

GENERAL NOTES

Design Specifications: AASHTO (1953) with minor modifications.

Design Live Load: H/15

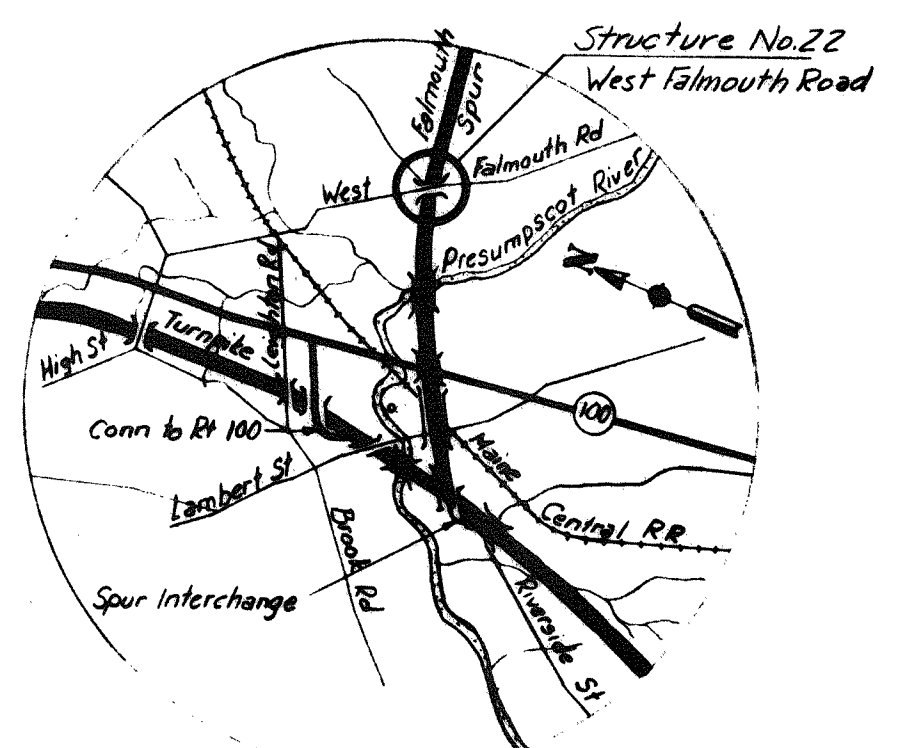
Maximum Soil Pressures:

Abutments: 2.9 Tons/Sq. Ft.

Piers: 4.1 Tons/Sq. Ft.

REFERENCES

Standard Drawing Number	Title	Substructure Contractor	Superstructure		
			Steel Fabricator	Steel Erector	Floor Contractor
1A	Standard Abutment Details	✓	✓	✓	✓
2	Standard Pier Details	✓	✓	✓	✓
3	Standard Abutment Drainage Details	✓	✓	✓	✓
5	Standard Handrail, Bearing Devices, and Miscellaneous Details	✓	✓	✓	✓
6	Standard Diaphragm Details	✓	✓	✓	✓
8	Standard Type 'A' Splices for 30W Beams	✓	✓	✓	✓
11A	Type 'X' and 'Y' Expansion Joints Expanding Length to 100 Feet.	✓	✓	✓	✓



DRAWING NO. 22.01.03

BY	DATE	4	As-Built	H.B.H. 124.54
MADE	A.E.R.	2-23-54	3	Redrawn (P.G. lowered) W.E.M. 128.54
TRACED			2	P.G. Elev. Change H.B.H. 526.54
CHECKED	DDG	3-2-54	1	Revised Boring No. 00-93A A.E.R. 3-9-54
IN CHARGE OF	I.D.S.K.	No.	REVISION	BY DATE

MAINE TURNPIKE AUTHORITY

MAINE TURNPIKE

SECTION 2 — PORTLAND TO AUGUSTA

STRUCTURE NO. 22 FALMOUTH SPUR UNDER WEST FALMOUTH ROAD STA. 80+51.15

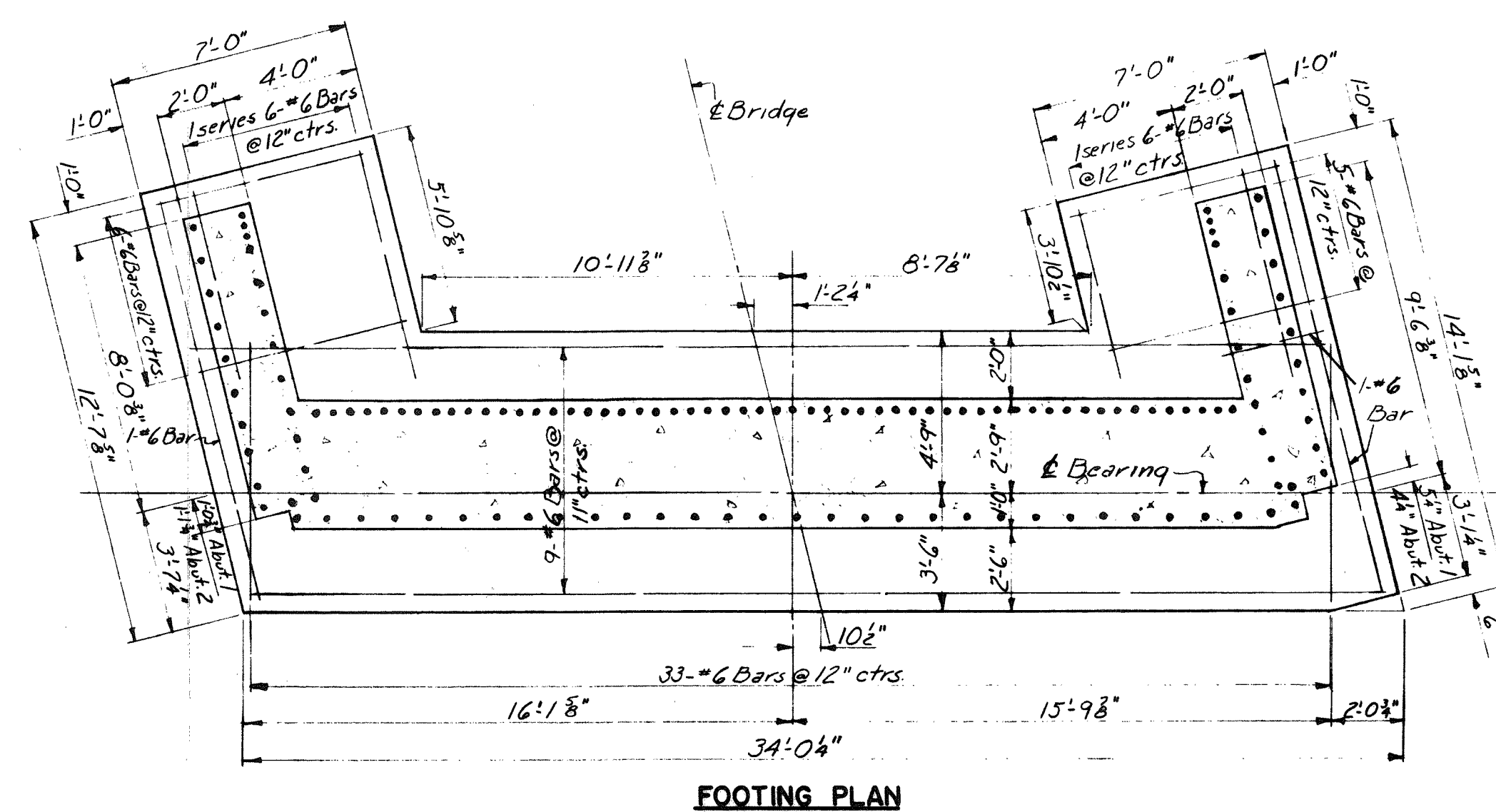
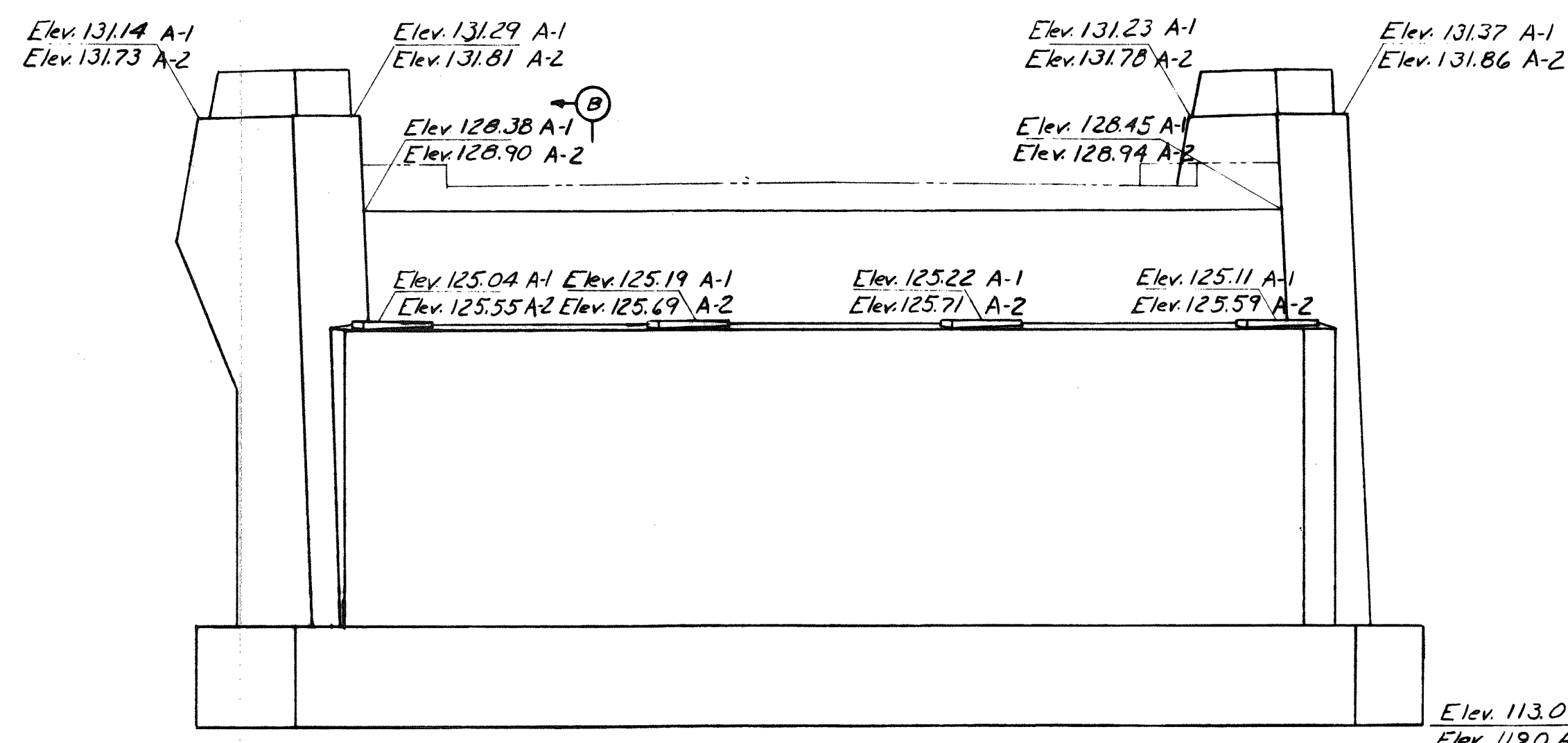
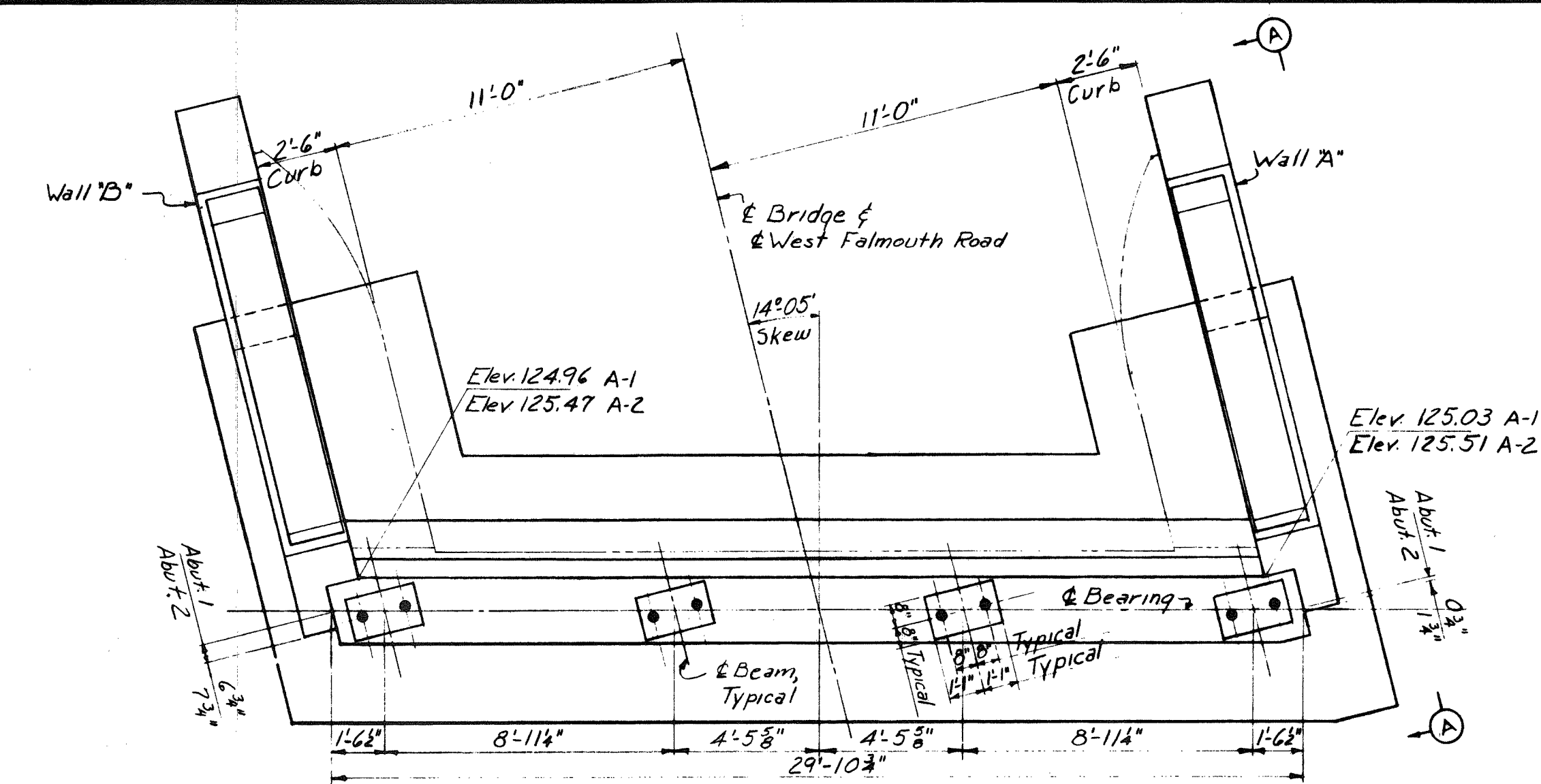
GENERAL PLAN AND ELEVATION

HOWARD, NEEDLES, TAMMEN & BERGENDOFF CONSULTING ENGINEERS

NEW YORK KANSAS CITY

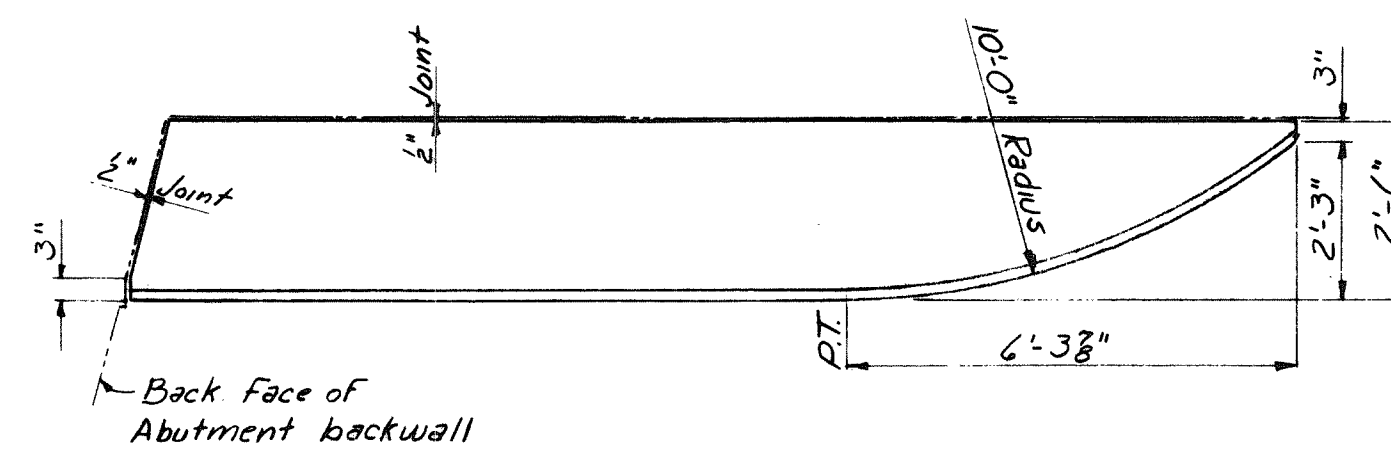
SCALE: 3/8" = 1'-0"

CONTRACT NO. SHEET NO. 113 OF 322

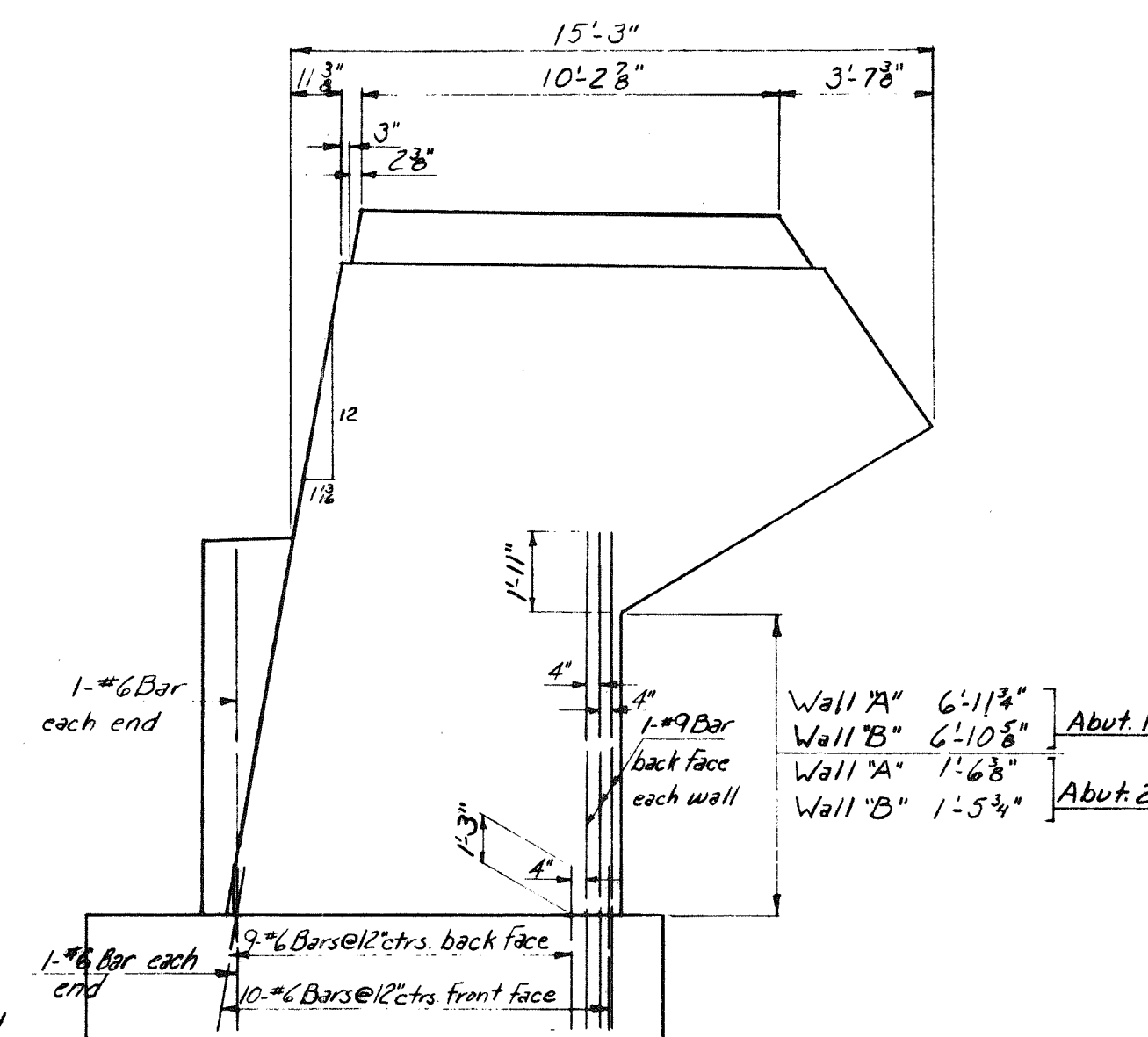


FOOTING PLAN

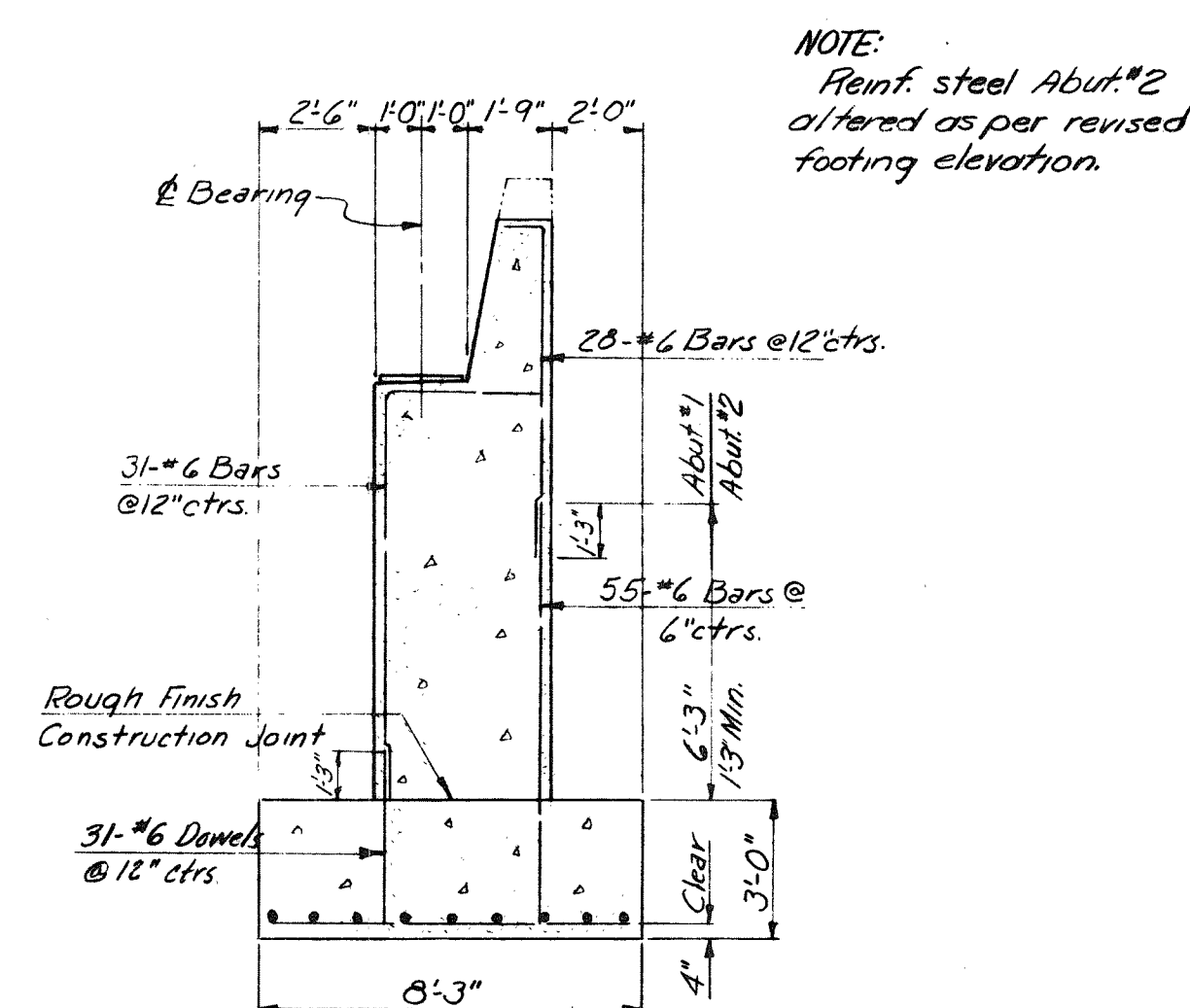
ABUTMENTS



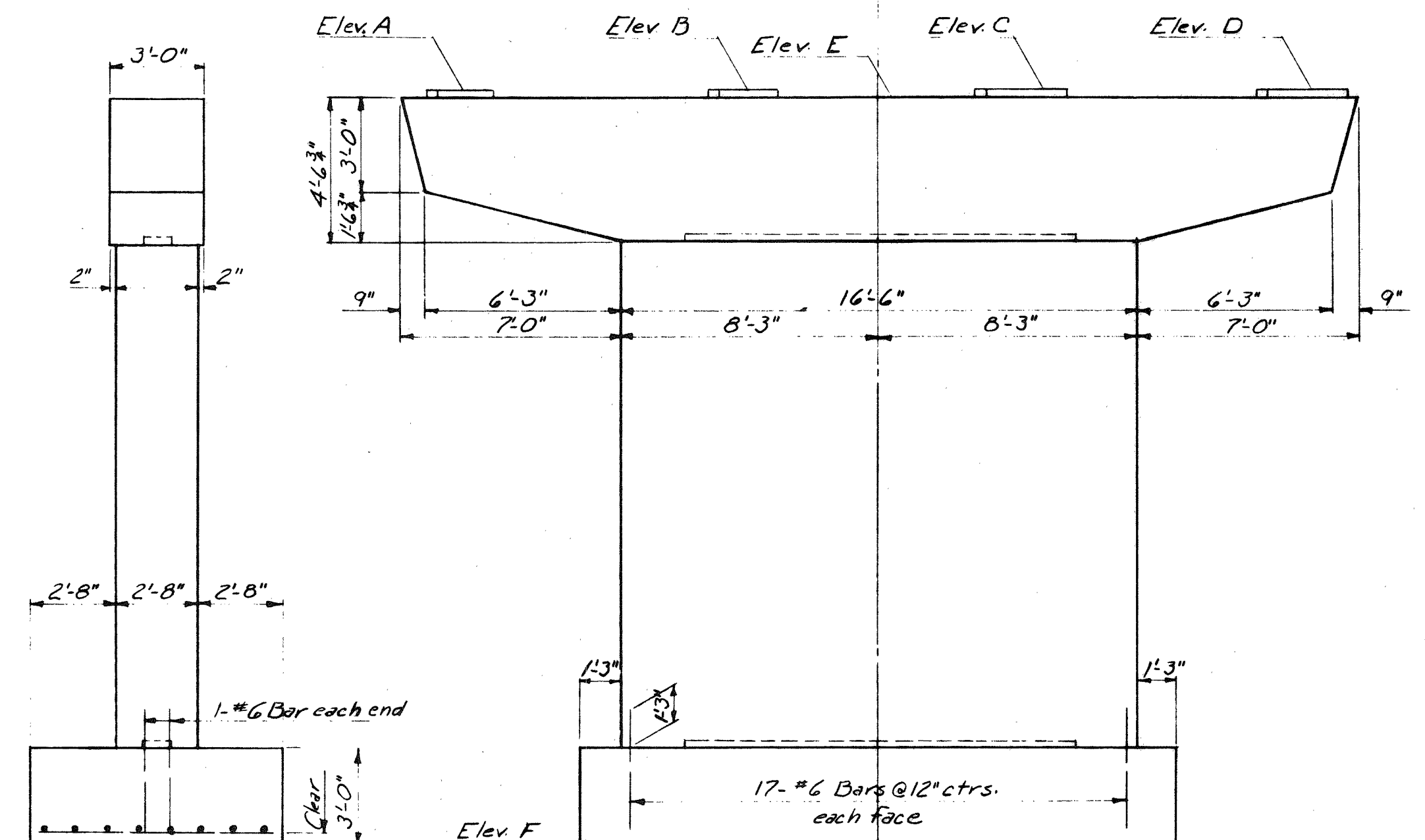
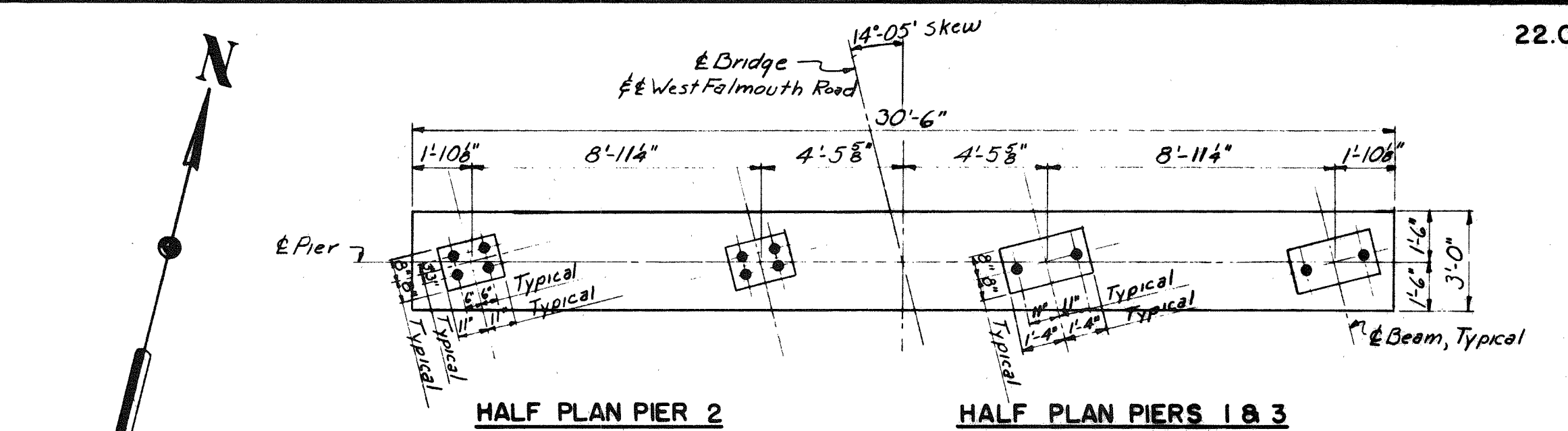
CURB DETAIL
Scale: $\frac{3}{8}" = 1'-0"$
Opposite Curb, Opposite Hand



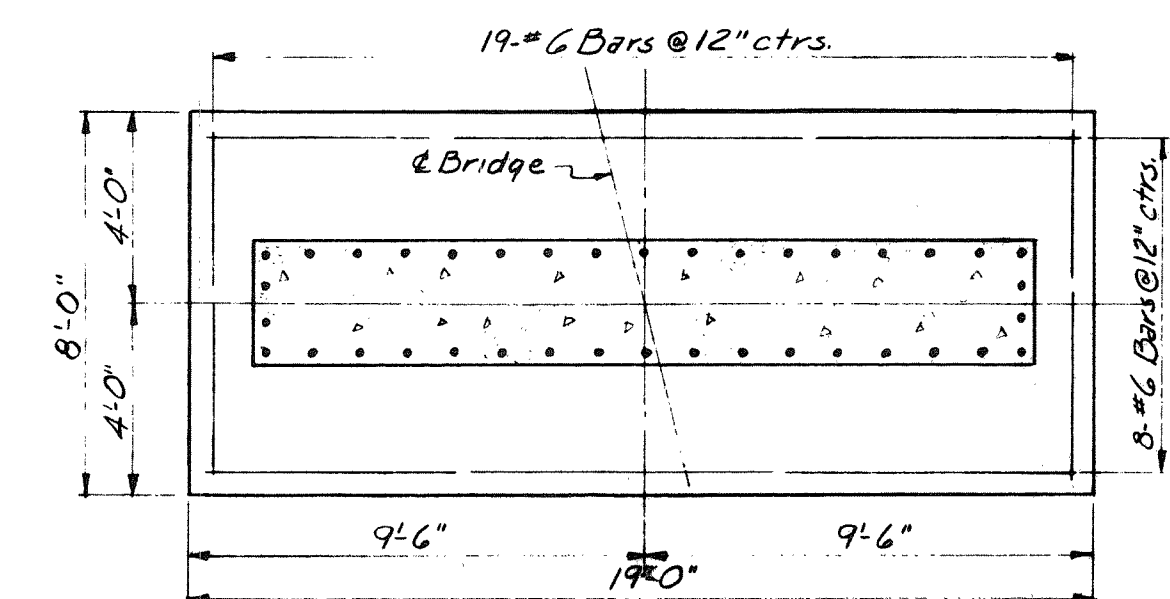
ELEVATION A-A



SECTION B-B



ELEVATION



FOOTING PLAN

PIERS

PIER ELEVATIONS			
Elev	Pier 1	Pier 2	Pier 3
A	125.26	125.71	125.56
B	125.39	125.84	125.68
C	125.41	125.84	125.47
D	125.30	125.73	125.55
E	125.24	125.69	125.53
F	102.0	105.0	102.0

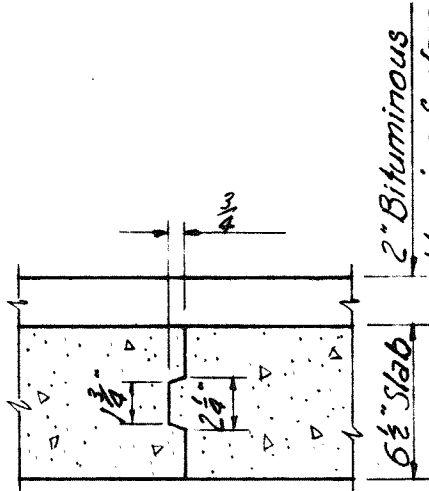
Note:
Letters A-1 and A-2 after Abutment Elevations refer to Abutments 1 and 2 respectively.
- Abutment footings shall be adjusted as necessary to provide a minimum of 6" penetration into solid undisturbed rock.
For reinforcing steel not shown see Standard Drawings 1A and 2.

DRAWING NO. 22.02.03

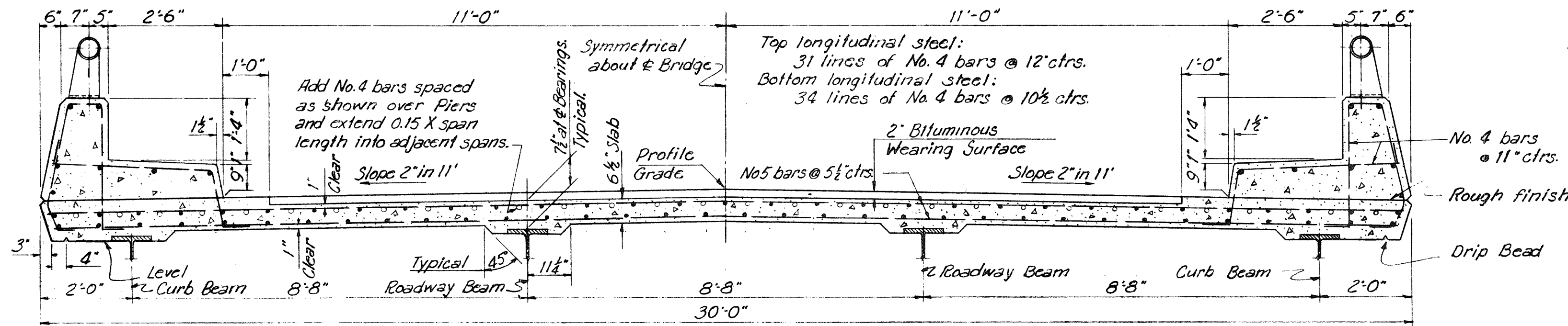
	BY	DATE					
MADE	A.E.R.	2-23-54					
TRACED			2	As-Built	HBH	12-15-	
CHECKED	D.D.G.	3-2-54	1	Redrawn	R.S.G.	6-28-	
IN CHARGE OF	I.D.S.K.		No.	REVISION	BY	DATE	

MAINE TURNPIKE AUTHORITY MAINE TURNPIKE	
SECTION 2 — PORTLAND TO AUGUSTA	
STRUCTURE NO. 22 WEST FALMOUTH ROAD STA. 80 + 51.5	FALMOUTH SPUR UNDER
SUBSTRUCTURE	
HOWARD, NEEDLES, TAMMEN & BERGENDOFF CONSULTING ENGINEERS	SCALE: 4" = 1'-0" except as shown CONTRACT NO. _____
NEW YORK	KANSAS CITY
SHEET NO. <u>114</u> OF <u>382</u>	

Note: The depth of concrete over the tops of beams shall be varied as necessary to insure uniform floor thickness between haunches and conformity of final concrete surfaces of floor, curbs and parapet walls with required elevations after deflection under full dead load.



BRIDGE FLOOR CONSTRUCTION JOINT

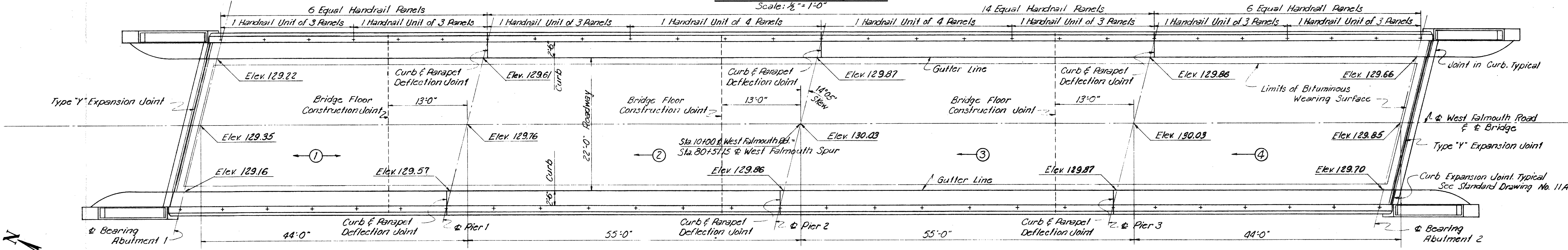


Note: Top of parapet slopes 1/4\"/>

Note: Elevations shown on Floor Plan are to top of Bridge Floor. Sequence and direction of placing concrete is noted thus: 1

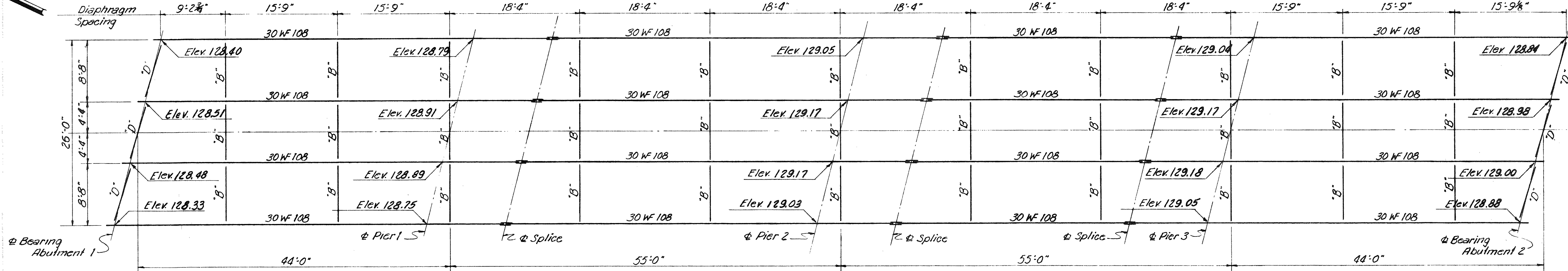
BRIDGE FLOOR CROSS SECTION

Scale: 1/2\"/>



PLAN

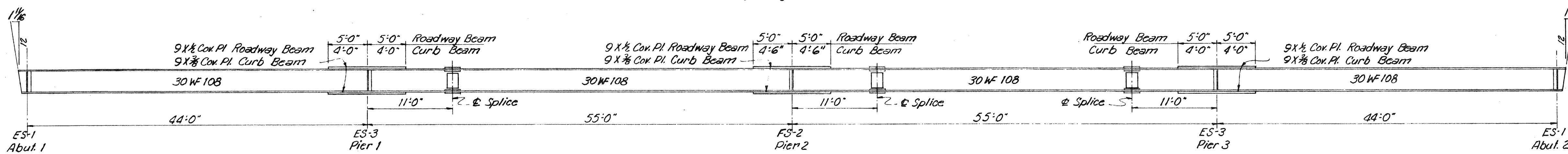
Scale: 1/8\"/>



FRAMING PLAN

Scale: 1/8\"/>

Note: Elevations shown on Framing Plan are to top of beam flanges.



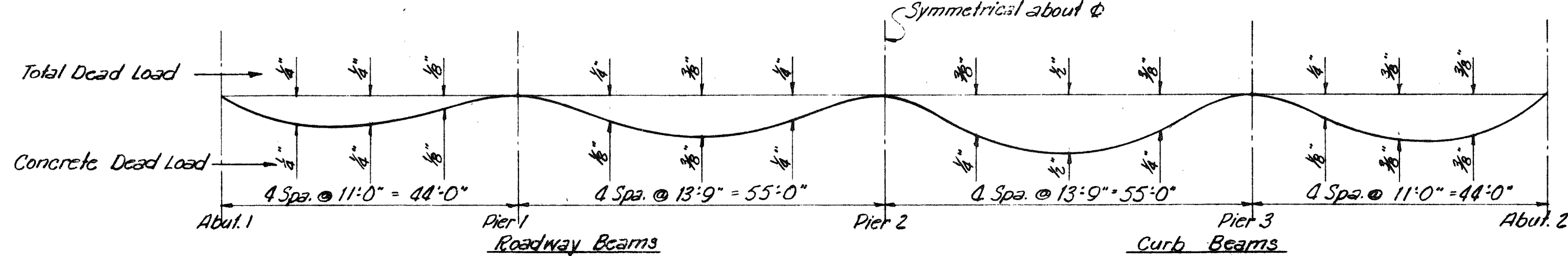
TYPICAL BEAM ELEVATION

Scale: 1/8\"/>

Note: Bevels shown are with respect to beam axis. All bearing stiffeners are 7 X 4 X 3/8\"/>

DRAWING NO. 22.03.03

BY	DATE			
MADE	AER	2-23-54	3	As-Built
TRACED			2	Grade Changed
CHECKED	DDG	3-1-54	1	Curb & Parapet Reinforcing Steel
IN CHARGE OF	IOSK		No.	REVISION
			BY	DATE



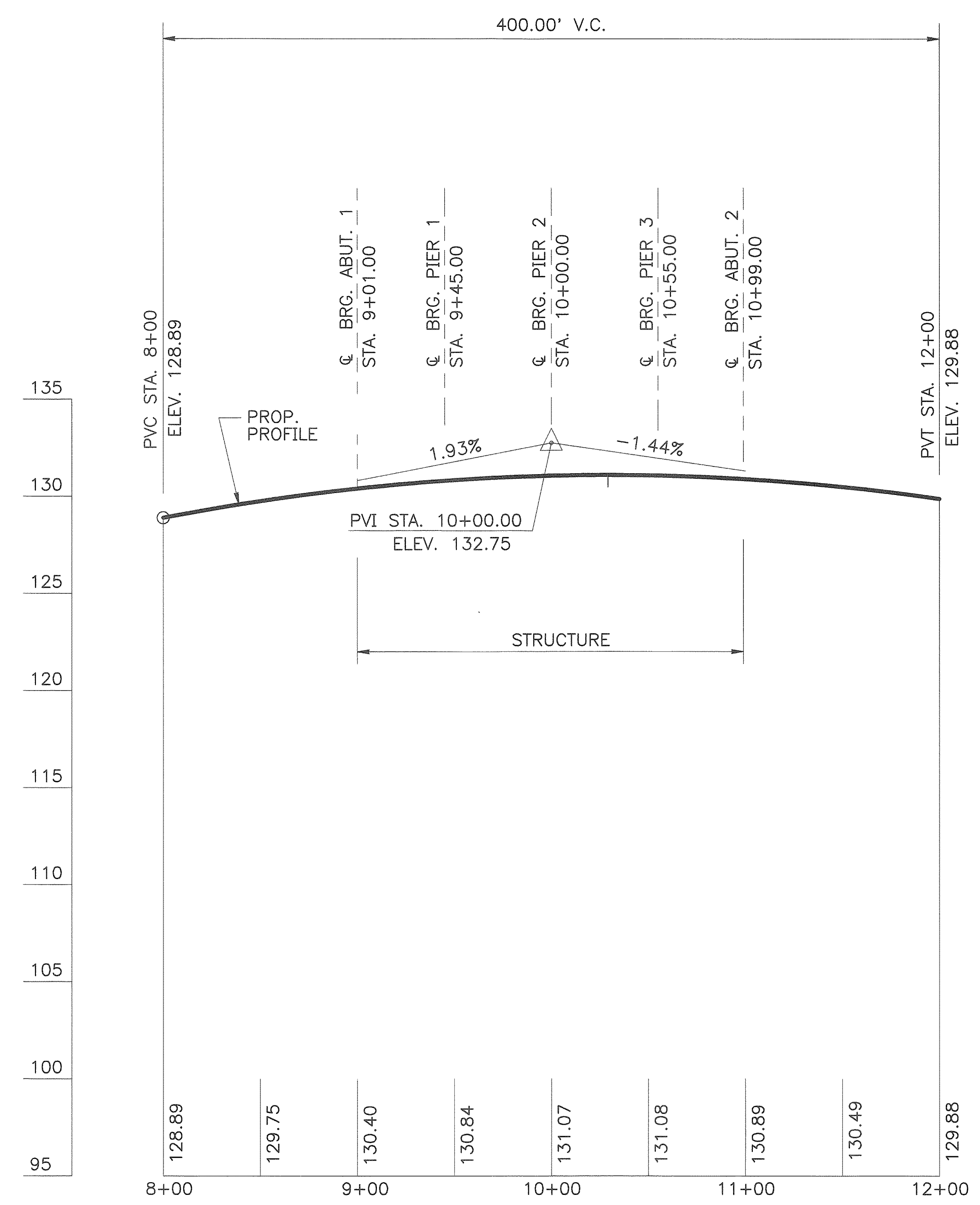
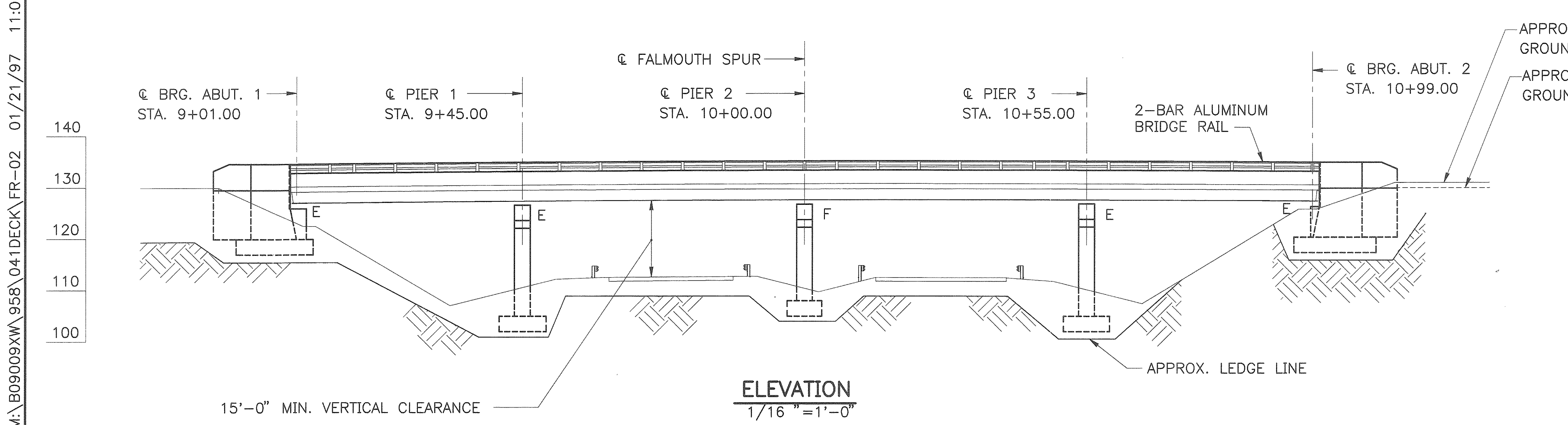
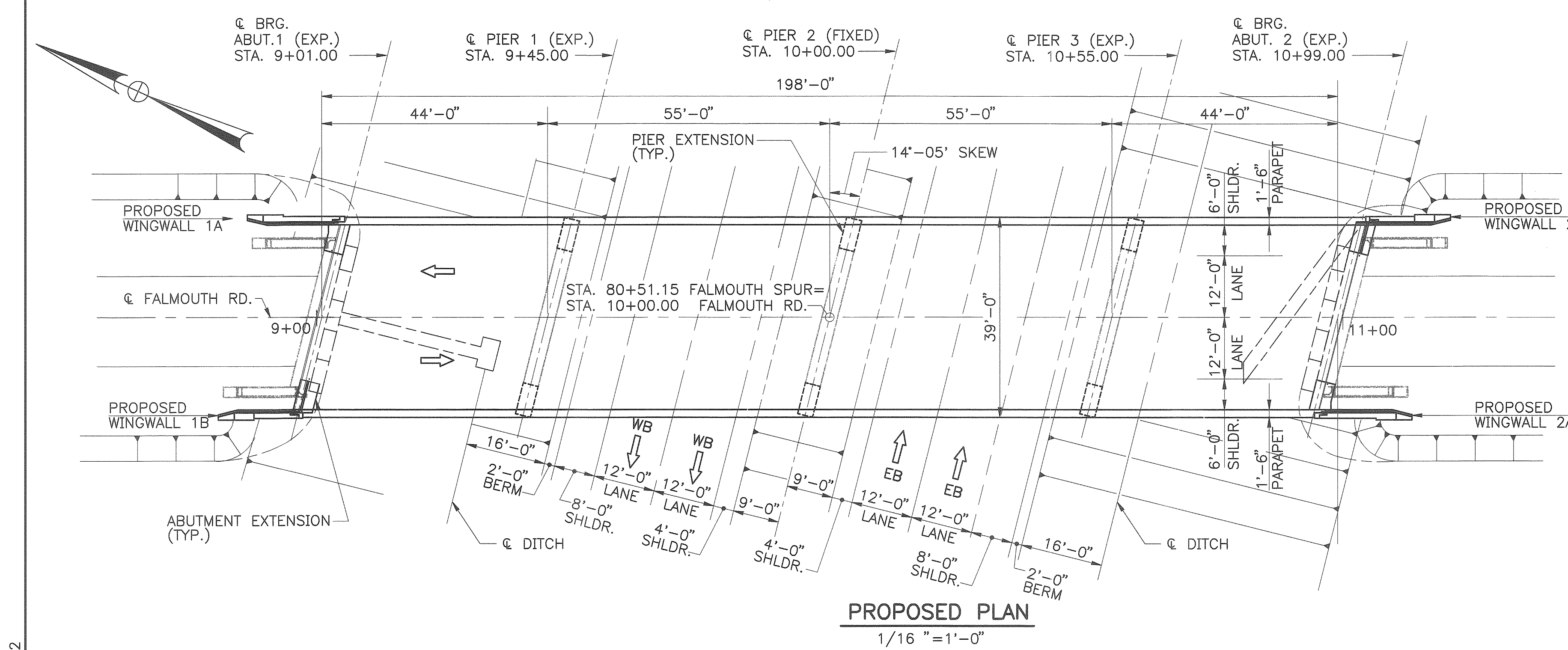
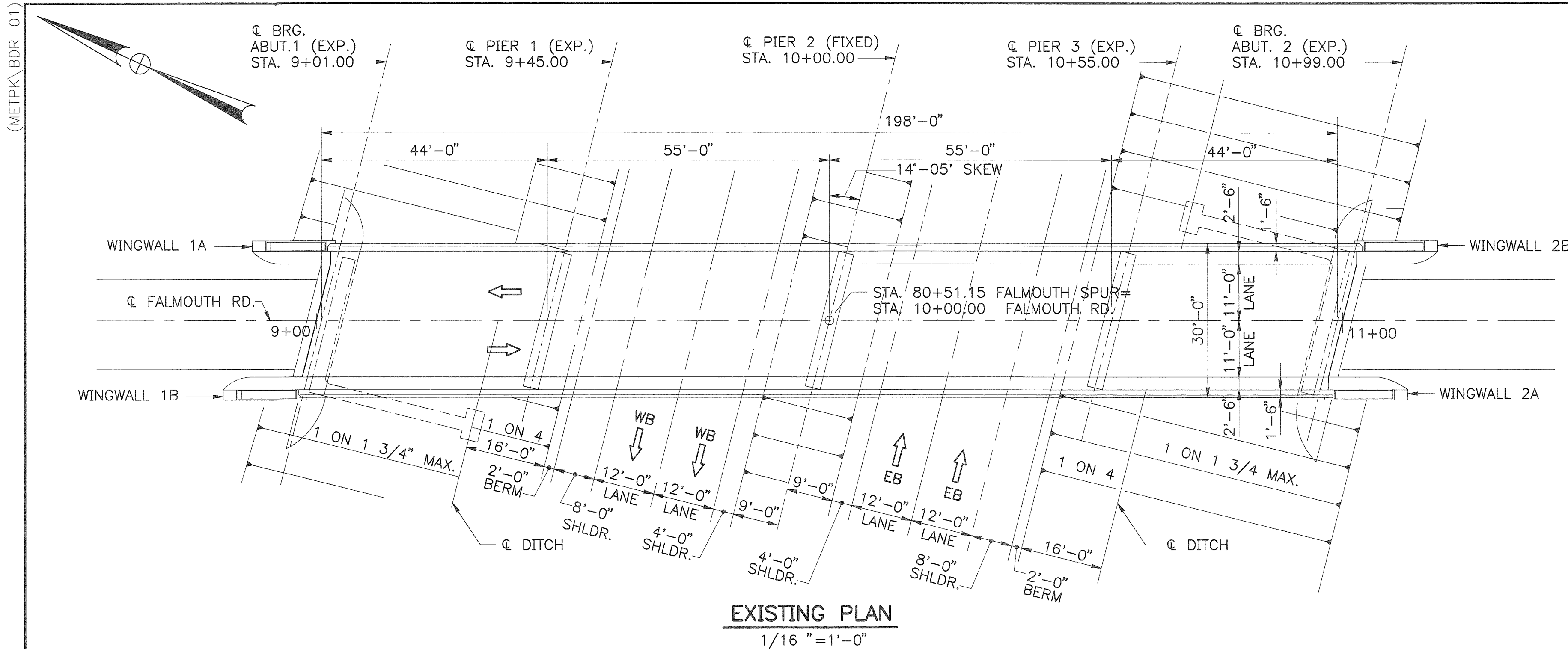
DEFLECTION DIAGRAM

No Scale

MAINE TURNPIKE AUTHORITY
SECTION 2— PORTLAND TO AUGUSTA
STRUCTURE NO. 22 FALMOUTH SPUR UNDER
WEST FALMOUTH ROAD
STA. 80+51.15
SUPERSTRUCTURE
HOWARD, NEEDLES, TAMMEN & BERGENDOFF
CONSULTING ENGINEERS
NEW YORK KANSAS CITY
SCALE: As Shown
CONTRACT NO. _____
SHEET NO. 115 OF 382

(METPK BDR-01)

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PROPOSED PROFILE
HORIZ. 1" = 50'
VERT. 1" = 5'

No.	Revision	By	Date	In Charge Of
		Designed	AD	1/97
		Drawn	CSL	1/97
		Checked	JMH	1/97

No.	Revision	By	Date	In Charge Of
				RAL

Maine Turnpike Authority
Maine Turnpike

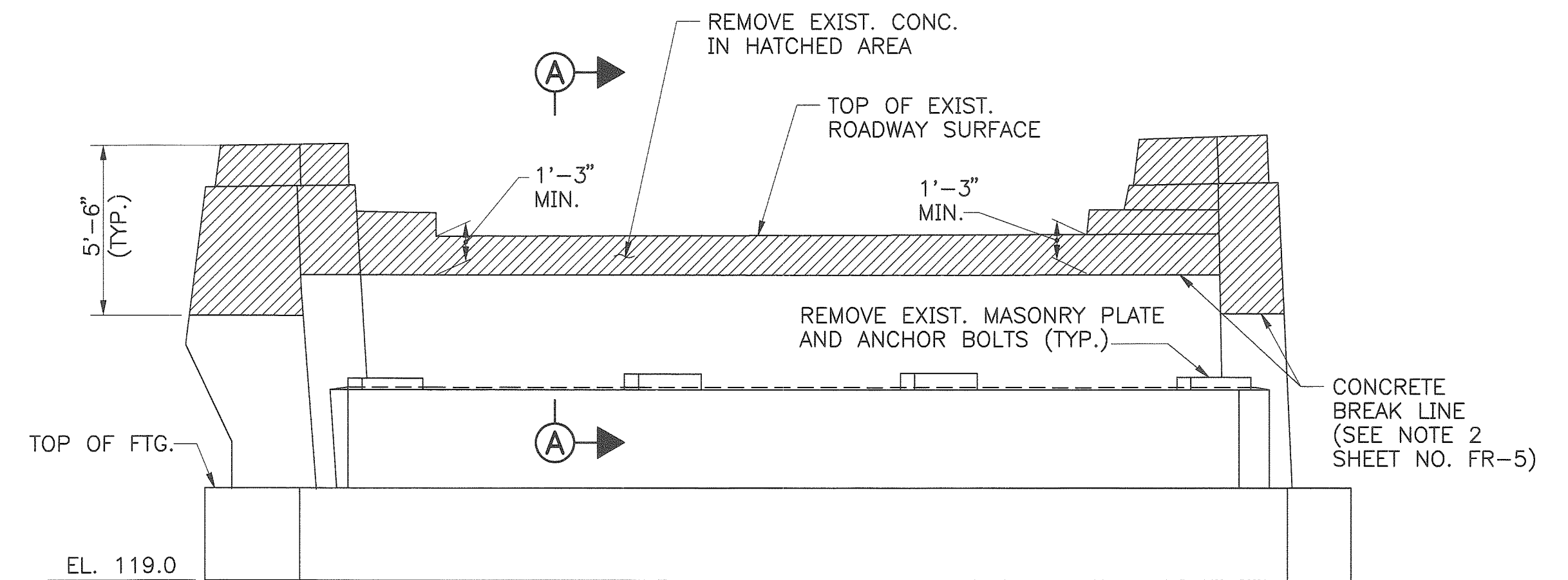
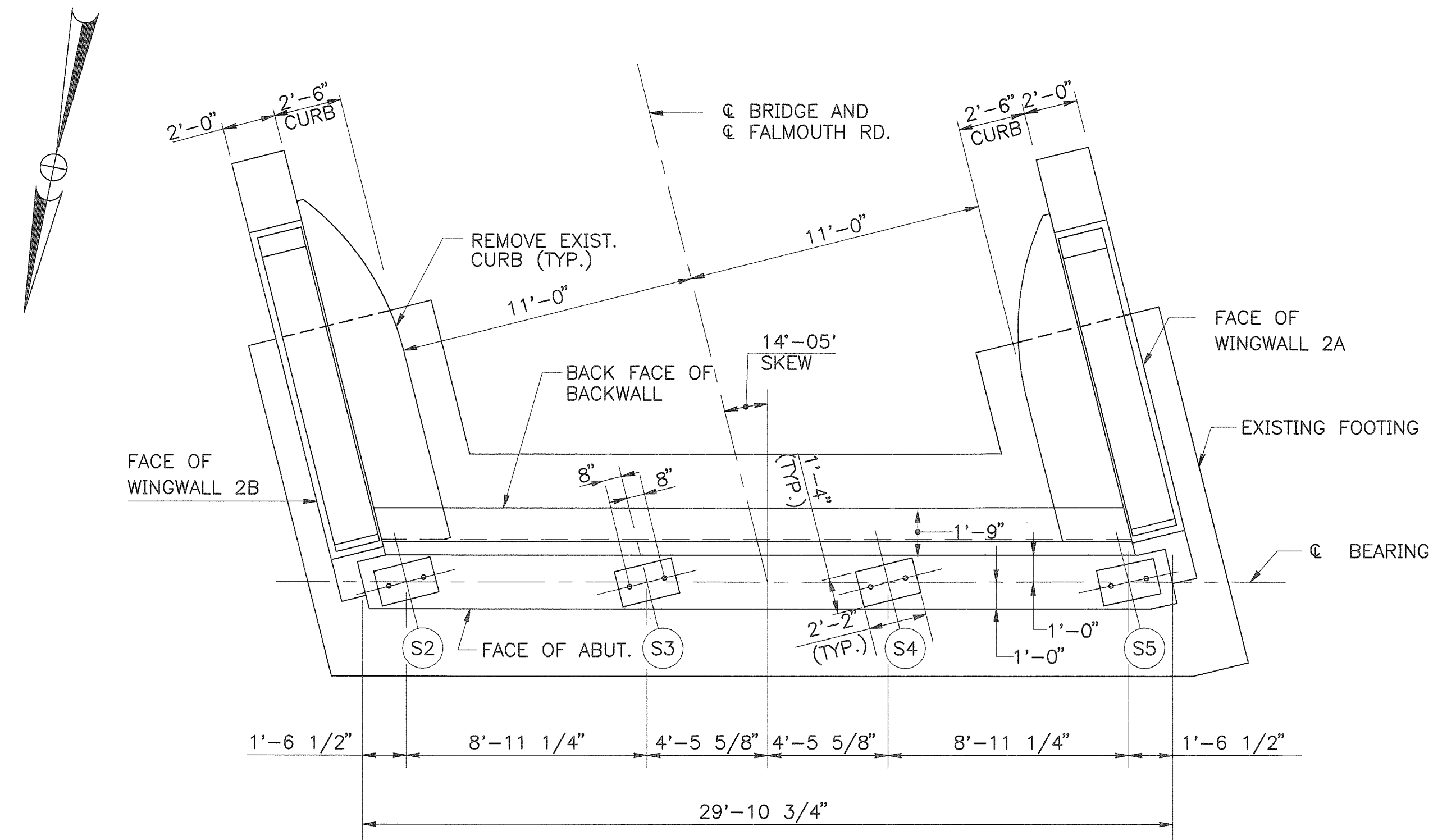
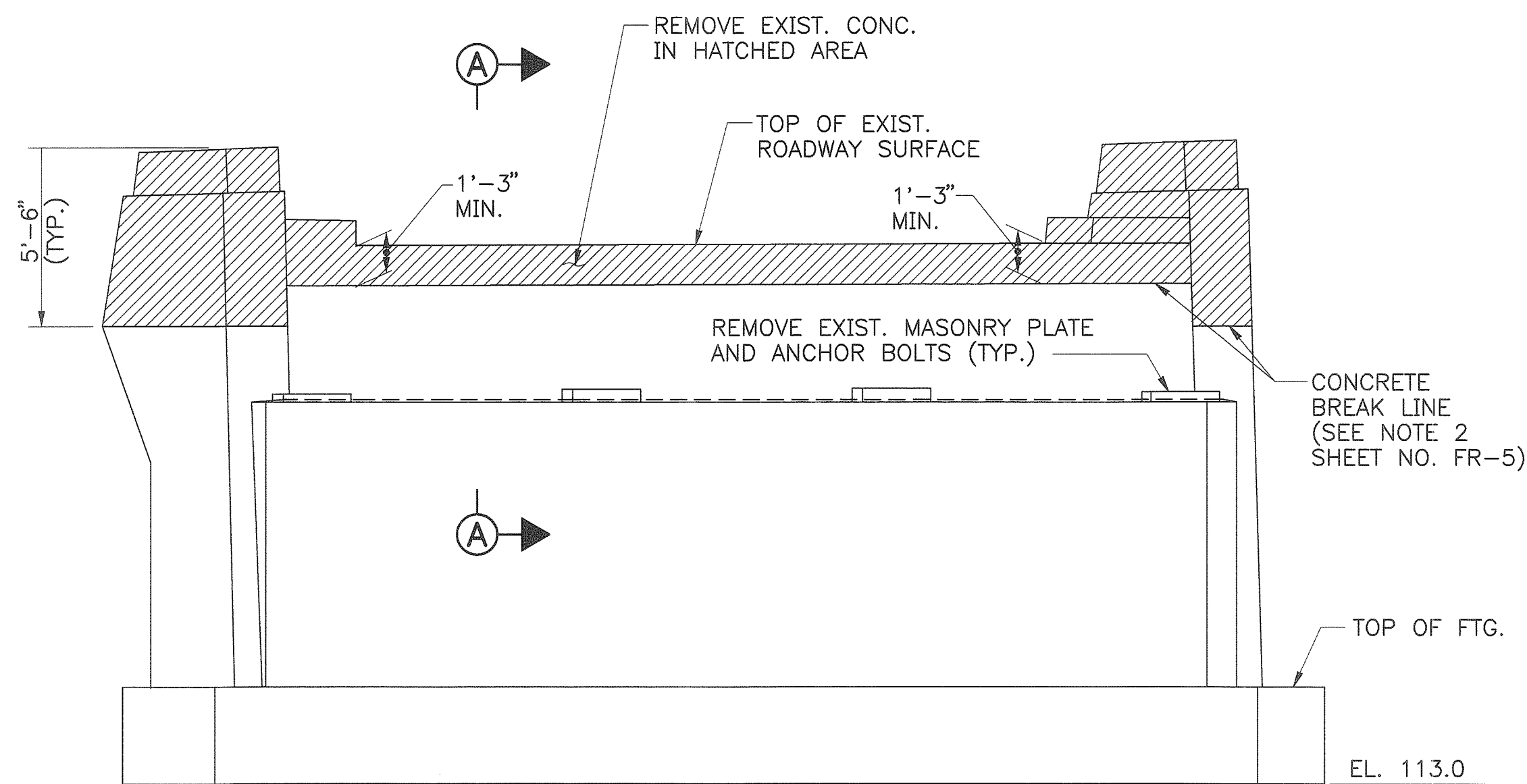
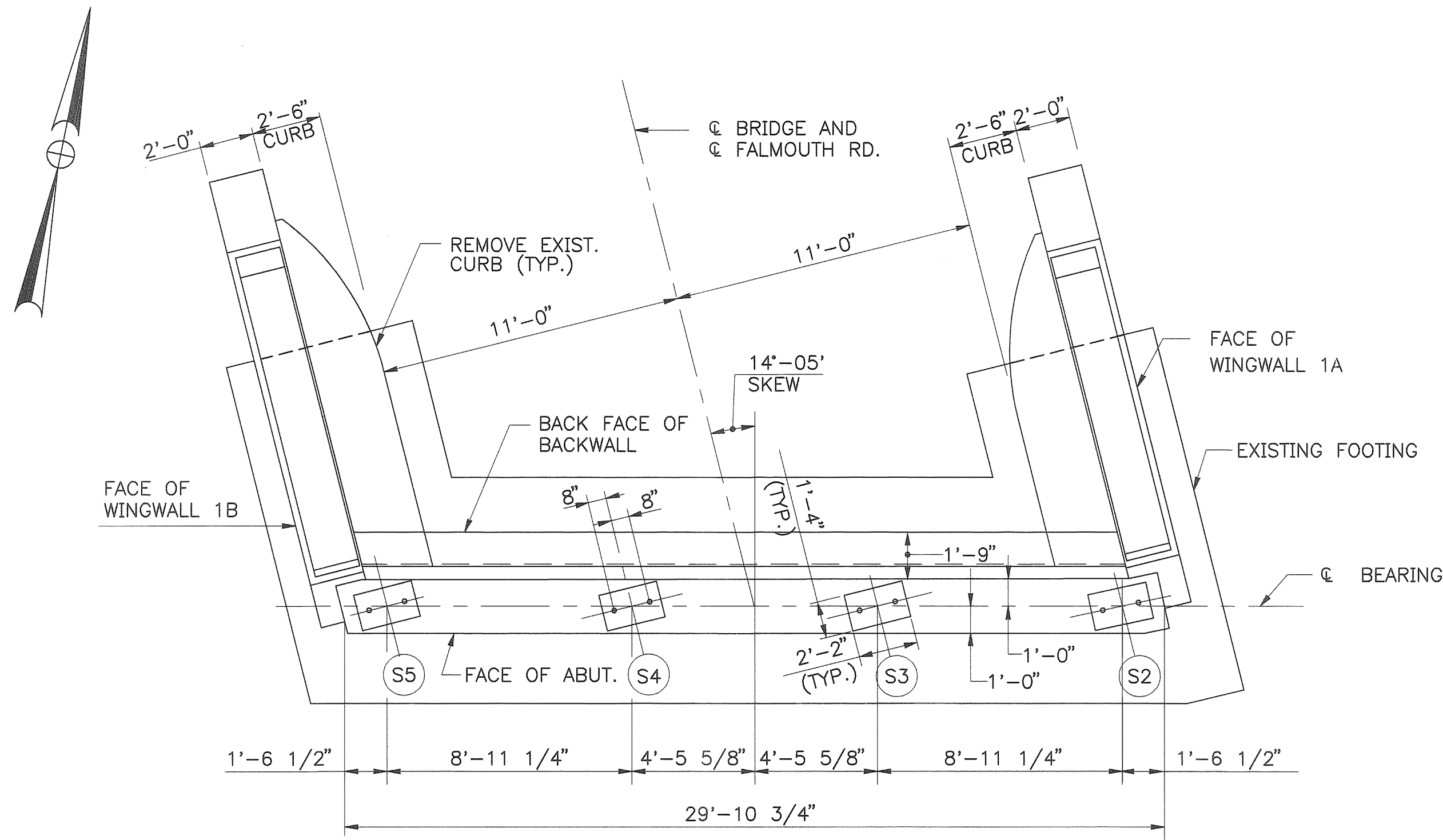
FALMOUTH ROAD UNDERPASS

GENERAL PLAN AND ELEVATION

HNTB HOWARD NEEDLES TAMMEN & BERGENDOFF, INC.
ARCHITECTS ENGINEERS PLANNERS

Contract 97.9

Sheet No. FR-2
32 of 56



				By	Date
			Designed	AD	1/97
			Drawn	LMR	1/97
			Checked	JMH	1/97
No.	Revision	By	Date	In Charge Of	RAL

Maine Turnpike Authority
Maine Turnpike

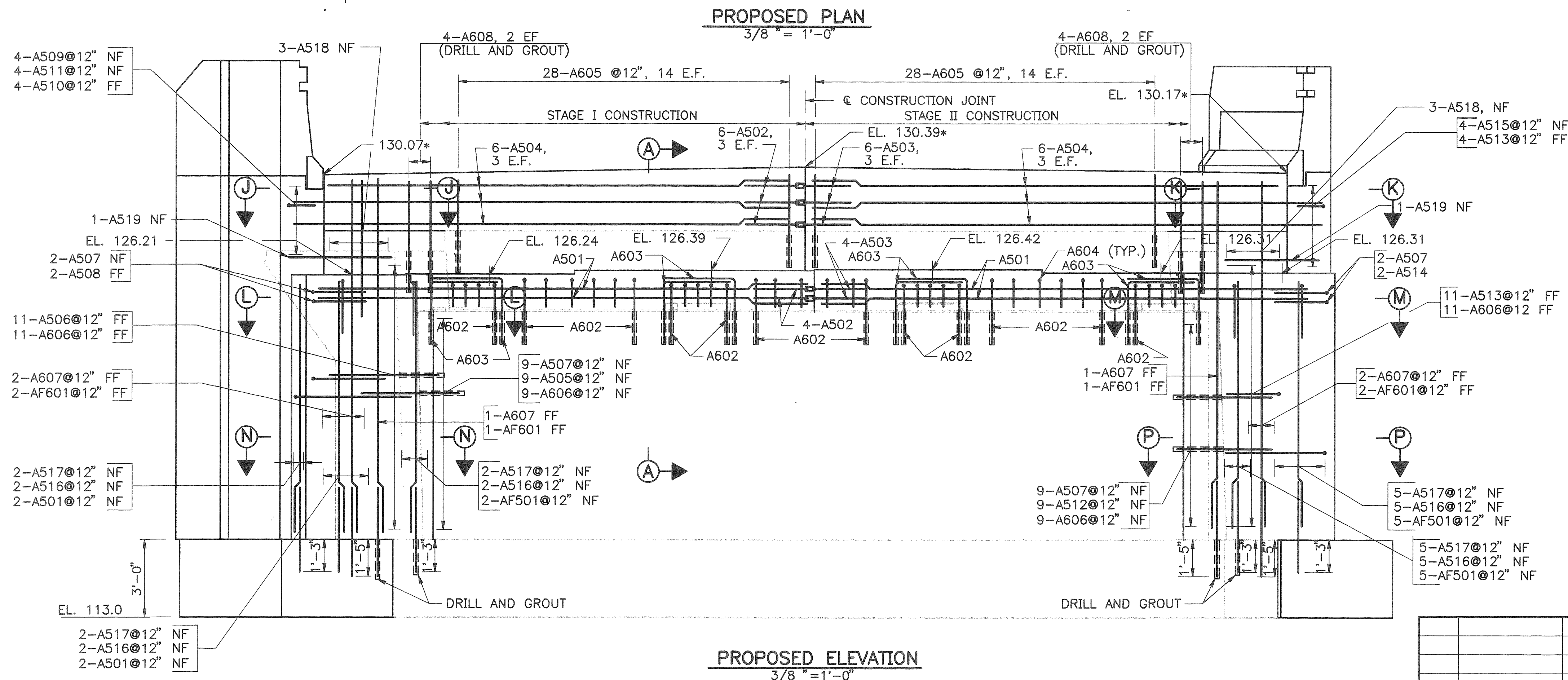
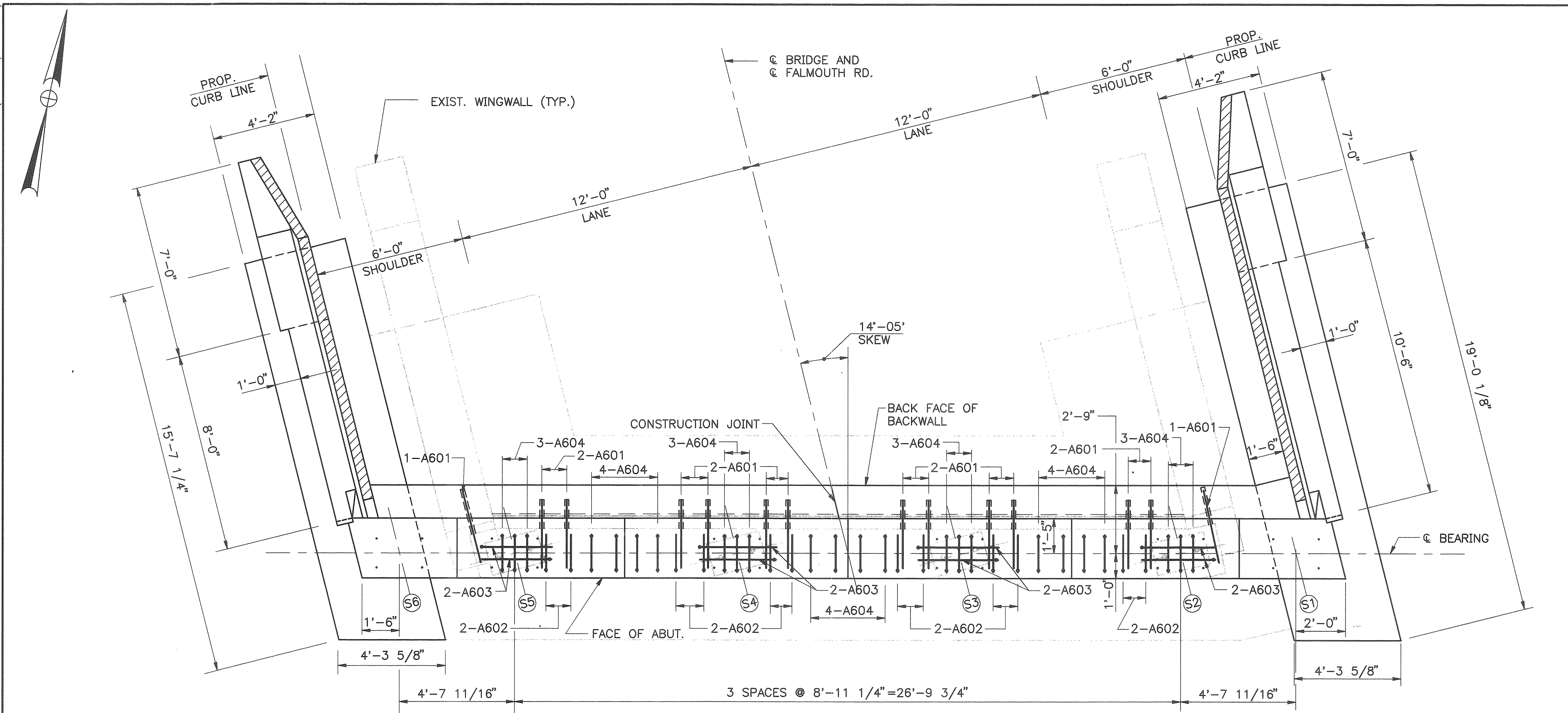
Trans
pass

Contract 97.9

Sheet No. FR-4
34 of 56

HOWARD NEEDLES TAMMEN & BERGENDOFF, INC
ARCHITECTS ENGINEERS PLANNERS

EXISTING ABUTMENTS



ABUTMENT NOTES

- FOR WINGWALL ELEVATIONS AND SECTIONS, SEE SHEET NOS. FR-8 AND FR-9.
- SAW CUT 1" MIN. DEEP BEFORE REMOVING EXISTING CONCRETE.
- A601 OR B601 BARS SHALL BE LOCATED SO AS NOT TO INTERFERE WITH THE INSTALLATION OF ANCHOR BOLTS FOR THE MASONRY PLATE.
- FOR ROADWAY EXPANSION JOINT DETAIL, SEE SHEET NOS. FR-18 AND FR-19.
- FOR LIMITS OF CONCRETE PROTECTIVE COATING, SEE SHEET NO. FR-9.
- FOR SECTIONS J-J, K-K, L-L, M-M, N-N, AND P-P, SEE SHEET NO. FR-7.
- EXCAVATION FOR BACKWALL MODIFICATION SHALL BE PAID FOR UNDER ITEM 203.20 TO THE LIMITS SHOWN.
- * ELEVATIONS SHOWN ARE AT THE FRONT FACE OF BACKWALL.
- REMOVAL OF THE EXISTING ARMOR JOINT SHALL BE INCIDENTAL TO ITEM 202.122.
- REMOVAL OF THE EXISTING MASONRY PLATE, ANCHOR BOLTS AND BEARING ASSEMBLY SHALL BE INCIDENTAL TO ITEM 202.12.
- FOR EXISTING AND PROPOSED SECTION A-A, SEE SHEET NO. FR-7.
- BRIDGE SEAT ELEVATIONS SHOWN ON THE ABUTMENT SHEETS ARE BASED ON POT BEARINGS MANUFACTURED BY STRUCTURAL ACCESSORIES, INC. OF TERRYVILLE, CT. IF THE CONTRACTOR SELECTS A BEARING FROM ANOTHER APPROVED BEARING MANUFACTURER, AFFECTED DETAILS AND ELEVATIONS SHALL BE ADJUSTED TO ACCOMMODATE THE SELECTED BEARINGS.
- ABUTMENT FOOTINGS SHALL BE ADJUSTED AS NECESSARY TO PROVIDE A MINIMUM OF 6" PENETRATION INTO SOLID UNDISTURBED ROCK.
- ANCHOR BOLT LAYOUT FOR ABUTMENT 1 AND 2 SHALL BE SIMILAR TO ANCHOR BOLT LAYOUT DETAIL SHOWN ON SHEET NO. FR-11 FOR PIERS 1, 2 AND 3.

Maine Turnpike Authority
Maine Turnpike



FALMOUTH ROAD UNDERPASS

ABUTMENT 1
MODIFICATIONS



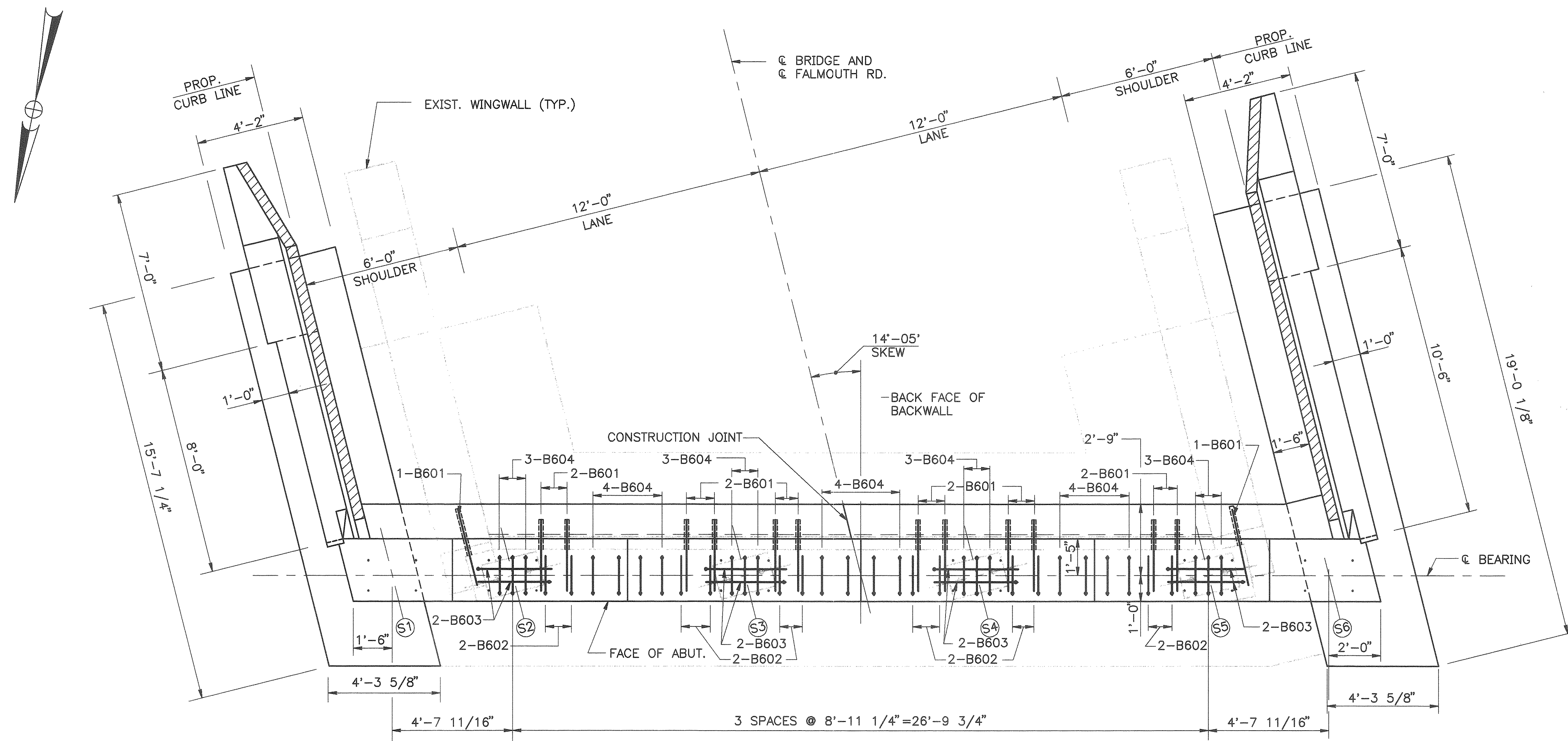
HOWARD NEEDLES TAMMEN & BERGENDOFF, INC.
ARCHITECTS ENGINEERS PLANNERS

Contract 97.9

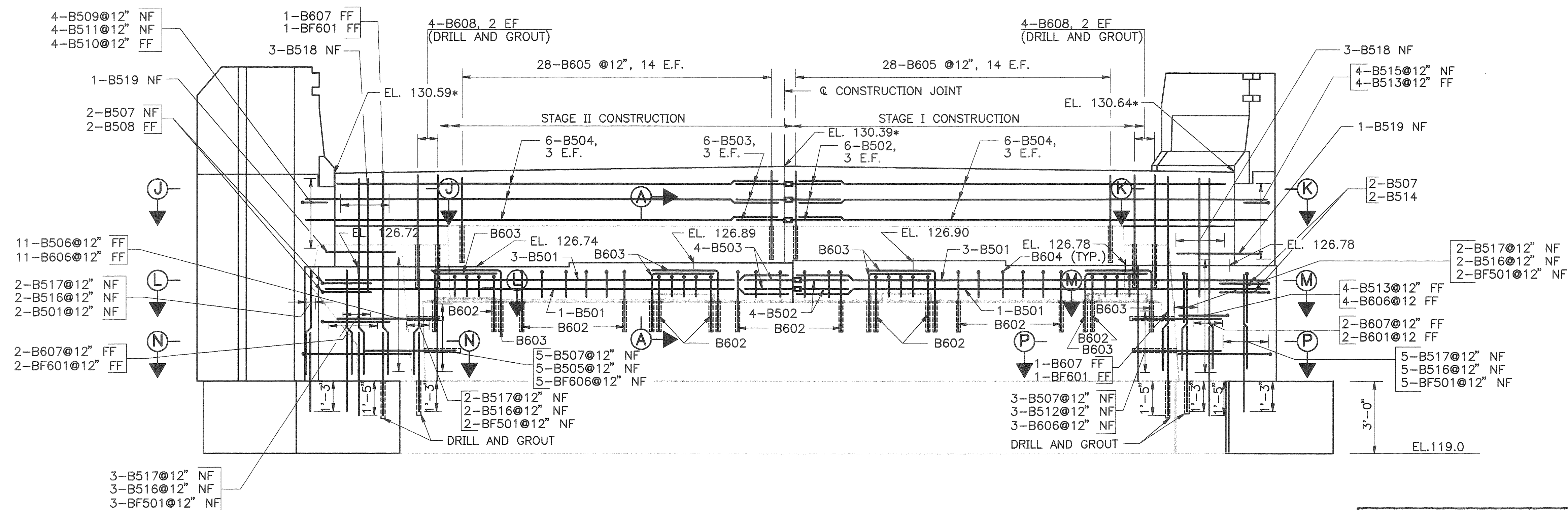
Sheet No. FR-5
35 of 56

No.	Revision	By	Date	In Charge Of
		Designed	AD	1/97
		Drawn	CSL	1/97
		Checked	JMH	1/97
		In Charge Of	RAL	

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PROPOSED PLAN
3/8" = 1'-0"




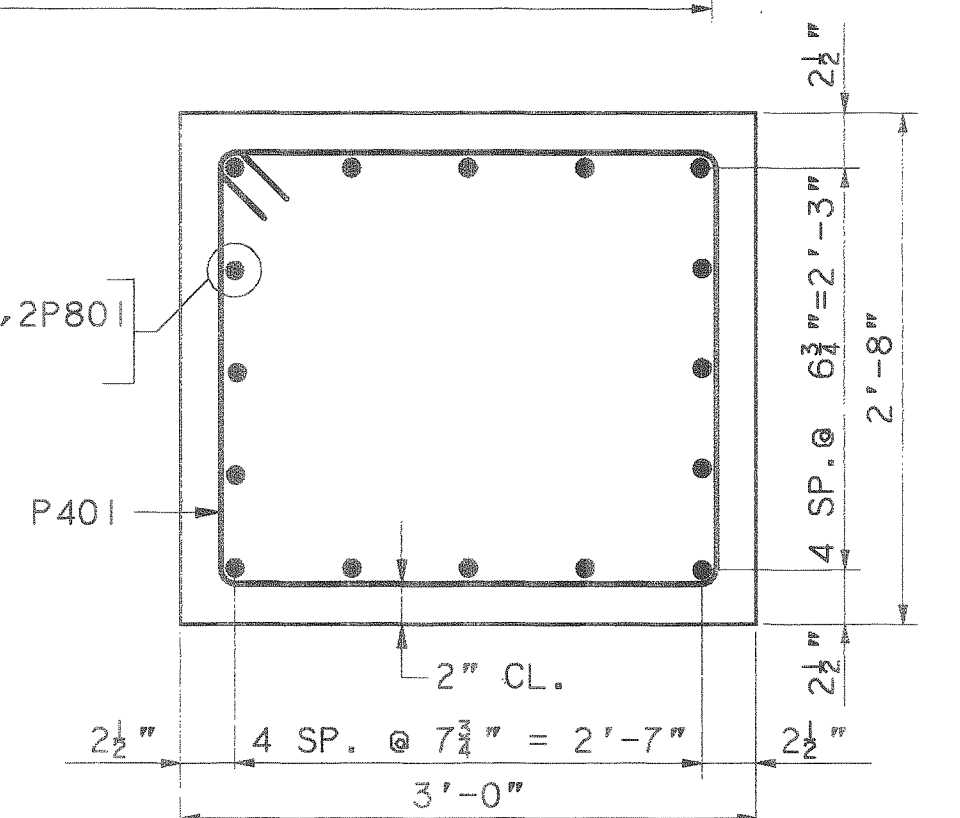
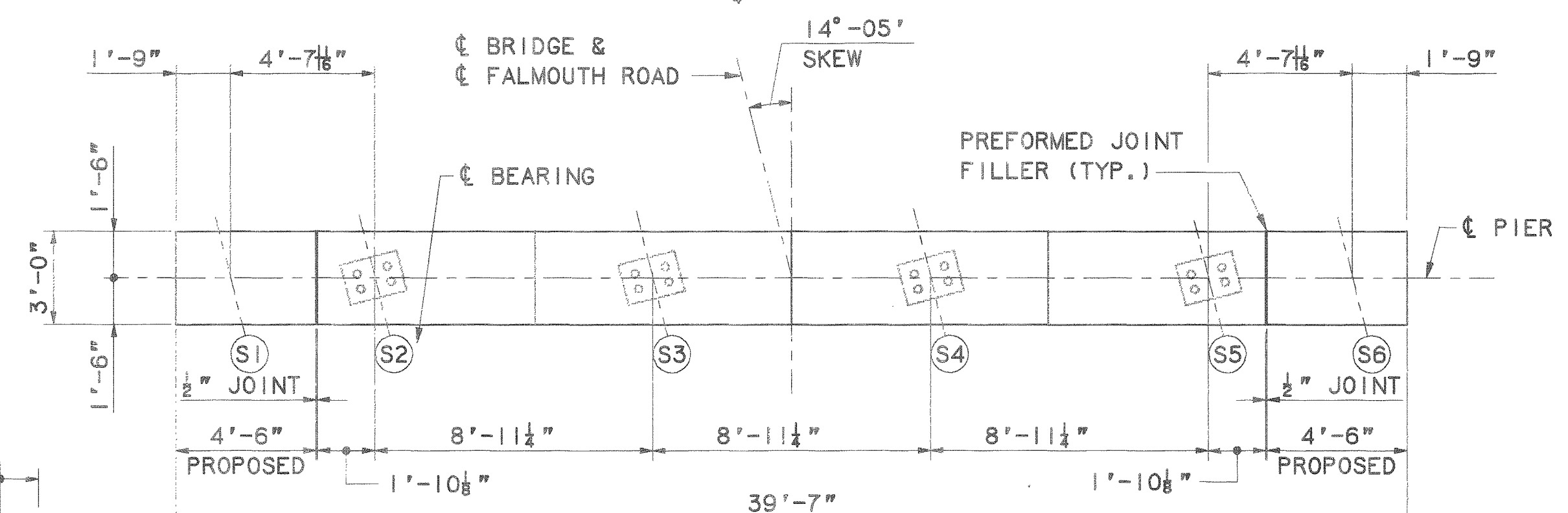
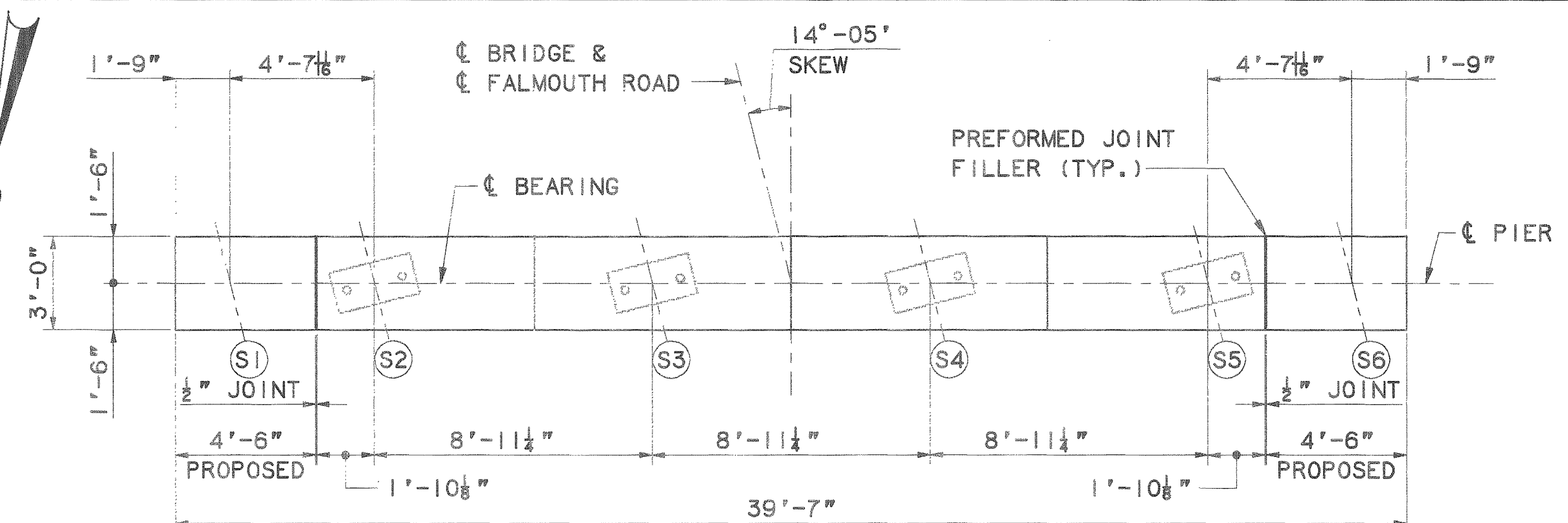
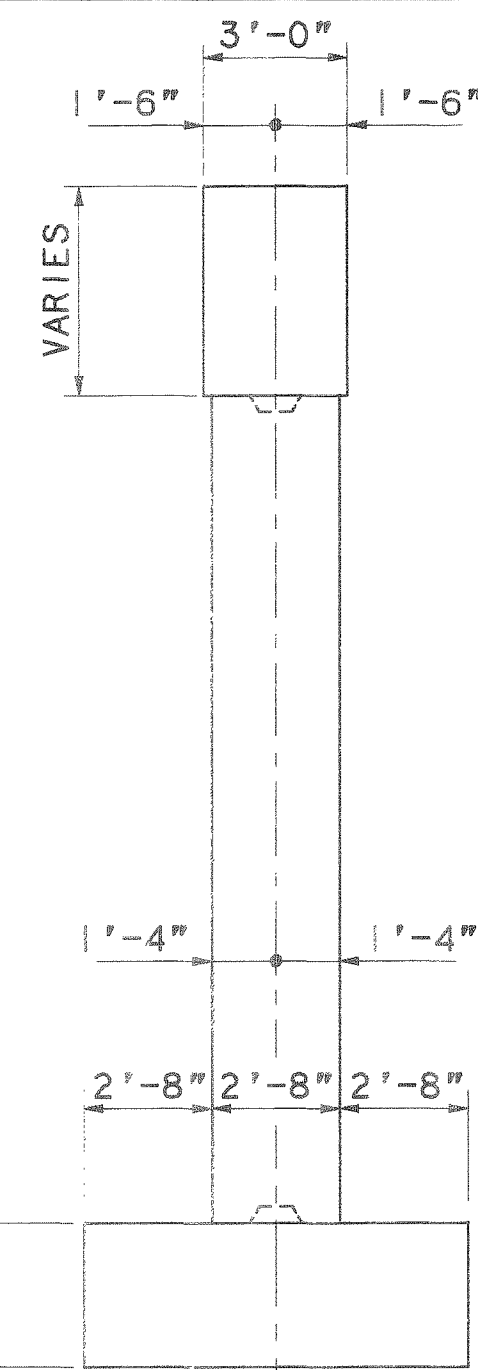
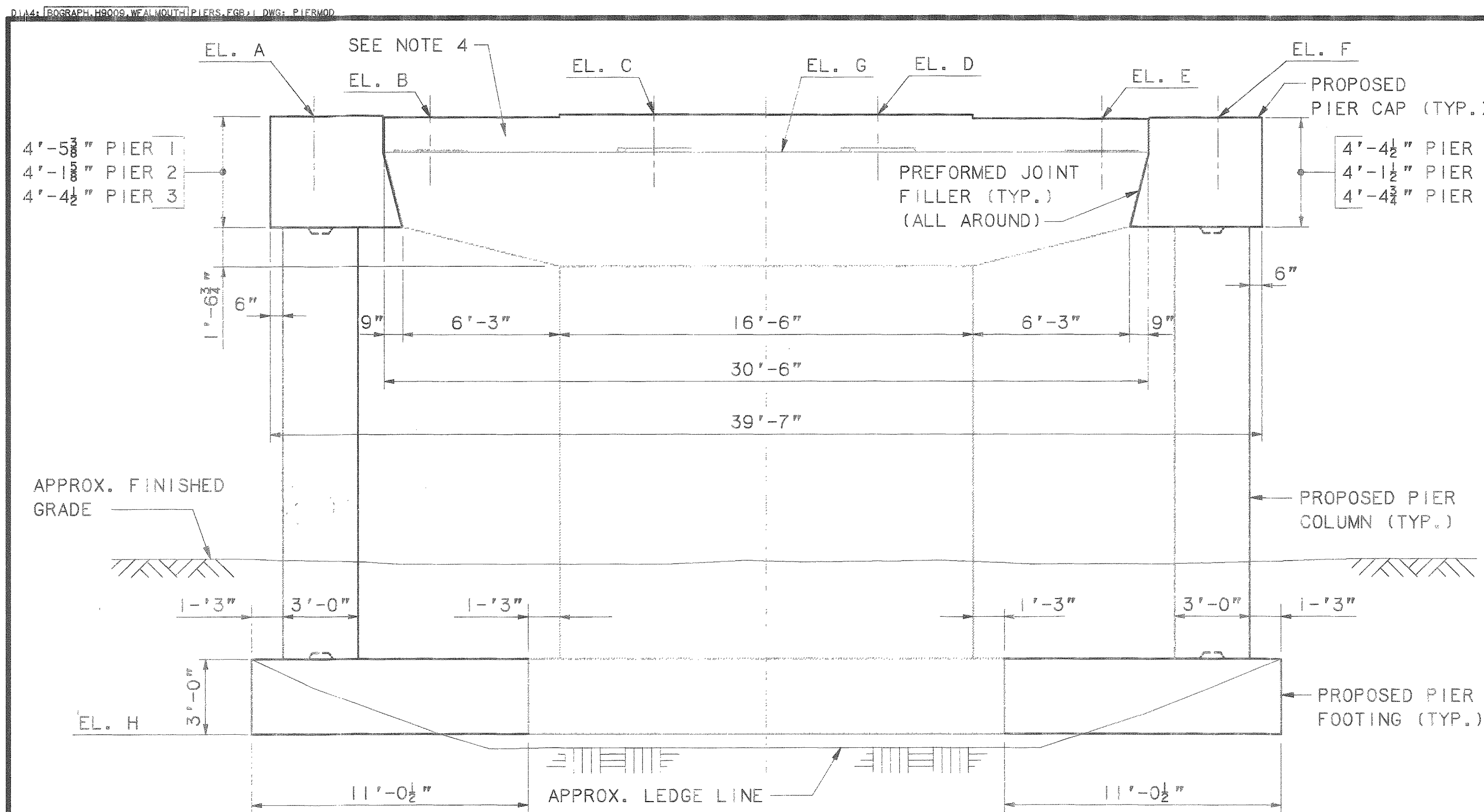
PROPOSED ELEVATION
3/8" = 1'-0"

NOTE

1. FOR ABUTMENT NOTES, SEE SHEET NO. FR-5.

					By	Date
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				Drawn	CSL	1/9
				Checked	JMH	1/9
No.	Revision	By	Date	In Charge Of:	RAL	

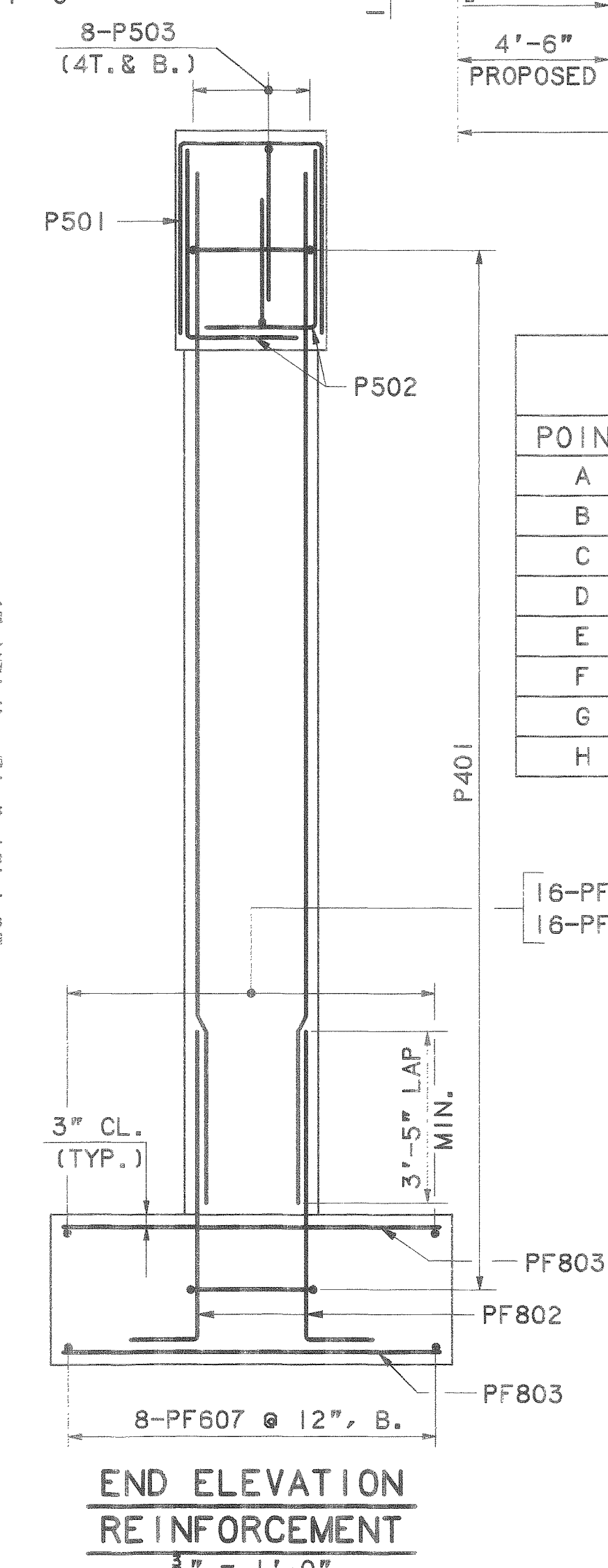
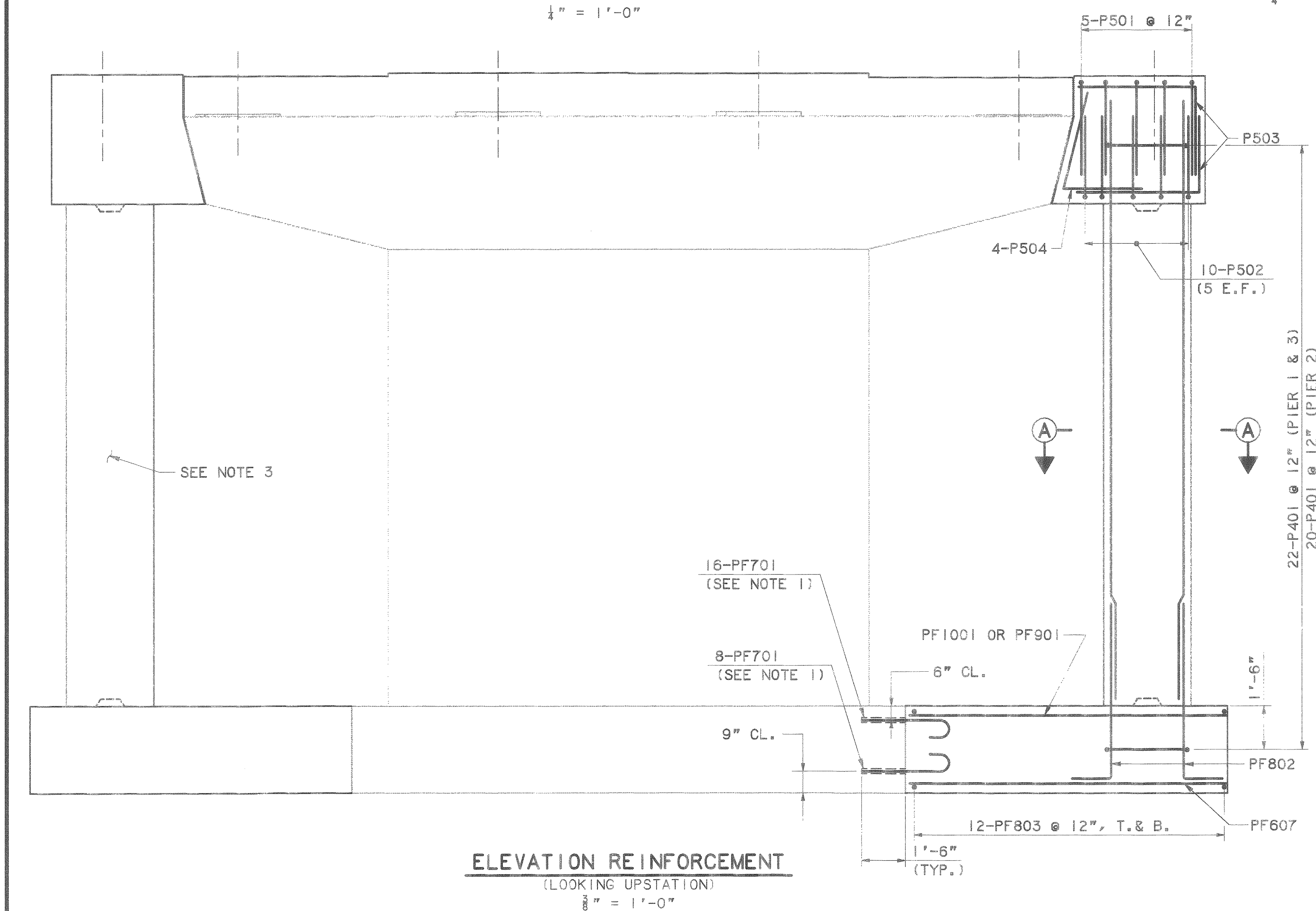
<p>Maine Turnpike Authority</p> <p>Maine Turnpike</p>	
 <p>Transpass</p>	<p>FALMOUTH ROAD UNDERPASS</p> <p>ABUTMENT 2</p> <p>MODIFICATIONS</p>
<p>HNTB</p> <p>HOWARD NEEDLES TAMMEN & BERGENDOFF, INC. ARCHITECTS ENGINEERS PLANNERS</p>	
<p>Contract 97.9</p>	<p>Sheet No. FR-6</p> <p>36 of 56</p>




ELEVATIONS			
POINT	PIER 1	PIER 2	PIER 3
A	126.69	126.83	126.92
B	126.62	126.98	126.86
C	126.73	127.09	126.99
D	126.71	127.09	126.99
E	126.57	126.96	126.88
F	126.62	126.81	126.94
G	125.24	125.69	125.53
H	102.00	105.00	102.00

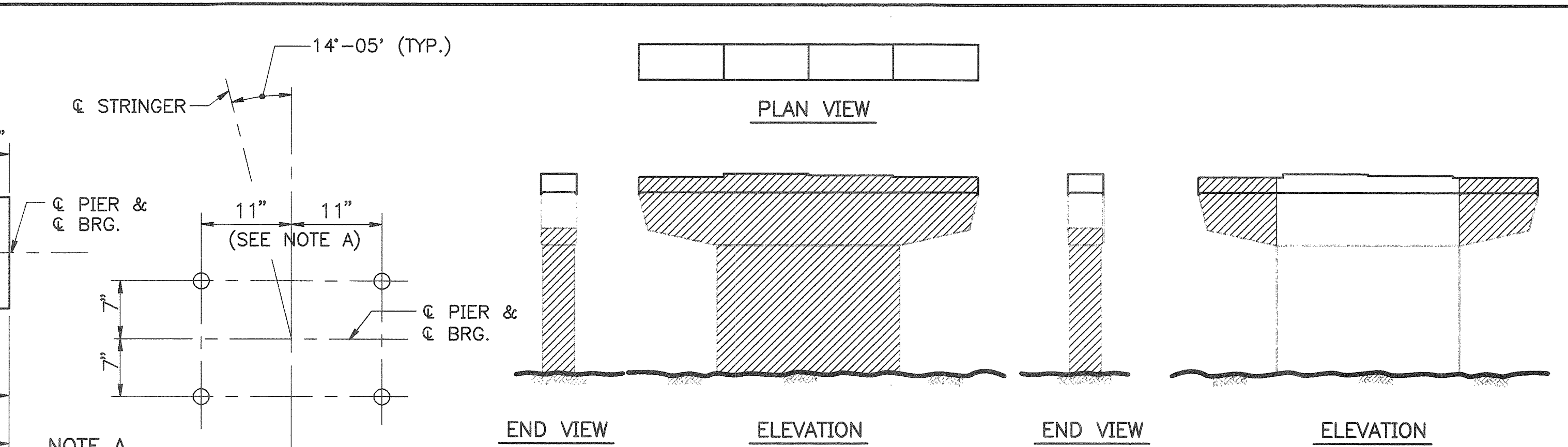
NOTES

1. DOWELS AS SHOWN SHALL BE DRILLED AND GROUTED INTO EXISTING FOOTINGS RESPECTIVELY. TOP DOWELS TO BE SPACED AT 6", SIMILAR TO TOP LONGITUDINAL BARS ON THE PROPOSED FOOTING. BOTTOM DOWELS TO BE SPACED AT 12", SIMILAR TO BOTTOM LONGITUDINAL BARS ON THE PROPOSED FOOTING.
2. FOR REINFORCING STEEL SCHEDULE, SEE SHEET FR-25.
3. REINFORCING STEEL SIMILAR TO PROPOSED COLUMN AND FOOTING SHOWN DETAILED BUT OPPOSITE HAND.
4. SEE SHEET NO. FR-11, FOR DETAILS ALONG THE EXISTING PIER CAPS RESPECTIVELY.



					By:	Date:
				Designed	AD	1/97
				Drawn	LS	1/97
				Checked	JMH	1/97
No.	Revision	By:	Date:	In charge of:	RAL	

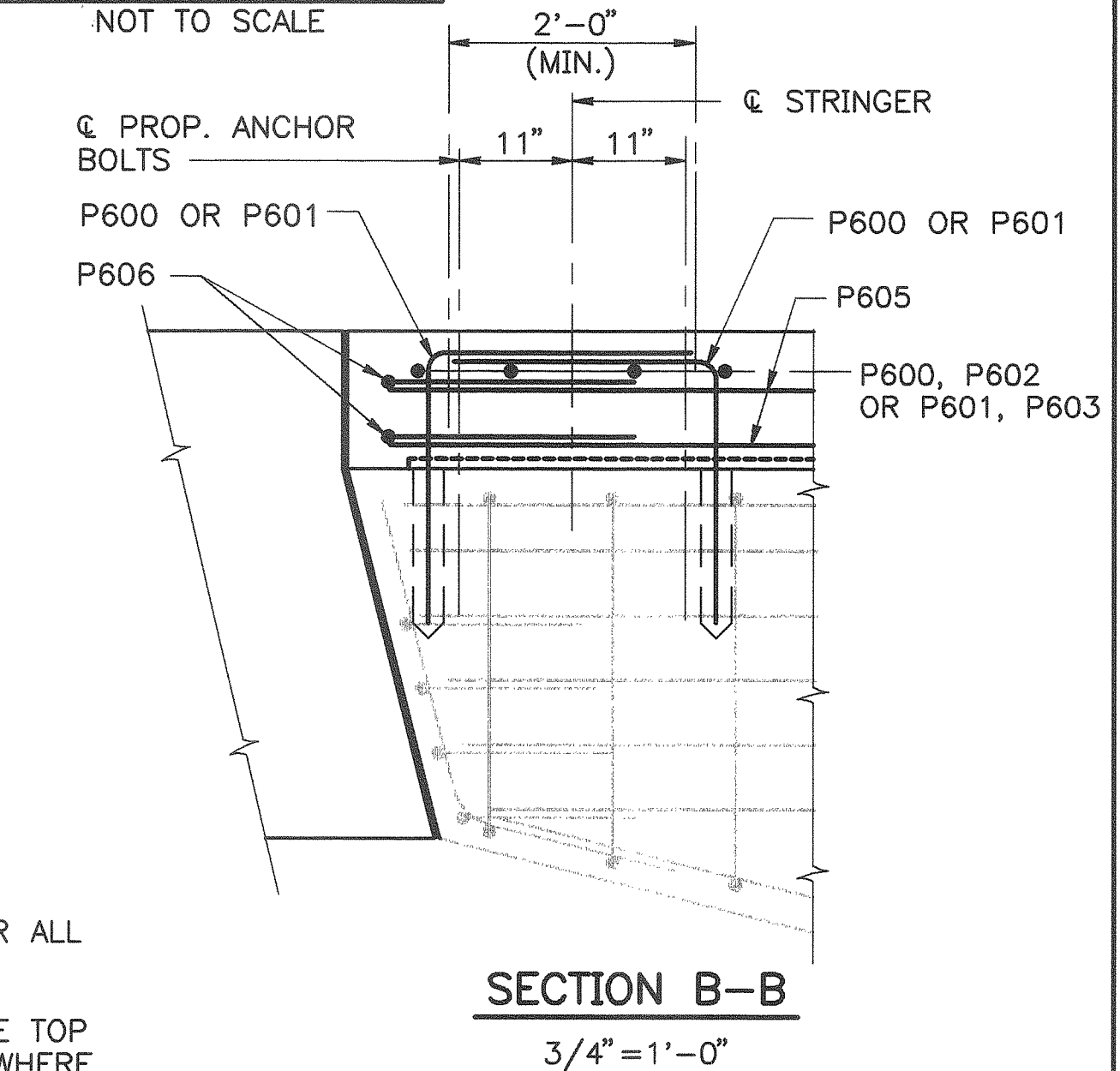
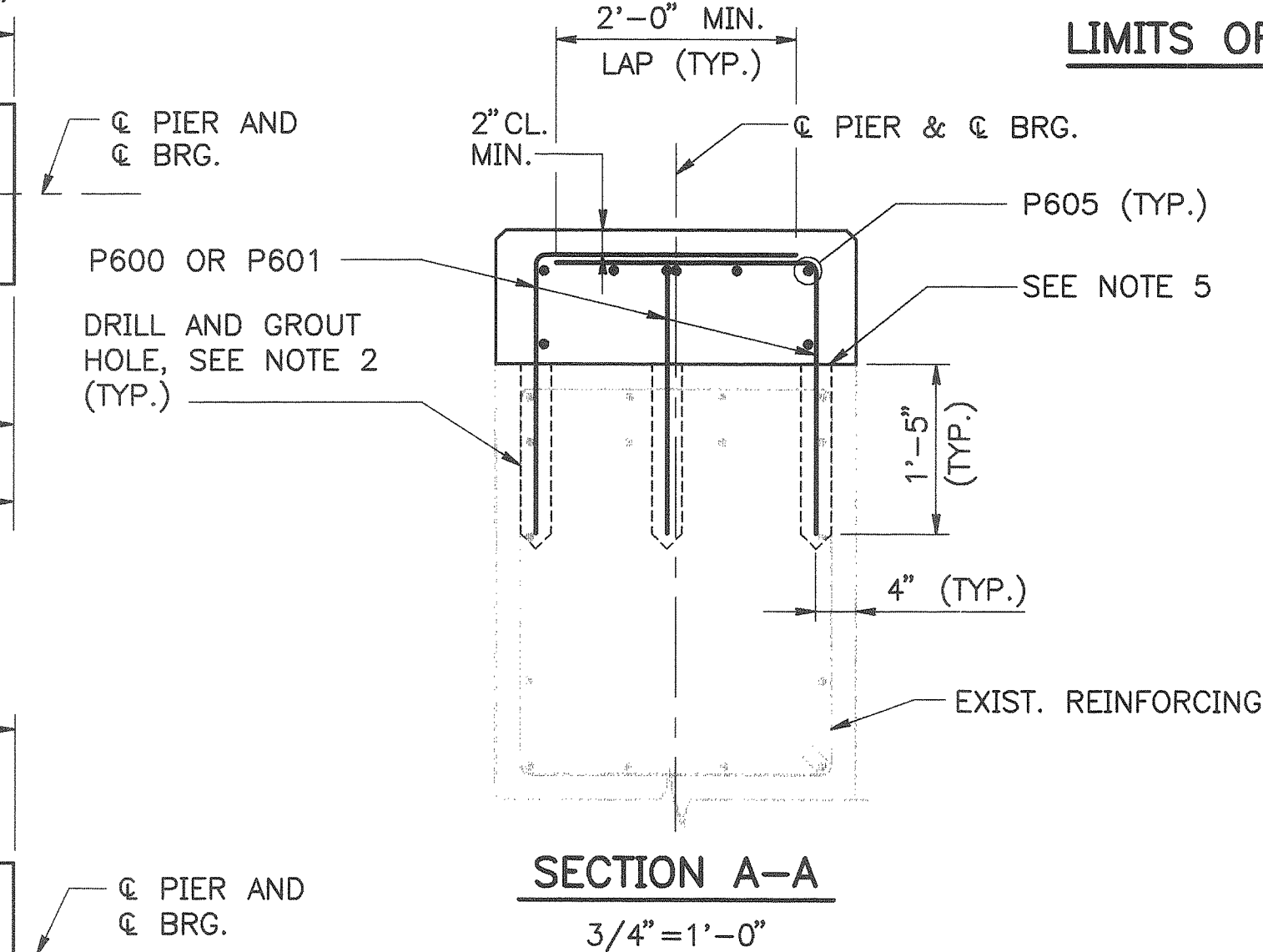
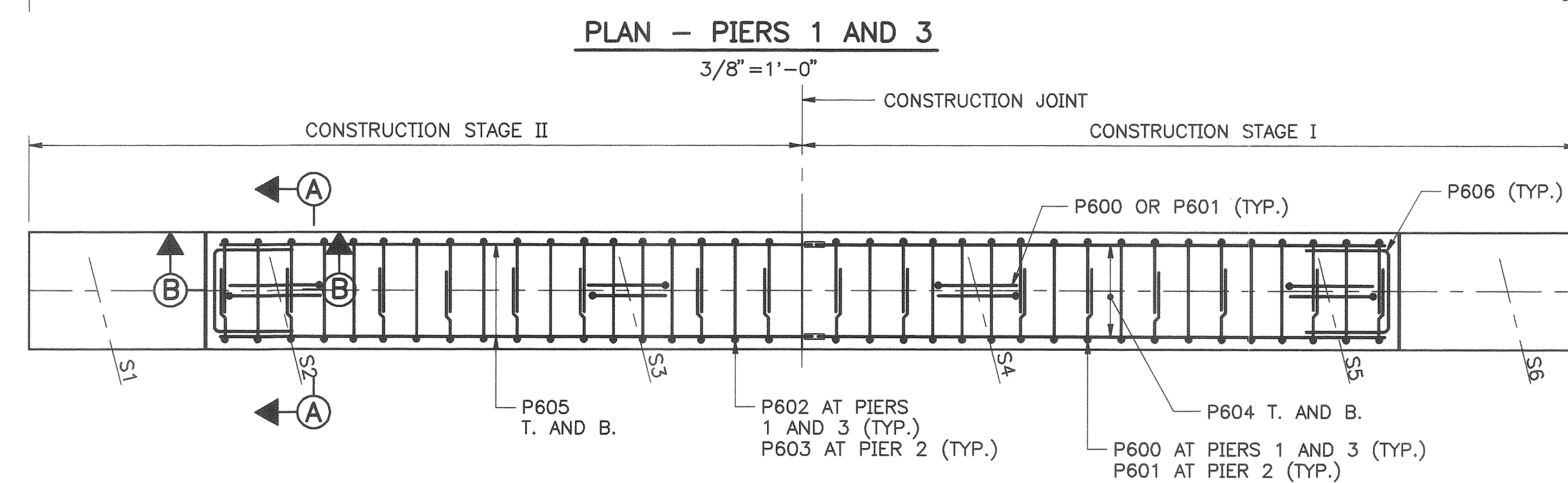
	Maine Turnpike Authority Maine Turnpike	FALMOUTH ROAD UNDERPASS PIER MODIFICATIONS I
	HNTB	HOWARD NEEDLES TAMMEN & BERGENDOFF, INC. ARCHITECTS ENGINEERS PLANNERS
Contract 97.9	Sheet No. FR-10 <div style="display: flex; align-items: center; justify-content: center;"> <div style="border: 1px solid black; padding: 5px 20px;">40</div> <div style="margin: 0 10px;">of</div> <div style="border: 1px solid black; padding: 5px 20px;">56</div> </div>	



NOTES

1. ALL EXPOSED SURFACES ON PIER 2, EXCEPT TOP OF PIER CAP AND SIDES OF PIER CAP AS SHOWN IN END VIEW SHALL BE COATED WITH PROTECTIVE COATING.
2. ONLY EXISTING PIERS ARE SHOWN ABOVE FOR CLARITY PURPOSES. FOR LIMITS ON PROPOSED PIER EXTENSIONS, SEE SHEET NO. FR-10.

PIERS 1, 2, AND 3



$$3/8'' = 1' - 0''$$


(LOOKING UP STATION)

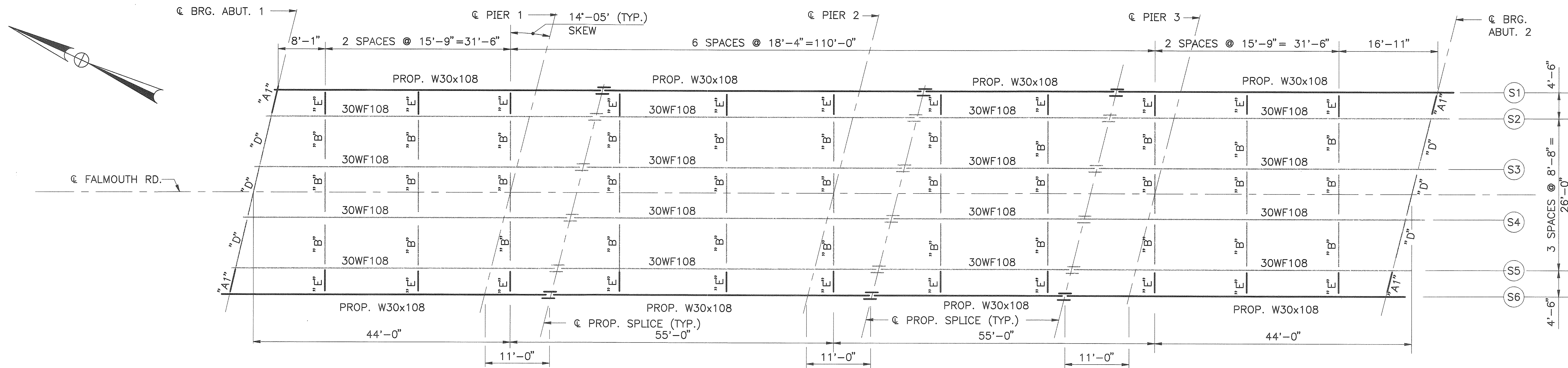
ELEVATIONS			
POINT	PIER 1	PIER 2	PIER 3
A	126.69	126.83	126.92
B	126.62	126.98	126.86
C	126.73	127.09	126.99
D	126.71	127.09	126.99
E	126.57	126.96	126.88
F	126.62	126.81	126.94
G	125.24	125.69	125.53

- ## NOTES
1. REINFORCING SHOWN IS TYPICAL FOR ALL PIERS, UNLESS OTHERWISE NOTED.
 2. THE CONTRACTOR SHALL EXPOSE THE TOP LAYER OF THE REINFORCING STEEL WHERE DRILLING IS REQUIRED PRIOR TO ANY DRILLING. THE CONTRACTOR SHALL REPORT INTERFERENCE OF DOWELS WITH EXISTING REINFORCING TO THE ENGINEER. CARE SHALL BE TAKEN NOT TO DAMAGE EXISTING REINFORCING STEEL. (DRILLING AND GROUTING HOLES SHALL BE INCIDENTAL TO ITEM 502.23)
 3. REMOVAL OF THE EXISTING MASONRY PLATE, ANCHOR BOLTS AND BEARING ASSEMBLY SHALL BE INCIDENTAL TO ITEM 202.12.
 4. SEE SHEET NO. FR-10 FOR PROPOSED REINFORCING STEEL.
 5. REMOVE ALL DETERIORATED OR LOOSE CONCRETE. ROUGHEN AND CLEAN TO ELIMINATE BOND INHIBITING MATERIALS ON ALL CONCRETE SURFACES, INCLUDING LOCATIONS WHERE EXPOSING THE TOP LAYER OF REINFORCING IS NOT REQUIRED. CLEAN ALL EXPOSED REINFORCING STEEL USING AN ACCEPTABLE METHOD APPROVED BY THE ENGINEER AND THEN APPLY AN EPOXY COATING TO BARS. APPLY A BONDING AGENT TO THE EXISTING SUBSTRATE, BY FOLLOWING THE MANUFACTURER'S INSTRUCTIONS AND APPROVED BY THE ENGINEER, PRIOR TO PLACEMENT OF NEW CONCRETE. (INCIDENTAL TO ITEM 502.23.)

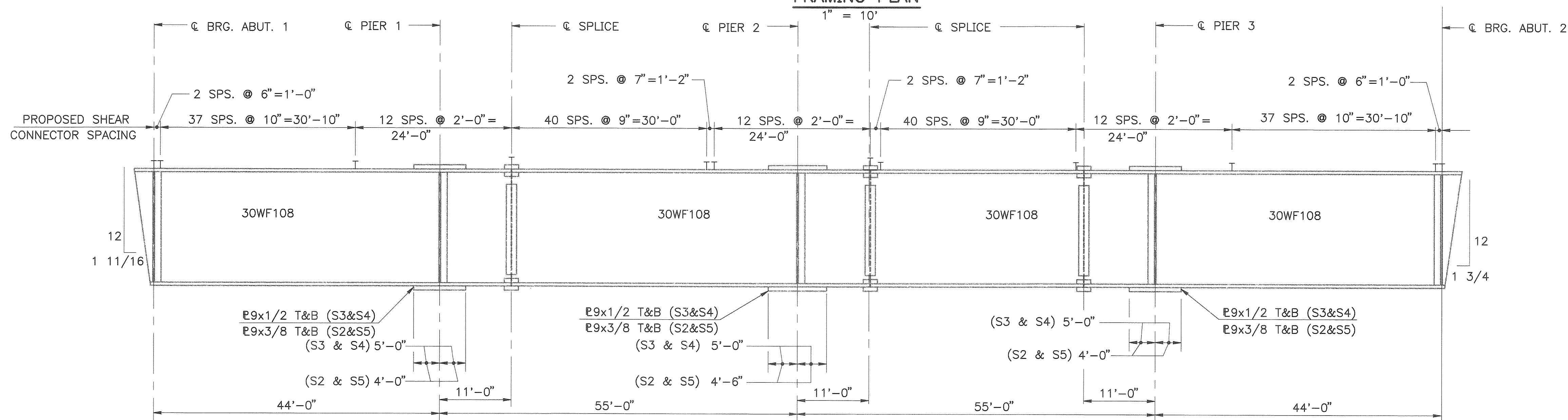
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				Designed	AD	1/
				Drawn	CSL	1/
				Checked	JMH	1/
No.	Revision	By	Date	In Charge Of:	RAL	

<p>Maine Turnpike Authority</p> <p>Maine Turnpike</p>	
 <p>Trans pass</p>	<p>FALMOUTH ROAD UNDERPASS</p> <p>PIER MODIFICATIONS II</p>
<p>HNTB</p>	<p>HOWARD NEEDLES TAMMEN & BERGENDOFF, INC</p> <p>ARCHITECTS ENGINEERS PLANNERS</p>
<p>Contract 97.9</p>	<p>Sheet No. FR-11</p> <p>41 of 56</p>

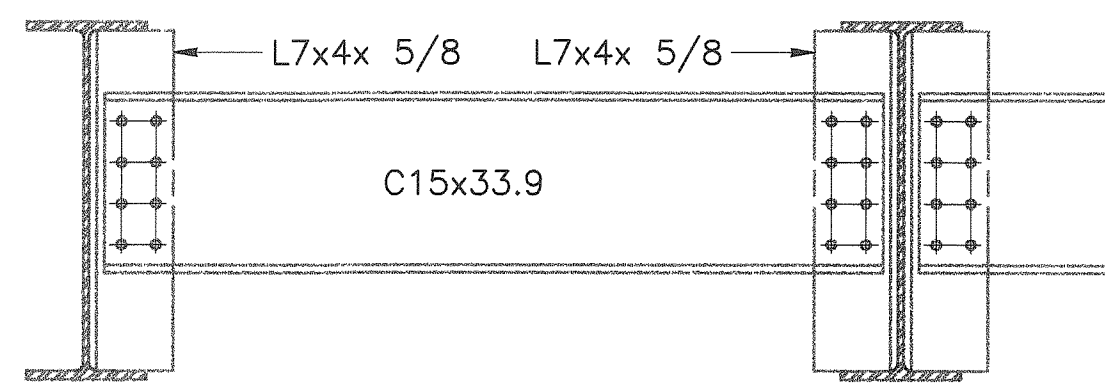
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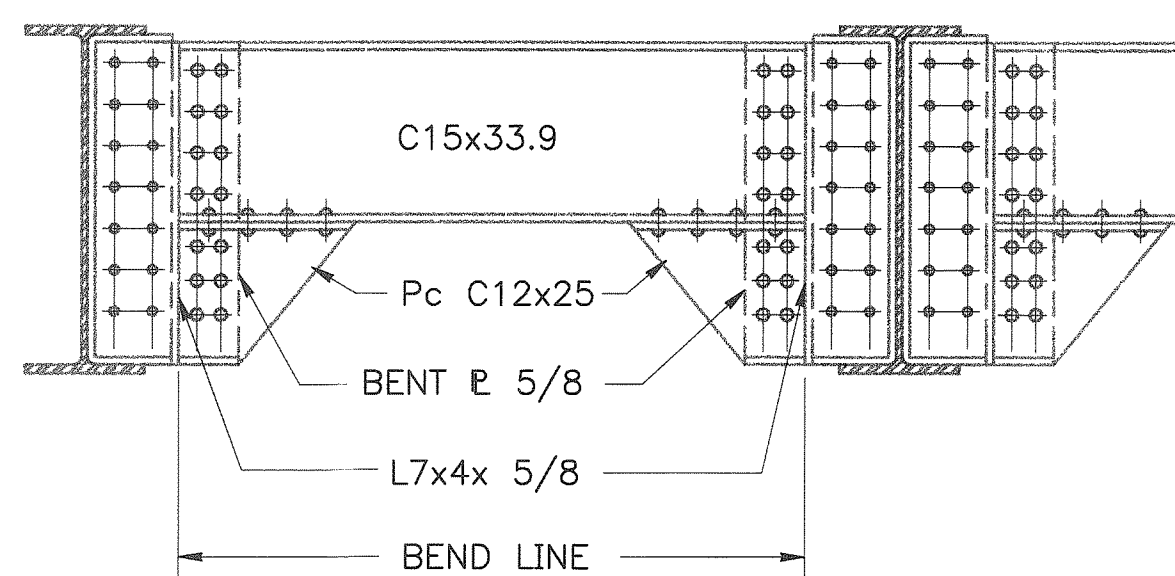
FRAMING PLAN



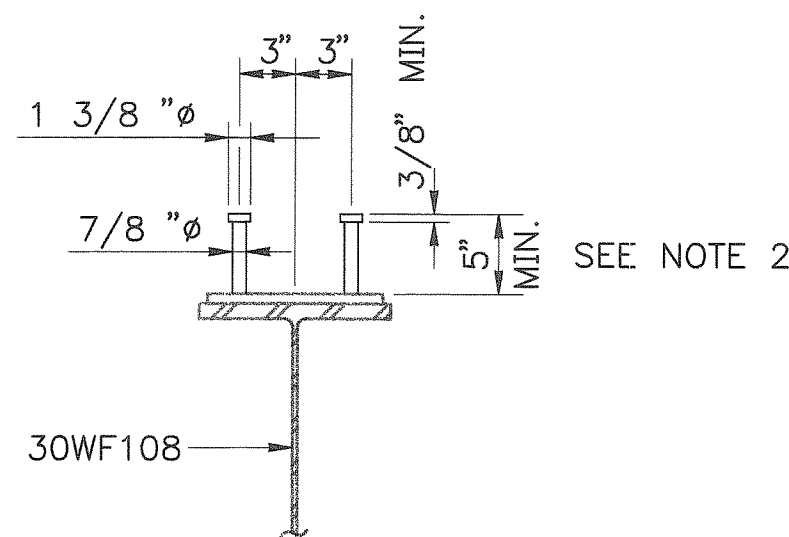
STRINGER S2 - S5 ELEVATION (EXISTING)



DIAPHRAGM TYPE B (EXISTING)



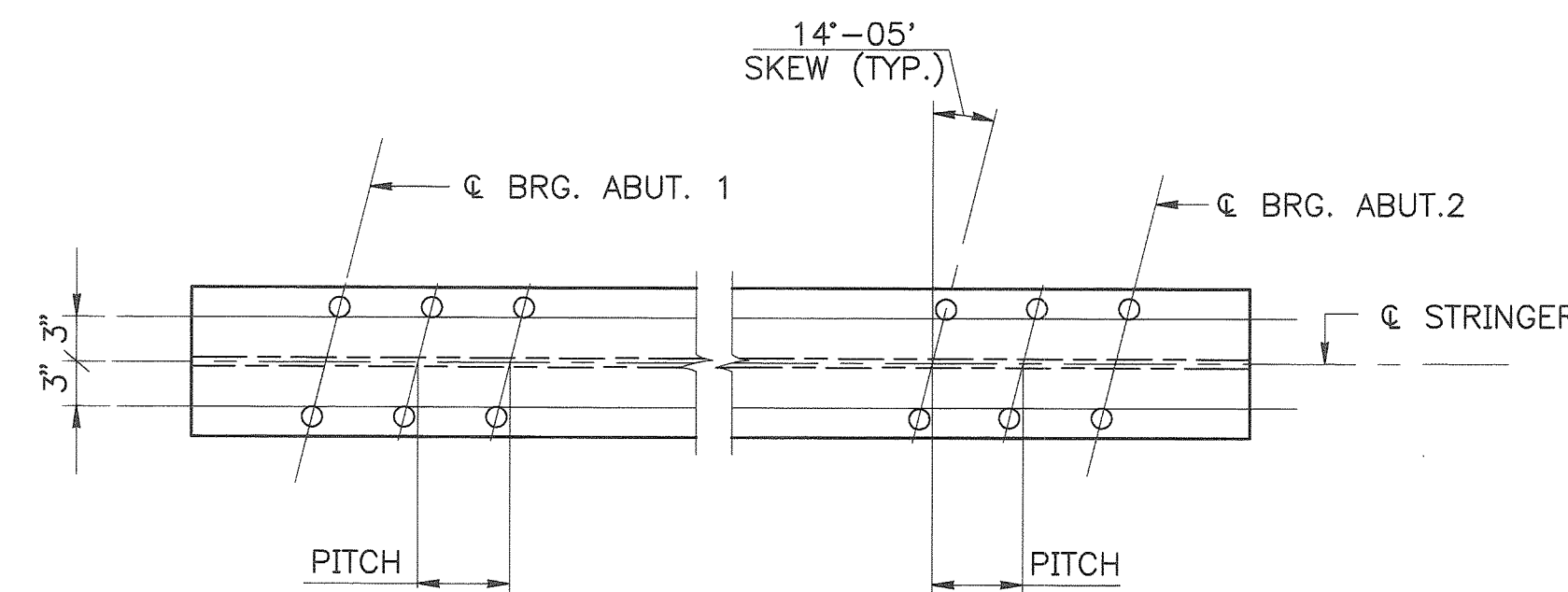
DIAPHRAGM TYPE D (EXISTING)



SHEAR CONN. NOTES

1. SEE STRINGER ELEVATION FOR PROPOSED SHEAR CONNECTOR SPACING
2. 7" FOR EXTRA DEPTH HAUNCH, SEE SHEET FR-16.

PROPOSED SHEAR CONNECTOR DETAIL





SHEAR CONNECTOR LAYOUT

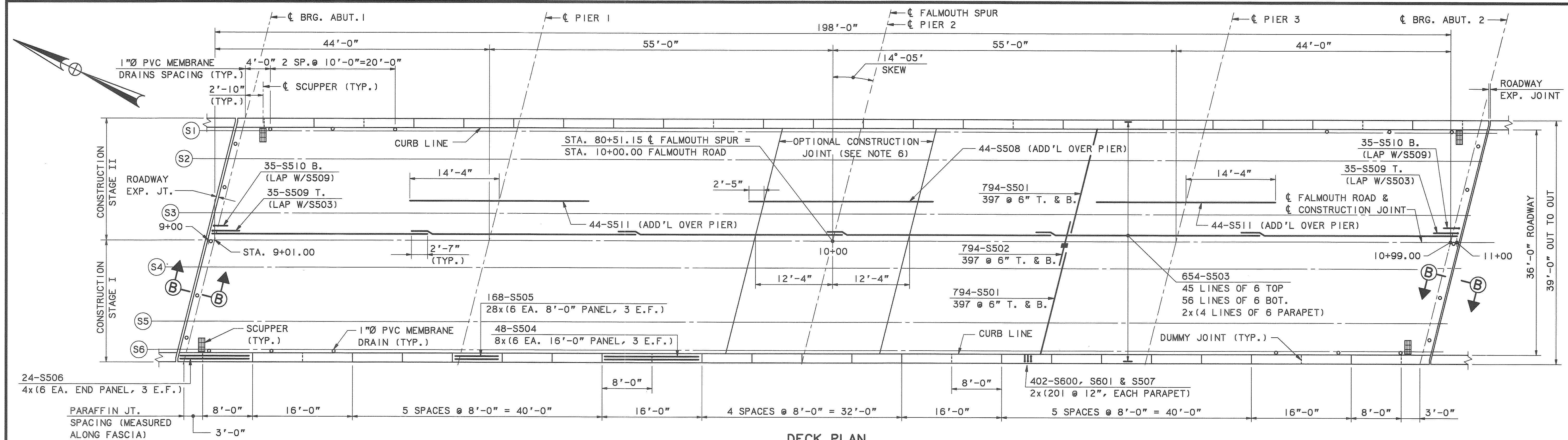
N.T.S.

NOTES

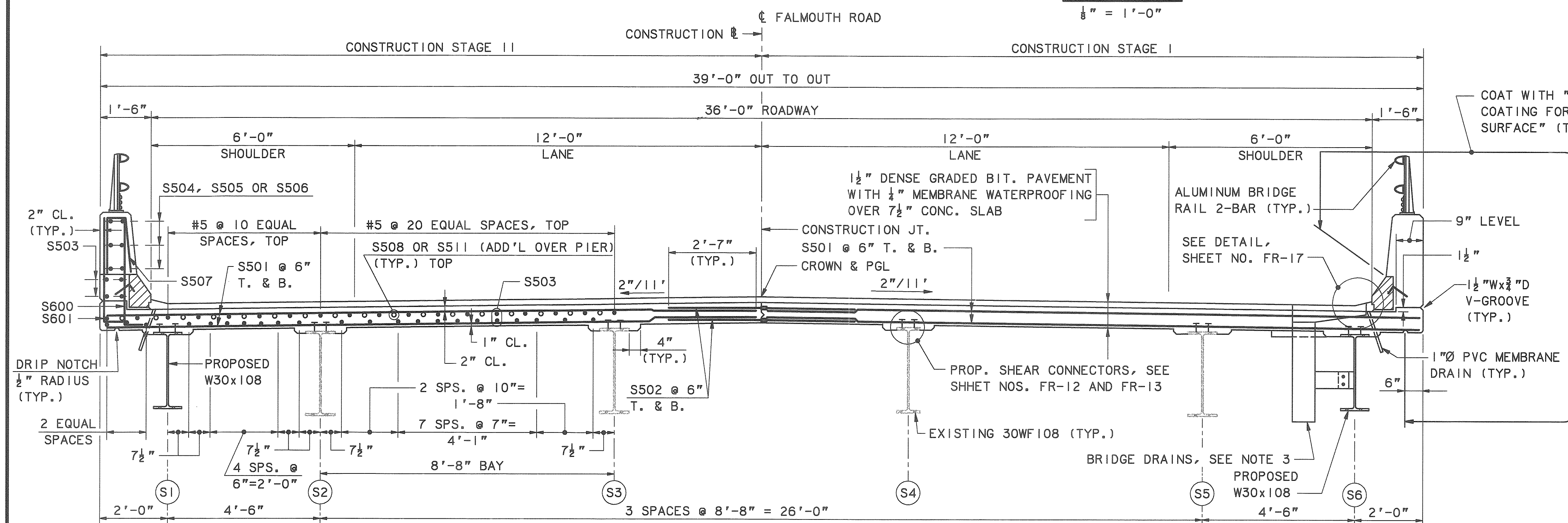
1. EXISTING DIAPHRAGMS ARE CONNECTED TO THE STRINGERS WITH THE USE OF BOLTS AND/OR RIVETS. ALL BOLTS AND RIVETS, WHICH ARE REMOVED TO JACK THE SUPERSTRUCTURE, SHALL BE REPLACED WITH NEW 7/8" AASHTO M164 HIGH STRENGTH BOLTS, NUTS AND WASHERS, WHICH SHALL BE INCIDENTAL TO ITEM 504.721, JACKING EXISTING SUPERSTRUCTURE. ALL DIAPHRAGM HOLES SHALL BE GROUND SMOOTH.
2. PROPOSED SHEAR CONNECTORS ARE REQUIRED AND SHOWN ON THE EXISTING STRINGER ELEVATIONS. SHEAR CONNECTORS ARE PAID FOR UNDER ITEM 505.09.
3. FOR PROPOSED STRINGERS S1 AND S6 PLUS DIAPHRAGM TYPES "A1" AND "E", SEE SHEET NO. FR-13.
4. ALL DIAPHRAGM BOLTED CONNECTIONS BETWEEN STAGE I AND II SHALL BE MADE AFTER STAGE II IS LOADED.

Maine Turnpike Authority Maine Turnpike		FALMOUTH ROAD UNDERPASS FRAMING PLAN AND STRINGER ELEVATION	
		 HOWARD NEEDLES TAMMEN & BERGENDOFF, INC ARCHITECTS ENGINEERS PLANNERS	
Contract 97.9		Sheet No. FR-12 42 of 56	

No.	Revision	By	Date
		Designed	AD 1/97
		Drawn	CSL 1/97
		Checked	JMH 1/97
		In Charge Of	RAL



DECK PLAN



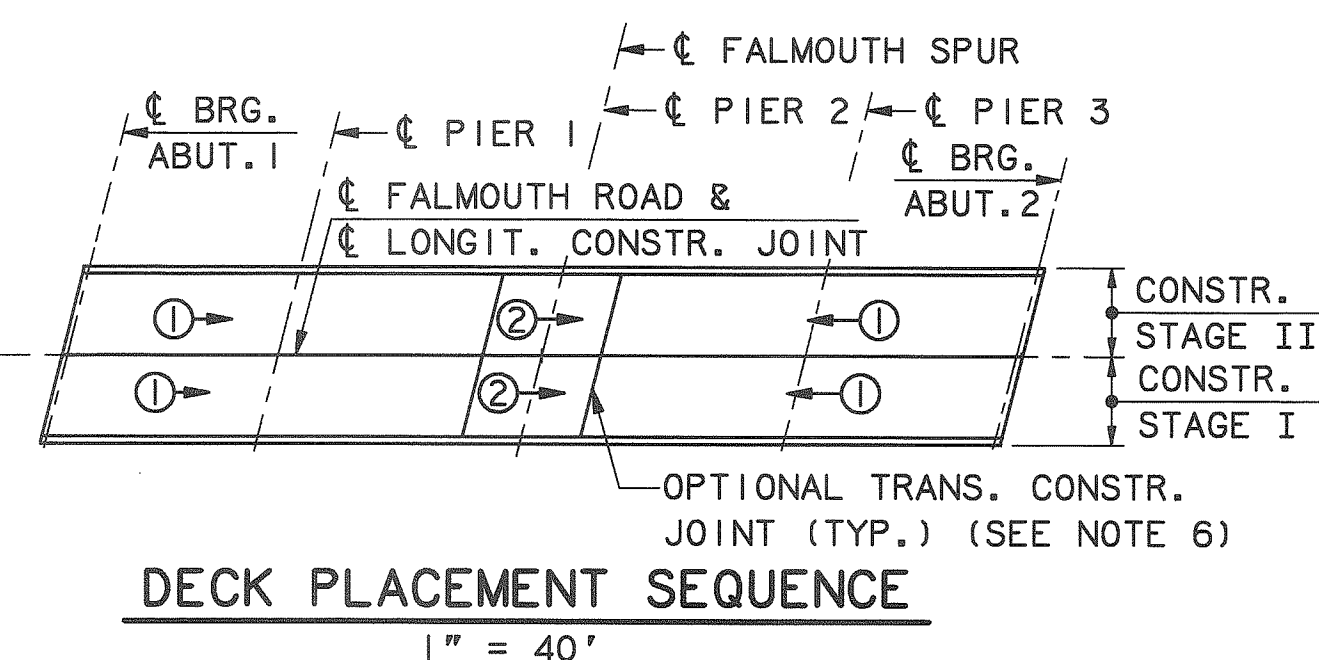
TYPICAL SECTION

(LOOKING UPSTATION)

$$\frac{1}{2}'' = 1' - 0''$$

SUPERSTRUCTURE NOTES



1. ADJUST REINFORCING STEEL TO FIT AROUND THE DRAINS IN A MANNER APPROVED BY THE ENGINEER. DO NOT CUT TRANSVERSE REINFORCING BARS. CUT LONGITUDINAL REINFORCING STEEL IN THE FIELD TO SUIT, THEN EPOXY COAT CUT ENDS.
2. FOR STEEL REINFORCING SCHEDULE, SEE SHEET NO. FR-25.
3. FOR SCUPPER AND DRAIN DETAILS, SEE SHEET NO. FR-16.
4. FOR 2-BAR ALUMINUM BRIDGE RAIL DETAILS, SEE SHEET NO. FR-20.
5. FOR SLAB DETAILS, SEE SHEET NOS. FR-16 AND FR-17.
6. FOR ROADWAY EXPANSION JOINT DETAILS, SEE SHEET NOS. FR-18 AND FR-19.
7. THE CONCRETE DECK SHALL BE GIVEN A SMOOTH BULL FLOAT OR WOOD FLOAT FINISH.
8. FOR SECTION B-B, SEE SHEET NO. FR-19.
9. CURB JOINTS SHALL LINE UP WITH PARAFFIN AND DUMMY JOINTS.
10. ONLY DOWEL BAR SPLICERS SHALL BE USED WITHIN CONCRETE DECK SLAB FOR STAGE I CONSTRUCTION AT THE LONGITUDINAL CONSTRUCTION JOINT. DOWEL-IN BARS SHALL THEN BE USED WITHIN CONCRETE DECK SLAB FOR STAGE II CONSTRUCTION.
11. FOR RAIL POST SPACING, SEE SHEET NO. FR-17.



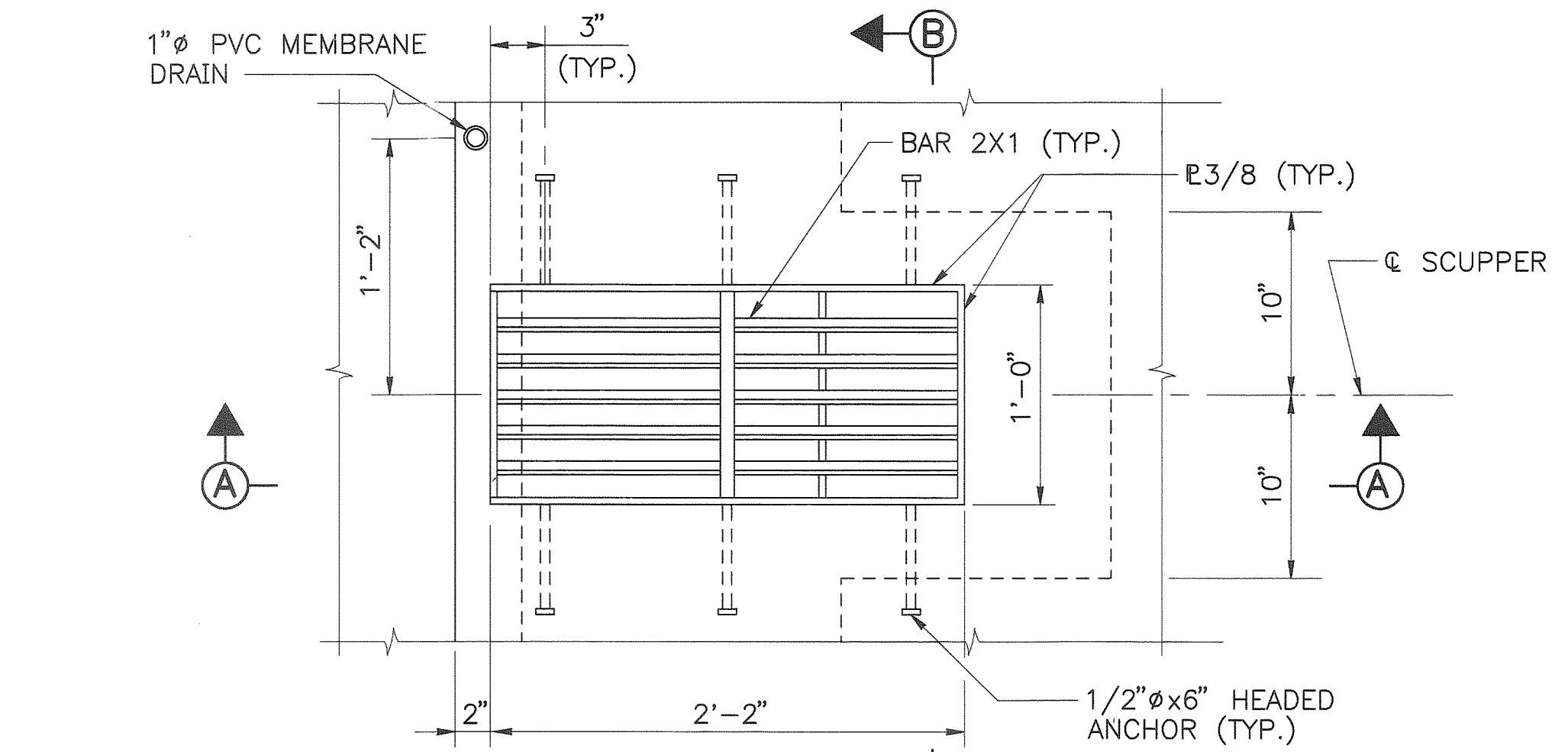
PLACEMENT NOTES

1. THE NUMBERS IN CIRCLES INDICATE PLACING SEQUENCE. THE ARROWS INDICATE DIRECTION OF PLACEMENT.
2. THE FORMWORK FOR THE CONSTRUCTION JOINTS SHALL REMAIN IN PLACE UNTIL A MINIMUM OF 48 HOURS HAS ELAPSED AFTER PLACEMENT OF THE SLAB. AFTER WHICH, REMOVAL OF FORMWORK MEETING THE REQUIREMENTS FOR FORM REMOVAL OF SECTION 502 (STRUCTURAL CONCRETE) OF THE STANDARD SPECIFICATIONS, MAY PROCEED.
3. POURS DESIGNATED BY THE SAME NUMBER DO NOT NECESSARILY HAVE TO BE POURED THE SAME DAY. A WAITING PERIOD OF 72 HOURS IS NECESSARY BETWEEN ADJACENT POURS.
4. STAY IN PLACE FORMS ARE NOT ALLOWED TO BE USED.
5. BEGIN PLACEMENT AT THE LOW END OF THE BLOCK.
6. THE OPTIONAL TRANSVERSE CONSTRUCTION JOINT MAY BE OMITTED PROVIDED THAT THE CONCRETE OF THE ENTIRE DECK SLAB REMAINS PLASTIC UNTIL THE COMPLETION OF THE LAST PLACEMENT AND ALL OTHER SPECIFICATIONS FOLLOWED.

					By: Date
				Designed	AD 1/97
				Drawn	LS 1/97
				Checked	JMH 1/97
No.	Revision	By:	Date:	In charge of:	RAI

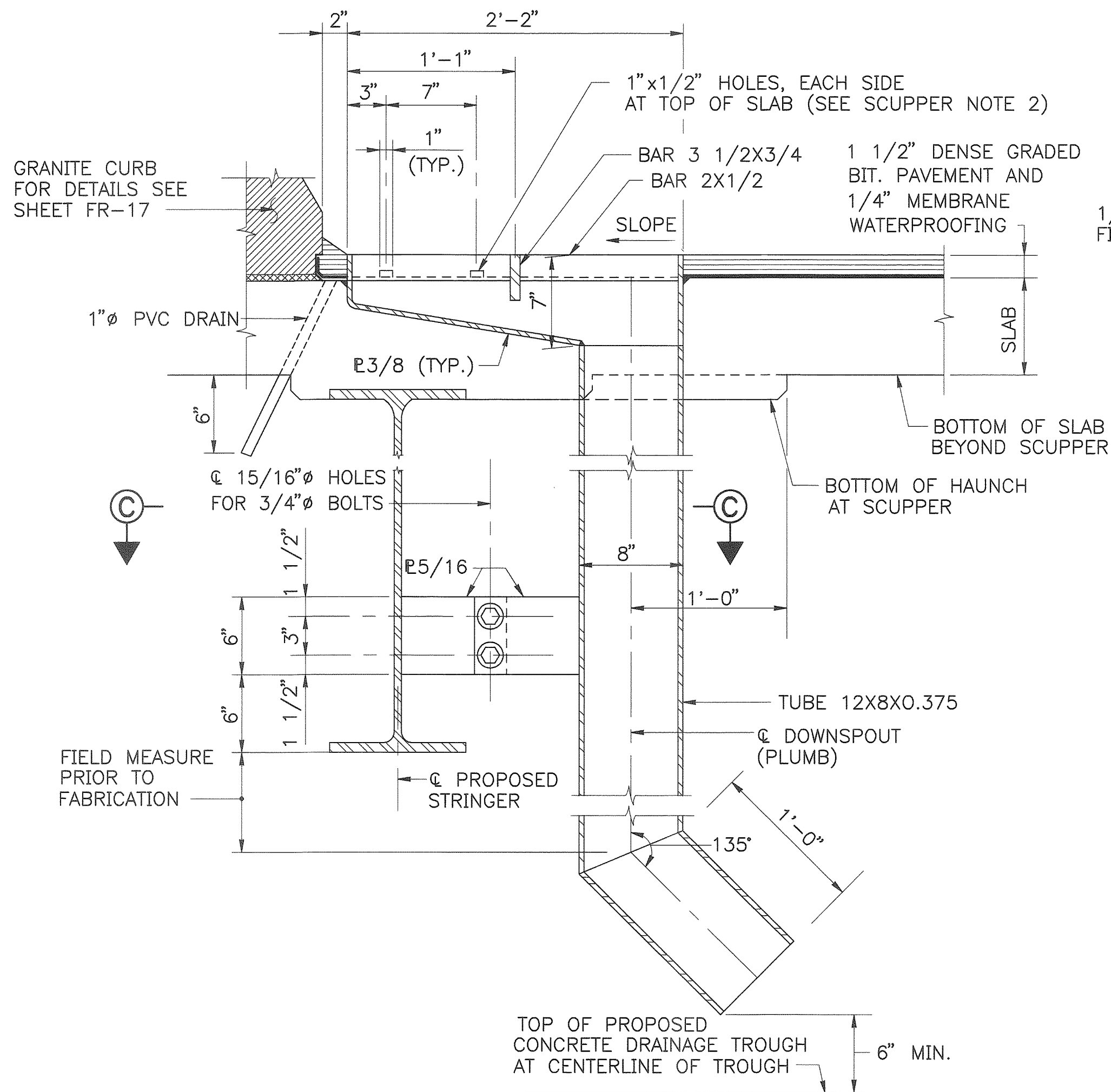
	Maine Turnpike Authority Maine Turnpike	FALMOUTH ROAD UNDERPASS DECK PLAN AND TYPICAL SECTION
		HOWARD NEEDLES TAMMEN & BERGENDOFF, INC. ARCHITECTS ENGINEERS PLANNERS
Contract 97.9	Sheet No. FR-15 <u>45</u> of <u>56</u>	

M:\B09009\958\041DECK\FR-13 01/18/97 14:10 (METPK) BDR-01)



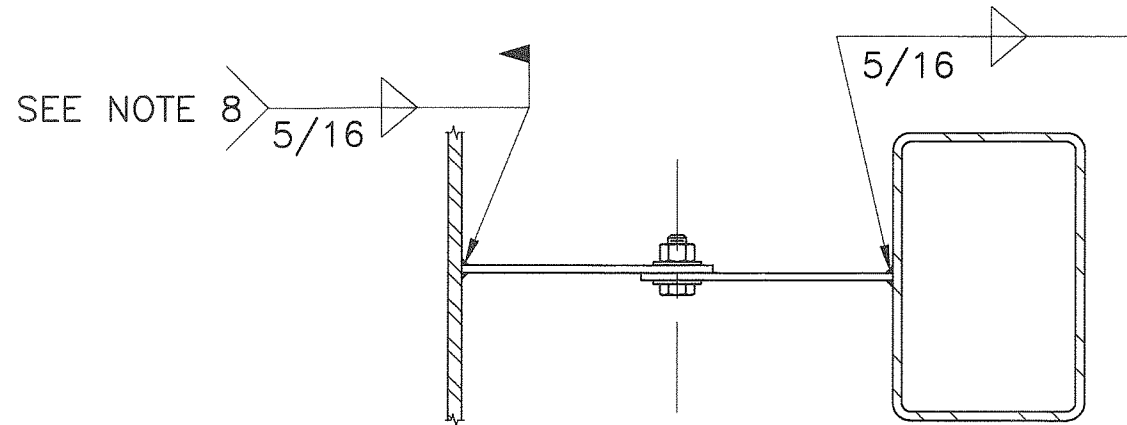
PLAN - SCUPPER

1 1/2" = 1'-0"



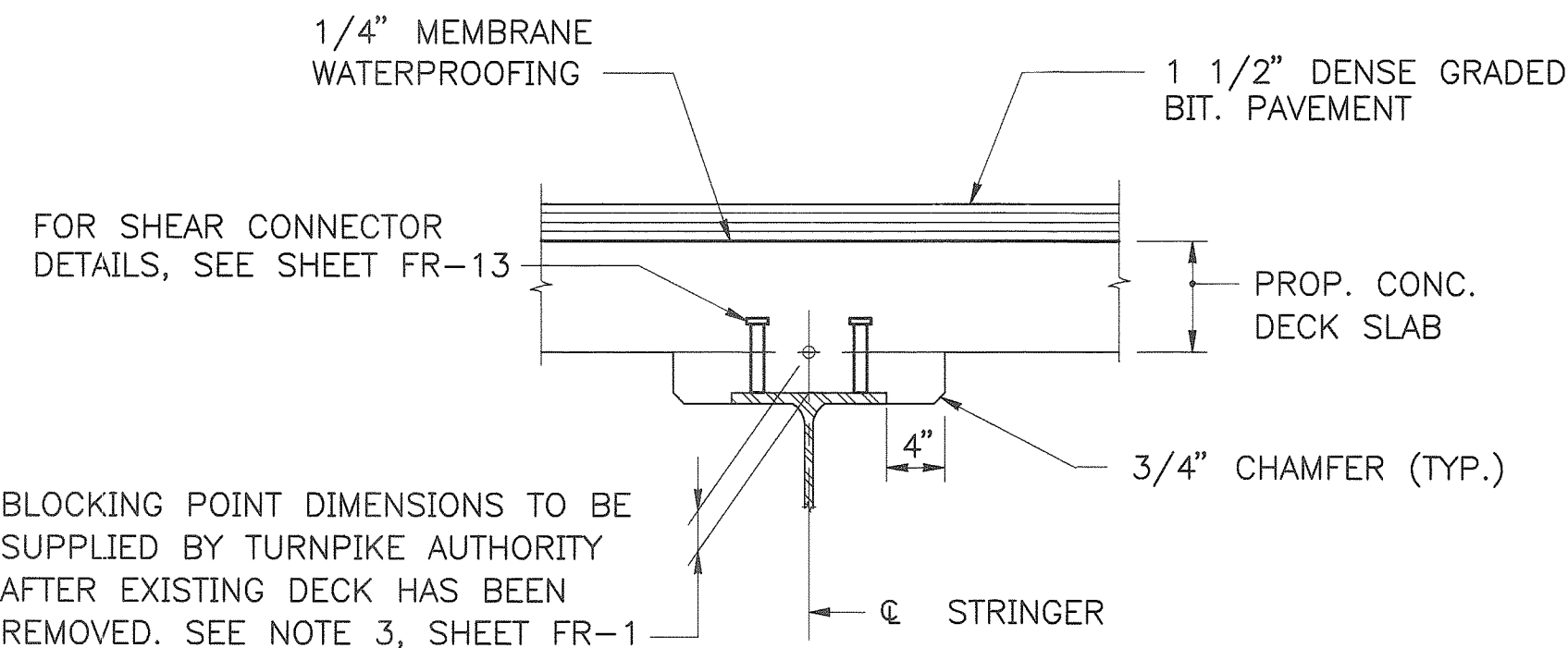
SECTION A-A

1 1/2" = 1'-0"



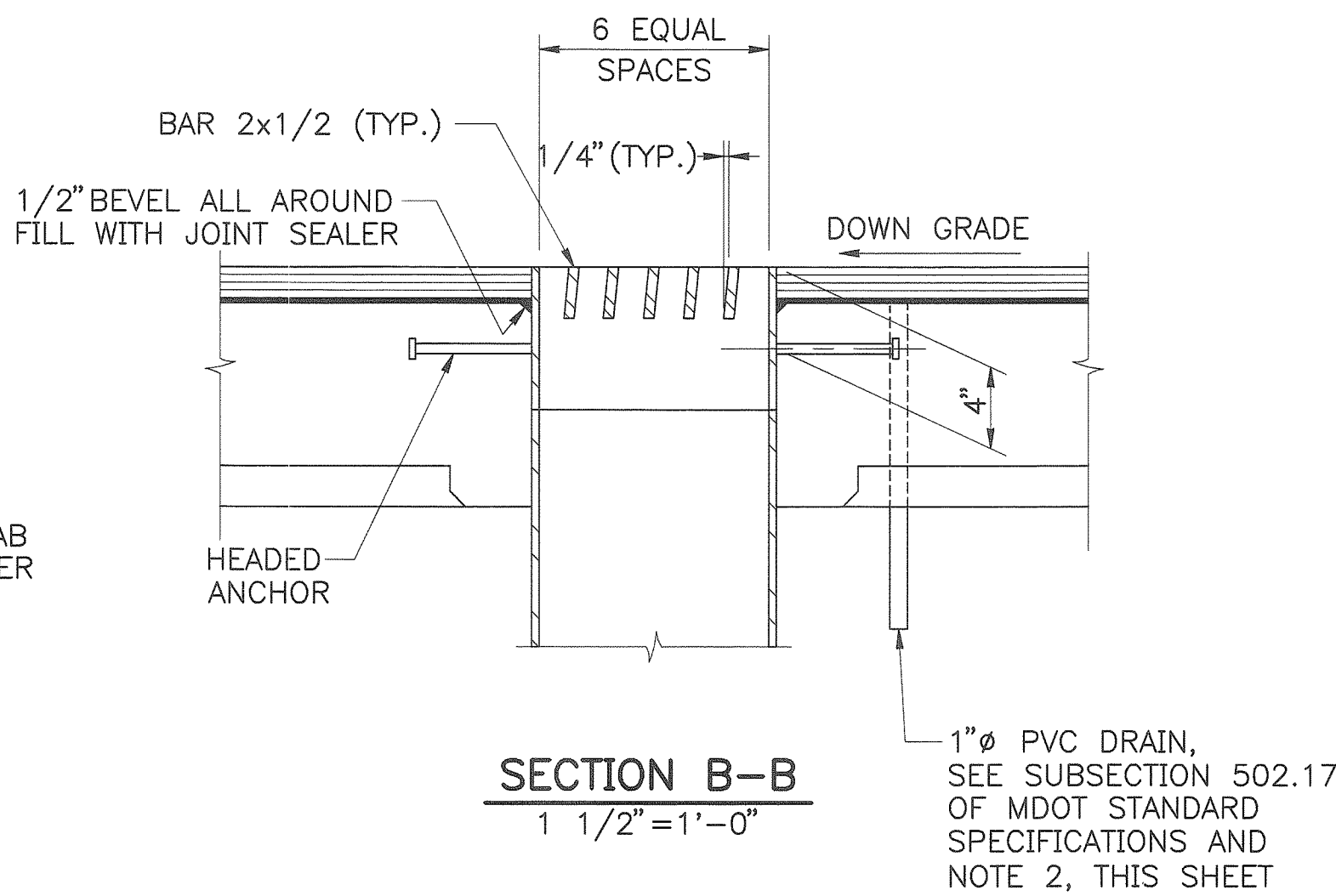
SECTION C-C

1 1/2" = 1'-0"



BLOCKING POINT DETAIL

1" = 1'-0"

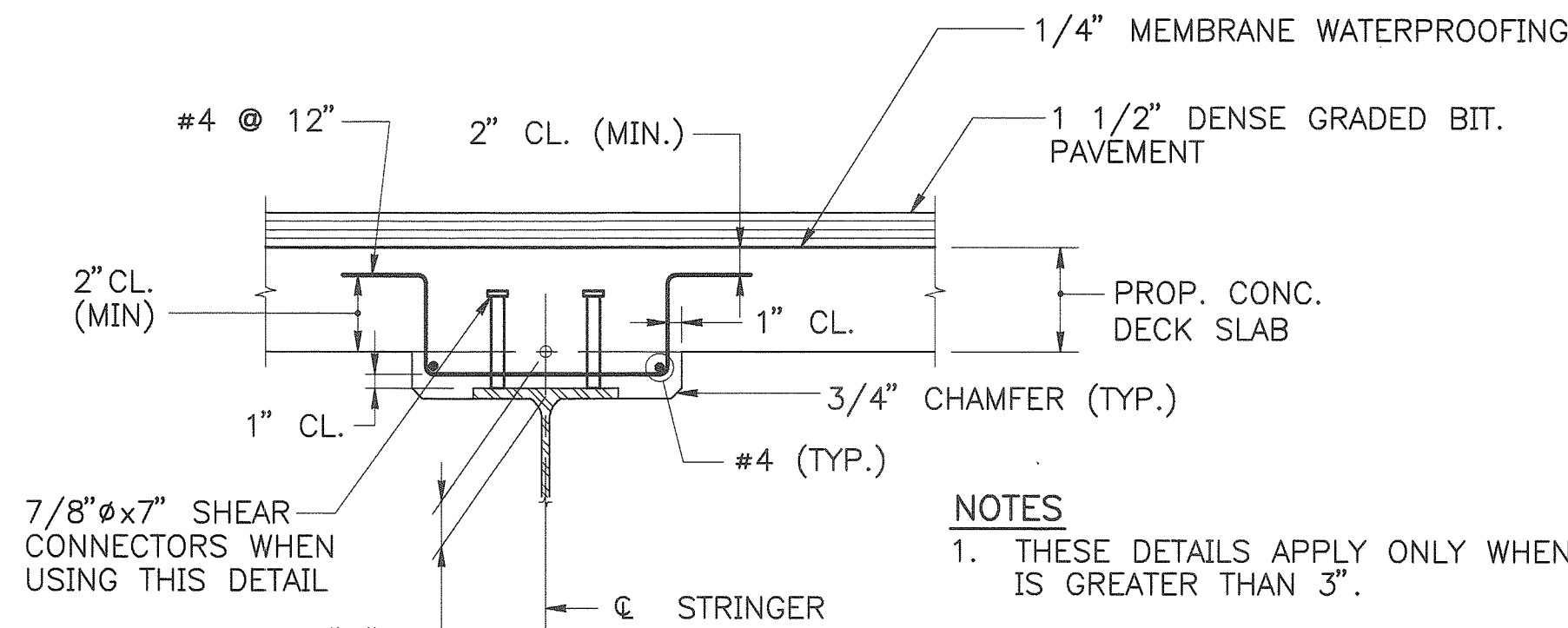


SECTION B-B

1 1/2" = 1'-0"

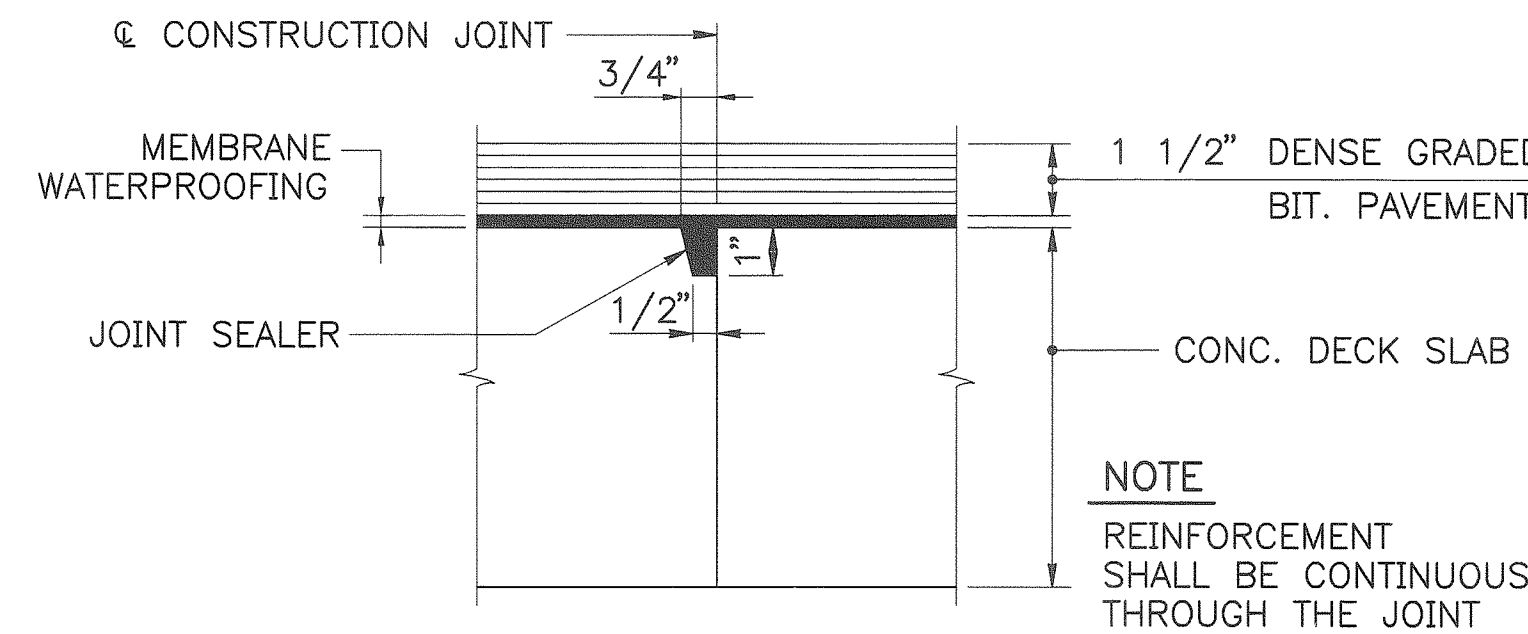
SCUPPER NOTES

1. ALL WELDS SHALL BE CONTINUOUS 1/4" FILLET WELDS EXCEPT AS NOTED.
2. DO NOT COVER DECK DRAINS WITH MEMBRANE WATERPROOFING. DEPRESS DRAINS 1/2" BELOW TOP OF SLAB, PROVIDE 23 GAUGE GALVANIZED SCREENS (1/4" MESH) OVER DRAINS.
3. SCUPPERS TO BE GALVANIZED AFTER FABRICATION. GALVINIZING SHALL CONFORM TO ASTM A153.
4. ALL PLATES SHALL CONFORM TO ASTM A709, GRADE 36.
5. STRUCTURAL TUBES SHALL CONFORM TO ASTM A501.
6. PAYMENT FOR SCUPPERS, PVC DRAINS AND SCREENS SHALL BE INCIDENTAL TO ITEM NO. 502.261.
7. FOR LOCATION OF SCUPPERS AND 1" DRAINS, SEE SHEET NO. FR-15.
8. FIELD REPAIR GALVANIZING WITH ZINC RICH PAINT ACCORDING TO SECTION 506 OF THE STANDARD SPECIFICATIONS. (PAYMENT INCIDENTAL TO ITEM NO. 502.261).



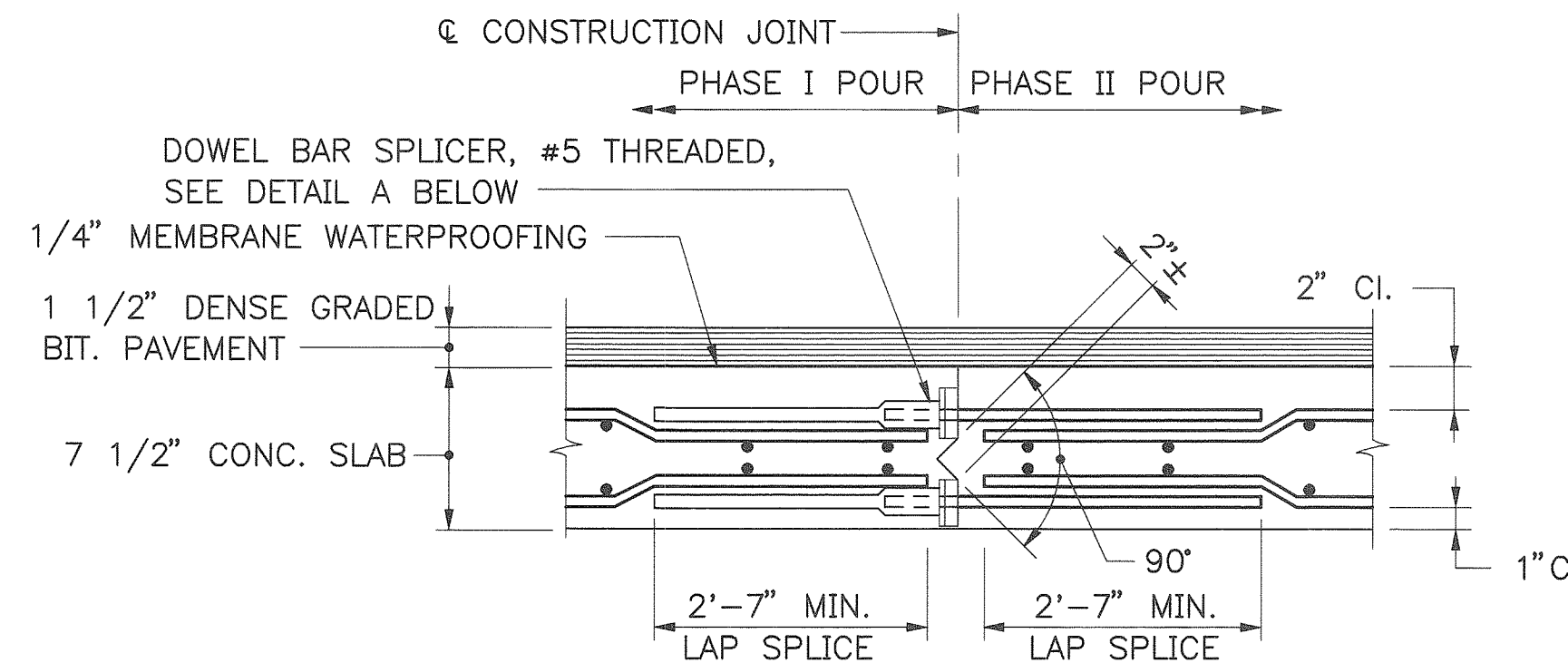
EXTRA DEPTH HAUNCH DETAILS

1" = 1'-0"



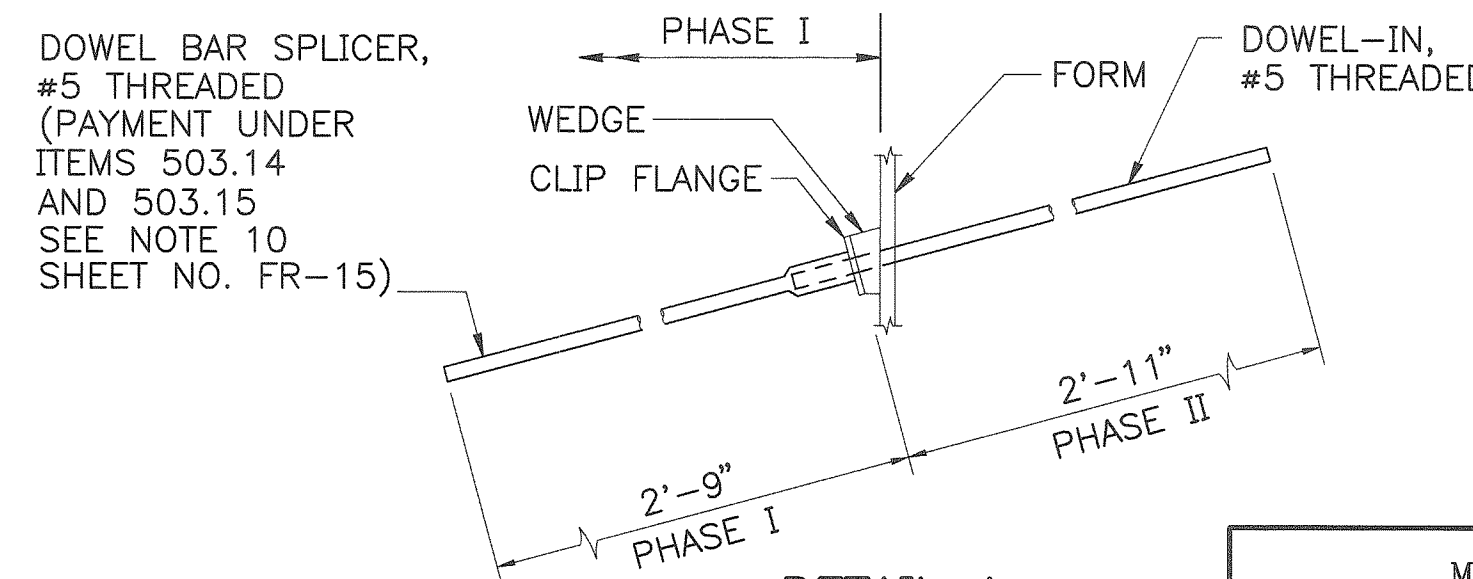
TRANSVERSE CONSTRUCTION JOINT DETAIL

3" = 1'-0"





LONGITUDINAL CONSTRUCTION JOINT DETAIL

NOT TO SCALE

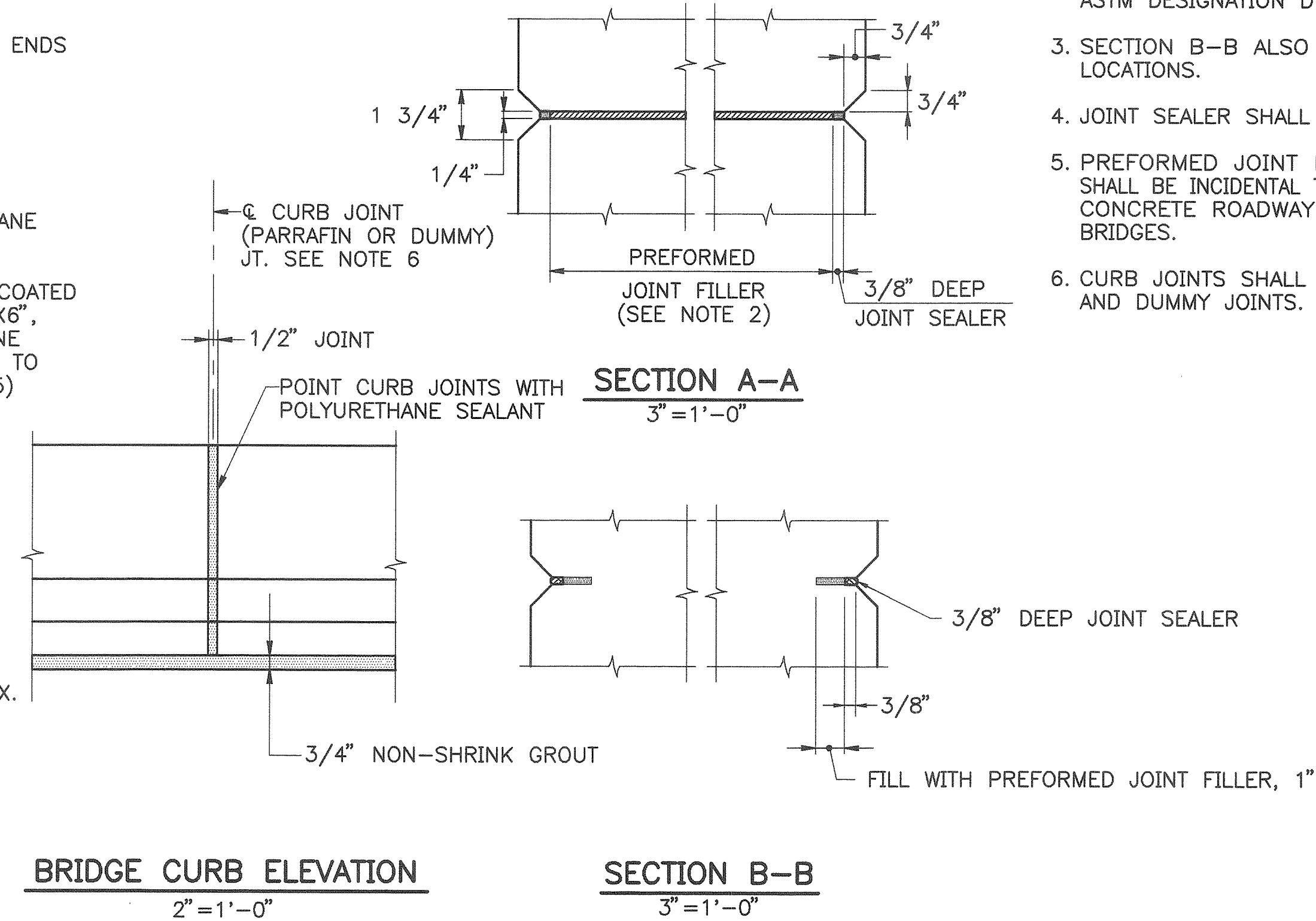
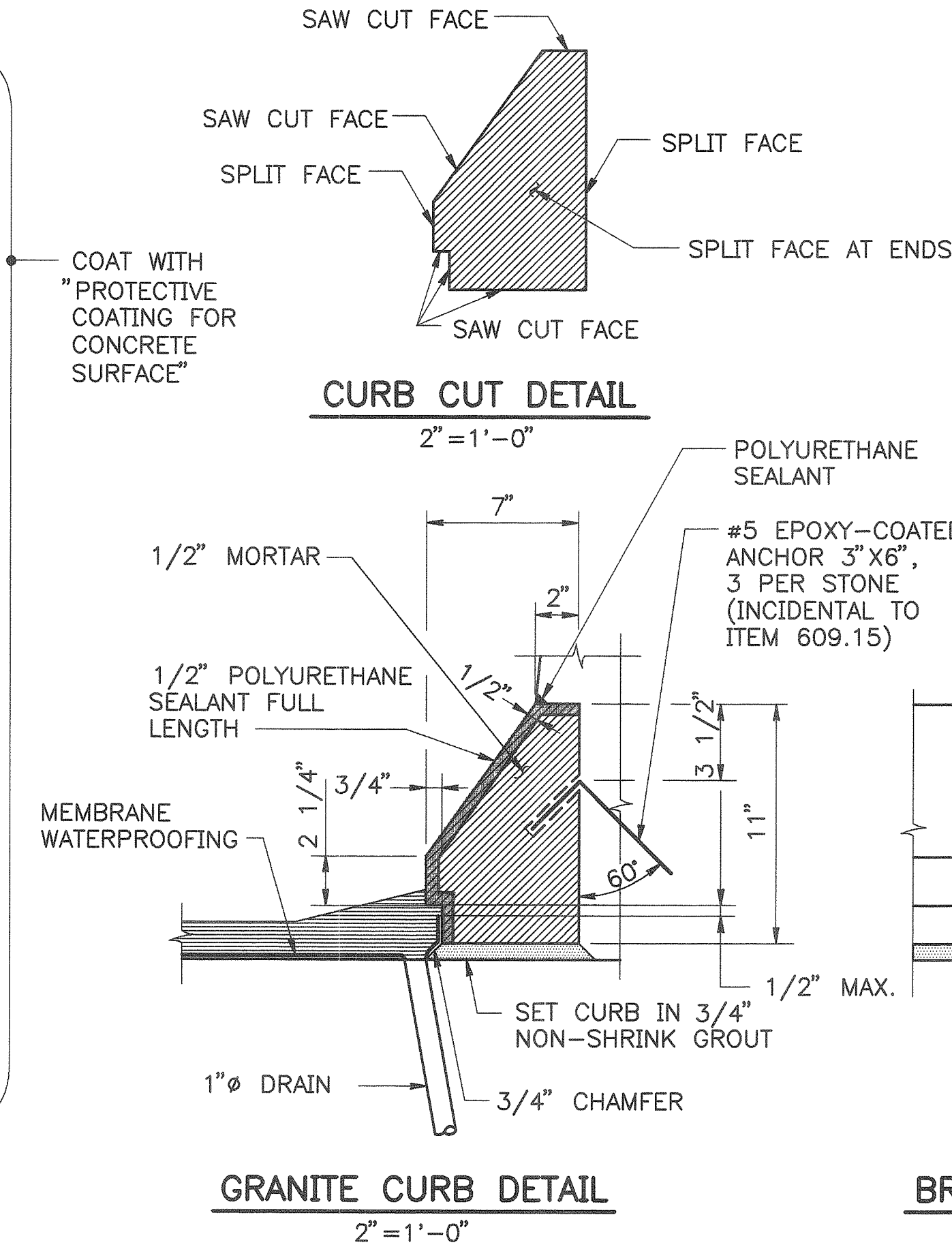
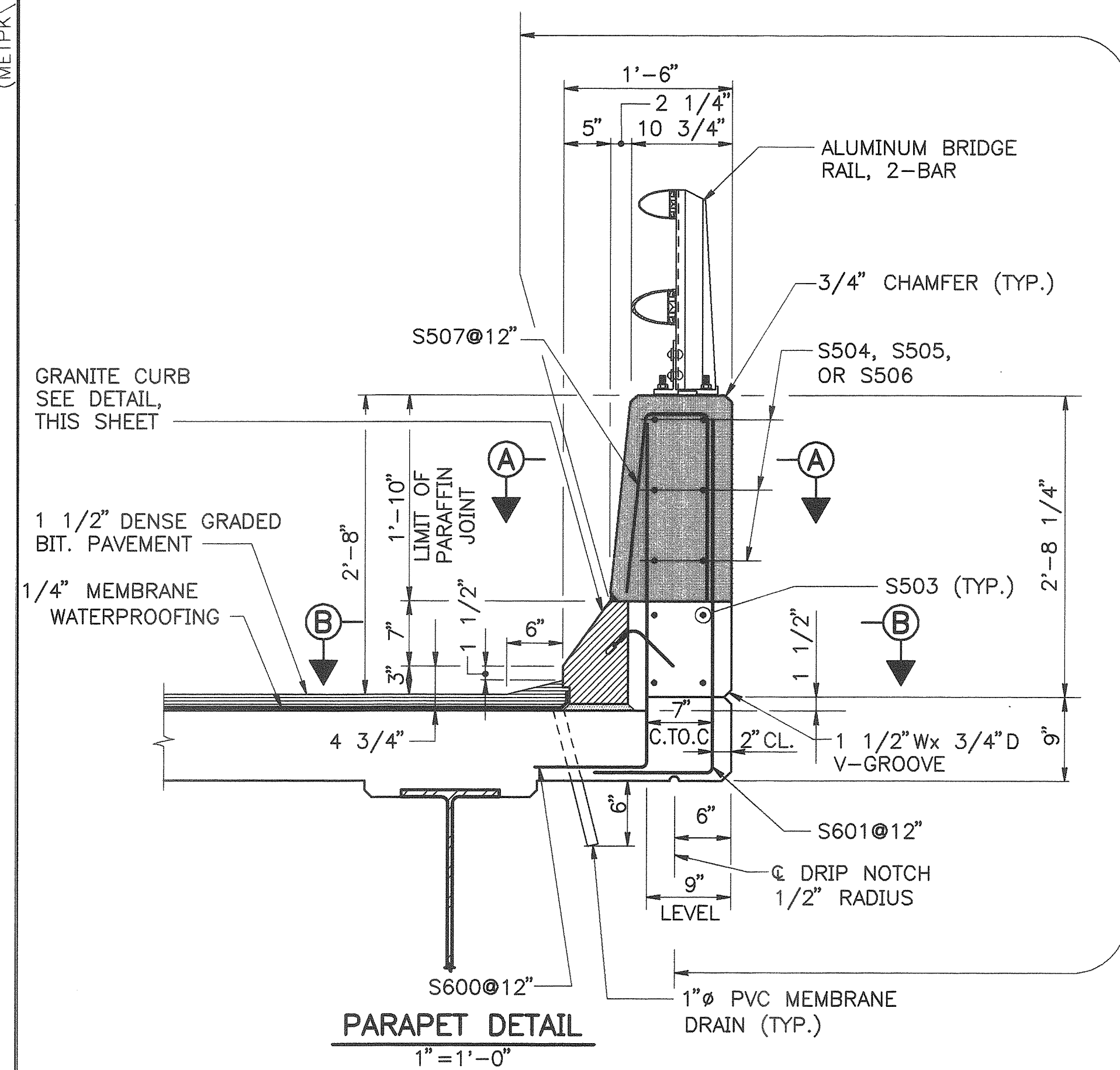


DETAIL A
NO SCALE

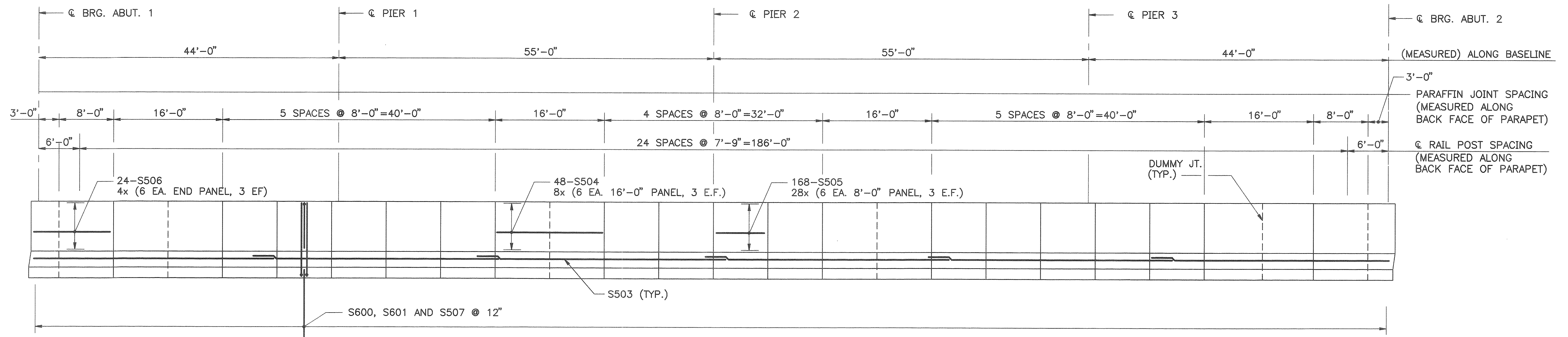
Maine Turnpike Authority Maine Turnpike		FALMOUTH ROAD UNDERPASS	
		SLAB DETAILS I	
		HOWARD NEEDLES TAMMEN & BERGENDOFF, INC ARCHITECTS ENGINEERS PLANNERS	
Contract 97.9		Sheet No. FR-16 46 of 56	

No.	Revision	By	Date	In Charge Of:	RAL
		Designed	AD	1/97	
		Drawn	CSL	1/97	
		Checked	JMH	1/97	

M:\B09009\XW\958\04\DECK\FR-14 01/22/97 17:07 (METPK\BDR-01)



- PARAFFIN AND DUMMY JOINT NOTES**
1. CONCRETE SHALL BE PLACED SIMULTANEOUSLY ON BOTH SIDES OF JOINT.
 2. PREFORMED JOINT FILLER SHALL CONFORM TO ASTM DESIGNATION D1751.
 3. SECTION B-B ALSO APPLIES TO DUMMY JOINT LOCATIONS.
 4. JOINT SEALER SHALL BE SIKAFLEX 1A.
 5. PREFORMED JOINT FILLER AND JOINT SEALER SHALL BE INCIDENTAL TO ITEM 502.261, STRUCTURAL CONCRETE ROADWAY AND PARAPETS ON STEEL BRIDGES.
 6. CURB JOINTS SHALL BE ALIGNED WITH PARAFFIN AND DUMMY JOINTS.



PARAPET ELEVATION
(WEST ELEVATION SHOWN, EAST ELEVATION SIMILAR)
HOR. 1/8" = 1'-0"
VERT. 1/2" = 1'-0"

Maine Turnpike Authority Maine Turnpike	
FALMOUTH ROAD UNDERPASS SLAB DETAILS II	
Contract 97.9	
Sheet No. FR-17 47 of 56	

No.	Revision	By	Date	In Charge Of	RAL
		Designed	AD	1/97	
		Drawn	CSL	1/97	
		Checked	JMH	1/97	

HOWARD NEEDLES TAMMEN & BERGENDOFF, INC.
ARCHITECTS ENGINEERS PLANNERS



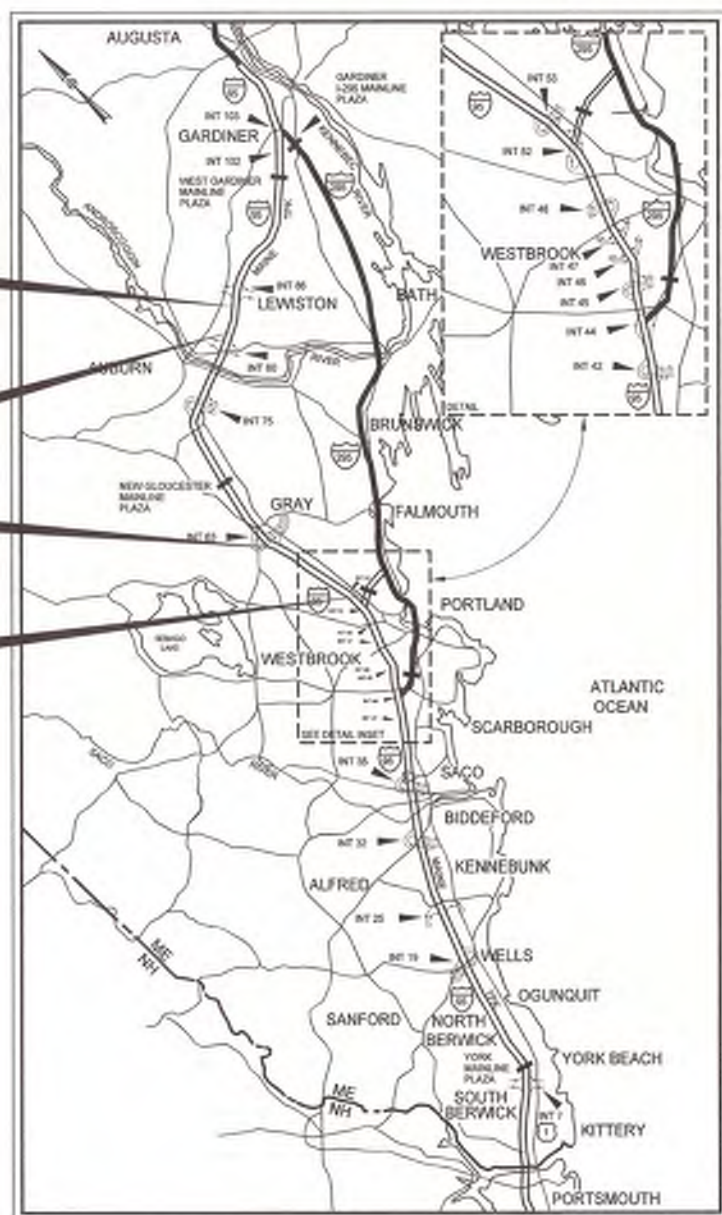
THE GOLD STAR
MEMORIAL HIGHWAY

MAINE TURNPIKE AUTHORITY

DANIEL E. WATHEN, CHAIR
DIANE M. DOYLE, VICE CHAIR
GERARD P. CONLEY, SR., MEMBER
JAMES F. CLOUTIER, MEMBER
JOHN E. DORITY, MEMBER
ROBERT D. STONE, MEMBER
BRUCE A. VAN NOTE, MEMBER EX-OFFICIO

S. PETER MILLS, EXECUTIVE DIRECTOR

CONTRACT 2014.15 BRIDGE AND CULVERT REPAIRS 4 LOCATIONS (MM F1.7 TO MM 84.3)



LOCATION MAP

INDEX OF SHEETS

SHEET NO.	DESCRIPTION
1	TITLE SHEET
2	QUANTITIES
3	GENERAL NOTES
4 - 6	TRAFFIC CONTROL DETAILS
7 - 8	CENTER RD. UNDERPASS - MILE 62.9 - DETOUR PLANS
9 - 12	LISBON RD. UNDERPASS - MILE 84.3 - DETOUR PLANS
13 - 14	MAINTENANCE OF TRAFFIC SIGN SUMMARY
15	LIMIT OF DISTURBANCE PLAN
16	MILE F1.7, MILE 62.9 & MILE 84.3 - DECK PLAN AND TRANSVERSE SECTION
17	FALMOUTH RD. UNDERPASS - MILE F1.7 - BRIDGE PHASING
18	FALMOUTH RD. UNDERPASS - MILE F1.7 - JOINT REPAIR
19	CENTER RD. UNDERPASS - MILE 62.9 - JOINT REPAIRS
20	LISBON RD. UNDERPASS - MILE 84.3 - JOINT REPAIRS
21 - 23	FALMOUTH RD. UNDERPASS - MILE F1.7 - SUBSTRUCTURE REPAIR
24 - 26	CENTER RD. UNDERPASS - MILE 62.9 - SUBSTRUCTURE REPAIR
27 - 29	LISBON RD. UNDERPASS - MILE 84.3 - SUBSTRUCTURE REPAIR
30	NO-NAME RIVER CULVERT - MILE 82.5 - GENERAL SITE PLAN
31	NO-NAME RIVER CULVERT - MILE 82.5 - WINGWALL REPAIRS
32	MISCELLANEOUS DETAILS
33	PRESSURE TREATED WOOD DRAIN TROUGH DETAILS
EXISTING AS-BUILTS AND STANDARDS:	
34 - 40	FALMOUTH RD. UNDERPASS - MILE F1.7 (FALMOUTH RD.)
41 - 44	CENTER RD. UNDERPASS - MILE 62.9 (OLD PORTLAND RD., CENTER ST.)
45 - 47	NO-NAME RIVER CULVERT - MILE 82.5 (NO NAME STREAM)
48 - 51	LISBON RD. UNDERPASS - MILE 84.3 (SABATTUS RD., OLD LISBON RD.)
52	ALUMINUM BRIDGE RAILING STANDARD

APPROVED:

MAINE TURNPIKE AUTHORITY

Peter S. Merrill
PETER S. MERRILL, P.E., CHIEF OPERATIONS OFFICER

3/24/14
DATE

Stephen R. Farrel
STEPHEN R. FARRELL, P.E., DIRECTOR OF ENGINEERING & BUILDING MAINTENANCE

3/24/14
DATE



TYLIN INTERNATIONAL

Norman L. Baker
NORMAN L. BAKER, PE
CONSULTANT PROJECT MANAGER

MARCH 24, 2014
DATE

Date: 3/24/2014

Filename: ... \BRIDGE\WSTA\002_Quantities.dgn

ITEM NO.	ITEM DESCRIPTION	UNIT	FALMOUTH RD. UNDERPASS (MILE F1.7)	CENTER RD. UNDERPASS (MILE 62.9)	NO-NAME RIVER CULVERT (MILE 82.5)	LISBON RD. UNDERPASS (MILE 84.3)	TOTAL QUANTITY
202.202	REMOVING PAVEMENT SURFACE	SY	200	160		140	500
202.2021	REMOVING PAVEMENT SURFACE - BRIDGE DECK	SY	805	810		600	2215
403.210	HOT MIX ASPHALT, 9.5 MM NOMINAL MAXIMUM SIZE	TON	119	82		63	264
409.15	BITUMINOUS TACK COAT, APPLIED	GAL	51	49		37	137
507.0928	ALUMINUM BRIDGE RAILING, RAIL SECTION REPLACEMENT	LF	28			38	66
507.095	ALUMINUM BRIDGE RAILING - SPLICE MODIFICATION	EA	20	32		20	72
508.141	HIGH PERFORMANCE WATERPROOFING MEMBRANE (FALMOUTH ROAD) (805 SY)	LS	1				1
508.142	HIGH PERFORMANCE WATERPROOFING MEMBRANE (CENTER ROAD) (810 SY)	LS		1			1
508.143	HIGH PERFORMANCE WATERPROOFING MEMBRANE (LISBON ROAD) (600 SY)	LS				1	1
511.07	COFFERDAM	LS			1		1
515.201	PIGMENTED PROTECTIVE COATING FOR CONCRETE SURFACES	SY	480	450		320	1250
515.202	CLEAR PROTECTIVE COATING FOR CONCRETE SURFACES	SY	440	500		430	1370
518.10	ABUTMENT REPAIRS	SF	115	125	170	15	425
518.15	CULVERT SURFACE PATCH REPAIR - ABOVE WATERLINE	SF			200		200
518.20	PIER REPAIRS	SF	85	95		40	220
518.61	REPAIR OF VERTICAL SURFACES > 7.9 INCHES	CY			3		3
518.6317	EPOXY INJECTED CRACK REPAIRS	LF	20	55	30		105
518.75	FASCIA AND OVERHANG REPAIRS	SF	30	70	45	30	175
518.80	PARTIAL DEPTH CONCRETE DECK REPAIRS	SF	425	440		205	1070
518.81	FULL DEPTH CONCRETE DECK REPAIRS	SF	40	40		30	110
520.2211	EXPANSION DEVICE REPAIR (FALMOUTH ROAD)	EA	2				2
520.2212	EXPANSION DEVICE REPAIR (CENTER ROAD)	EA		2			2
520.2213	EXPANSION DEVICE REPAIR (LISBON ROAD)	EA				2	2
526.301	TEMPORARY CONCRETE BARRIER, TYPE 1	LS	0.58	0.42			1
527.341	WORK ZONE CRASH CUSHIONS - TL-3	UNIT	1	2			3
527.342	WORK ZONE CRASH CUSHIONS - TL-2	UNIT	2				2
603.91	PRESSURE TREATED WOOD DRAIN TROUGH	LF	100	119		60	279
619.1202	TEMPORARY MULCH	LS	0.25	0.25	0.25	0.25	1
627.712	4 INCH WHITE OR YELLOW PAVEMENT MARKING LINE	LF	1280	915		800	2995
627.76	TEMPORARY PAVEMENT MARKING LINE, WHITE OR YELLOW	LS	1				1
627.77	REMOVING EXISTING PAVEMENT MARKING	SF	650				650
643.72	TEMPORARY TRAFFIC SIGNAL	LS	1				1
652.30	FLASHING ARROW BOARD	EA	1	1	1	1	4
652.312	TYPE III BARRICADE	EA		2		2	4
652.33	DRUM	EA	50	55	50	41	196
652.34	CONE	EA	20	20		20	60
652.35	CONSTRUCTION SIGNS	SF	650	970	370	1040	3030
652.361	MAINTENANCE OF TRAFFIC CONTROL DEVICES	LS	0.45	0.35	0.06	0.14	1
652.38	FLAGGER	HR	90				90
652.412	PORTABLE-CHANGEABLE MESSAGE SIGN	MO	2	2		2	6
652.45	TRUCK MOUNTED ATTENUATOR	CD	20	20		20	60
656.632	30 INCH TEMPORARY SILT FENCE	LF	50	50	50	50	200
656.64	BOOM SUPPORTED FLOATING SILT FENCE	LF			300		300
659.10	MOBILIZATION	LS			1		1

Scale:				
NOT TO SCALE				
No.	Revision	By	Date	

Designed by:					
TY·LIN INTERNATIONAL					
CONSULTANT PROJECT MANAGER: Norman L. Baker					
	By	Date		By	Date
Designed	DSM	3/14	Checked	CPT	3/14
Drawn	DSM	3/14	In Charge of	NLB	3/14

T.Y. Lin International
12 Northbrook Drive
Building A, Suite One
Falmouth, Maine 04105
TEL: (207) 781-4721
FAX: (207) 781-4753



THE GOLD STAR
MEMORIAL HIGHWAY

MTA PROJECT MANAGER: Ralph C. Norwood IV

BRIDGE AND CULVERT REPAIRS
4 LOCATIONS (MILE F1.7 TO MILE 84.3)

QUANTITIES

Date: 3/24/2014

Filename: ... \BRIDGE\MSTA\003_GEN_NOTES.dgn

SPECIFICATIONS:

DESIGN
AASHTO LRFD BRIDGE DESIGN SPECIFICATIONS
6TH EDITION

CONSTRUCTION
STATE OF MAINE, DEPARTMENT OF TRANSPORTATION
STANDARD SPECIFICATIONS FOR HIGHWAYS AND BRIDGES,
DECEMBER 2002

STATE OF MAINE DEPARTMENT OF TRANSPORTATION
STANDARD DETAILS FOR HIGHWAYS AND BRIDGES,
DECEMBER 2002 WITH LATEST REVISIONS

AASHTO LRFD BRIDGE CONSTRUCTION SPECIFICATIONS,
THIRD EDITION WITH 2013 INTERIMS

DESIGN LOADING:

LIVE LOAD
HL-93

MATERIALS:

CONCRETE
CONCRETE JOINT REPAIR SHALL BE CLASS AAA.

ALL OTHER PATCHING AND REPAIR AREAS SHALL
BE CLASS AAA-MODIFIED.

REINFORCING STEEL
AASHTO M31, GRADE 60 EPOXY-COATED.

BASIC DESIGN STRESSES:

CONCRETE
CLASS AAA, $f'_c = 4,500$ P.S.I.

REINFORCING STEEL
 $f_y = 60,000$ P.S.I.

GENERAL NOTES:

- FOR ADDITIONAL DETAILS REFERENCED OR NOT SHOWN IN THESE DRAWINGS, SEE THE STATE OF MAINE, DEPARTMENT OF TRANSPORTATION STANDARD DETAILS, HIGHWAYS AND BRIDGES, DECEMBER 2002 WITH UPDATES.
- COPIES OF THE AS-BUILT PLANS ARE ON FILE AT THE MAINE TURNPIKE AUTHORITY. A PORTION OF THESE PLANS ARE INCLUDED IN THIS CONTRACT FOR THE CONTRACTOR'S CONVENIENCE. THE COMPLETENESS AND ACCURACY OF THESE PLANS IS NOT GUARANTEED.
- THE CONTRACTOR IS REQUIRED TO MAINTAIN ACCESS TO ALL DRIVEWAYS AND SIDE ROADS DURING CONSTRUCTION UNLESS NOTED OTHERWISE ON THE PLANS.
- REINFORCING STEEL SHALL HAVE A CLEAR COVER OF 2" MIN., UNLESS OTHERWISE NOTED.
- CHAMFER ALL EXPOSED CONCRETE EDGES $\frac{3}{4}$ " UNLESS OTHERWISE NOTED.
- ALL CONCRETE BRIDGE PARAPET FACES AND WINGWALL CONCRETE INSIDE FACE AND TOP FACE SHALL HAVE A RUBBED FINISH PRIOR TO THE APPLICATION OF THE CLEAR PROTECTIVE COATING FOR CONCRETE SURFACE.
- SHIELDING REQUIRED DURING CONSTRUCTION SHALL NOT PROJECT BELOW THE BOTTOM FLANGES OF GIRDERS.
- ALL PAINT ON EXISTING STEEL, DAMAGED BY CONTRACTOR'S OPERATIONS, SHALL BE REPAIRED TO THE SATISFACTION OF THE RESIDENT AT THE CONTRACTOR'S EXPENSE.

PAVING NOTES:

- ANY NECESSARY CLEANING OF EXISTING PAVEMENT PRIOR TO PAVING SHALL BE INCIDENTAL TO THE RELATED PAVING ITEMS.
- RECYCLED BITUMINOUS PAVEMENT (RAP) SHALL NOT BE PLACED ON ANY PORTION OF THE EXISTING BRIDGE DECK.
- ALL COLD JOINTS WITH TEMPERATURES LESS THAN 120°F SHALL BE SEALED WITH JOINT SEALANT CONFORMING TO FEDERAL SPECIFICATION SS-S-1401C.

SUBSTRUCTURE REPAIR PROCEDURAL NOTES:

- CONCRETE REPAIRS, INCLUDING THE ASSOCIATED REMOVALS, WILL BE PAID UNDER 518 ITEMS.

REMOVAL PROCEDURE:

- PRIOR TO THE START OF THE CONCRETE REMOVALS, THE RESIDENT AND THE CONTRACTOR SHALL SOUND THE CONCRETE AND AGREE ON THE REMOVAL LIMITS. SHOULD THE REMOVAL AREA LIMITS APPEAR TO CHANGE DURING THE DEMOLITION PROCESS, THE CONTRACTOR SHALL NOTIFY THE RESIDENT. THE RESIDENT AND CONTRACTOR SHALL AGREE ON THE REVISED PAY LIMITS PRIOR TO THE CONTRACTOR CONTINUING THE REMOVALS.
- PERFORM 1 INCH DEEP SAWCUTS ALONG LIMITS OF REMOVAL.
- CHIP CONCRETE TO DEPTH REQUIRED PER MAINEDOT STANDARD SPECIFICATION 518.

CONCRETE SURFACE PATCH/REPAIR PROCEDURE:

- PREPARE AND PATCH REPAIR AREAS WITH CLASS AAA MODIFIED CONCRETE. SEE SPECIFICATIONS FOR MATERIAL, PREPARATION, PLACEMENT, AND CURING REQUIREMENTS.
- PERFORM GENERAL FINISHING (SEE BELOW).

EPOXY INJECTION CRACK REPAIR:

- PREPARE CONCRETE SURFACE AND CRACK AREA BY CLEANING SUBSTRATE WITH WIRE BRUSH TO REMOVE LAITANCE AND CONTAMINANTS. BLAST CLEAN THE CRACK AND CONCRETE SURFACE; DO NOT USE WATER.
- SET INJECTION PORTS ALONG THE LENGTH OF THE CRACK USING A HIGH-MODULUS EPOXY ADHESIVE. ONCE SET, SEAL PORTS AND CRACK WITH SAME EPOXY ADHESIVE.
- WHEN THE EPOXY ADHESIVE SEAL HAS CURED, INJECT A HIGH-MODULUS, LOW-VISCOSITY EPOXY SUCH AS SIKADUR 35, SIKADUR 55, OR APPROVED EQUAL.

GENERAL FINISHING:

- CONTRACTOR SHALL REMOVE TECTYL COATING WHERE PRESENT PRIOR TO APPLYING PROTECTIVE CONCRETE COATING. WORK SHALL BE INCIDENTAL TO THE SPECIFIED ITEM IN S.P. 518.
- ALL EXPOSED SURFACES SHALL BE COATED WITH A PROTECTIVE COATING SUITABLE FOR CONCRETE SURFACES AFTER PATCHING IS COMPLETE AND PATCH MATERIALS HAVE CURED. SEE S.P. 515 FOR LIMITS OF CLEAR AND PIGMENTED COATING.

LIST OF ABBREVIATIONS

ABUT.	ABUTMENT
ADDL.	ADDITIONAL
ASCG.	AGGREGATE SUBBASE COURSE GRAVEL
ALT.	ALTERNATE
APPROX.	APPROXIMATE
BOT.	BOTTOM
BRG.	BEARING
CL.	CLEAR
℄.	CENTERLINE
CONC.	CONCRETE
CONSTR.	CONSTRUCTION
DEMO.	DEMOLITION
DIA.	DIAMETER
DIAPH.	DIAPHRAGM
EA.	EACH
EB	EASTBOUND
E.F.	EACH FACE
EL.	ELEVATION
EQ.	EQUAL
EXIST.	EXISTING
EXP.	EXPANSION
F.F.	FAR FACE
JT.	JOINT
MAX.	MAXIMUM
MIN.	MINIMUM
MTA	MAINE TURNPIKE AUTHORITY
NB	NORTHBOUND
N.F.	NEAR FACE
N.T.S.	NOT TO SCALE
O.H.W.	ORDINARY HIGH WATER
PED.	PEDESTAL
PGL	PROFILE GRADE LINE
℄	PLATE
PROP.	PROPOSED
RDWY.	ROADWAY
SHLD.	SHOULDER
SB	SOUTHBOUND
SP.	SPACES
STA.	STATION
T.&B.	TOP & BOTTOM
TPKE.	TURNPIKE
TYP.	TYPICAL
U.O.N.	UNLESS OTHERWISE NOTED
VERT.	VERTICAL
WB	WESTBOUND
W.P.	WORKING POINT

Scale:

NOT TO SCALE

No.	Revision	By	Date

Designed by:

TYLIN

INTERNATIONAL

CONSULTANT PROJECT MANAGER: Norman L. Baker

	By	Date		By	Date
Designed	DSM	3/14	Checked	SBK	3/14
Drawn	DSM	3/14	In Charge of	NLB	3/14

MAINE

TURNPIKE

★

THE GOLD STAR
MEMORIAL HIGHWAY

MTA PROJECT MANAGER: Ralph C. Norwood IV

BRIDGE AND CULVERT REPAIRS
4 LOCATIONS (MILE F1.7 TO MILE 84.3)

GENERAL NOTES

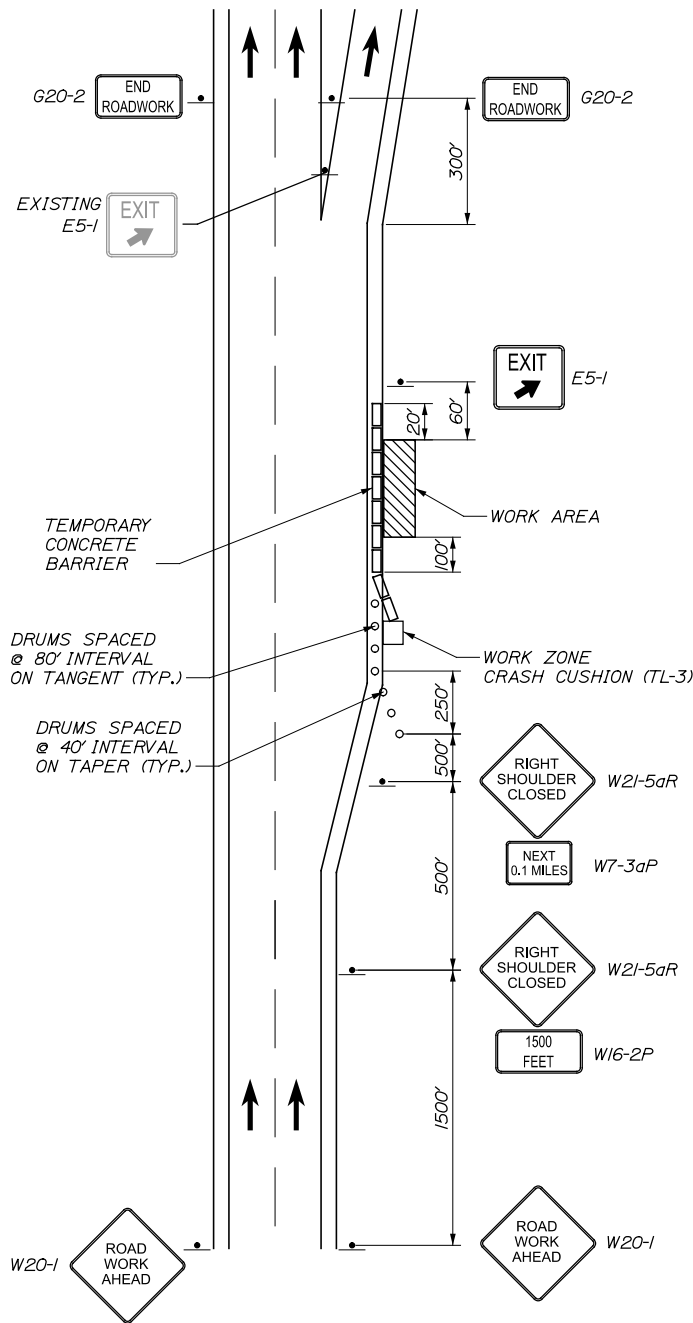
SHEET NUMBER: SN-01

CONTRACT: 2014.15

3 OF 52

Date: 3/24/2014

Filename: ...\\MSTA\\004_MOT_DET_01.dgn



TURNPIKE NORTHBOUND SHOULDER
CLOSURE AT CENTER ROAD UNDERPASS

GENERAL MAINTENANCE OF TRAFFIC NOTES:

1. CONTRACTOR SHALL REFER TO THE CURRENT EDITION OF THE MUTCD, FOR TEMPORARY TRAFFIC CONTROL MEASURES AND TYPICAL APPLICATIONS FOR OTHER WORK ZONE ARRANGEMENTS ON TURNPIKE MAINLINE AND OVERPASS ROADWAYS.
2. FOR SIGN DETAILS, SEE MAINTENANCE OF TRAFFIC SIGN SUMMARY SHEET.
3. ALL WORK SHALL CONFORM TO THE 2002 MAINE DEPARTEMENT OF TRANSPORTATION STANDARD SPECIFICATIONS FOR HIGHWAY AND BRIDGES, REVISION OF 2002, EXCEPT AS MODIFIED BY THE MAINE TURNPIKE AUTHORITY'S GENERAL AND SPECIAL PROVISIONS.
4. THESE PLANS SHOW THE GENERAL CONDITION FOR TURNPIKE MAINLINE AND INTERCHANGE TRAFFIC CONTROL DURING CONSTRUCTION. SLIGHT MODIFICATIONS IN CONSTRUCTION PROCEDURE MAY OCCUR AND MAY REQUIRE SOME MINOR ADJUSTMENTS TO BE MADE IN THE FIELD. ALL PROCEDURES MUST BE APPROVED BY THE RESIDENT.
5. THE CONTRACTOR SHALL REMOVE ALL PAVEMENT MARKINGS THAT CONFLICT WITH PROPOSED PAVEMENT MARKINGS IN ACCORDANCE WITH THE SPECIFICATIONS AND MUTCD. PAYMENT SHALL BE MADE UNDER ITEM 627.77 - REMOVING EXISTING PAVEMENT MARKING.
6. TEMPORARY PAVEMENT MARKING SHALL BE PAINTED, UNLESS OTHERWISE NOTED. PAYMENT FOR MARKINGS SHALL BE UNDER ITEM 627.76 - TEMPORARY PAVEMENT MARKING LINE, WHITE OR YELLOW. TEMPORARY PAVEMENT MARKINGS ON FINAL PAVEMENT SURFACE OR EXISTING PAVEMENT TO REMAIN SHALL BE TEMPORARY PAVEMENT MARKING TAPE TO BE REMOVED PRIOR TO FINAL PAVEMENT MARKING.
7. EXPOSED BARRIER ENDS SHALL BE PROTECTED BY A WORK ZONE CRASH CUSHION. PAYMENT WILL BE UNDER ITEM 527.341 - WORK ZONE CRASH CUSHION - TL-3 OR ITEM 527.342 - WORK ZONE CRASH CUSHION - TL-2.
8. AT NO TIME SHALL SHOULDER CLOSURES ON BOTH SIDES OF THE TRAVEL LANES OCCUR ON THE SAME SIDE OF THE MAINLINE.
9. WORK WITHIN SHOULDER CLOSURES SHALL BE CONTINUOUS TO MINIMIZE IMPACTS TO MAINLINE TRAFFIC.
10. CONTRACTOR SHALL COORDINATE ALL MAINTENANCE OF TRAFFIC WITH ADJACENT PROJECTS.
11. WHEN TRUCK MOUNTED ATTENUATORS ARE INCLUDED IN THE CONTRACT (INCIDENTAL OR PAY ITEM), THEY SHALL BE LOCATED WITHIN THE WORK ZONE, NOT THE BUFFER ZONE.
12. LOCATIONS OF TEMPORARY TRAFFIC CONTROL DEVICES, SIGNS, AND PAVEMENT MARKINGS ARE APPROXIMATE. FINAL LOCATIONS WILL BE DETERMINED IN THE FIELD BY THE RESIDENT ENGINEER.
13. PORTABLE-CHANGABLE MESSAGE SIGNS SHALL BE USED DURING PERIOD OF FULL BRIDGE CLOSURE ONLY. THESE SHALL BE LOCATED FOR EACH APPROACH TO EACH BRIDGE WHEN CLOSED.
14. ALL PAVEMENT STRIPING AND SIGNING SHALL BE IN ACCORDANCE WITH THE "MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES", U.S.D.O.T., FHWA, LATEST EDITION.

ABBREVIATIONS FOR ALL M.O.T. PLANS

BWLL= BROKEN WHITE LANE LINE
SWEL= SOLID WHITE EDGE LINE
SYEL= SOLID YELLOW EDGE LINE
TBWLL= TEMPORARY BROKEN WHITE LANE LINE
TDYCL= TEMPORARY DOUBLE YELLOW CENTER LINE
TSWEL= TEMPORARY SOLID WHITE EDGE LINE
TSYEL= TEMPORARY SOLID YELLOW EDGE LINE

TEMPORARY SIGNAL NOTES:

1. LOCATIONS OF TEMPORARY TRAFFIC SIGNAL SYSTEMS ARE APPROXIMATE. FINAL LOCATIONS WILL BE DETERMINED IN THE FIELD BY THE RESIDENT ENGINEER.
2. TRAFFIC SIGNAL WORK SHALL BE COMPLETED IN A MANNER AND ORDER THAT WILL CAUSE THE MINIMUM DISRUPTION TO TRAFFIC.
3. ALL SIGNAL SECTIONS SHALL HAVE LED LENSES 12 INCHES IN DIAMETER WITH 5 IN BACK PLATES.
4. TEMPORARY TRAFFIC SIGNAL SYSTEMS MAY BE SPAN-WIRE, MAST-ARM, OR MOBILE TRAILOR MOUNTED SIGNAL HEADS WITH ALL NECESSARY POWER SUPPLY AND SIGNAL TIMING CONTROL TYPE AND POSITION OF TEMPORARY SIGNAL SYSTEM SHALL BE SUCH THAT THE SIGNALS ARE ALSO VISIBLE TO VEHICLES IN DRIVEWAYS IN VICINITY OF WORK AREA.
5. THE BOTTOM OF THE HOUSING OF NEW SIGNAL FACES SHALL BE AT LEAST 16 FT BUT NO MORE THAN 19 FT ABOVE THE PAVEMENT GRADE AT THE CENTER OF THE ROADWAY.
6. THE CONTRACTORS SHALL MEET ALL REQUIREMENTS OF THE UTILITY COMPANIES WHEN MODIFYING/INSTALLING SERVICE CONNECTIONS AND WHEN INSTALLING EQUIPMENT ON THEIR POLES OR NEAR THEIR WIRES. THE CONTRACTOR IS RESPONSIBLE FOR FURNISHING AND INSTALLING POWER METERS, IF REQUIRED, AND ASSURING THE POWER METER LOCATION AND INSTALLATION CONFORMS TO THE LOCAL UTILITY REQUIREMENTS. THIS WORK SHALL BE INCIDENTAL TO ITEM 643.72, TEMPORARY TRAFFIC SIGNAL. THE SERVICE POLE, WIRING, METER SOCKET, AND DISCONNECT/POWER PANEL SHALL BE INCIDENTAL TO ITEM 643.72, TEMPORARY TRAFFIC SIGNAL.
7. ALL POWER AND CONTROL SPLICES WILL BE MADE IN THE CABINETS MEETING MAINEDOT SPECIFICATIONS.
8. THE CONTRACTOR SHALL NOT BE ALLOWED TO ATTACH ANY SIGNING OR SPAN WIRE TO EXISTING UTILITY POLES.
9. THE CONTRACTOR SHALL INSTALL ALL SPAN WIRE, POLES, SIGNS AND ALL OTHER TEMPORARY TRAFFIC CONTROL ELEMENTS WHILE MAINTAINING A MINIMUM 10 FT. CLEARANCE FROM ALL OVERHEAD LINES.
10. THE CONTRACTOR SHALL VERIFY THAT THERE ARE NO UNDERGROUND UTILITY CONFLICTS PRIOR TO INSTALLING TEMPORARY POLES. A MINIMUM 2 FOOT CLEARANCE FROM ALL UNDERGROUND UTILITIES WILL BE REQUIRED.

Scale:			
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No.	Revision	By	Date

Designed by:					
TYLIN INTERNATIONAL					
CONSULTANT PROJECT MANAGER: Norman L. Baker					
	By	Date		By	Date
Designed	DRR	3/14	Checked	TSK	3/14
Drawn	JD	3/14	In Charge of	NLB	3/14

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THE GOLD STAR
MEMORIAL HIGHWAY

MTA PROJECT MANAGER: Ralph C. Norwood IV

BRIDGE AND CULVERT REPAIRS
4 LOCATIONS (MILE F1.7 TO MILE 84.3)

TRAFFIC CONTROL DETAILS - 1 OF 3

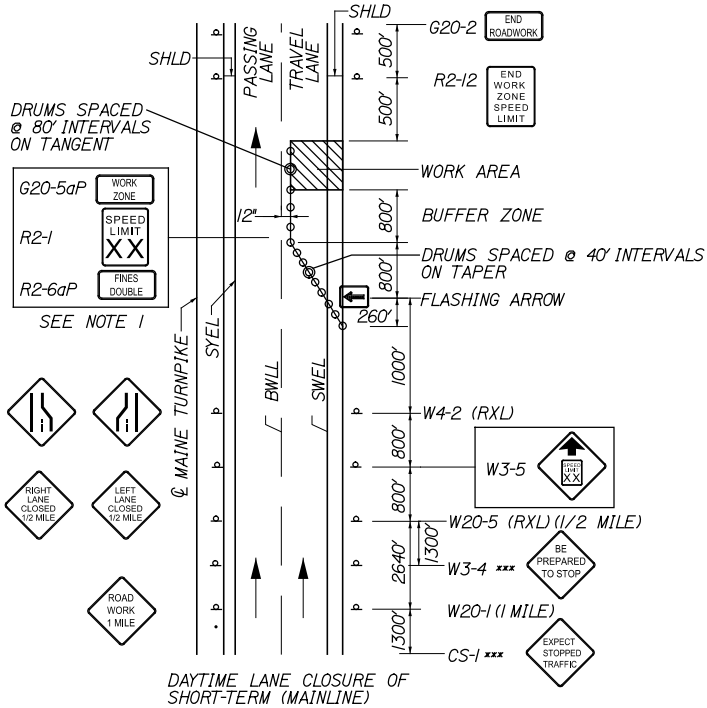
CONTRACT: 2014.15

SHEET NUMBER: MT-01

4 OF 52

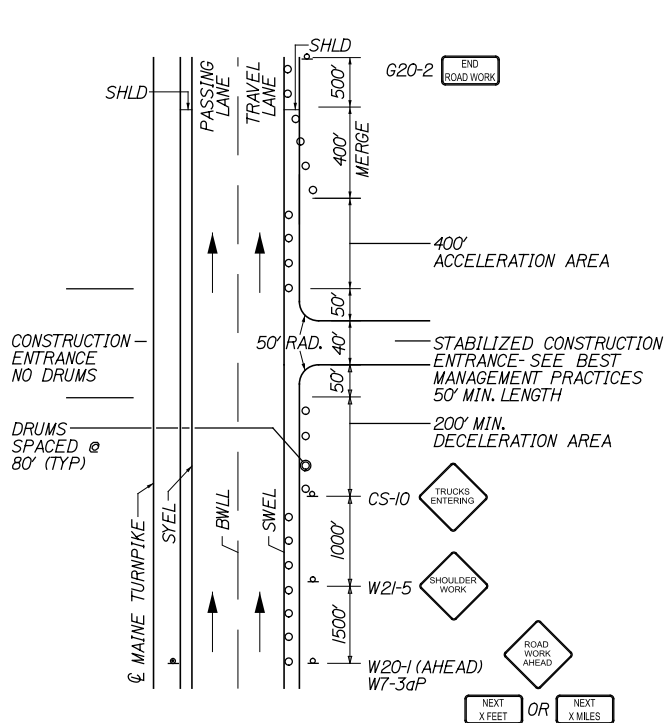
Date: 3/24/2014

Filename: ...MSTA\005_MOT_DET_02.dgn



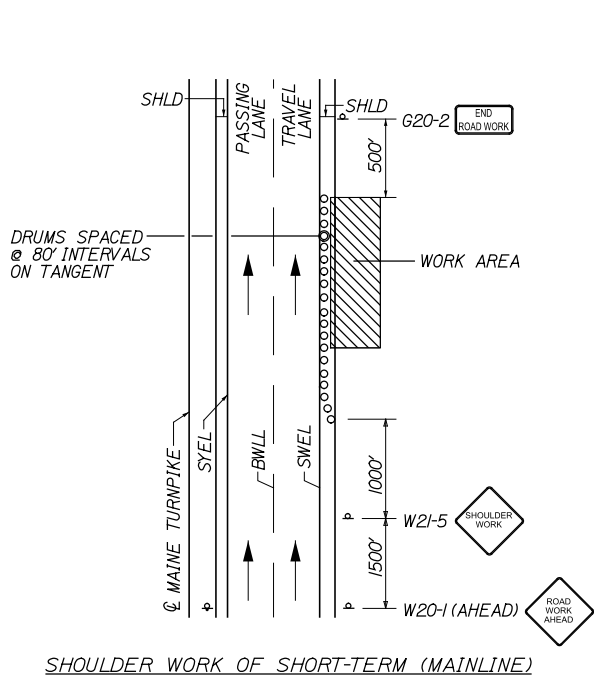
- NOTES:**
1. FOR EASEL SET UP ONLY, THIS SIGN IS NOT BRACKETED. ONE SIGN ASSEMBLY IS PLACED AT THE END OF THE TAPER.
 2. SIGNS DESIGNATED WITH *** SHALL BE USED DURING STOPPAGES OF TRAFFIC.

SINGLE LANE CLOSURE- SIGNS ON EASELS



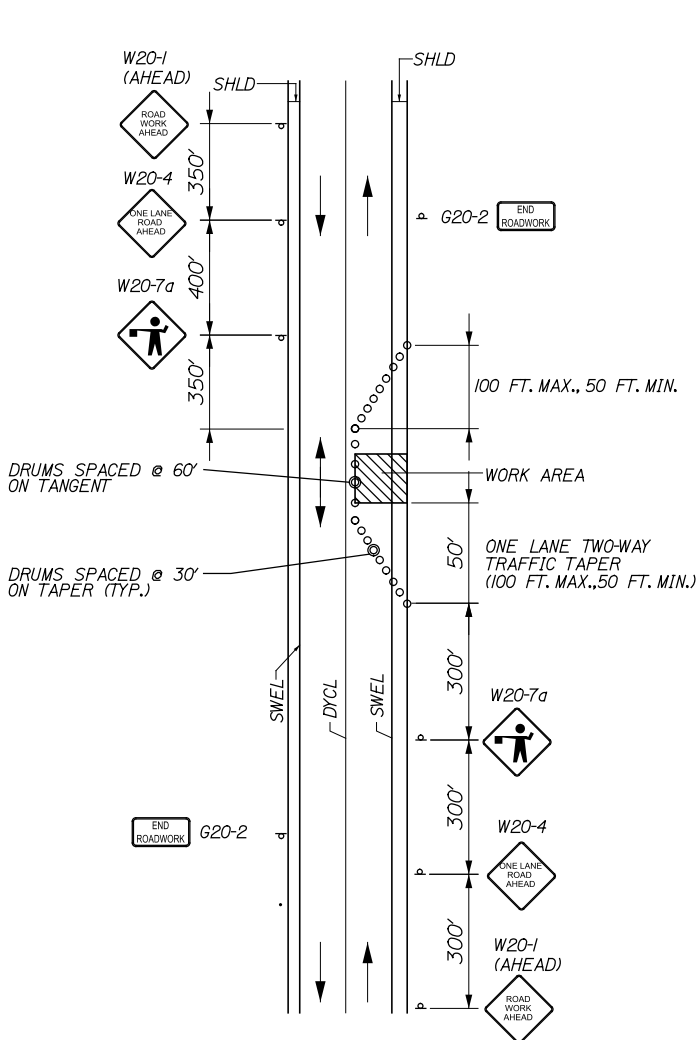
- NOTES:**
1. SIGN G20-2 IS NOT REQUIRED IF ENTRANCE IS WITHIN THE WORK ZONE.
 2. "TRUCKS ENTERING" SIGN SHALL ALSO BE USED AT LOCATIONS WHERE TRUCKS ENTER THE WORK ZONE FROM THE TRAVEL LANE.

STABILIZED CONSTRUCTION ENTRANCE



- NOTE:**
- THE W20-1 AND G20-2 SIGNS ARE NOT REQUIRED IF THE SHOULDER CLOSURE IS WITHIN A PREVIOUSLY ESTABLISHED WORK ZONE.

SHOULDER WORK



- NOTES:**
1. FLAGGERS SHALL BE LOCATED SO THEY ARE CLEARLY VISIBLE TO APPROACHING DRIVERS.

LOCAL ROAD SINGLE LANE CLOSURE

- NOTE:**
- SEE TRAFFIC CONTROL DETAILS - 1 OF 3 FOR GENERAL MAINTENANCE OF TRAFFIC NOTES AND ABBREVIATIONS FOR ALL M.O.T. PLANS.

Scale:			
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No.	Revision	By	Date

Designed by:			
T.Y. LIN INTERNATIONAL			
CONSULTANT PROJECT MANAGER: Norman L. Baker			
	By	Date	
Designed	DRR	3/14	Checked TSK 3/14
Drawn	JD	3/14	In Charge of NLB 3/14

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THE GOLD STAR
MEMORIAL HIGHWAY

MTA PROJECT MANAGER: Ralph C. Norwood IV

BRIDGE AND CULVERT REPAIRS
4 LOCATIONS (MILE F1.7 TO MILE 84.3)

TRAFFIC CONTROL DETAILS - 2 OF 3

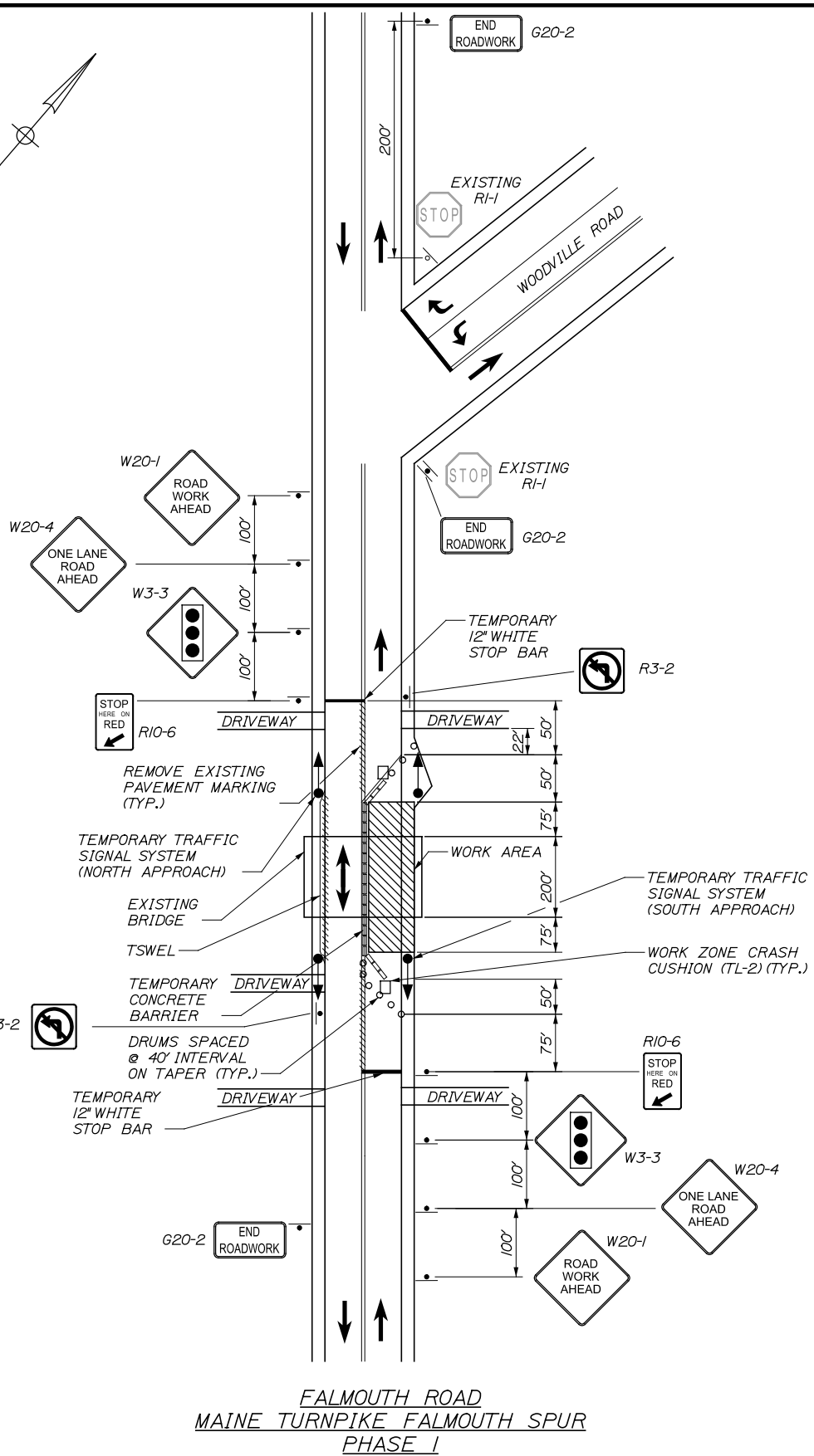
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CONTRACT: 2014.15

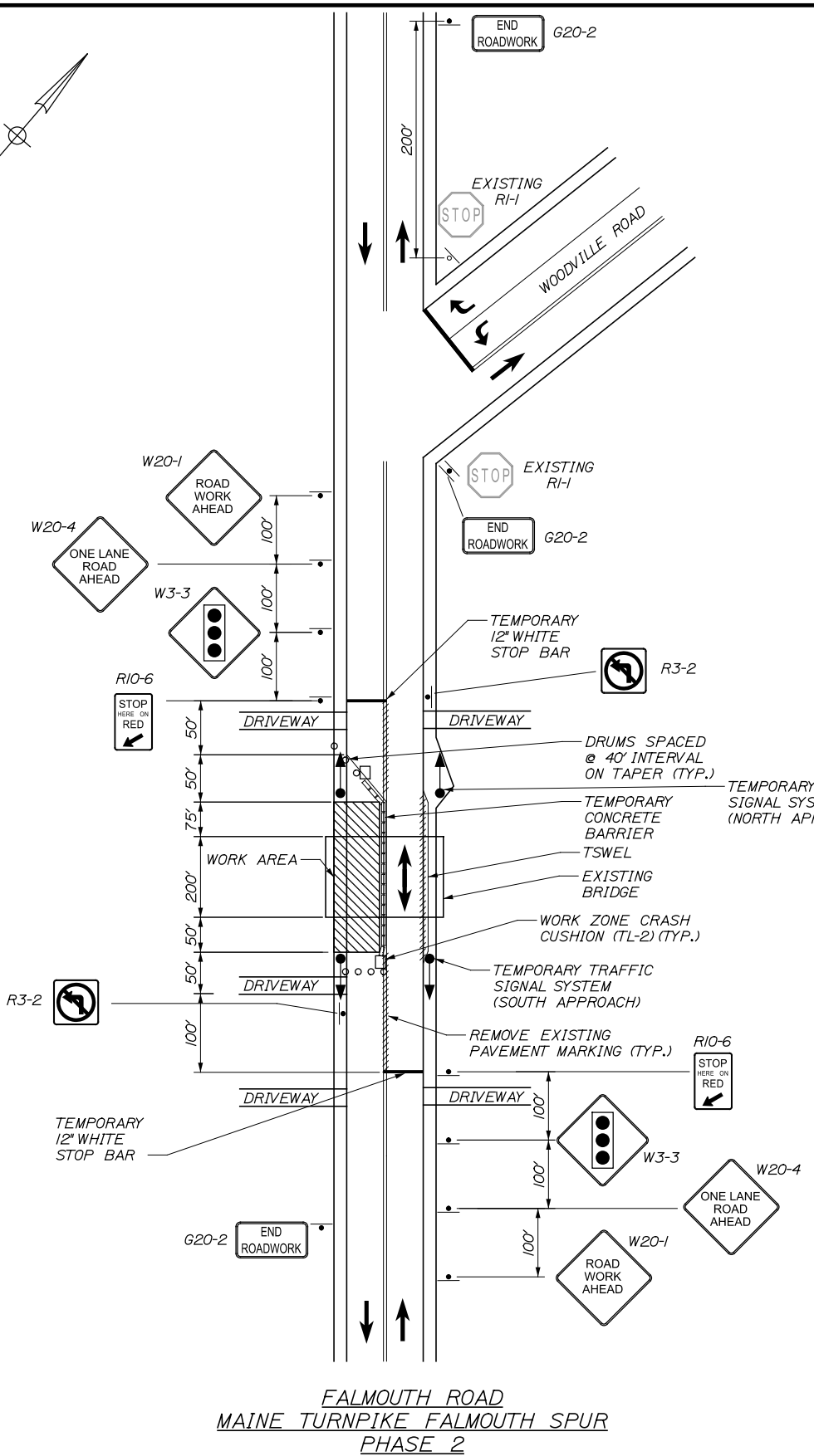
5 OF 52

Date: 3/24/2014

Filename: ...MSTA\006_MOT_DET_03.dgn



FALMOUTH ROAD
MAINE TURNPIKE FALMOUTH SPUR
PHASE 1



FALMOUTH ROAD
MAINE TURNPIKE FALMOUTH SPUR
PHASE 2

- GENERAL NOTES:**
1. FOR SIGN DETAILS SEE MAINTENANCE OF TRAFFIC SIGN SUMMARY SHEETS.
 2. CONTRACTOR SHALL SET TIMING OF TWO TEMPORARY TRAFFIC SIGNALS PER TIMING DATA PROVIDED ON THIS SHEET. CONTRACTOR SHALL MONITOR AND ADJUST SIGNAL TIMING AS NECESSARY TO MINIMIZE TRAFFIC DELAYS.
 3. SEE ADDITIONAL NOTES FOR TEMPORARY SIGNAL WORK ON TRAFFIC CONTROL DETAILS, SHEET 1 OF 3.

SIGNAL TIMING

INTERSECTION	-	SOUTH APPROACH	-	-	-	-	-	NORTH APPROACH	-
MOVEMENT	-	NBT	-	-	-	-	-	SBT	-
PHASE	1	2	3	4	5	6	7	8	9
MIN GREEN	-	4.0	-	-	-	-	-	4.0	-
YELLOW	-	3.5	-	-	-	-	-	3.5	-
ALL RED	-	19.0	-	-	-	-	-	19.0	-
MAX SPLIT	-	60.0	-	-	-	-	-	60.0	-
VEH EXTENSION	-	3.0	-	-	-	-	-	3.0	-

Scale: NOT TO SCALE

No.	Revision	By	Date

TY LIN INTERNATIONAL
CONSULTANT PROJECT MANAGER: Norman L. Baker

By	Date	By	Date
Designed	DRR 3/14	Checked	TSK 3/14
Drawn	JD 3/14	In Charge of	NLB 3/14

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THE GOLD STAR MEMORIAL HIGHWAY

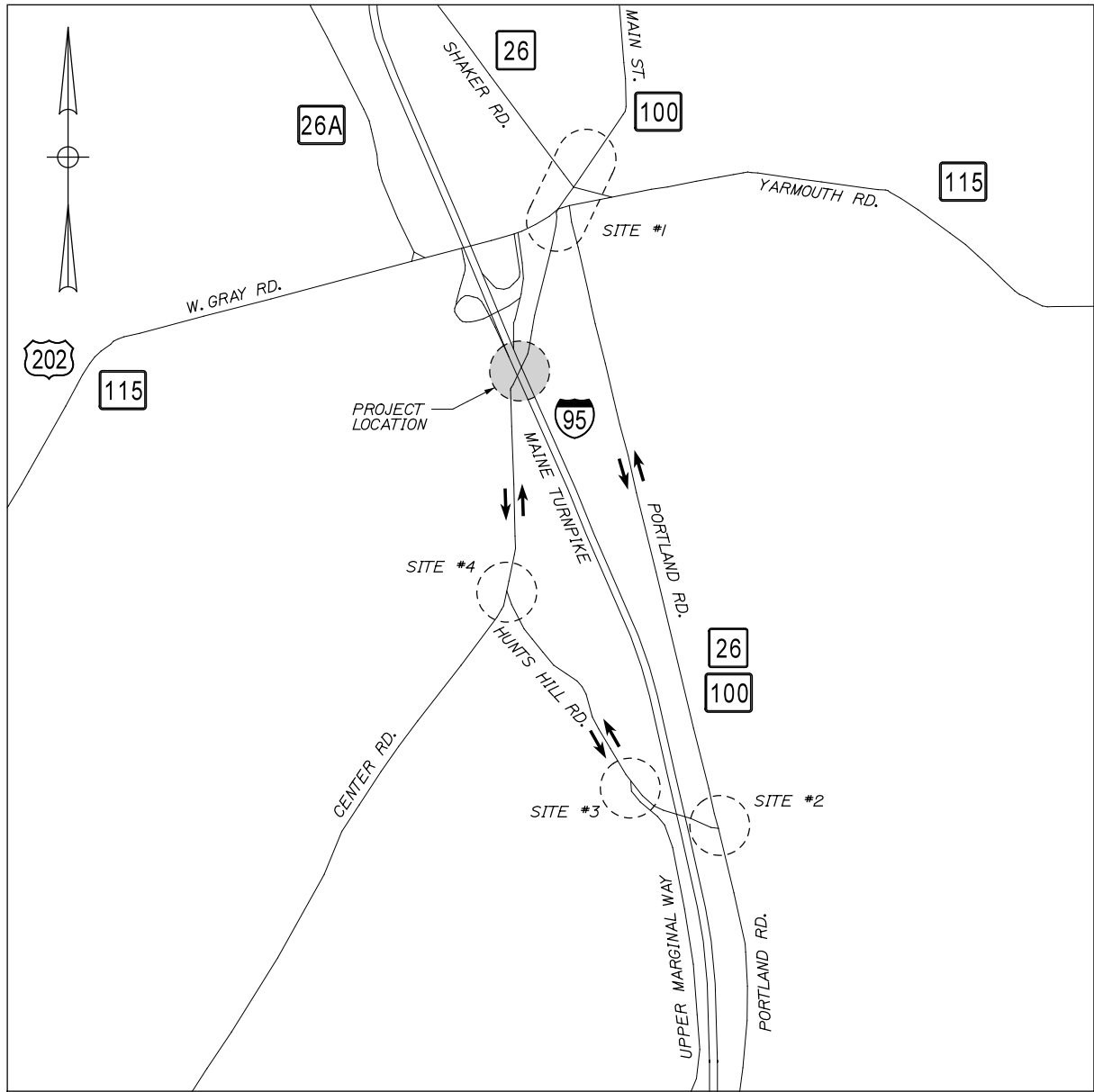
MTA PROJECT MANAGER: Ralph C. Norwood IV

BRIDGE AND CULVERT REPAIRS
4 LOCATIONS (MILE F1.7 TO MILE 84.3)
TRAFFIC CONTROL DETAILS - 3 OF 3

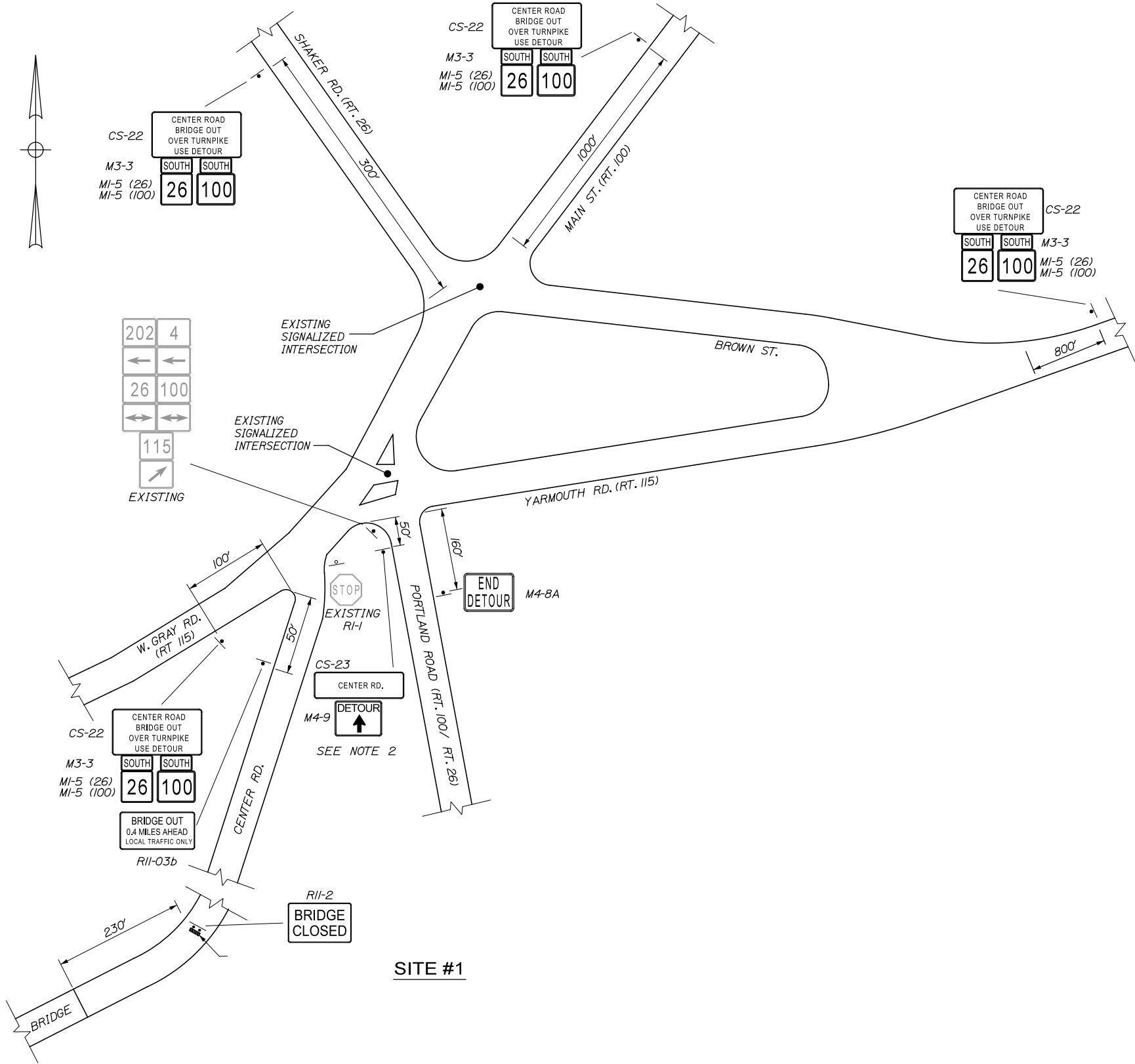
SHEET NUMBER: MT-03
CONTRACT: 2014.15
6 OF 52

Date: 3/24/2014

Filename: ...MSTA\007_62.9_MOT_01.dgn



- NOTES:**
- 1. REFER TO THE MAINTENANCE OF TRAFFIC SHEETS FOR ALL TURNPIKE MAINLINE WORK.
 - 2. SIGNS CS-23 AND M4-9 WILL BE LARGER SIGNS SHOWN ON SIGN SUMMARY SHEET IN LOCATION NOTED.



Scale:			
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No.	Revision	By	Date

Designed by:					
T.Y. LIN INTERNATIONAL					
CONSULTANT PROJECT MANAGER: Norman L. Baker					
	By	Date		By	Date
Designed	DRR	3/14	Checked	TSK	3/14
Drawn	JD	3/14	In Charge of	NLB	3/14

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**THE GOLD STAR
MEMORIAL HIGHWAY**

MTA PROJECT MANAGER: Ralph C. Norwood IV

BRIDGE AND CULVERT REPAIRS
4 LOCATIONS (MILE F1.7 TO MILE 84.3)
CENTER RD. UNDERPASS - MILE 62.9
DETOUR PLAN - 1 OF 2

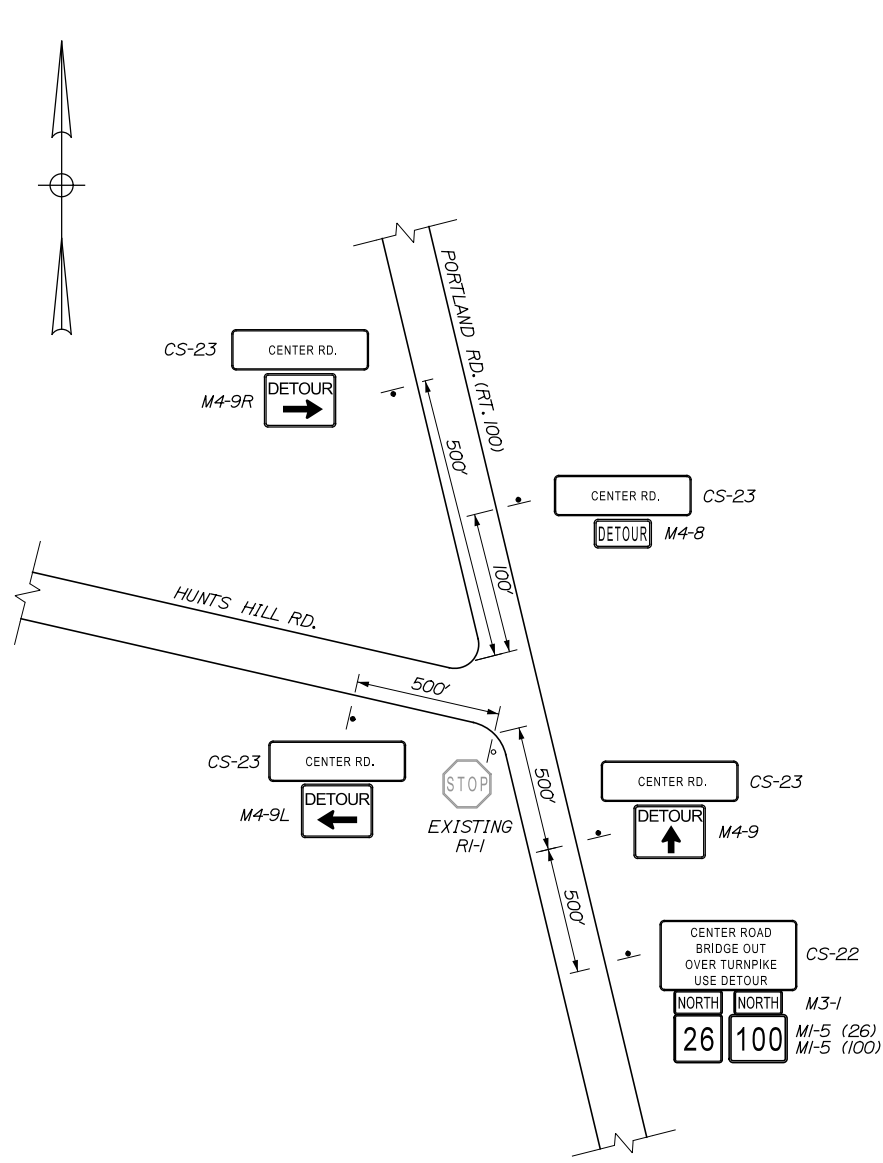
CONTRACT: 2014.15

SHEET NUMBER: MT-04

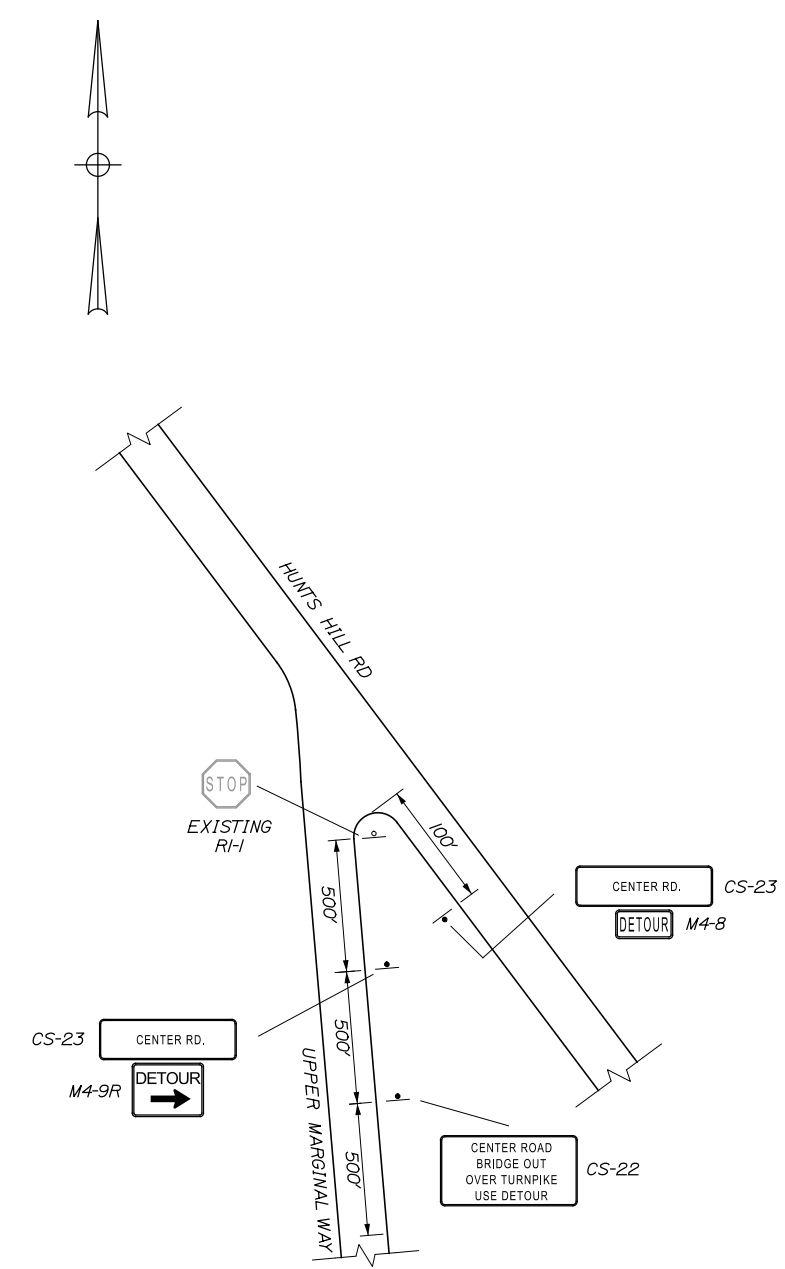
7 OF 52

Date: 3/24/2014

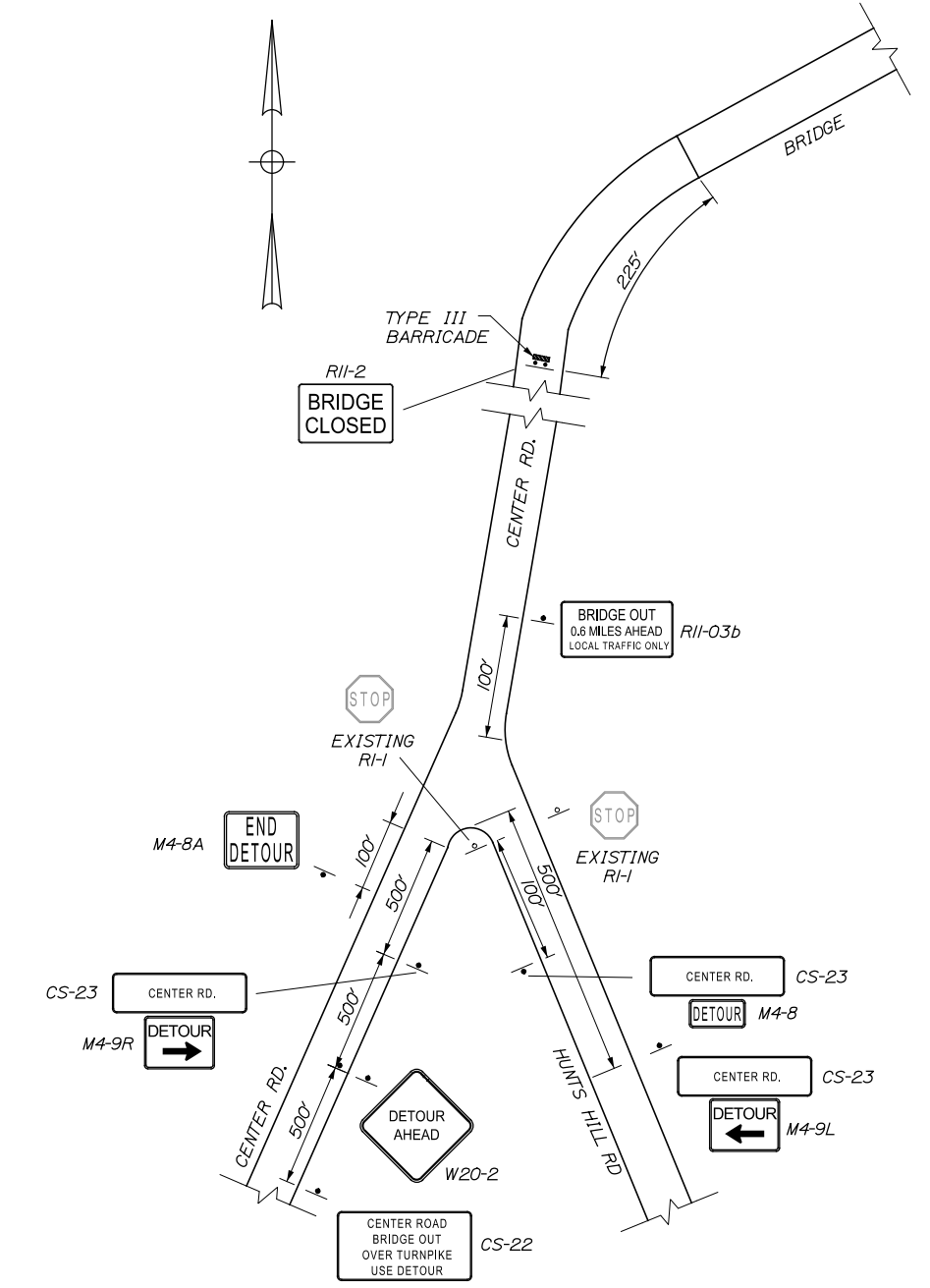
Filename: ...MSTA\008_62.9_MOT_02.dgn



SITE #2



SITE #3




SITE #4

Scale:				
NOT TO SCALE				
No.	Revision	By	Date	

Designed by:					
T.Y. LIN INTERNATIONAL					
CONSULTANT PROJECT MANAGER: Norman L. Baker					
	By	Date		By	Date
Designed	DRR	3/14	Checked	TSK	3/14
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THE GOLD STAR
MEMORIAL HIGHWAY

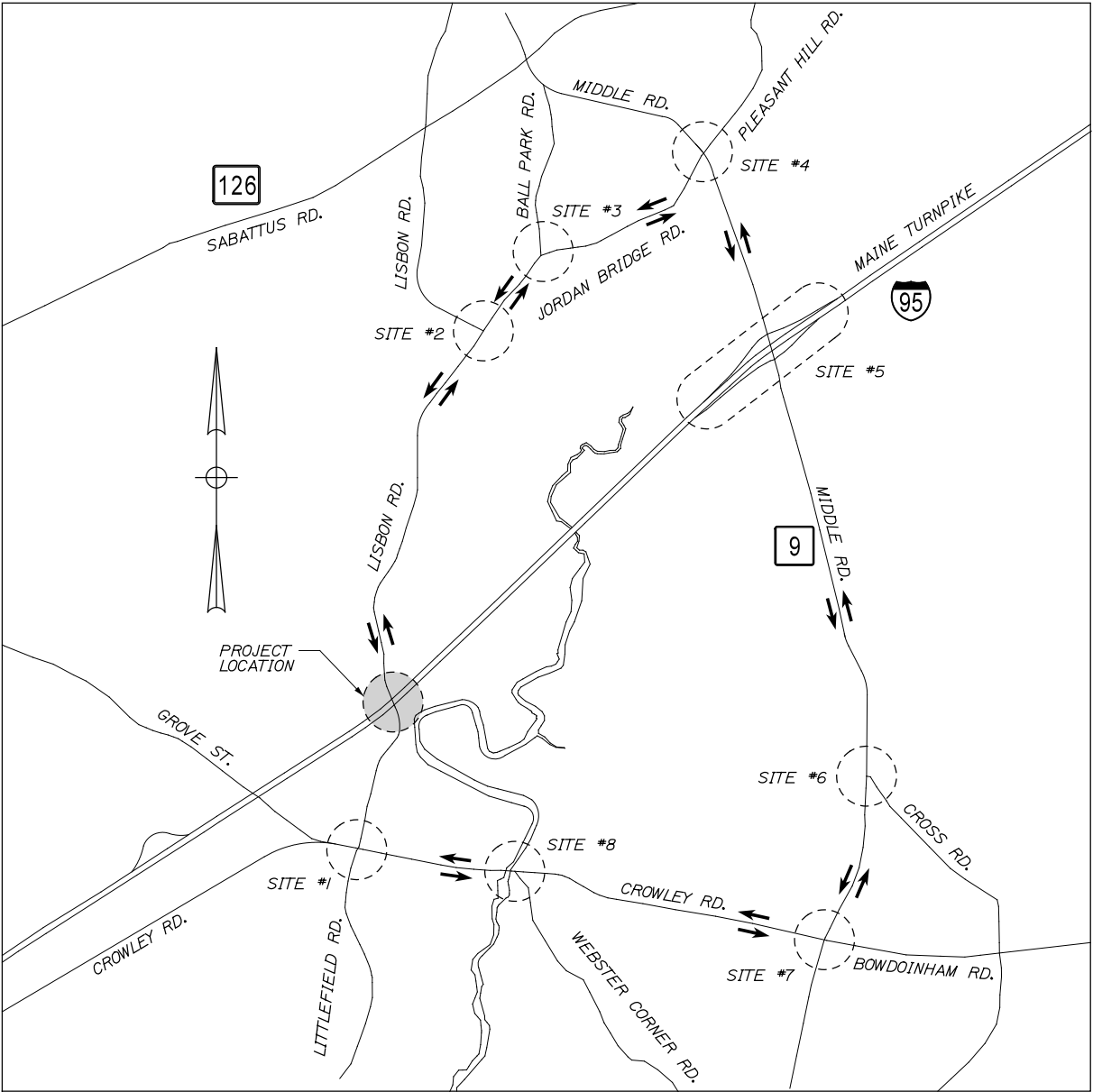
MTA PROJECT MANAGER: Ralph C. Norwood IV

BRIDGE AND CULVERT REPAIRS
4 LOCATIONS (MILE F1.7 TO MILE 84.3)
CENTER RD. UNDERPASS - MILE 62.9
DETOUR PLAN - 2 OF 2

SHEET NUMBER: MT-05
8 OF 52

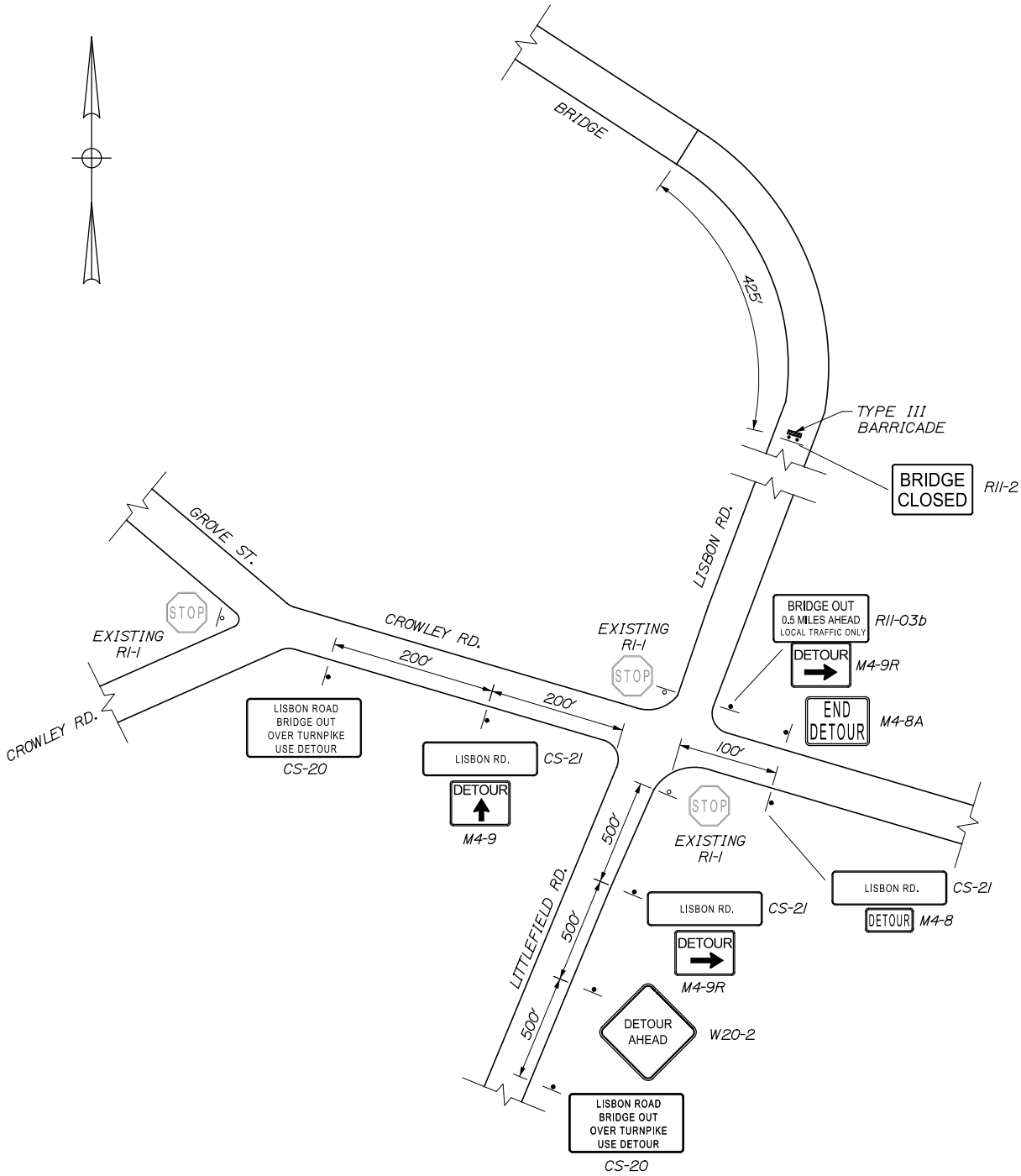
CONTRACT: 2014.15

Date: 3/24/2014



NOTES:

1. REFER TO THE MAINTENANCE OF TRAFFIC SHEETS FOR ALL TURNPIKE MAINLINE WORK.
2. CONTRACTOR IS ADVISED OF POTENTIAL CONSTRUCTION AND TEMPORARY TRAFFIC CONTROL AT SITE #4 NOTED ABOVE. CONTRACTOR SHALL COORDINATE WITH MAINE TURNPIKE AND MODIFY DETOUR MEASURES AT SITE #4 AS REQUIRED BY THE RESIDENT.



SITE #1

Filename: ...\\MSTA\\009_84.3_MOT_01.dgn

Scale:			
NOT TO SCALE			
No.	Revision	By	Date

Designed by:					
T.Y. LIN INTERNATIONAL					
CONSULTANT PROJECT MANAGER: Norman L. Baker					
	By	Date		By	Date
Designed	DRR	3/14	Checked	TSK	3/14
Drawn	JD	3/14	In Charge of	NLB	3/14

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THE GOLD STAR
MEMORIAL HIGHWAY

MTA PROJECT MANAGER: Ralph C. Norwood IV

BRIDGE AND CULVERT REPAIRS
4 LOCATIONS (MILE F1.7 TO MILE 84.3)
LISBON RD. UNDERPASS - MILE 84.3
DETOUR PLAN - 1 OF 4

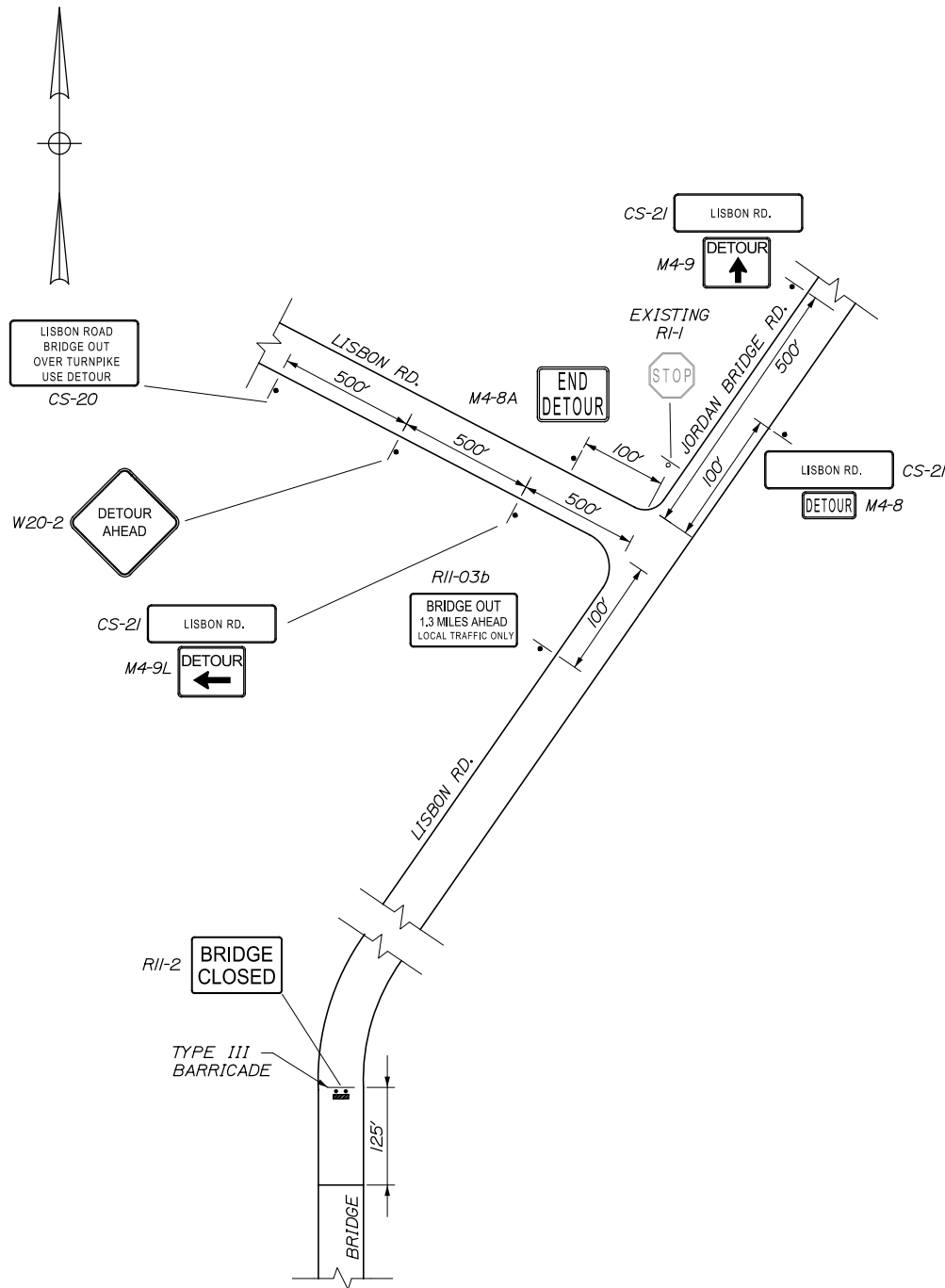
CONTRACT: 2014.15

SHEET NUMBER: MT-06

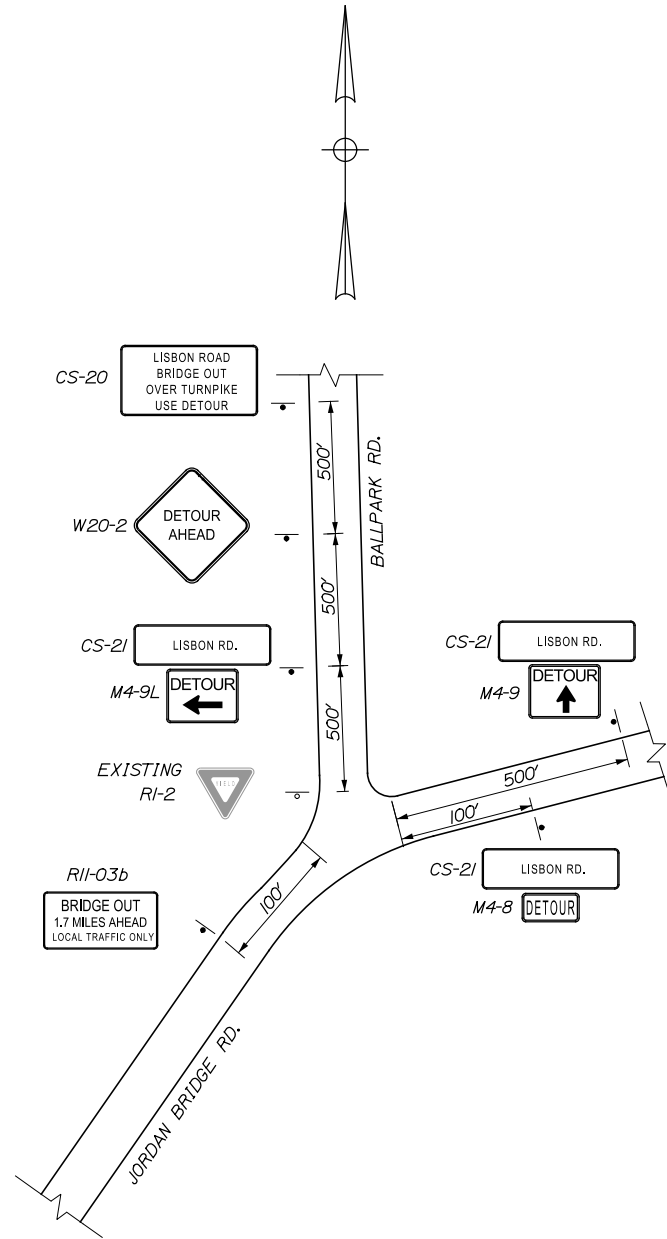
9 OF 52

Date: 3/24/2014

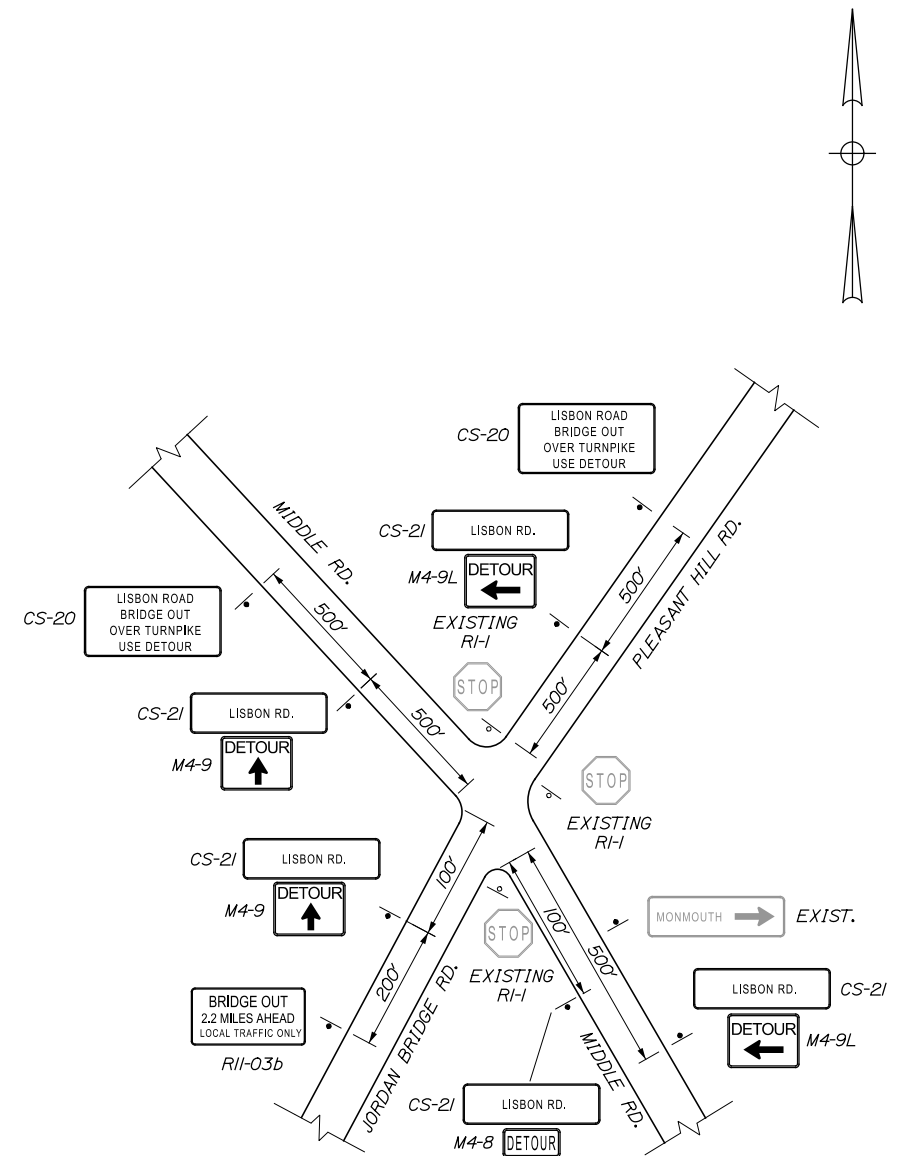
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SITE #2



SITE #3



SITE #4

Scale:				
NOT TO SCALE				
No.	Revision	By	Date	

Designed by:					
T.Y. LIN INTERNATIONAL					
CONSULTANT PROJECT MANAGER: Norman L. Baker					
	By	Date		By	Date
Designed	DRR	3/14	Checked	TSK	3/14
Drawn	JD	3/14	In Charge of	NLB	3/14

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THE GOLD STAR
MEMORIAL HIGHWAY

MTA PROJECT MANAGER: Ralph C. Norwood IV

BRIDGE AND CULVERT REPAIRS
4 LOCATIONS (MILE F1.7 TO MILE 84.3)
LISBON RD. UNDERPASS - MILE 84.3
DETOUR PLAN - 2 OF 4

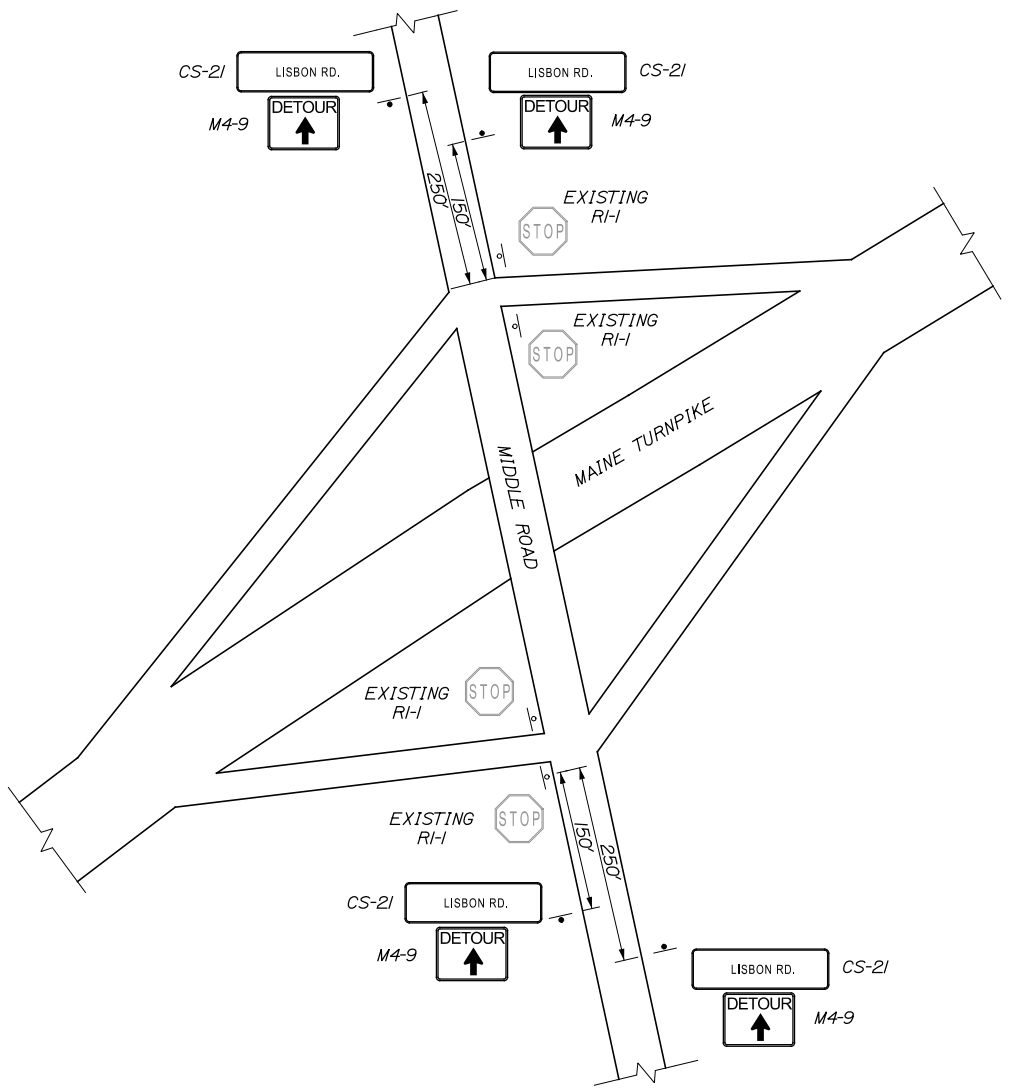
CONTRACT: 2014.15

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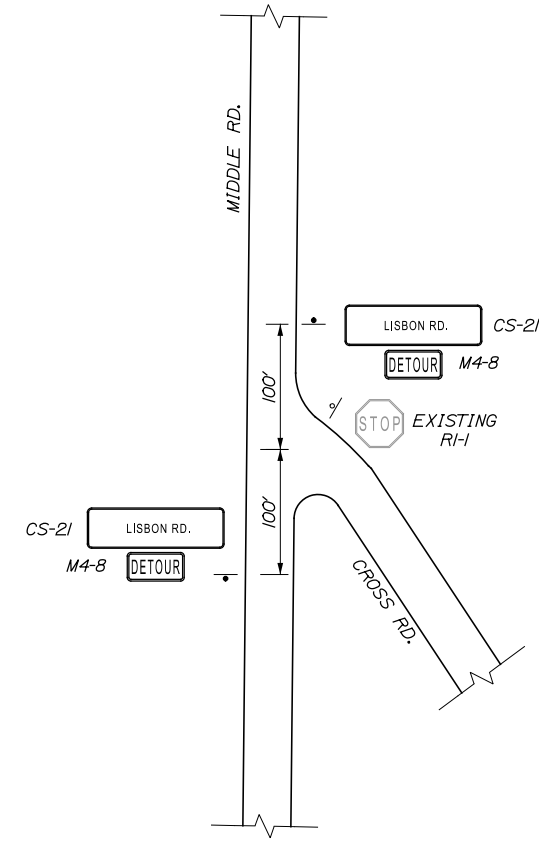
10 OF 52

Date: 3/24/2014

Filename: ...\\MSTA\011_84.3_MOT_03.dgn



SITE #5




SITE #6

Scale:				
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No.	Revision	By	Date	

Designed by:					
TY·LIN INTERNATIONAL					
CONSULTANT PROJECT MANAGER: Norman L. Baker					
	By	Date		By	Date
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**THE GOLD STAR
MEMORIAL HIGHWAY**

MTA PROJECT MANAGER: Ralph C. Norwood IV

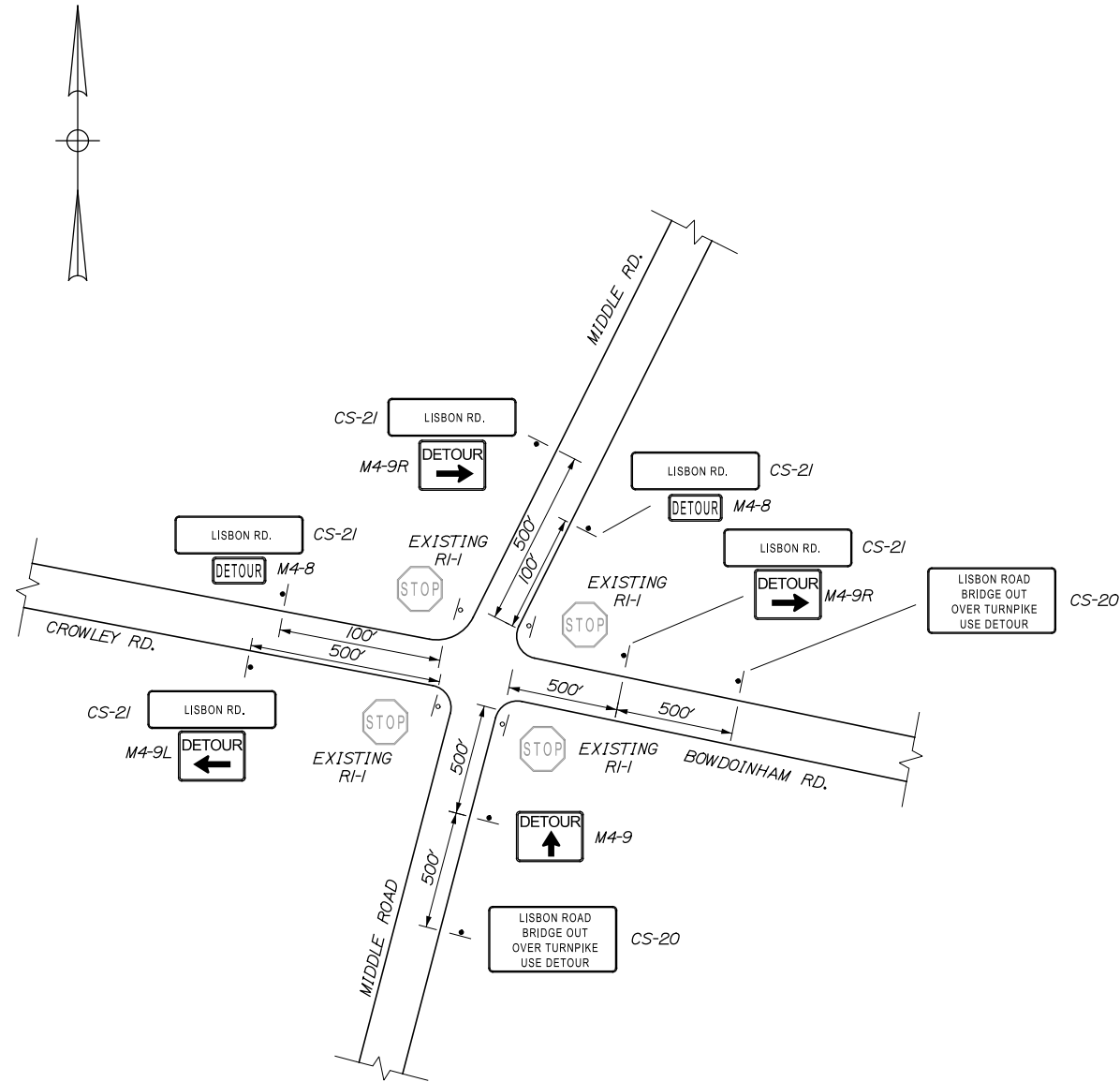
BRIDGE AND CULVERT REPAIRS
4 LOCATIONS (MILE F1.7 TO MILE 84.3)
LISBON RD. UNDERPASS - MILE 84.3
DETOUR PLAN - 3 OF 4

CONTRACT: 2014.15

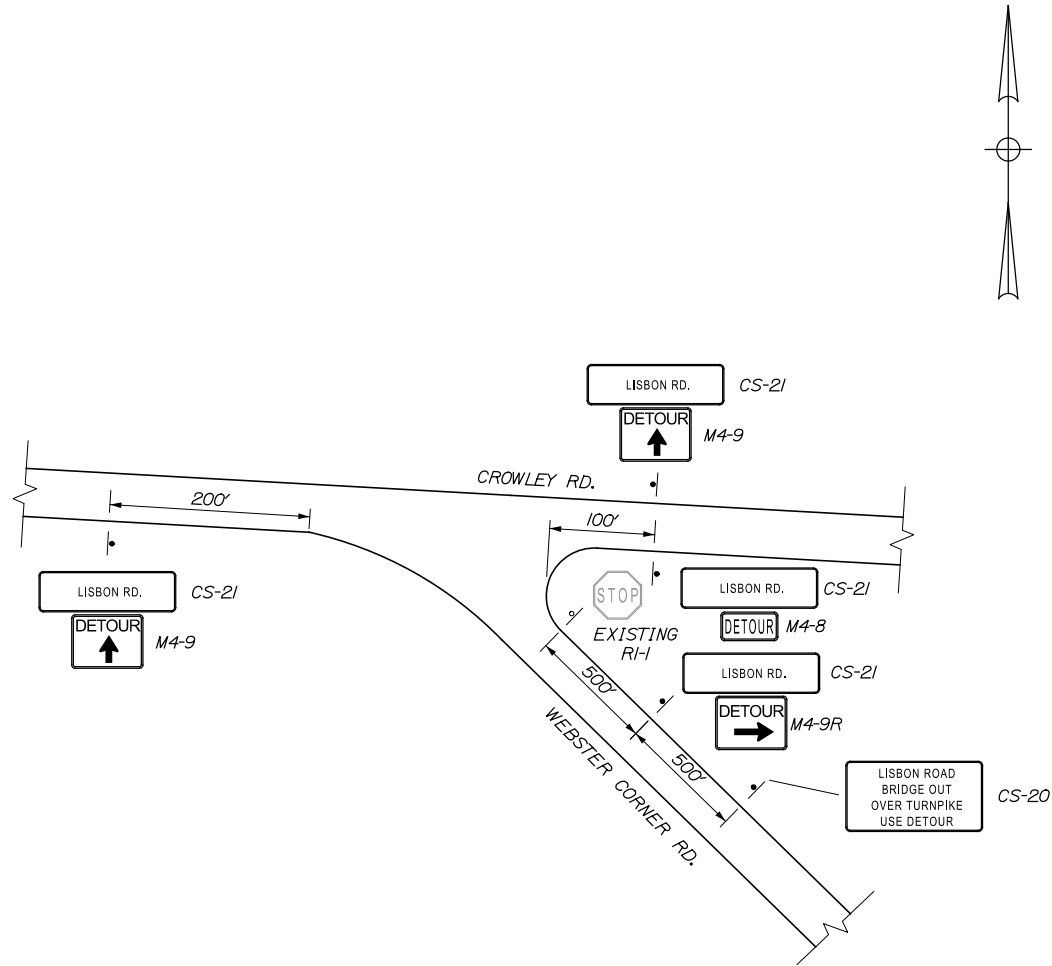
SHEET NUMBER: MT-08
11 OF 52

Date: 3/24/2014

Filename: ...MSTA\012_84.3_MOT_04.dgn



SITE #7



SITE #8

Scale:			
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No.	Revision	By	Date

Designed by:					
T.Y. LIN INTERNATIONAL					
CONSULTANT PROJECT MANAGER: Norman L. Baker					
	By	Date		By	Date
Designed	DRR	3/14	Checked	TSK	3/14
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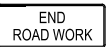









THE GOLD STAR
MEMORIAL HIGHWAY







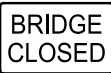
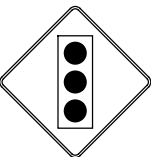


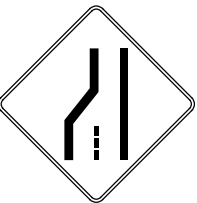
MTA PROJECT MANAGER: Ralph C. Norwood IV

BRIDGE AND CULVERT REPAIRS
4 LOCATIONS (MILE F1.7 TO MILE 84.3)
LISBON RD. UNDERPASS - MILE 84.3
DETOUR PLAN - 4 OF 4

Date: 3/24/2014

Filename: ...\\013_MOT_Sign_Summary_01.dgn

IDENTIFI- CATION NUMBER	SIZE OF SIGN		TEXT	TEXT DIMENSIONS (INCHES)			NUMBER OF SIGNS REQUIRED	COLOR		BORDER RADIUS	AREA IN SQUARE FEET	NOTES
	WIDTH	HEIGHT		LETTER HEIGHT	VERTICAL SPACING	ARROW RTE. MKR.		BACK- GROUND	LEGEND BORDER			
CS-1	48"	48"		TEXT DIMENSIONS SHALL CONFORM TO "STANDARD HIGHWAY SIGNS" - 2009			8	ORANGE	BLACK		16.00 (128)	
CS-20	60"	36"		5C 5C 5C 5C	4" 4" 4"		9	WHITE	BLACK		15.00 (135)	
CS-21	60"	12"		5"			29	WHITE	BLACK		5.00 (145)	
CS-22	60"	36"		5C 5C 5C 5C	4" 4" 4"		7	WHITE	BLACK		15.00 (105)	
CS-23	60"	12"		5"			9	WHITE	BLACK		5.00 (45)	
	84"	18"		8"	5"		1				10.50 (10.5)	
E5-1	48"	36"		TEXT DIMENSIONS SHALL CONFORM TO "STANDARD HIGHWAY SIGNS" - 2009			1	ORANGE	BLACK		12.00 (12)	
G20-2	36" 48"	18" 24"					3 16	ORANGE	BLACK		4.5 (13.5) 8.00 (128)	
G20-5aP	36"	24"					8	ORANGE	BLACK		6.00 (48)	
M1-5 (26) (100)	24" 30"	24" 24"					5 5	ORANGE	BLACK		4.00 (20) 5.00 (25)	
M3-1	24"	12"					2	ORANGE	BLACK		2.00 (4)	
M3-3	24"	12"					8	ORANGE	BLACK		2.00 (16)	
M4-8	24"	12"					12	ORANGE	BLACK		2.00 (24)	
M4-8A	24"	18"					4	ORANGE	BLACK		3.00 (12)	
M4-9	30"	24"					14	ORANGE	BLACK		5.00 (70)	
	48"	36"					1				12.00 (12)	
M4-9L	30"	24"					7	ORANGE	BLACK		5.00 (35)	
M4-9R	30"	24"					8	ORANGE	BLACK		5.00 (40)	

IDENTIFI- CATION NUMBER	SIZE OF SIGN		TEXT	TEXT DIMENSIONS (INCHES)			NUMBER OF SIGNS REQUIRED	COLOR		BORDER RADIUS	AREA IN SQUARE FEET	NOTES
	WIDTH	HEIGHT		LETTER HEIGHT	VERTICAL SPACING	ARROW RTE. MKR.		BACK- GROUND	LEGEND BORDER			
R2-1	24" 36"	30" 48"		TEXT DIMENSIONS SHALL CONFORM TO "STANDARD HIGHWAY SIGNS" - 2009			3 8	WHITE	BLACK		5.00 (15) 12.00 (96)	
R2-6aP	36"	24"					8	WHITE	BLACK		6.00 (48)	
R2-12	36"	54"					8	WHITE	BLACK		13.5 (108)	
R3-2	24"	24"					2	WHITE	BLACK		4.00 (8)	
R10-6	24"	36"					2	WHITE	BLACK		6.00 (12)	
R11-03b (0.4) (0.6) (0.5) (1.3) (1.7) (2.2)	60"	30"					6	WHITE	BLACK		12.50 (75)	
R11-2	48"	30"					4	WHITE	BLACK		10.00 (40)	
W3-3	36"	36"					2	ORANGE	BLACK		9.00 (18)	
W3-4	48"	48"					8	ORANGE	BLACK		16.00 (128)	
W3-5	48"	48"					8	ORANGE	BLACK		16.00 (128)	
W4-2 (LEFT) (RIGHT)	48"	48"					8 8	ORANGE	BLACK		16.00 (128) 16.00 (128)	

Scale:				
NOT TO SCALE				
No.	Revision	By	Date	

Designed by:					
TY·LIN INTERNATIONAL					
CONSULTANT PROJECT MANAGER: Norman L. Baker					
	By	Date		By	Date
Designed	DRR	3/14	Checked	TSK	3/14
Drawn	JD	3/14	In Charge of	NLB	3/14

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THE GOLD STAR
MEMORIAL HIGHWAY

MTA PROJECT MANAGER: Ralph C. Norwood IV

BRIDGE AND CULVERT REPAIRS
4 LOCATIONS (MILE F1.7 TO MILE 84.3)
MAINTENANCE OF TRAFFIC
SIGN SUMMARY - 1 OF 2

CONTRACT: 2014.15

SHEET NUMBER: MT-10

13 OF 52

Date: 3/24/2014

Filename: ...\\014_MOT_Sign_Summary_02.dgn

IDENTIFI- CATION NUMBER	SIZE OF SIGN		TEXT	TEXT DIMENSIONS (INCHES)			NUMBER OF SIGNS REQUIRED	COLOR		BORDER RADIUS	AREA IN SQUARE FEET	NOTES
	WIDTH	HEIGHT		LETTER HEIGHT	VERTICAL SPACING	ARROW RTE. MKR.		BACK- GROUND	LEGEND BORDER			
W7-3aP (0.1)	36"	30"	<div>NEXT XX MILES</div>	TEXT DIMENSIONS SHALL CONFORM TO "STANDARD HIGHWAY SIGNS" - 2009			4	ORANGE	BLACK		7.50 (30)	
W16-2P (1500)	30"	24"	<div>XX FEET</div>				1	ORANGE	BLACK		5.00 (5)	
W20-5 (LEFT)	48"	48"	<div>LEFT LANE CLOSED 1/2 MILE</div>				8	ORANGE	BLACK		16.00 (128)	
(RIGHT)							8				16.00 (128)	
W20-7a	36"	36"	<div></div>				2	ORANGE	BLACK		9.00 (18)	
W21-5	48"	48"	<div>SHOULDER WORK</div>				4	ORANGE	BLACK		16.00 (64)	
W21-5aR (RIGHT)	48"	48"	<div>RIGHT SHOULDER CLOSED</div>				5	ORANGE	BLACK		16.0 (80)	
W21-5aL (LEFT)	48"	48"					3				16.0 (48)	
W21-5bR (RIGHT) (1000)	48"	48"	<div>RIGHT SHOULDER CLOSED 500 FT</div>				3	ORANGE	BLACK		16.0 (48)	
W21-5bL (LEFT) (1000)	48"	48"					3				16.0 (48)	
W20-1 (AHEAD)	36"	36"	<div>ROAD WORK AHEAD</div>				2	ORANGE	BLACK		9.00 (18)	
(AHEAD)	48"	48"					16				16 (256)	
(1 MILE)	48"	48"					6				16 (96)	
W20-2	36"	36"	<div>DETOUR AHEAD</div>	<div>↓</div>	<div>↓</div>	<div>↓</div>	4	ORANGE	BLACK		9.00 (36)	

IDENTIFI- CATION NUMBER	SIZE OF SIGN		TEXT	TEXT DIMENSIONS (INCHES)			NUMBER OF SIGNS REQUIRED	COLOR		BORDER RADIUS	AREA IN SQUARE FEET	NOTES
	WIDTH	HEIGHT		LETTER HEIGHT	VERTICAL SPACING	ARROW RTE. MKR.		BACK- GROUND	LEGEND BORDER			
W20-4	36"	36"	<div>ONE LANE ROAD AHEAD</div>	TEXT DIMENSIONS SHALL CONFORM TO "STANDARD HIGHWAY SIGNS" - 2009			4	ORANGE	BLACK		9.00 (36)	

Scale:				
NOT TO SCALE				
No.	Revision	By	Date	

Designed by:					
<div>TYLININTERNATIONAL</div>					
CONSULTANT PROJECT MANAGER: Norman L. Baker					
	By	Date		By	Date
Designed	DRR	3/14	Checked	TSK	3/14
Drawn	JD	3/14	In Charge of	NLB	3/14

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THE GOLD STAR
MEMORIAL HIGHWAY

MTA PROJECT MANAGER: Ralph C. Norwood IV

BRIDGE AND CULVERT REPAIRS
4 LOCATIONS (MILE F1.7 TO MILE 84.3)
MAINTENANCE OF TRAFFIC
SIGN SUMMARY - 2 OF 2

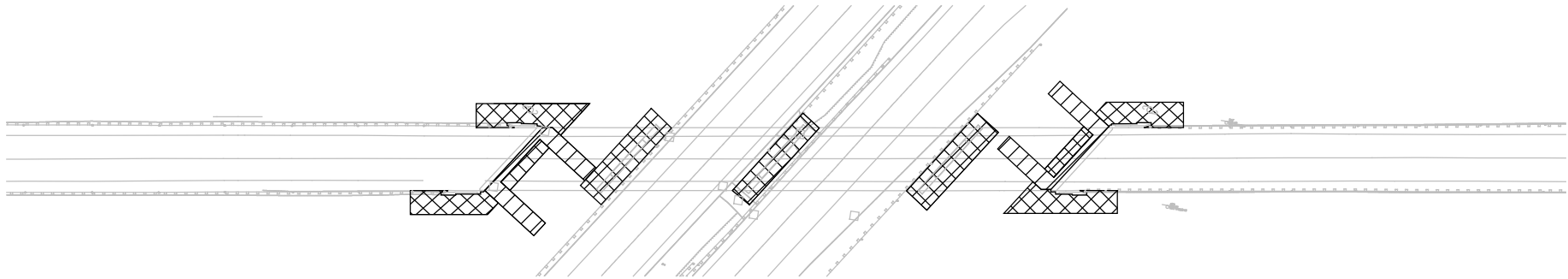
CONTRACT: 2014.15

SHEET NUMBER: MT-11

14 OF 52

Date: 3/24/2014

Filename: ...\\HIGHWAY\\MSTA\\015_DIST_PLAN.dgn



TYPICAL LIMIT OF DISTURBANCE PLAN

LEGEND:



CONTRACT LIMIT OF DISTURBANCE



CONTRACTOR'S LIMIT OF DISTURBANCE
AREA FOR ACCESS AND STORAGE

AREAS:

CONTRACT LIMIT OF DISTURBANCE:

FALMOUTH ROAD = 0.03 ACRES
CENTER ROAD = 0.03 ACRES
NO-NAME RIVER = 0.02 ACRES
LISBON ROAD = 0.03 ACRES

CONTRACT TOTAL = 0.11 ACRES

CONTRACTOR'S LIMIT OF DISTURBANCE
AREA FOR ACCESS AND STORAGE:

FALMOUTH ROAD = 0.15 ACRES
CENTER ROAD = 0.15 ACRES
NO-NAME RIVER = 0.10 ACRES
LISBON ROAD = 0.15 ACRES

CONTRACT TOTAL = 0.55 ACRES

NOTES:

1. EROSION AND SEDIMENT CONTROL DEVICES REQUIRED FOR THE CONTRACTOR'S ACCESS LOCATIONS AND STORAGE AREAS WILL NOT BE MEASURED FOR PAYMENT.
2. THE CONTRACTOR WILL BE ALLOWED TO STORE MATERIAL AND EQUIPMENT ON GRAVEL AND PAVEMENT WITHIN THE CLOSED PORTIONS OF FALMOUTH ROAD, CENTER ROAD, AND LISBON ROAD. THESE AREAS ARE EITHER INCLUDED IN THE LIMITS OF DISTURBANCE OR HAVE BEEN PREVIOUSLY SURFACED AND DO NOT ADD TO THE LIMITS OF DISTURBANCE.
3. SEE SPECIFICATIONS FOR CONTRACTOR'S SUBMITTAL REQUIREMENTS IF INCREASING THE LIMITS OF DISTURBANCE ABOVE THE PROJECT ESTIMATED QUANTITIES.
4. TYPICAL LIMITS OF DISTURBANCE SHOWN FOR UNDERPASS LOCATIONS. SEE SHEET "NO-NAME RIVER CULVERT - MILE 82.5 GENERAL SITE PLAN" FOR NO-NAME RIVER CULVERT DISTURBANCE LIMITS. AMOUNTS OF DISTURBANCE ARE LISTED ON THIS SHEET.



No.	Revision	By	Date

Designed by:

TY·LININTERNATIONAL

CONSULTANT PROJECT MANAGER: Norman L. Baker

	By	Date		By	Date
Designed	DSM	3/14	Checked	SBK	3/14
Drawn	JD	3/14	In Charge of	NLB	3/14

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**THE GOLD STAR
MEMORIAL HIGHWAY**

MTA PROJECT MANAGER: Ralph C. Norwood IV

**BRIDGE AND CULVERT REPAIRS
4 LOCATIONS (MILE F1.7 TO MILE 84.3)**

LIMIT OF DISTURBANCE PLAN

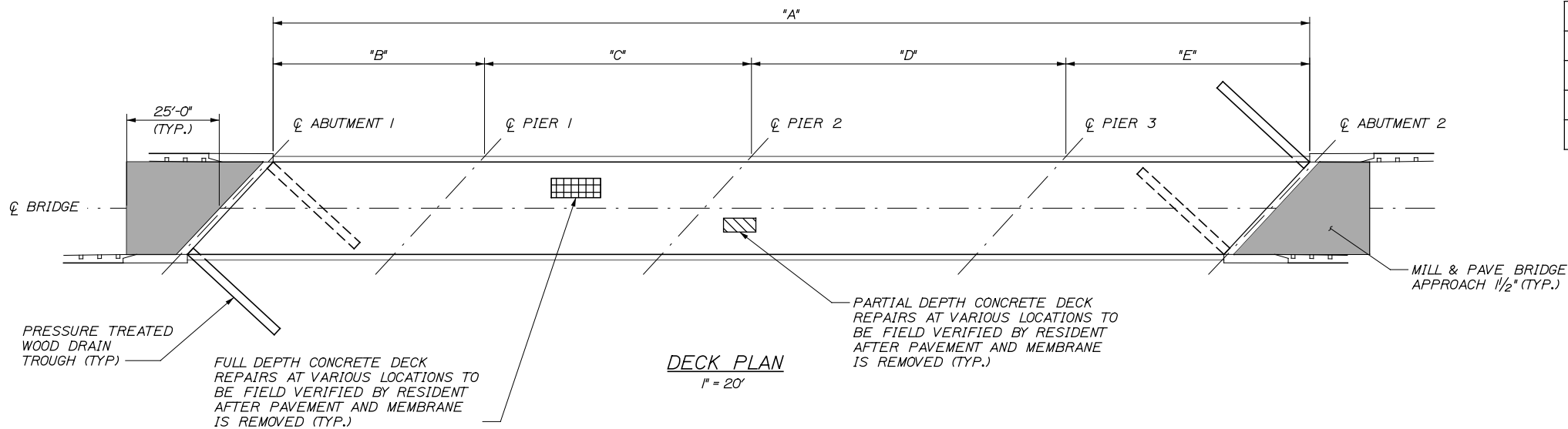
SHEET NUMBER: LD-01

CONTRACT: 2014.15

15 OF 52

Date: 3/24/2014

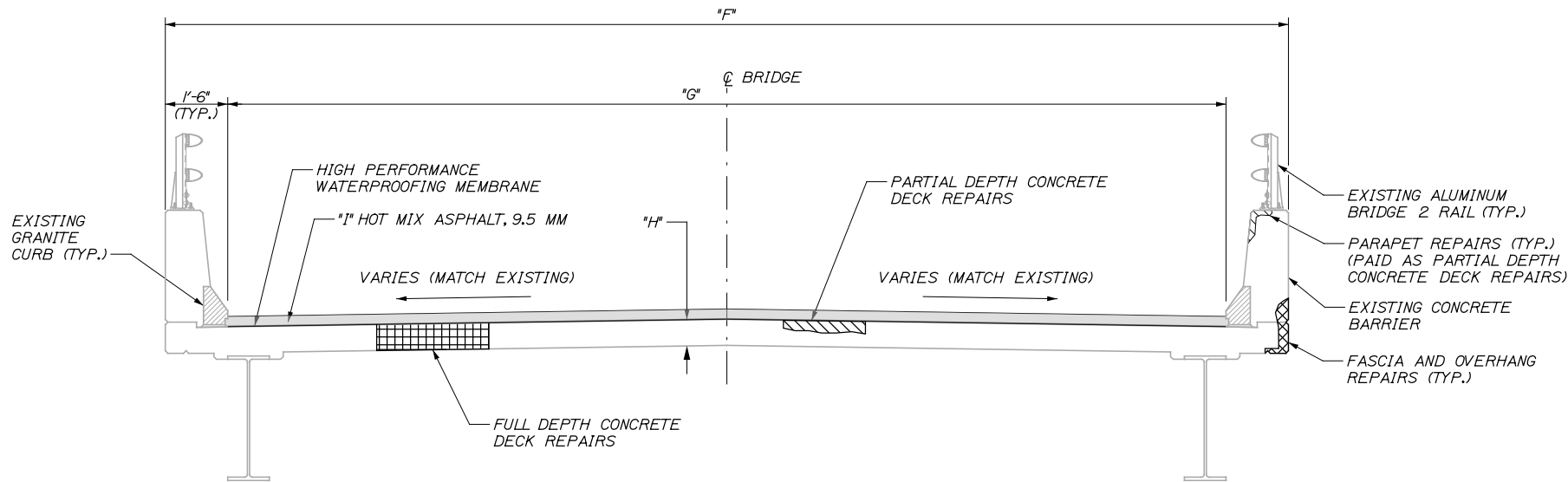
Filename: ...\\MSTA\\016_3Bridges_Plan_Typ.dgn



DECK PLAN DIMENSIONS					
	"A"	"B"	"C"	"D"	"E"
FALMOUTH ROAD MM F1.7	198'-0"	44'-0"	55'-0"	55'-0"	44'-0"
CENTER ROAD MM 62.9	252'-0"	52'-0"	69'-0"	77'-0"	54'-0"
LISBON ROAD MM 84.3	213'-0"	44'-0"	56'-0"	61'-0"	52'-0"

- NOTES**
- CONTRACTOR SHALL REPAIR AREAS OF CONCRETE DECK DETERIORATION AS IDENTIFIED BY THE RESIDENT. RESIDENT SHALL IDENTIFY AND MARK AREAS FOR DECK REPAIR AFTER THE WEARING SURFACE AND MEMBRANE ARE REMOVED. CONTRACTOR SHALL ALSO REPAIR DETERIORATED AREAS OF FASCIA AND FASCIA OVERHANGS. COORDINATE WORK WITH THE RESIDENT. PAYMENT FOR REPAIR WORK SHALL BE UNDER THE 518 PAY ITEMS.
 - CONTRACTOR SHALL REPAIR AREAS OF DETERIORATED GRANITE CURB BEDDING MORTAR ON THE DECK AS REQUIRED. LOCATIONS OF GRANITE CURB BEDDING MORTAR SHALL BE FIELD DETERMINED BY THE RESIDENT. COSTS SHALL BE INCIDENTAL TO 518 PAY ITEMS.
 - SEAL SHOULDER AREAS WITH TWO APPLICATIONS OF EMULSIFIED BITUMINOUS SEALER PER MAINEDOT STANDARD SPECIFICATION 403.03 AFTER COMPLETING ALL DECK PAVING WORK.
 - RETROFIT ALL EXISTING ALUMINUM BRIDGE RAIL SPLICES NOT IN CONFORMANCE WITH THE ALUMINUM BRIDGE RAILING - SPLICE MODIFICATION DETAIL. PAYMENT TO BE MADE UNDER ITEM 507.095.
 - FIELD VERIFY BROKEN ALUMINUM BRIDGE RAIL AND REPLACE. RAIL TO BE PAID FOR UNDER ITEM 507.0928.
 - ALL PVC DECK WEEPS WHICH DRAIN ONTO PIERS, ABUTMENTS OR STRUCTURAL STEEL SHALL BE EXTENDED AND DIVERTED AWAY FROM BRIDGE COMPONENTS BELOW DECK, AS DIRECTED BY THE RESIDENT. PAYMENT SHALL BE BY THE VARIOUS LABOR AND EQUIPMENT ITEMS.
 - ALL EXISTING PARAPET JOINTS SHALL BE CLEANED AND RECAULKED AS DIRECTED BY THE RESIDENT WITH AN APPROVED JOINT SEALER. COSTS SHALL BE INCIDENTAL TO 518 PAY ITEMS.
 - REMOVE SAND BUILDUP BENEATH GUARDRAILS AT LISBON ROAD OVERPASS NORTHWEST ABUTMENT. REMOVE BUILDUP APPROXIMATELY 25 FEET BACK FROM ABUTMENT OR AS DIRECTED BY THE RESIDENT. COSTS SHALL BE INCIDENTAL TO RELATED CONTRACT ITEMS.

- LEGEND**
- LIMITS OF FULL DEPTH CONCRETE DECK REPAIR (REPAIR AREAS TO BE FIELD VERIFIED)
 - LIMITS OF PARTIAL DEPTH CONCRETE DECK REPAIR (REPAIR AREAS TO BE FIELD VERIFIED)
 - LIMITS OF FASCIA AND FASCIA OVERHANG REPAIR (REPAIR AREAS TO BE FIELD VERIFIED)



TRANSVERSE SECTION DIMENSIONS				
	"F"	"G"	"H"	"I"
FALMOUTH ROAD MM F1.7	39'-0"	36'-0"	0'-7 1/2"	0'-2 1/4"
CENTER ROAD MM 62.9	31'-6"	28'-6"	0'-7"	0'-1 1/2"
LISBON ROAD MM 84.3	28'-0"	25'-0"	0'-8"	0'-1 1/2"

TRANSVERSE SECTION
(INTERIOR GIRDERS NOT SHOWN)
1/2" = 1'-0"

ESTIMATED REPAIR QUANTITIES			
	PARTIAL DEPTH CONCRETE DECK REPAIRS (S.F.)	FULL DEPTH CONCRETE DECK REPAIRS (S.F.)	FASCIA AND FASCIA OVERHANG REPAIRS (S.F.)
FALMOUTH ROAD MM F1.7	425	40	30
CENTER ROAD MM 62.9	440	40	70
LISBON ROAD MM 84.3	205	30	30

Scale: AS NOTED			
No.	Revision	By	Date

Designed by: TYLIN INTERNATIONAL					
CONSULTANT PROJECT MANAGER: Norman L. Baker					
	By	Date		By	Date
Designed	DSM	3/14	Checked	SBK	3/14
Drawn	TJP	3/14	In Charge of	NLB	3/14

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**THE GOLD STAR
MEMORIAL HIGHWAY**

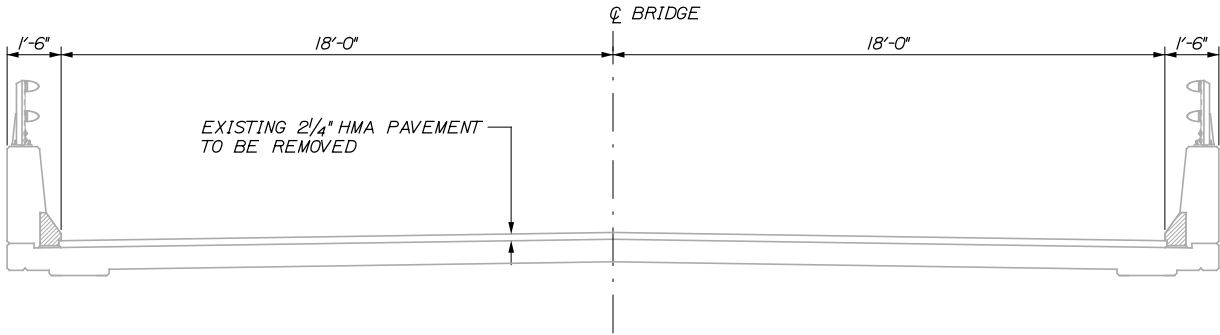
MTA PROJECT MANAGER: Ralph C. Norwood IV

BRIDGE AND CULVERT REPAIRS
4 LOCATIONS (MILE F1.7 TO MILE 84.3)
MILE F1.7, MILE 62.9 & MILE 84.3
DECK PLAN & TRANSVERSE SECTION

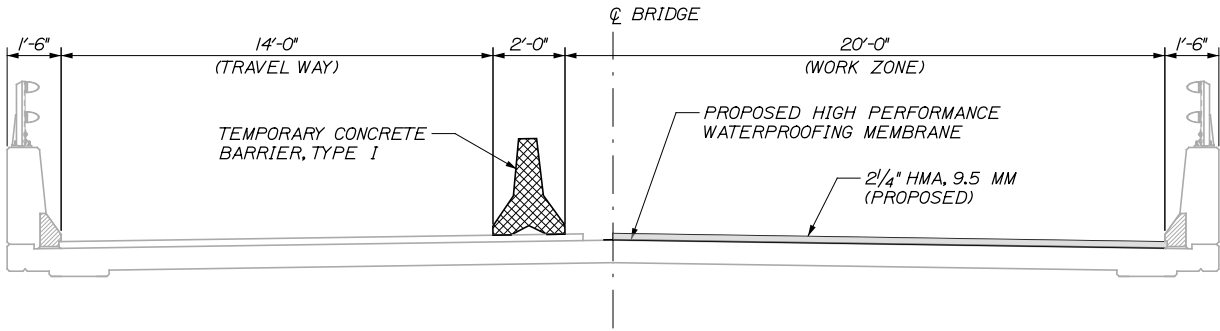
CONTRACT: 2014.15 SHEET NUMBER: S-01 16 OF 52

Date: 3/24/2014

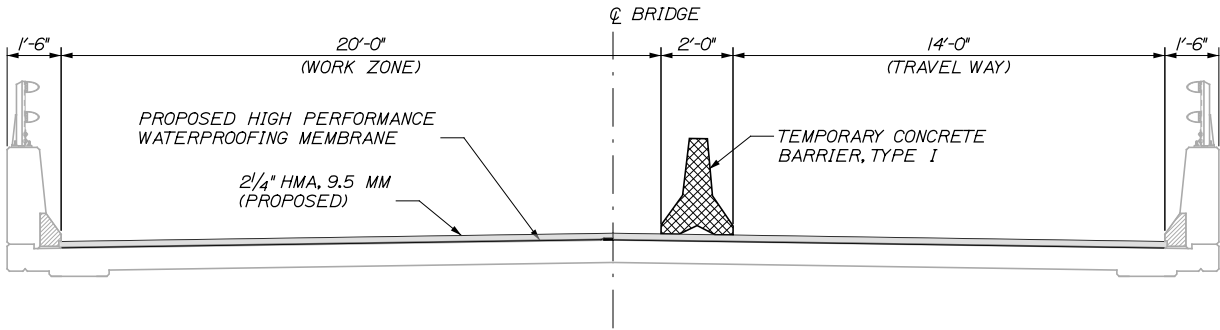
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EXISTING BRIDGE



PHASE 1



PHASE 2

Scale:				
3/8" = 1'-0"				
No.	Revision	By	Date	

Designed by:					
<div>T.Y. LIN INTERNATIONAL</div>					
CONSULTANT PROJECT MANAGER: Norman L. Baker					
	By	Date		By	Date
Designed	DSM	3/14	Checked	SBK	3/14
Drawn	JD	3/14	In Charge of	NLB	3/14

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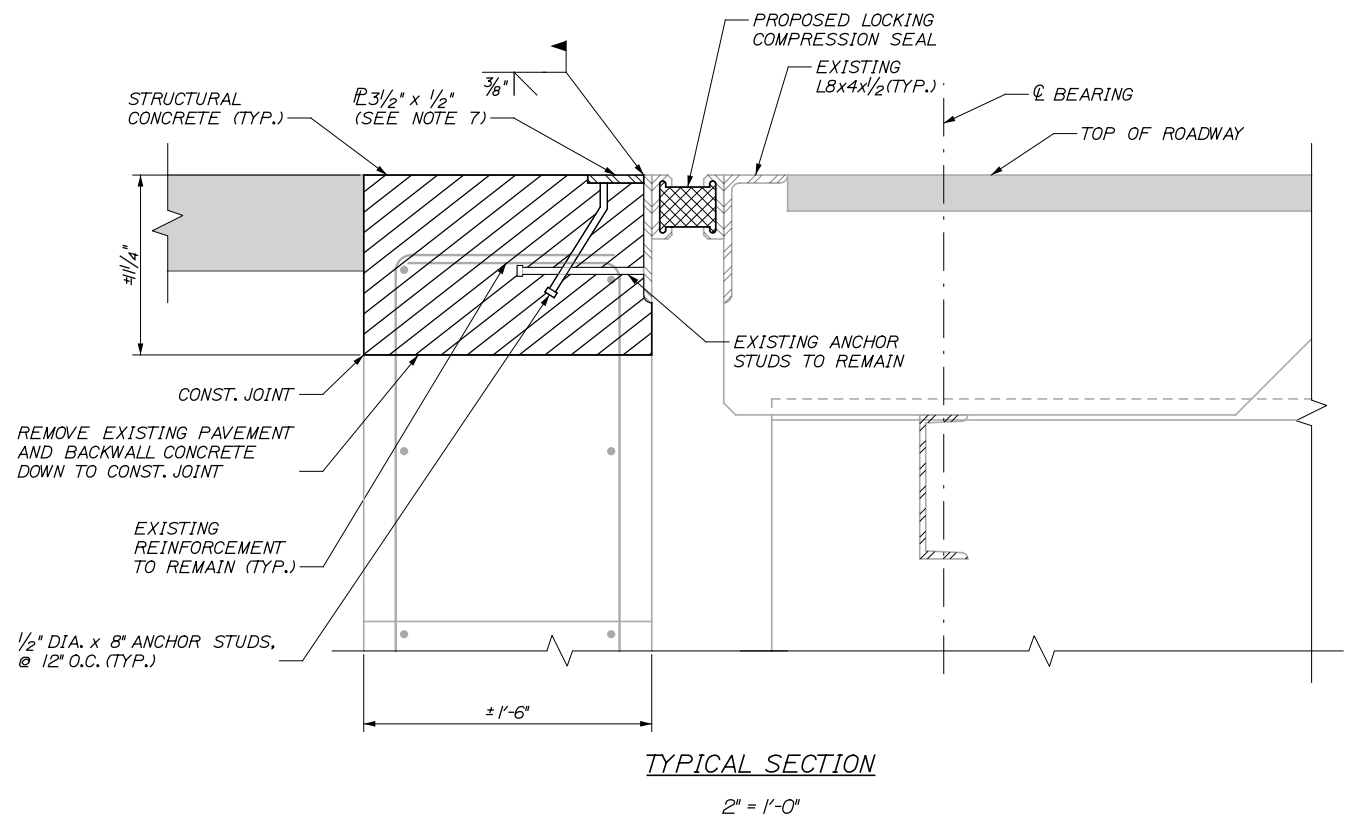
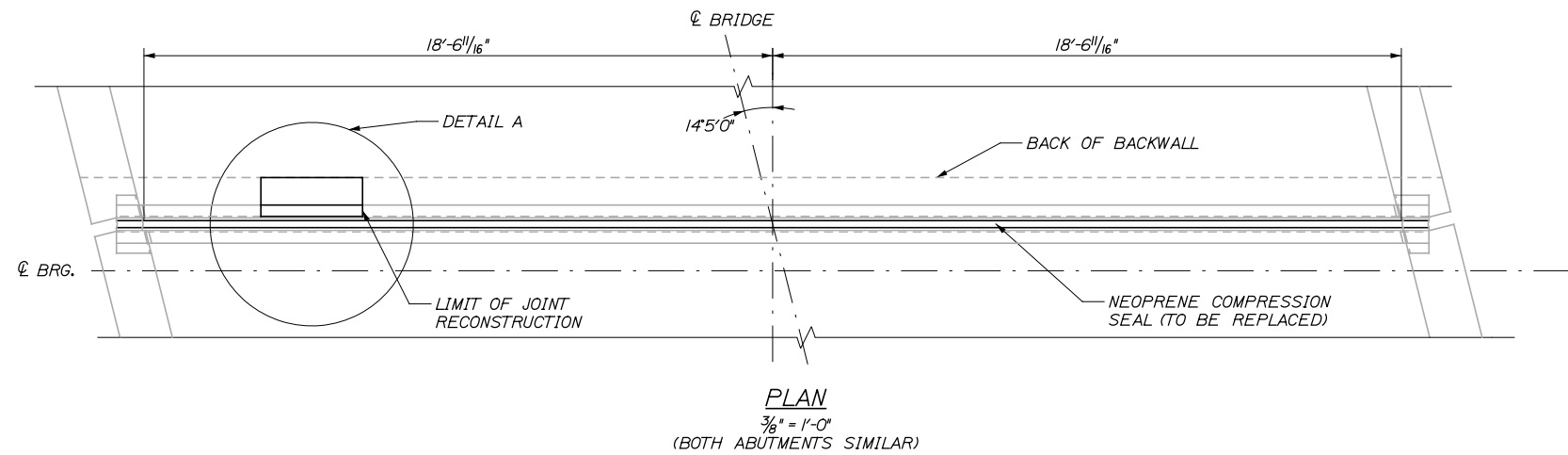
**THE GOLD STAR
MEMORIAL HIGHWAY**

MTA PROJECT MANAGER: Ralph C. Norwood IV

BRIDGE AND CULVERT REPAIRS
 4 LOCATIONS (MILE F1.7 TO MILE 84.3)
 FALMOUTH RD. UNDERPASS - MILE F1.7
 BRIDGE PHASING

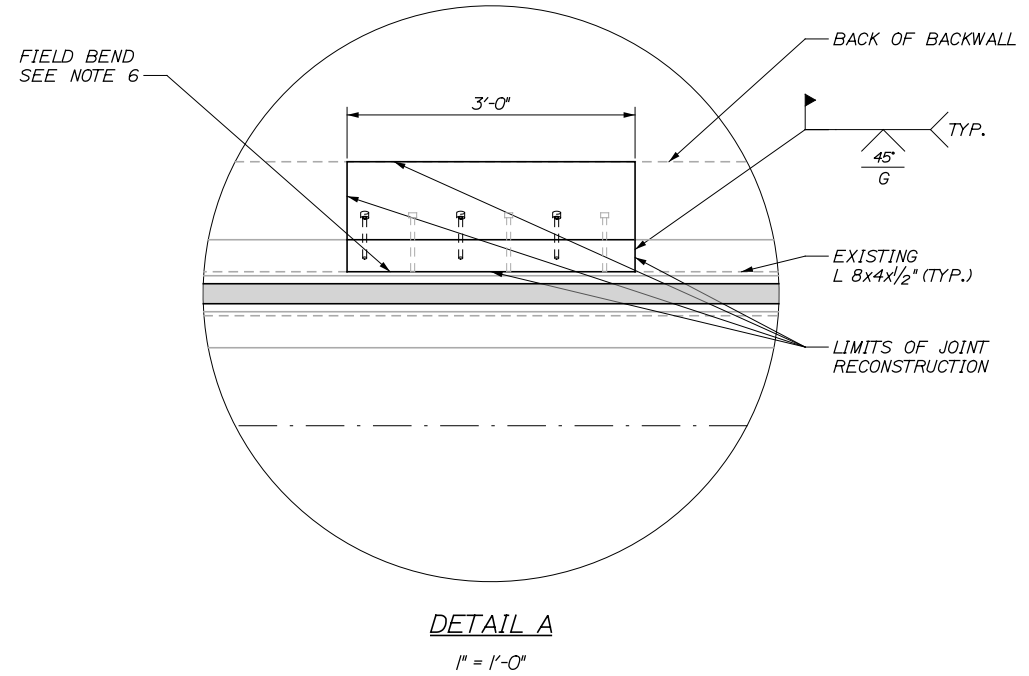
Date: 3/24/2014

Filename: ...\\018_F1.7_Joint_Repairs.dgn



EXPANSION JOINT NOTES:


1. THE CONTRACTOR SHALL APPLY AN EPOXY BONDING AGENT SELECTED FROM MAINEDOT'S QUALIFIED PRODUCTS LIST TO ALL STEEL SURFACES OF THE EXPANSION JOINT THAT WILL BE EMBEDDED IN CONCRETE BEFORE PLACING THE BACKWALL CONCRETE.
2. ALL STEEL COMPONENTS SHALL BE AASHTO M270 GRADE 36, UNLESS OTHERWISE NOTED.
3. FOR CLARITY, NOT ALL REINFORCING STEEL HAS BEEN SHOWN IN ALL DETAILS.
4. THE NEOPRENE LOCKING COMPRESSION SEALS TO BE FURNISHED SHALL HAVE A MINIMUM MOVEMENT RANGE OF 2 INCHES. SEALS SHALL BE "WABO D SERIES" AS MANUFACTURED BY WATSON BOWMAN ACME.
5. CONTRACTOR SHALL FIELD VERIFY LOCKING COMPRESSION SEAL OPENING AND INSTALL REPLACEMENT SEAL AT A TEMPERATURE SUCH THAT INSTALLATION IS FEASIBLE.
6. CONTRACTOR SHALL FIELD BEND VERTICAL LEG OF EXISTING ANGLE THAT WILL REMAIN TO BE PLUMB AND IN LINE WITH UNDAMAGED JOINT.
7. CONTRACTOR SHALL REMOVE HORIZONTAL LEG OF ANGLE IN DAMAGED REGION UP TO THE FACE OF THE VERTICAL LEG OF THE ANGLE, AND SHALL REPLACE IT WITH A NEW STEEL PLATE AS SHOWN, AFTER REPAIRING VERTICAL LEG PER NOTE 6.
8. EXACT LOCATION OF REPAIRS ON EACH DAMAGED JOINT ANGLE TO BE ESTABLISHED BY THE RESIDENT.



Scale:			
AS NOTED			
No.	Revision	By	Date

Designed by:					
TYLIN INTERNATIONAL					
CONSULTANT PROJECT MANAGER: Norman L. Baker					
	By	Date		By	Date
Designed	DSM	3/14	Checked	SBK	3/14
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**THE GOLD STAR
MEMORIAL HIGHWAY**

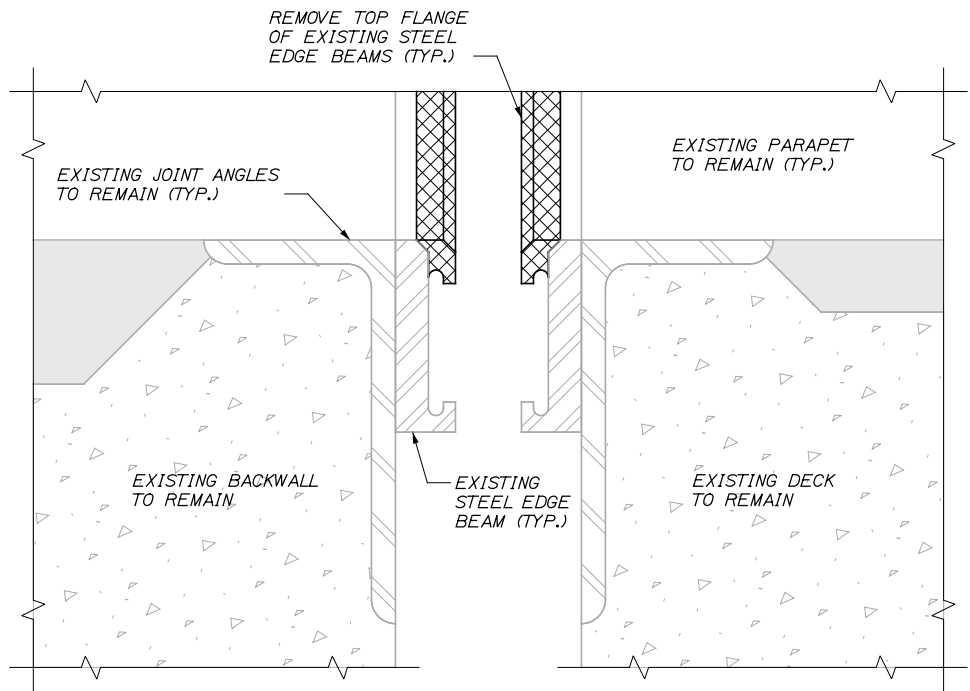
MTA PROJECT MANAGER: Ralph C. Norwood IV

BRIDGE AND CULVERT REPAIRS
4 LOCATIONS (MILE F1.7 TO MILE 84.3)
FALMOUTH RD. UNDERPASS - MILE F1.7
JOINT REPAIRS

SHEET NUMBER: S-03
CONTRACT: 2014.15
18 OF 52

Date: 3/24/2014

Filename: ...019_62.9_Joint_Repairs.dgn



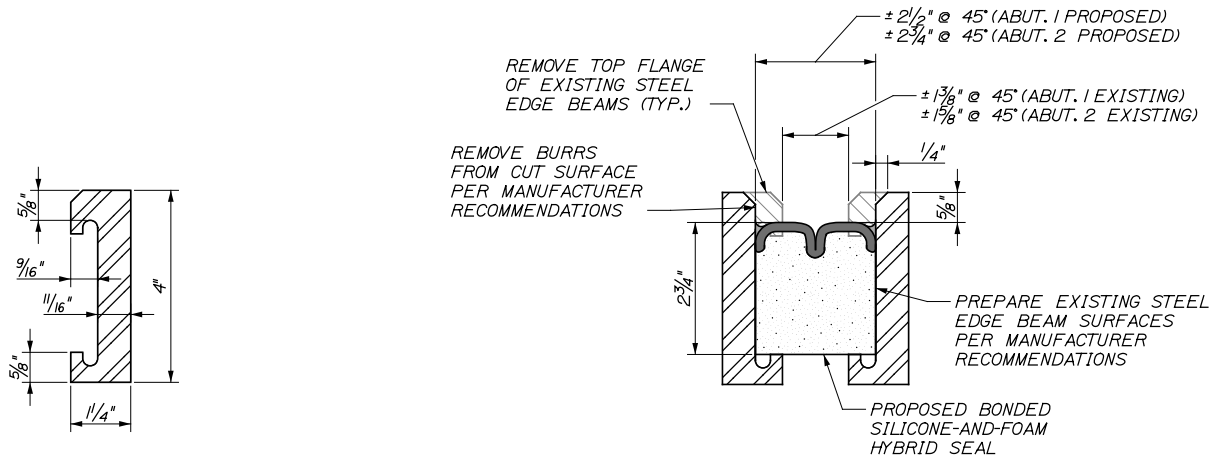
CENTER ROAD JOINT SECTION
AT PARAPET FACE

6" = 1'-0"

EXISTING SEAL AND PROPOSED SEALS NOT SHOWN FOR CLARITY



JOINT ARMOR CRACK REPAIR



CENTER ROAD EDGE BEAM MODIFICATION
AND PROPOSED SEAL TYPICAL SECTION

6" = 1'-0"

CENTER ROAD EXISTING
EDGE BEAM DIMENSIONS

6" = 1'-0"

CENTER RD. JOINT REPAIR NOTES

1. CONTRACTOR SHALL REPAIR JOINT ARMOR CRACKS WITH WELDS. LOCATIONS AS IDENTIFIED BY THE RESIDENT.
2. CONTRACTOR SHALL REPLACE EXISTING NEOPRENE COMPRESSION SEALS WITH NEW BONDED SILICONE-AND-FOAM HYBRID SEALS.
3. THE BONDED SILICONE-AND-FOAM HYBRID SEALS TO BE FURNISHED SHALL HAVE A MINIMUM MOVEMENT RANGE OF 1 1/2 INCHES.

Scale:			
AS NOTED			
No.	Revision	By	Date

Designed by:					
T.Y. LIN INTERNATIONAL					
CONSULTANT PROJECT MANAGER: Norman L. Baker					
	By	Date		By	Date
Designed	DSM	3/14	Checked	NLB	3/14
Drawn	DSM	3/14	In Charge of	NLB	3/14

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THE GOLD STAR
MEMORIAL HIGHWAY

MTA PROJECT MANAGER: Ralph C. Norwood IV

BRIDGE AND CULVERT REPAIRS
4 LOCATIONS (MILE F1.7 TO MILE 84.3)
CENTER RD. UNDERPASS - MILE 62.9
JOINT REPAIRS

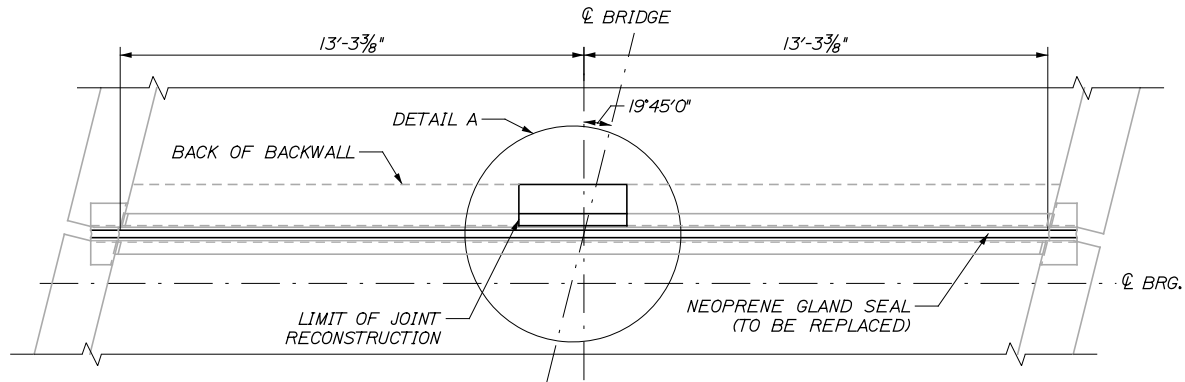
CONTRACT: 2014.15

SHEET NUMBER: S-04

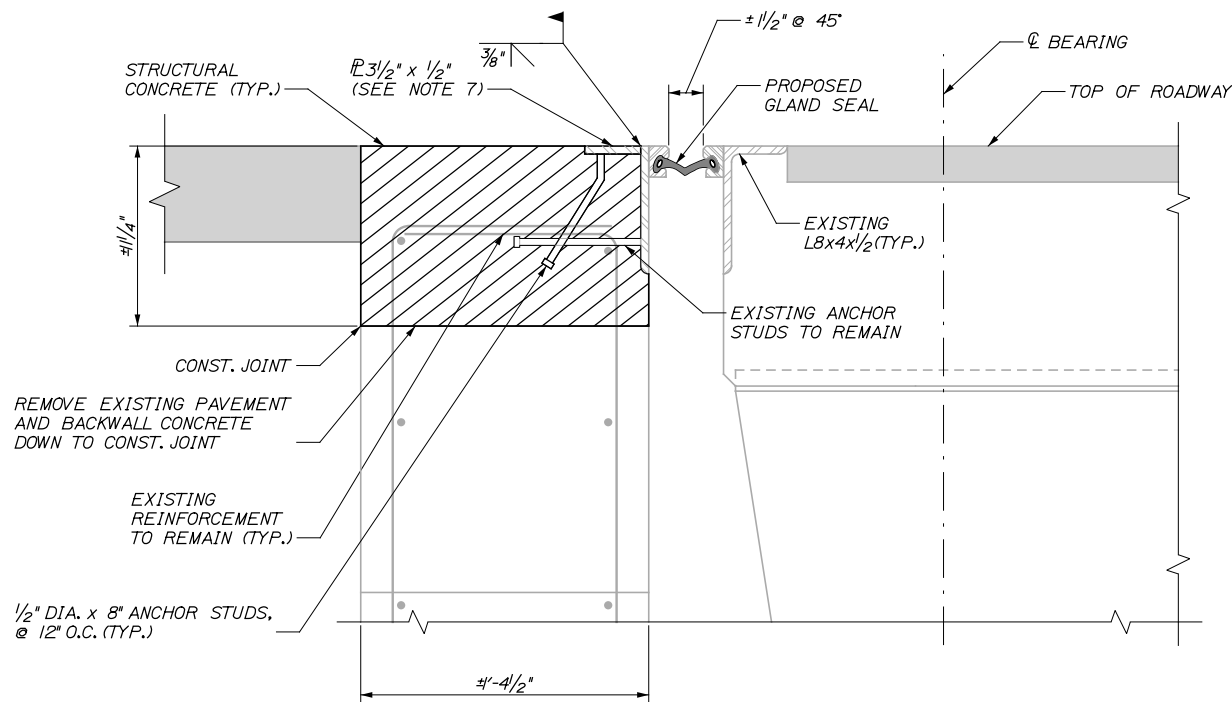
19 OF 52

Date: 3/24/2014

Filename: ...020_84.3_Joint_Repairs.dgn



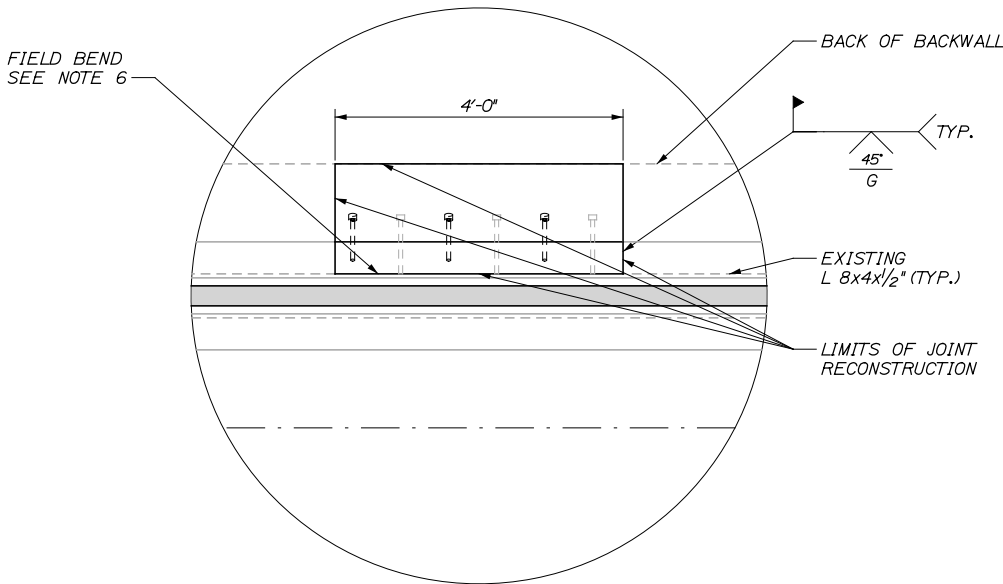
PLAN - ABUTMENT 2 (SOUTHEAST ABUTMENT)
NTS



TYPICAL SECTION
2" = 1'-0"

EXPANSION JOINT NOTES:

1. THE CONTRACTOR SHALL APPLY AN EPOXY BONDING AGENT SELECTED FROM MAINEDOT'S QUALIFIED PRODUCTS LIST TO ALL STEEL SURFACES OF THE EXPANSION JOINT THAT WILL BE EMBEDDED IN CONCRETE BEFORE PLACING THE BACKWALL CONCRETE.
2. ALL STEEL COMPONENTS SHALL BE AASHTO M270 GRADE 36, UNLESS OTHERWISE NOTED.
3. FOR CLARITY, NOT ALL REINFORCING STEEL HAS BEEN SHOWN IN ALL DETAILS.
4. THE GLAND SEALS TO BE FURNISHED SHALL HAVE A MINIMUM MOVEMENT RANGE OF 2 INCHES.
5. CONTRACTOR SHALL FIELD VERIFY EXISTING GLAND SEAL TYPE AND GEOMETRY AND PROVIDE A REPLACEMENT SEAL THAT WILL FIT EXISTING STEEL EXTRUSIONS.
6. CONTRACTOR SHALL FIELD BEND VERTICAL LEG OF EXISTING ANGLE THAT WILL REMAIN TO BE PLUMB AND IN LINE WITH UNDAMAGED JOINT.
7. CONTRACTOR SHALL REMOVE HORIZONTAL LEG OF ANGLE IN DAMAGED REGION UP TO THE FACE OF THE VERTICAL LEG OF THE ANGLE, AND SHALL REPLACE IT WITH A NEW STEEL PLATE AS SHOWN, AFTER REPAIRING VERTICAL LEG PER NOTE 6.
8. EXACT LOCATION OF REPAIRS ON THE DAMAGED JOINT ANGLE TO BE ESTABLISHED BY THE RESIDENT.
9. FILL HOLES IN THE TOP OF THE EXISTING JOINT ANGLES WITH PLUG WELDS. LOCATIONS TO BE ESTABLISHED BY THE RESIDENT.



DETAIL A
NTS

Scale:			
AS NOTED			
No.	Revision	By	Date

Designed by:					
TYLIN INTERNATIONAL					
CONSULTANT PROJECT MANAGER: Norman L. Baker					
	By	Date		By	Date
Designed	DSM	3/14	Checked	NLB	3/14
Drawn	DSM	3/14	In Charge of	NLB	3/14

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THE GOLD STAR
MEMORIAL HIGHWAY

MTA PROJECT MANAGER: Ralph C. Norwood IV

BRIDGE AND CULVERT REPAIRS
4 LOCATIONS (MILE F1.7 TO MILE 84.3)
LISBON RD. UNDERPASS - MILE 84.3
JOINT REPAIRS

SHEET NUMBER: S-05
CONTRACT: 2014.15
20 OF 52

Date: 3/24/2014





Filename: ...MSTA\021.F1.7.SUB_REPAIR_1.dgn

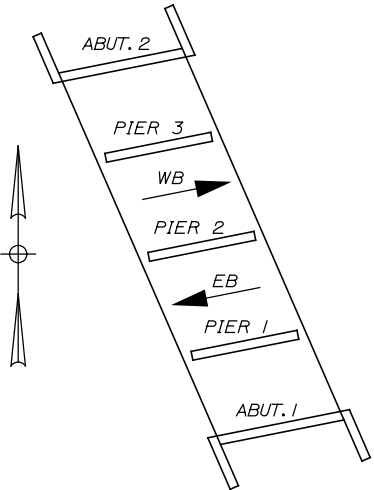
FALMOUTH RD. UNDERPASS - MM F1.7 ABUTMENTS ESTIMATED QUANTITY OF REPAIRS			
	ABUTMENT 1	ABUTMENT 2	TOTAL
ABUTMENT REPAIRS	75 S.F.	40 S.F.	115 S.F.
EPOXY INJECTED CRACK REPAIRS	0 L.F.	5 L.F.	5 L.F.

NOTES:

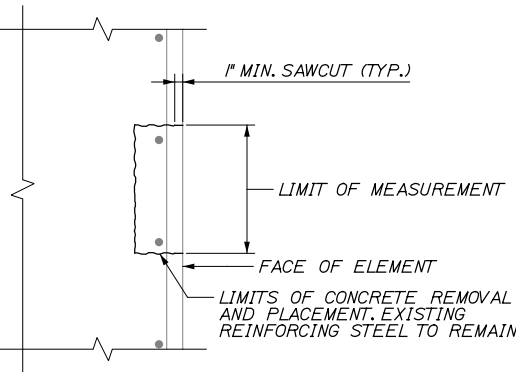
1. FOR PROCEDURAL NOTES SEE SHEET "GENERAL NOTES"

LEGEND:

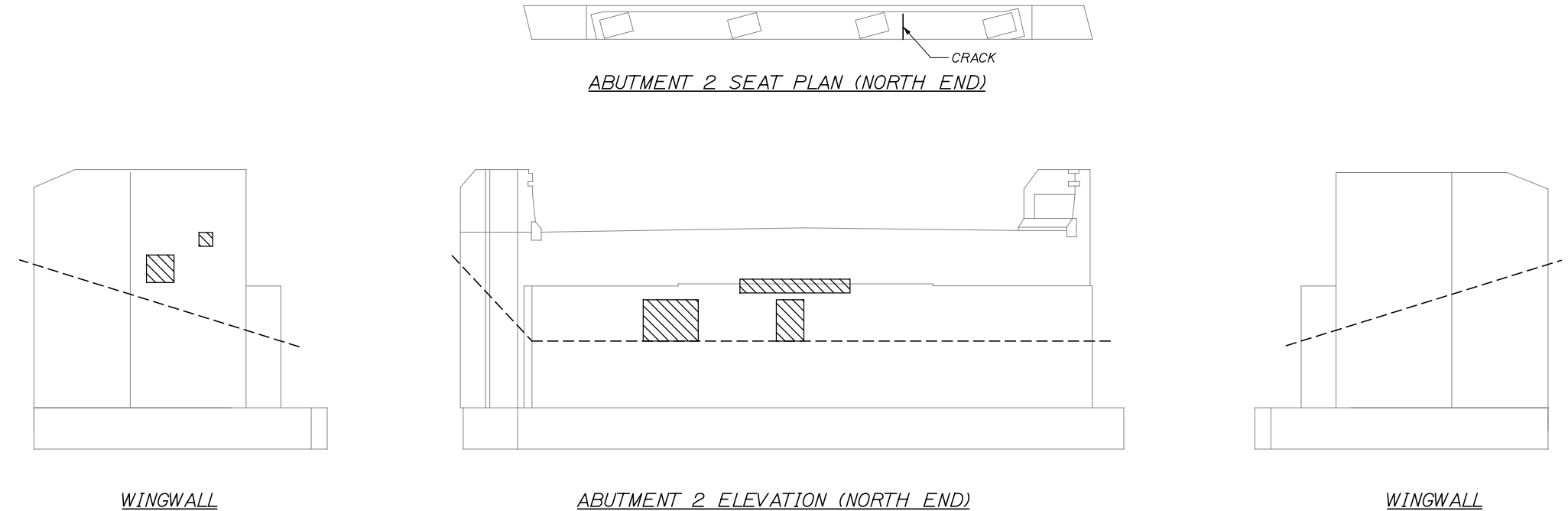
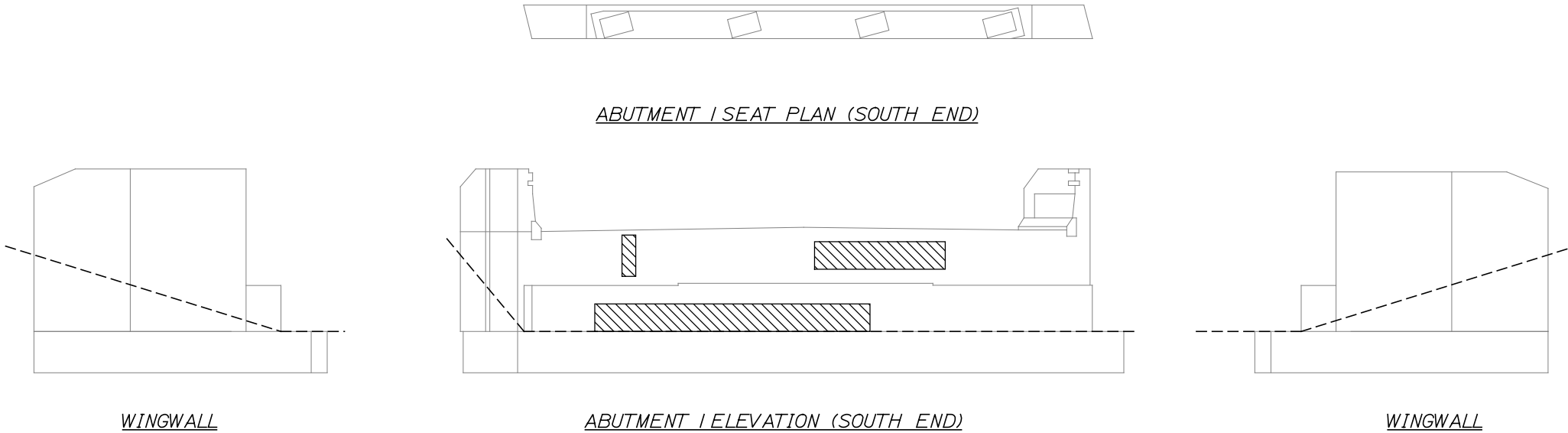
-  LIMIT OF CONCRETE SURFACE REPAIRS
-  LIMIT OF EPOXY INJECTED CRACK REPAIR
-  EXISTING CONCRETE
-  EXISTING AND PROPOSED GROUND



LOCATION PLAN



CONCRETE REPAIR DETAIL
1" = 1'-0"



Scale:			
NOT TO SCALE			
No.	Revision	By	Date

Designed by:					
TYLIN INTERNATIONAL					
CONSULTANT PROJECT MANAGER: Norman L. Baker					
	By	Date		By	Date
Designed	DSM	3/14	Checked	SBK	3/14
Drawn	JD	3/14	In Charge of	NLB	3/14

T.Y. Lin International
12 Northbrook Drive
Building A, Suite One
Falmouth, Maine 04105
TEL: (207) 781-4721
FAX: (207) 781-4753



THE GOLD STAR
MEMORIAL HIGHWAY

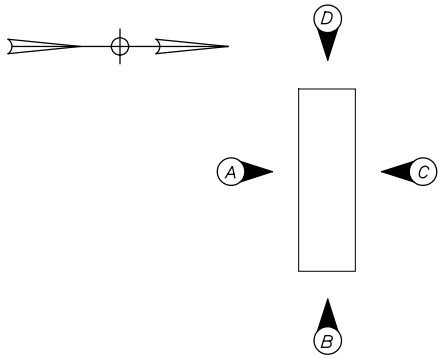
MTA PROJECT MANAGER: Ralph C. Norwood IV

BRIDGE AND CULVERT REPAIRS
4 LOCATIONS (MILE F1.7 TO MILE 84.3)
FALMOUTH RD. UNDERPASS - MILE F1.7
SUBSTRUCTURE REPAIR - 1 OF 3

SHEET NUMBER: S-06
CONTRACT: 2014.15
21 OF 52

Date: 3/24/2014

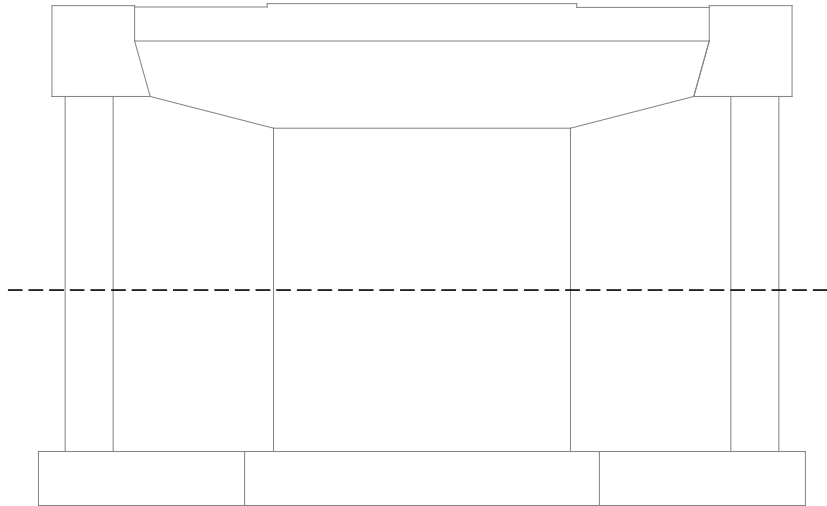
Filename: ...\\MSTA\\022_F1.7_SUB_REPAIR_2.dgn



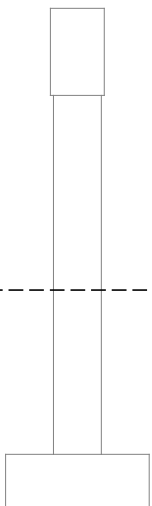
PIER ELEVATIONS



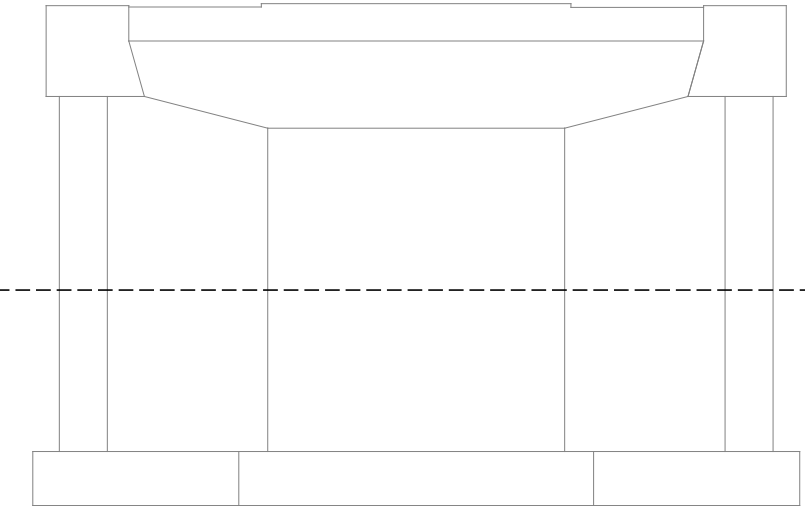
PLAN



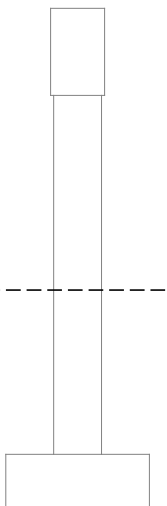
ELEVATION A



ELEVATION B



ELEVATION C



ELEVATION D

PIER 1 (SOUTH PIER)

FALMOUTH RD. UNDERPASS - MM F1.7 PIERS ESTIMATED QUANTITY OF REPAIRS				
	PIER 1	PIER 2	PIER 3	TOTAL
PIER REPAIRS	0 S.F.	60 S.F.	25 S.F.	85 S.F.
EPOXY INJECTED CRACK REPAIRS	0 L.F.	0 L.F.	15 L.F.	15 L.F.

NOTES:

- FOR CONCRETE REPAIR DETAIL SEE SHEET "FALMOUTH RD. UNDERPASS - MM, F1.7 SUBSTRUCTURE REPAIR - 1 OF 3"
- FOR PROCEDURAL NOTES SEE SHEET "GENERAL NOTES"

LEGEND:

- LIMIT OF CONCRETE SURFACE REPAIRS
- LIMIT OF EPOXY INJECTED CRACK REPAIR
- EXISTING CONCRETE
- EXISTING AND PROPOSED GROUND

Scale:			
NOT TO SCALE			
No.	Revision	By	Date

Designed by:					
CONSULTANT PROJECT MANAGER: Norman L. Baker					
	By	Date		By	Date
Designed	DSM	3/14	Checked	SBK	3/14
Drawn	JD	3/14	In Charge of	NLB	3/14

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Falmouth, Maine 04105
TEL: (207) 781-4721
FAX: (207) 781-4753



THE GOLD STAR
MEMORIAL HIGHWAY

MTA PROJECT MANAGER: Ralph C. Norwood IV

BRIDGE AND CULVERT REPAIRS
4 LOCATIONS (MILE F1.7 TO MILE 84.3)
FALMOUTH RD. UNDERPASS - MILE F1.7
SUBSTRUCTURE REPAIR - 2 OF 3

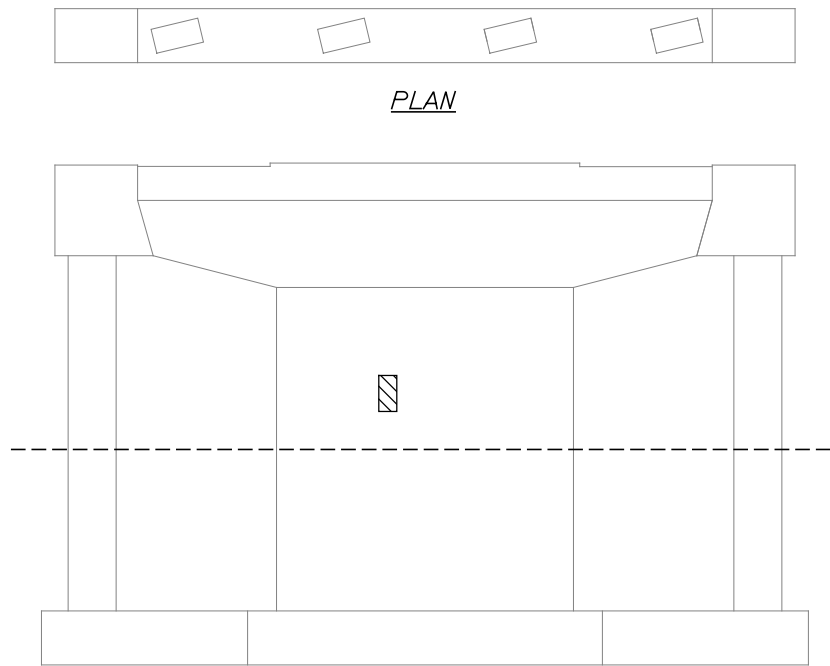
SHEET NUMBER: S-07

CONTRACT: 2014.15

22 OF 52

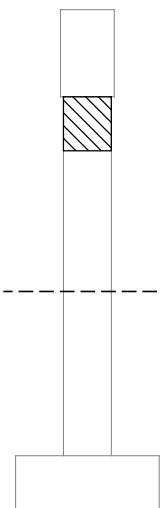
Date: 3/24/2014

Filename: ...MSTA\023_F1.7_SUB_REPAIR_3.dgn

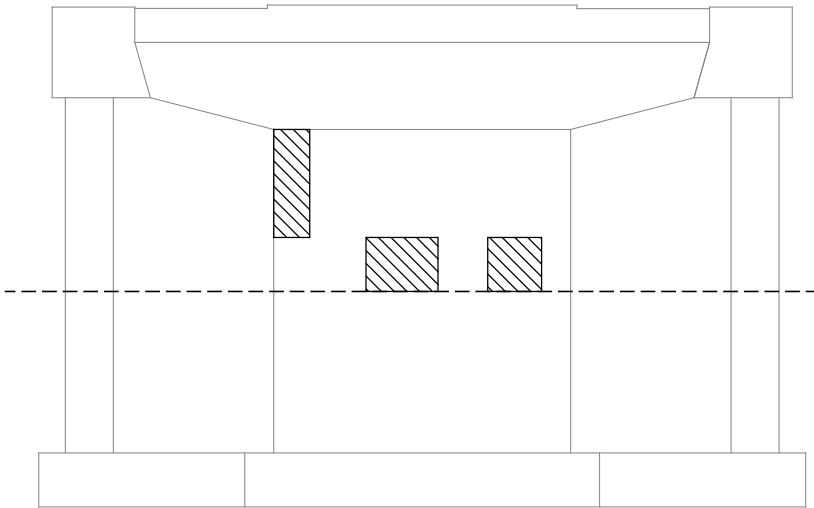


PLAN

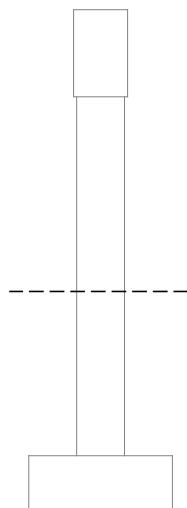
ELEVATION A



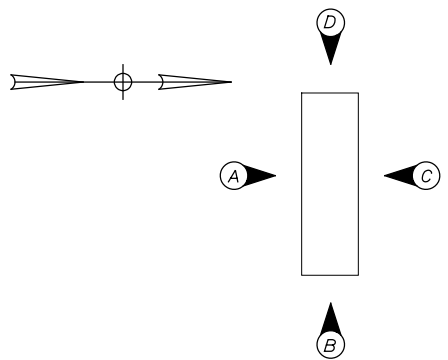
ELEVATION B



ELEVATION C



ELEVATION D



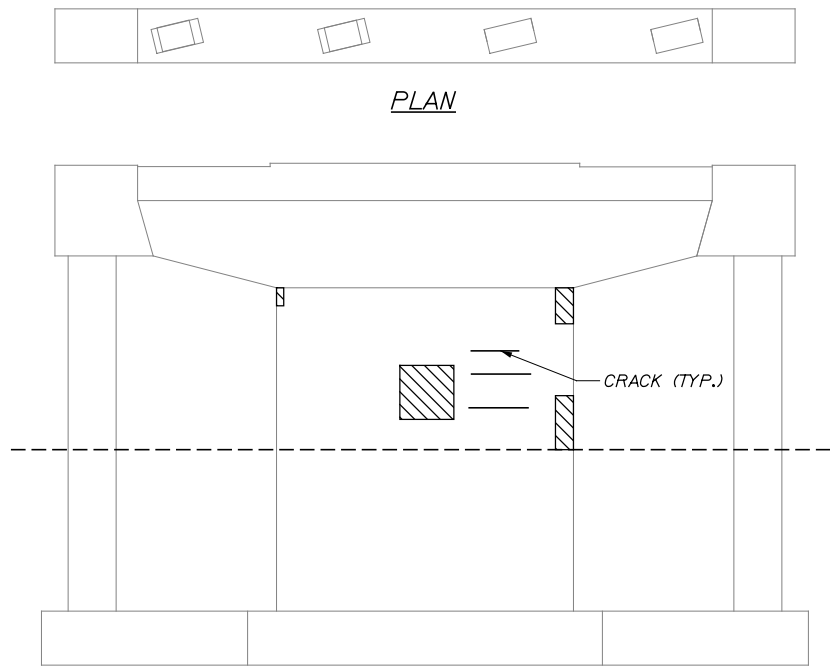
PIER ELEVATIONS

NOTES:

1. FOR CONCRETE REPAIR DETAIL SEE SHEET "FALMOUTH RD. UNDERPASS - MM. F1.7 SUBSTRUCTURE REPAIR - 1 OF 3"
2. FOR PROCEDURAL NOTES SEE SHEET "GENERAL NOTES"

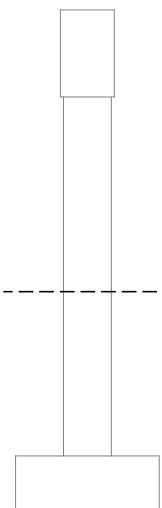
LEGEND:

- LIMIT OF CONCRETE SURFACE REPAIRS
- LIMIT OF EPOXY INJECTED CRACK REPAIR
- EXISTING CONCRETE
- EXISTING AND PROPOSED GROUND

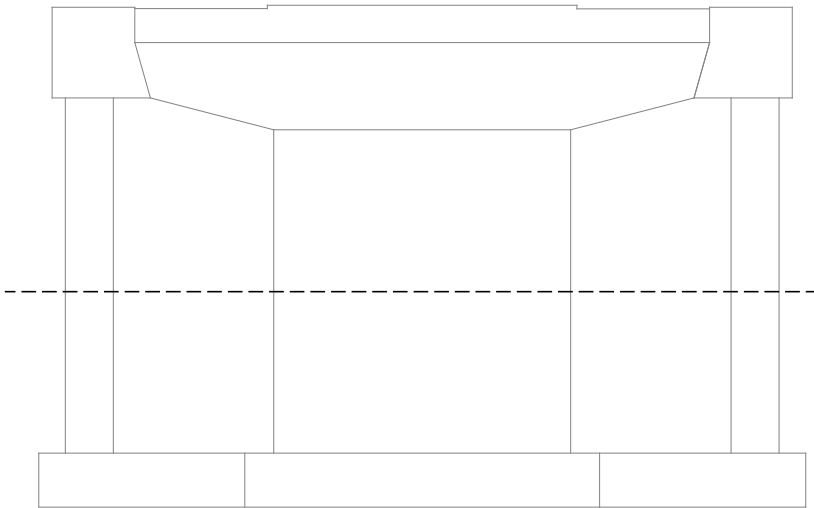


PLAN

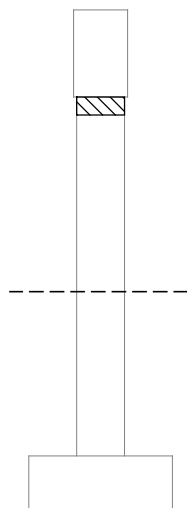
ELEVATION A



ELEVATION B



ELEVATION C



ELEVATION D

PIER 3 (NORTH PIER)

Scale:				
NOT TO SCALE				
No.	Revision	By	Date	

Designed by:					
TYLIN INTERNATIONAL					
CONSULTANT PROJECT MANAGER: Norman L. Baker					
	By	Date		By	Date
Designed	DSM	3/14	Checked	SBK	3/14
Drawn	JD	3/14	In Charge of	NLB	3/14

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Falmouth, Maine 04105
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FAX: (207) 781-4753



THE GOLD STAR
MEMORIAL HIGHWAY

MTA PROJECT MANAGER: Ralph C. Norwood IV

BRIDGE AND CULVERT REPAIRS
4 LOCATIONS (MILE F1.7 TO MILE 84.3)
FALMOUTH RD. UNDERPASS - MILE F1.7
SUBSTRUCTURE REPAIR - 3 OF 3

CONTRACT: 2014.15
SHEET NUMBER: S-08
23 OF 52

Date: 3/24/2014





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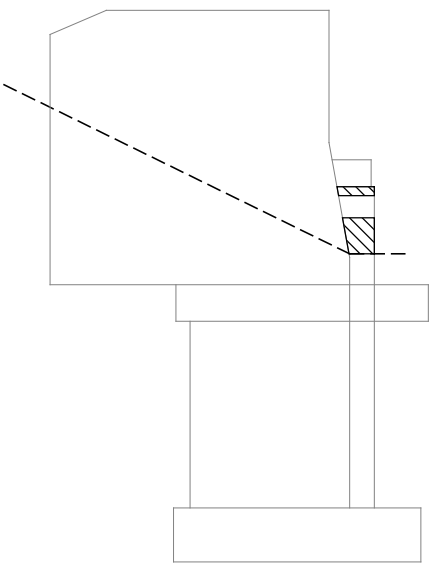
CENTER RD. UNDERPASS - MM 62.9 ABUTMENTS ESTIMATED QUANTITY OF REPAIRS			
	ABUTMENT 1	ABUTMENT 2	TOTAL
ABUTMENT REPAIRS	85 S.F.	40 S.F.	125 S.F.
EPOXY INJECTED CRACK REPAIRS	0 L.F.	5 L.F.	5 L.F.

NOTES:

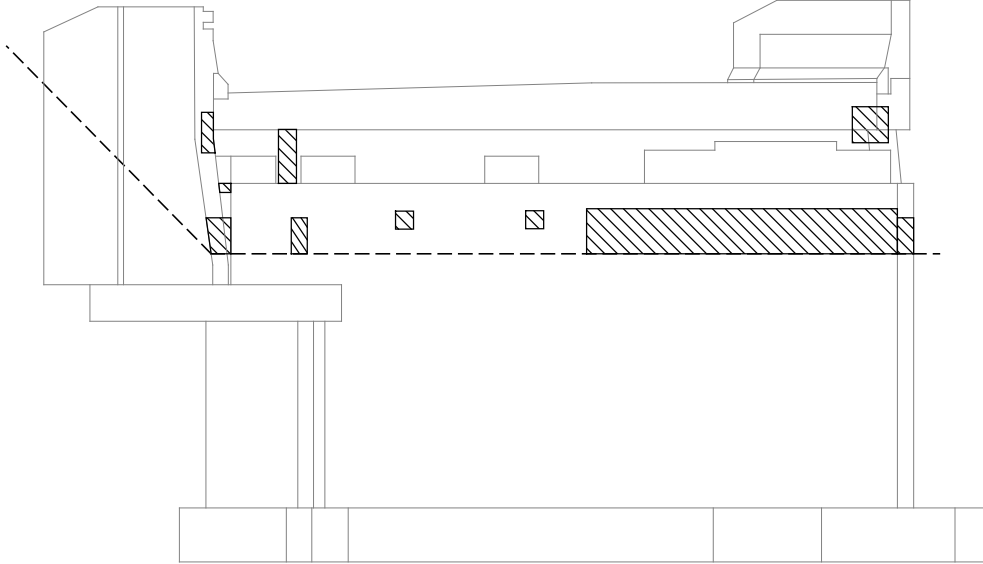
- FOR CONCRETE REPAIR DETAIL SEE SHEET "FALMOUTH RD. UNDERPASS - MM F1.7 SUBSTRUCTURE REPAIR - 1 OF 3"
- FOR PROCEDURAL NOTES SEE SHEET "GENERAL NOTES"

LEGEND:

-  LIMIT OF CONCRETE SURFACE REPAIRS
-  LIMIT OF EPOXY INJECTED CRACK REPAIR
-  EXISTING CONCRETE
-  EXISTING AND PROPOSED GROUND



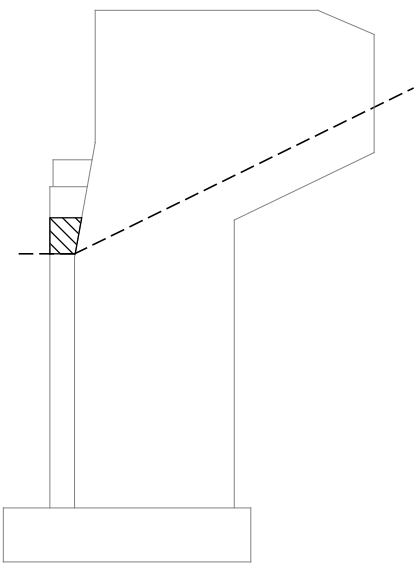
WINGWALL



ABUTMENT 1 ELEVATION (SOUTHWEST END)



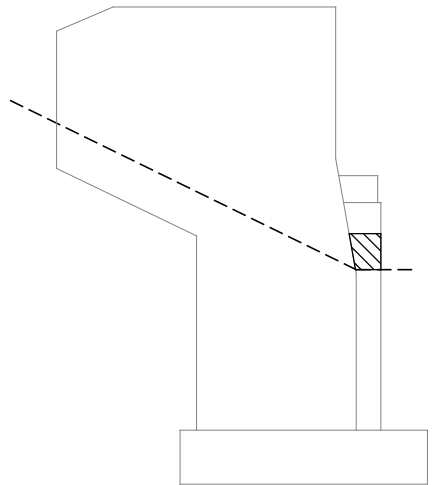
ABUTMENT 1 SEAT PLAN (SOUTHWEST END)



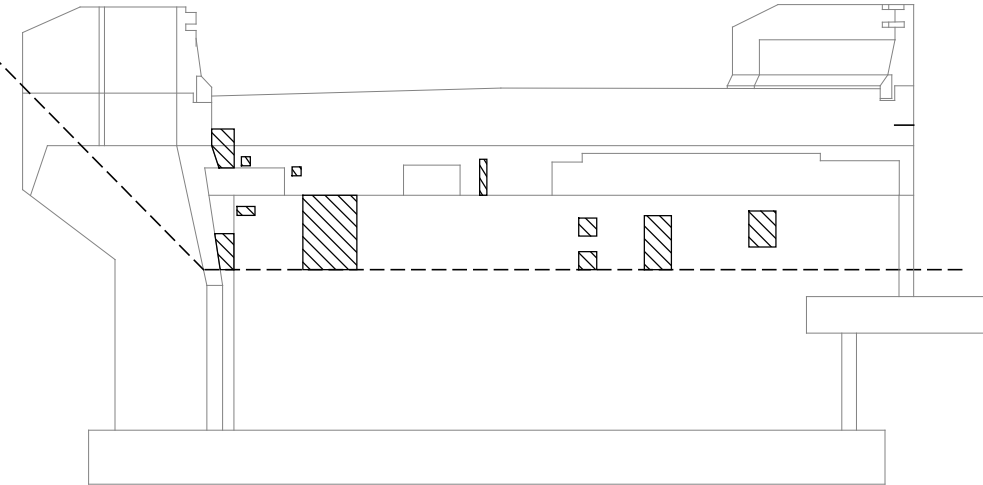
WINGWALL



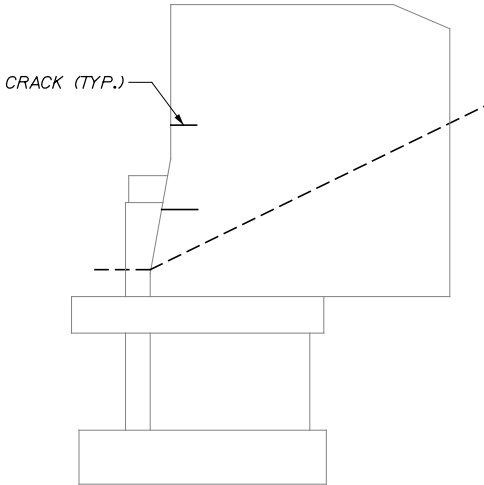
ABUTMENT 2 SEAT PLAN (NORTHEAST END)



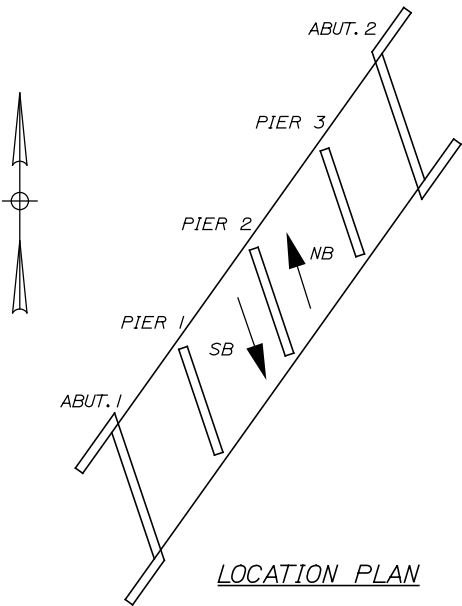
WINGWALL



ABUTMENT 2 ELEVATION (NORTHEAST END)



WINGWALL



Scale:			
NOT TO SCALE			
No.	Revision	By	Date

Designed by:					
TYLIN INTERNATIONAL					
CONSULTANT PROJECT MANAGER: Norman L. Baker					
	By	Date		By	Date
Designed	DSM	3/14	Checked	SBK	3/14
Drawn	JD	3/14	In Charge of	NLB	3/14

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FAX: (207) 781-4753



THE GOLD STAR
MEMORIAL HIGHWAY

MTA PROJECT MANAGER: Ralph C. Norwood IV

BRIDGE AND CULVERT REPAIRS
4 LOCATIONS (MILE F1.7 TO MILE 84.3)
CENTER RD. UNDERPASS - MILE 62.9
SUBSTRUCTURE REPAIR - 1 OF 3

CONTRACT: 2014.15

SHEET NUMBER: S-09

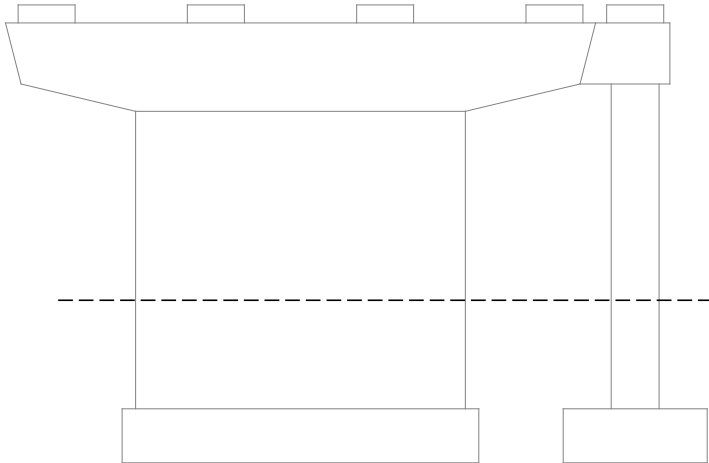
24 OF 52

Date: 3/24/2014

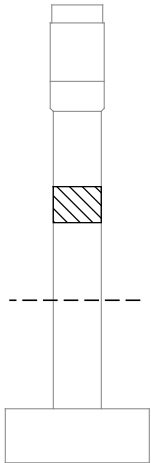
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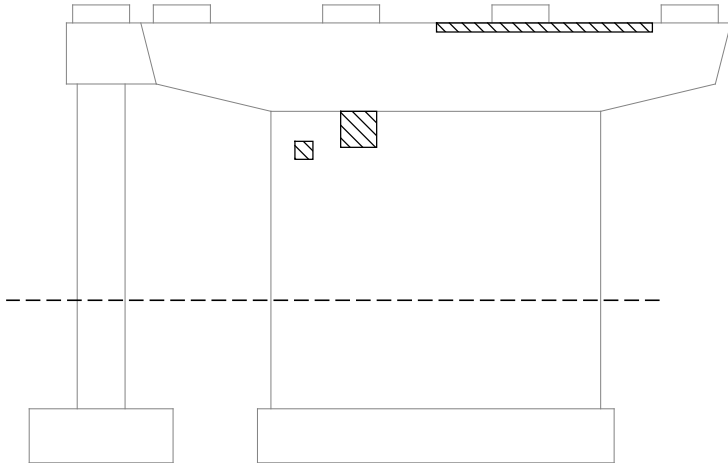
PLAN



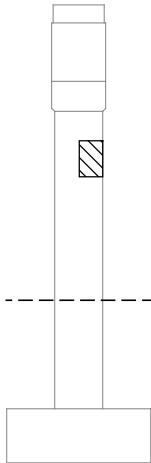
ELEVATION A



ELEVATION B

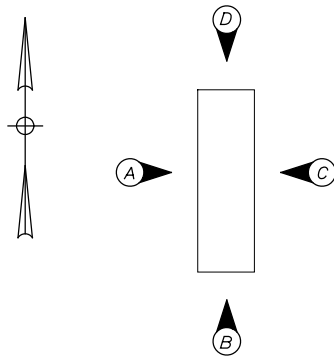


ELEVATION C



ELEVATION D

PIER 1 (SOUTHWEST PIER)



PIER ELEVATIONS

CENTER RD. UNDERPASS - MM 62.9 PIERS ESTIMATED QUANTITY OF REPAIRS				
	PIER 1	PIER 2	PIER 3	TOTAL
PIER REPAIRS	35 S.F.	25 S.F.	35 S.F.	95 S.F.
EPOXY INJECTED CRACK REPAIRS	0 L.F.	25 L.F.	25 L.F.	50 L.F.

NOTES:

- FOR CONCRETE REPAIR DETAIL SEE SHEET "FALMOUTH RD. UNDERPASS - MM F1.7 SUBSTRUCTURE REPAIR - 1 OF 3"
- FOR PROCEDURAL NOTES SEE SHEET "GENERAL NOTES"

LEGEND:

- LIMIT OF CONCRETE SURFACE REPAIRS
- LIMIT OF EPOXY INJECTED CRACK REPAIR
- EXISTING CONCRETE
- EXISTING AND PROPOSED GROUND

Scale:				
NOT TO SCALE				
No.	Revision	By	Date	

Designed by:					
CONSULTANT PROJECT MANAGER: Norman L. Baker					
	By	Date		By	Date
Designed	DSM	3/14	Checked	SBK	3/14
Drawn	JD	3/14	In Charge of	NLB	3/14

T.Y. Lin International
12 Northbrook Drive
Building A, Suite One
Falmouth, Maine 04105
TEL: (207) 781-4721
FAX: (207) 781-4753



THE GOLD STAR
MEMORIAL HIGHWAY

MTA PROJECT MANAGER: Ralph C. Norwood IV

BRIDGE AND CULVERT REPAIRS
4 LOCATIONS (MILE F1.7 TO MILE 84.3)
CENTER RD. UNDERPASS - MILE 62.9
SUBSTRUCTURE REPAIR - 2 OF 3

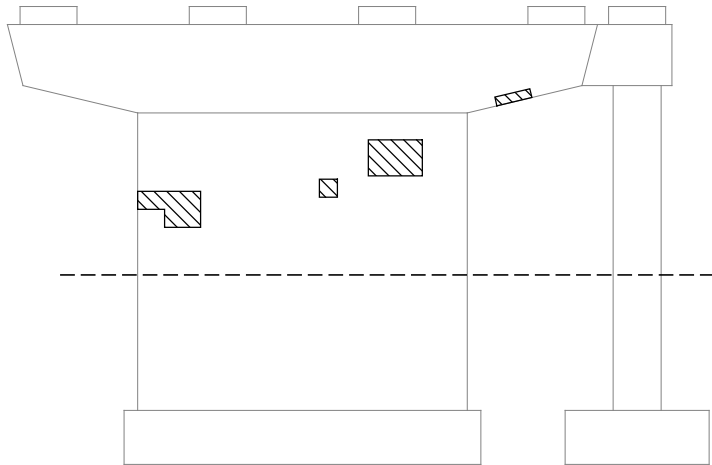
SHEET NUMBER: S-10
CONTRACT: 2014.15
25 OF 52

Date: 3/24/2014

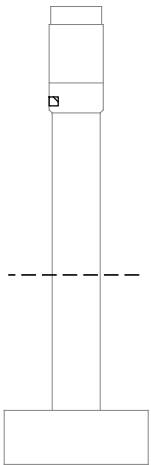
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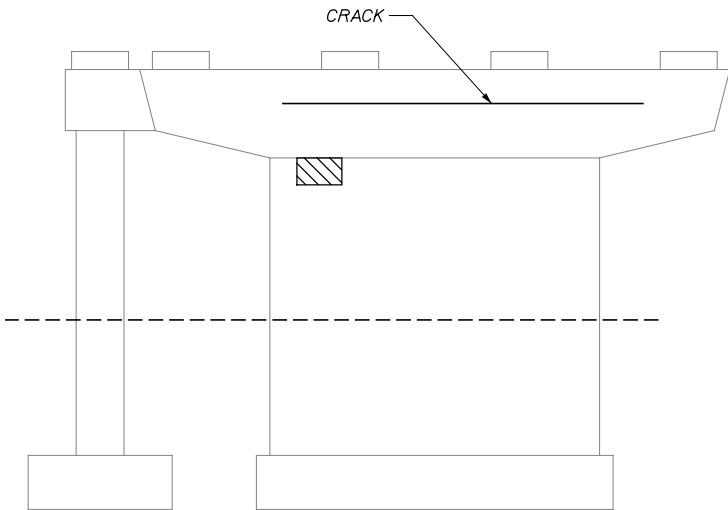
PLAN



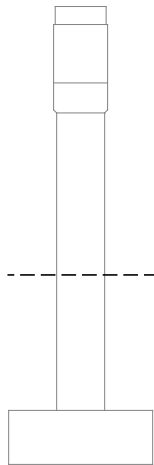
ELEVATION A



ELEVATION B



ELEVATION C

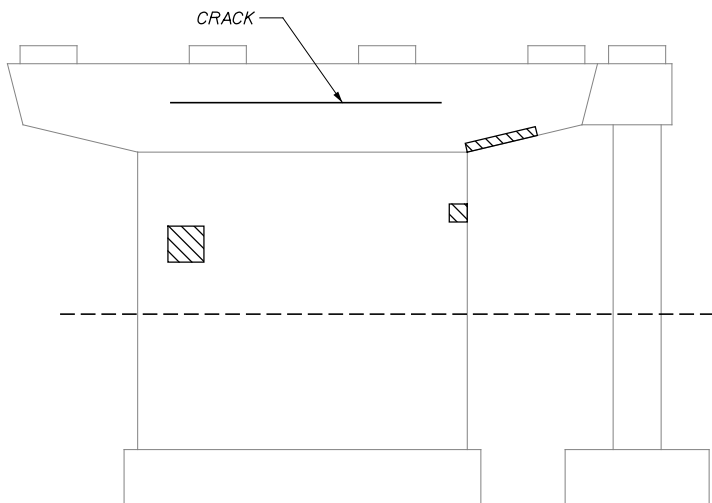


ELEVATION D

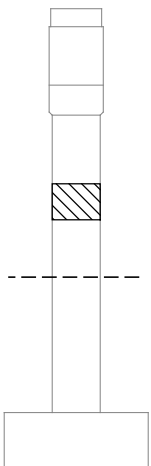
PIER 2 (CENTER PIER)



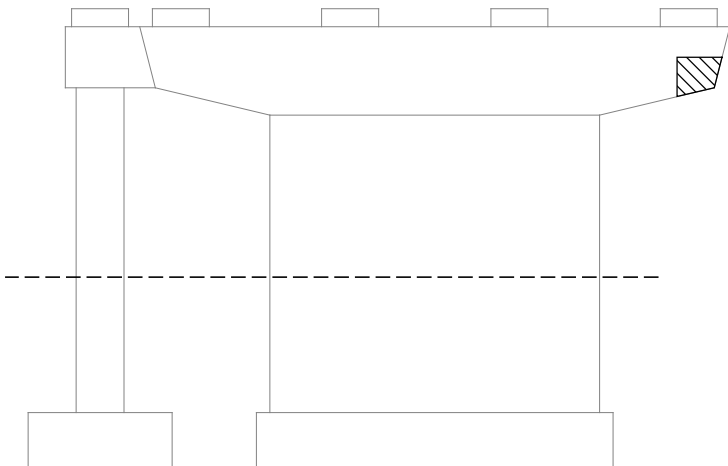
PLAN



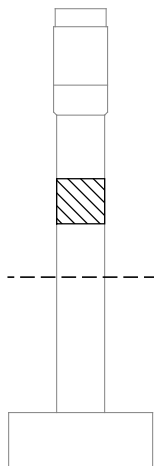
ELEVATION A



ELEVATION B

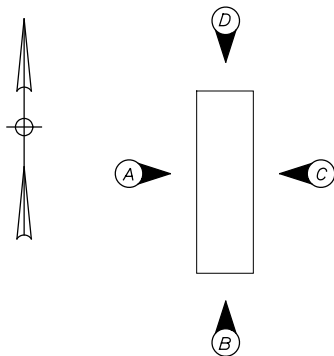


ELEVATION C



ELEVATION D

PIER 3 (NORTHEAST PIER)



PIER ELEVATIONS

NOTES:

- FOR CONCRETE REPAIR DETAIL SEE SHEET "FALMOUTH RD. UNDERPASS - MM. F1.7 SUBSTRUCTURE REPAIR - 1 OF 3"
- FOR PROCEDURAL NOTES SEE SHEET "GENERAL NOTES"

LEGEND:

- LIMIT OF CONCRETE SURFACE REPAIRS
- LIMIT OF EPOXY INJECTED CRACK REPAIR
- EXISTING CONCRETE
- EXISTING AND PROPOSED GROUND

Scale:				
NOT TO SCALE				
No.	Revision	By	Date	

Designed by:					
T.Y. LIN INTERNATIONAL					
CONSULTANT PROJECT MANAGER: Norman L. Baker					
	By	Date		By	Date
Designed	DSM	3/14	Checked	SBK	3/14
Drawn	JD	3/14	In Charge of	NLB	3/14

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Falmouth, Maine 04105
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THE GOLD STAR
MEMORIAL HIGHWAY

MTA PROJECT MANAGER: Ralph C. Norwood IV

BRIDGE AND CULVERT REPAIRS
4 LOCATIONS (MILE F1.7 TO MILE 84.3)
CENTER RD. UNDERPASS - MILE 62.9
SUBSTRUCTURE REPAIR - 3 OF 3

CONTRACT: 2014.15

SHEET NUMBER: S-11

26 OF 52

Date: 3/24/2014





Filename: ...MSTA\027_84.3_SUB_REPAIR_1.dgn

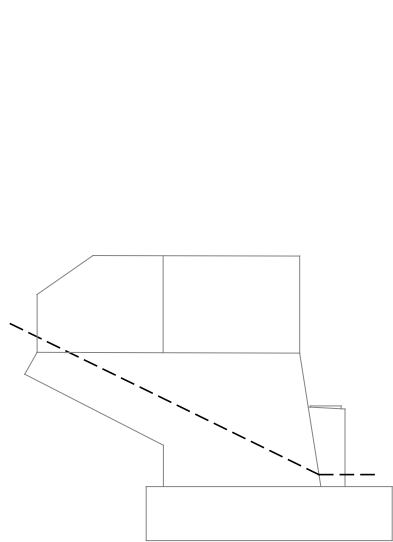
LISBON RD. UNDERPASS - MM 84.3 ABUTMENTS ESTIMATED QUANTITY OF REPAIRS			
	ABUTMENT 1	ABUTMENT 2	TOTAL
ABUTMENT REPAIRS	10 S.F.	5 S.F.	15 S.F.
EPOXY INJECTED CRACK REPAIRS	0 L.F.	0 L.F.	0 L.F.

NOTES:

- FOR CONCRETE REPAIR DETAIL SEE SHEET "FALMOUTH RD. UNDERPASS - MM F1.7 SUBSTRUCTURE REPAIR - 1 OF 3"
- FOR PROCEDURAL NOTES SEE SHEET "GENERAL NOTES"

LEGEND:

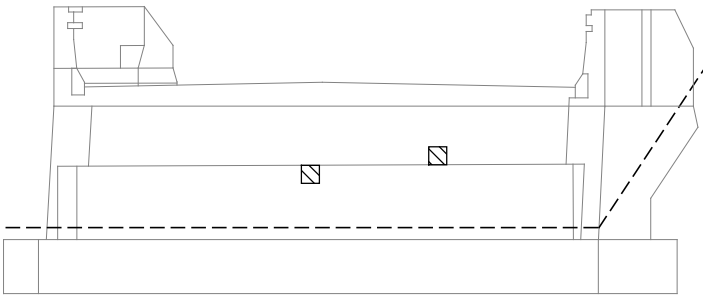
-  LIMIT OF CONCRETE SURFACE REPAIRS
-  LIMIT OF EPOXY INJECTED CRACK REPAIR
-  EXISTING CONCRETE
-  EXISTING AND PROPOSED GROUND



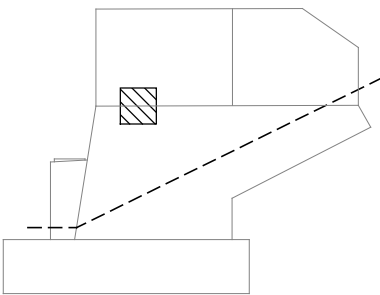
WINGWALL



ABUTMENT 1 SEAT PLAN (NORTHWEST END)



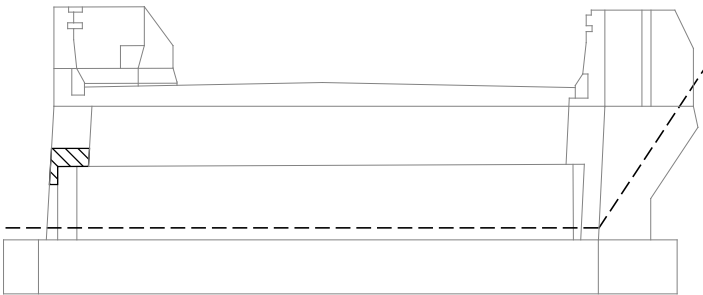
ABUTMENT 1 ELEVATION (NORTHWEST END)



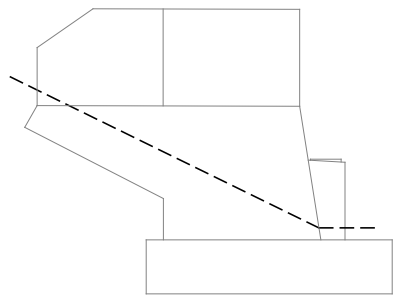
WINGWALL



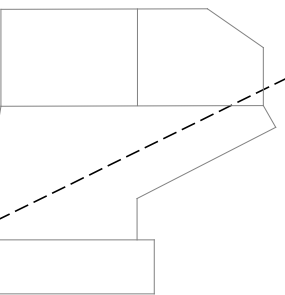
ABUTMENT 2 SEAT PLAN (SOUTHEAST END)



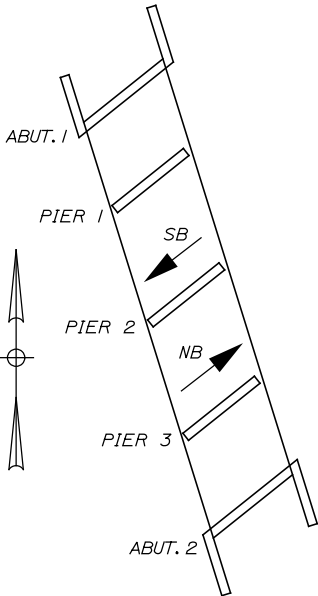
ABUTMENT 2 ELEVATION (SOUTHEAST END)



WINGWALL



WINGWALL



LOCATION PLAN

Scale:			
NOT TO SCALE			
No.	Revision	By	Date

Designed by:					
<div>T.Y. LIN INTERNATIONAL</div>					
CONSULTANT PROJECT MANAGER: Norman L. Baker					
	By	Date		By	Date
Designed	DSM	3/14	Checked	SBK	3/14
Drawn	JD	3/14	In Charge of	NLB	3/14

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**THE GOLD STAR
MEMORIAL HIGHWAY**

MTA PROJECT MANAGER: Ralph C. Norwood IV

BRIDGE AND CULVERT REPAIRS
 4 LOCATIONS (MILE F1.7 TO MILE 84.3)
 LISBON RD. UNDERPASS - MILE 84.3
 SUBSTRUCTURE REPAIR - 1 OF 3

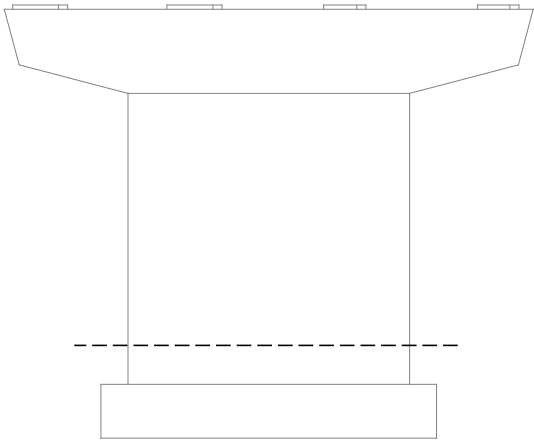
SHEET NUMBER: S-12
 CONTRACT: 2014.15
 27 OF 52

Date: 3/24/2014

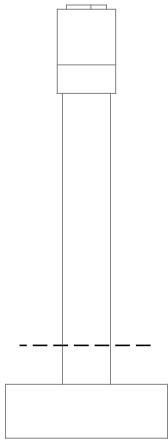
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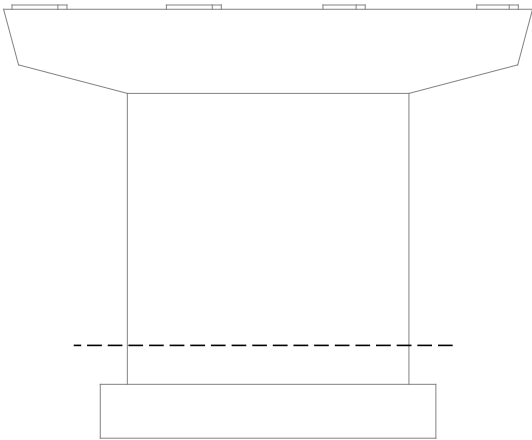
PLAN



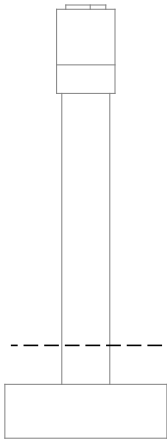
ELEVATION A



ELEVATION B

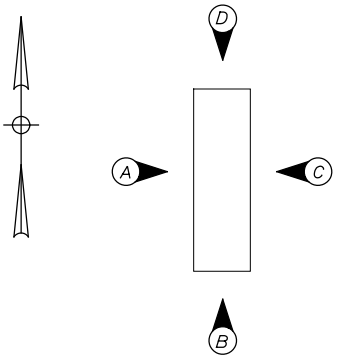


ELEVATION C



ELEVATION D

PIER 1 (NORTHWEST PIER)



PIER ELEVATIONS

LISBON RD. UNDERPASS - MM 84.3 PIERS ESTIMATED QUANTITY OF REPAIRS				
	PIER 1	PIER 2	PIER 3	TOTAL
PIER REPAIRS	0 S.F.	0 S.F.	40 S.F.	40 S.F.
EPOXY INJECTED CRACK REPAIRS	0 L.F.	0 L.F.	0 L.F.	0 L.F.

NOTES:

- FOR CONCRETE REPAIR DETAIL SEE SHEET "FALMOUTH RD. UNDERPASS - MM F1.7 SUBSTRUCTURE REPAIR - 1 OF 3"
- FOR PROCEDURAL NOTES SEE SHEET "GENERAL NOTES"

LEGEND:

- LIMIT OF CONCRETE SURFACE REPAIRS
- LIMIT OF EPOXY INJECTED CRACK REPAIR
- EXISTING CONCRETE
- EXISTING AND PROPOSED GROUND

Scale:			
NOT TO SCALE			
No.	Revision	By	Date

Designed by:					
TY·LIN INTERNATIONAL					
CONSULTANT PROJECT MANAGER: Norman L. Baker					
	By	Date		By	Date
Designed	DSM	3/14	Checked	SBK	3/14
Drawn	JD	3/14	In Charge of	NLB	3/14

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THE GOLD STAR
MEMORIAL HIGHWAY

MTA PROJECT MANAGER: Ralph C. Norwood IV

BRIDGE AND CULVERT REPAIRS
4 LOCATIONS (MILE F1.7 TO MILE 84.3)
LISBON RD. UNDERPASS - MILE 84.3
SUBSTRUCTURE REPAIR - 2 OF 3

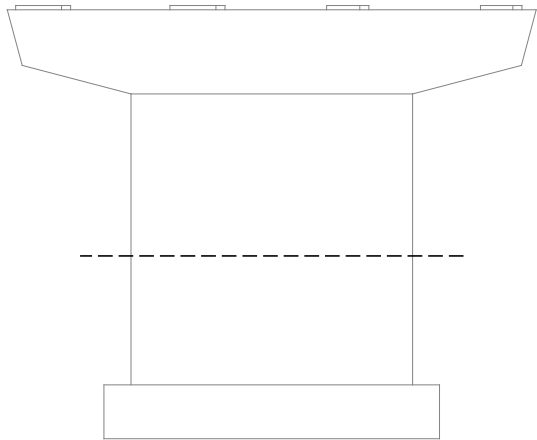
SHEET NUMBER: S-13
CONTRACT: 2014.15
28 OF 52

Date: 3/24/2014

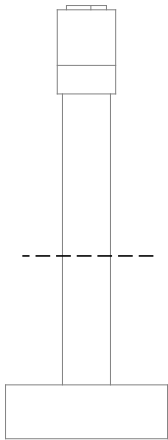
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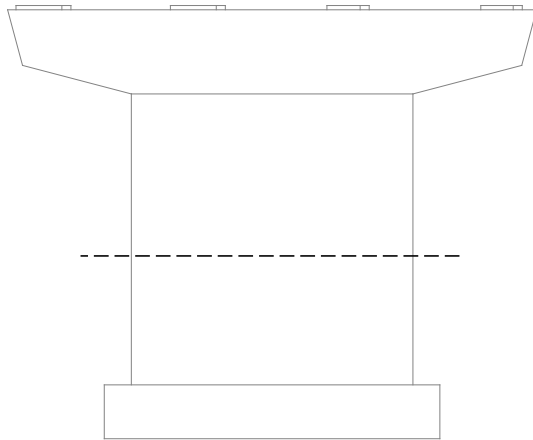
PLAN



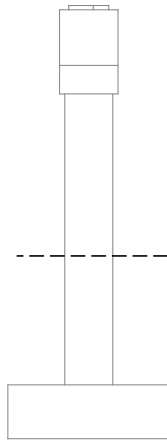
ELEVATION A



ELEVATION B



ELEVATION C

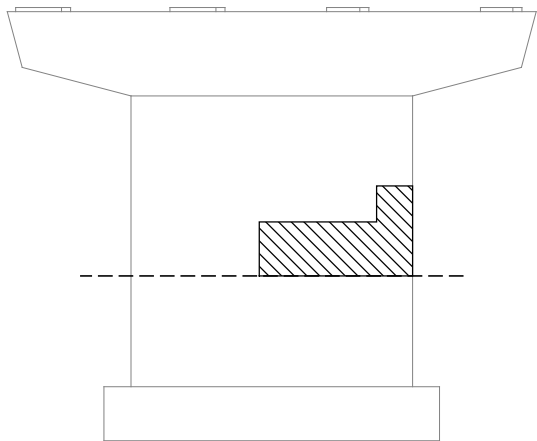


ELEVATION D

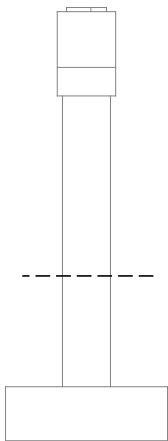
PIER 2 (CENTER PIER)



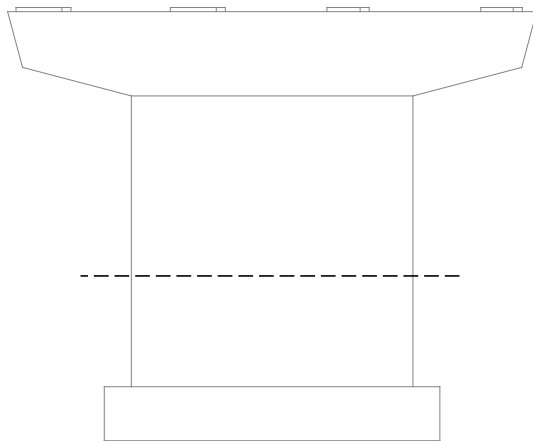
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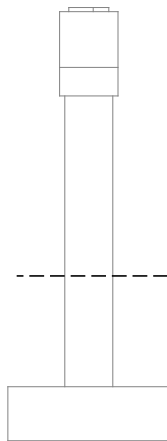
ELEVATION A



ELEVATION B

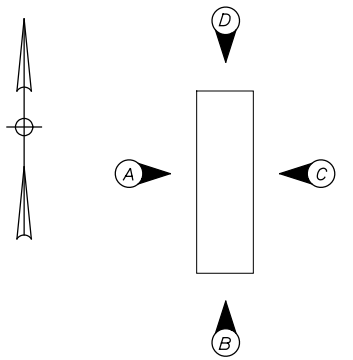


ELEVATION C



ELEVATION D

PIER 3 (SOUTHEAST PIER)



PIER ELEVATIONS

NOTES:

- FOR CONCRETE REPAIR DETAIL SEE SHEET "FALMOUTH RD. UNDERPASS - MM. F1.7 SUBSTRUCTURE REPAIR - 1 OF 3"
- FOR PROCEDURAL NOTES SEE SHEET "GENERAL NOTES"

LEGEND:

- LIMIT OF CONCRETE SURFACE REPAIRS
- LIMIT OF EPOXY INJECTED CRACK REPAIR
- EXISTING CONCRETE
- EXISTING AND PROPOSED GROUND

Scale:				
NOT TO SCALE				
No.	Revision	By	Date	

Designed by:					
TYLIN INTERNATIONAL					
CONSULTANT PROJECT MANAGER: Norman L. Baker					
	By	Date		By	Date
Designed	DSM	3/14	Checked	SBK	3/14
Drawn	JD	3/14	In Charge of	NLB	3/14

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THE GOLD STAR
MEMORIAL HIGHWAY

MTA PROJECT MANAGER: Ralph C. Norwood IV

BRIDGE AND CULVERT REPAIRS
4 LOCATIONS (MILE F1.7 TO MILE 84.3)
LISBON RD. UNDERPASS - MILE 84.3
SUBSTRUCTURE REPAIR - 3 OF 3

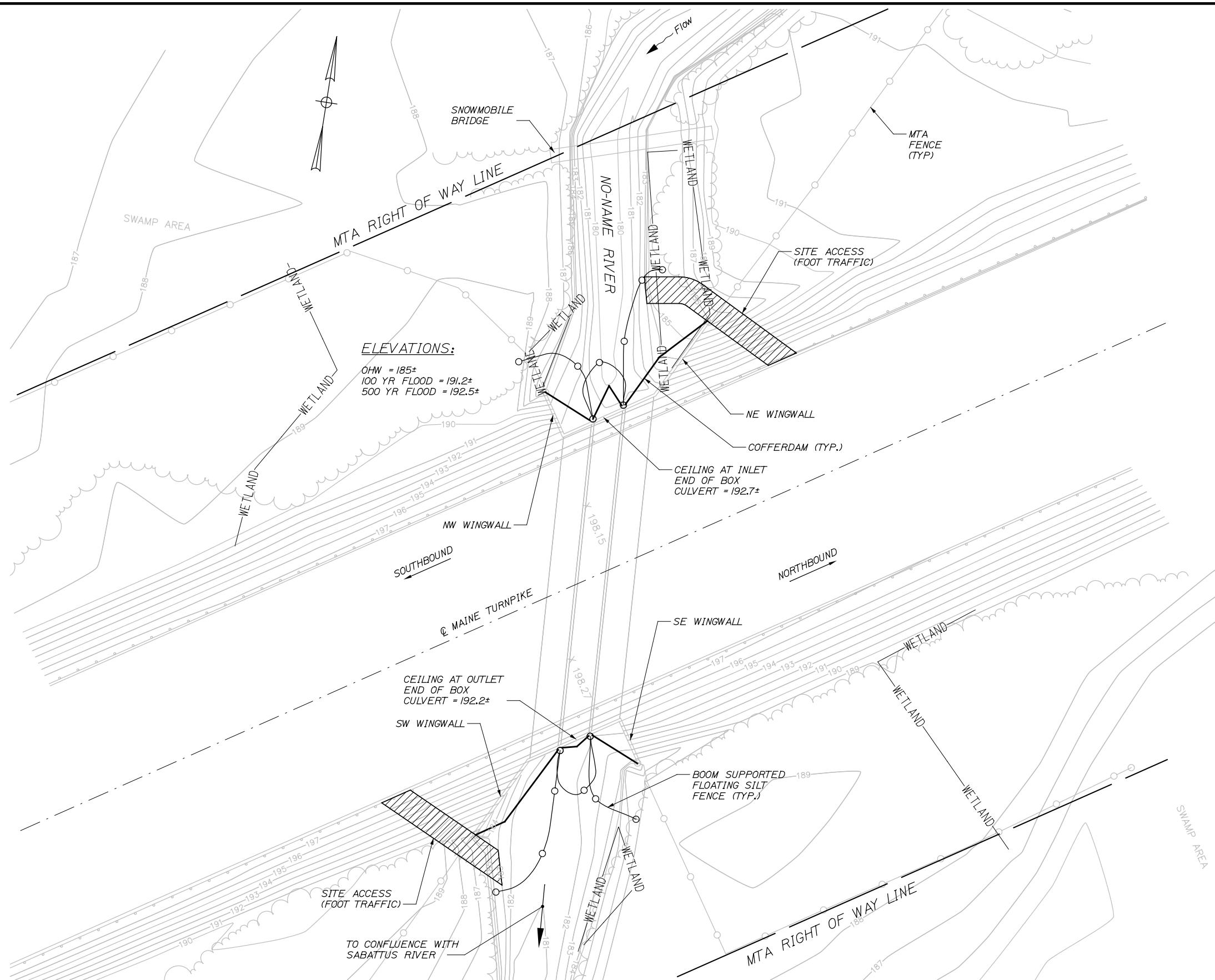
CONTRACT: 2014.15

SHEET NUMBER: S-14

29 OF 52

Date: 3/24/2014

Filename: ... \BRIDGE\MSTA\030_82.5_Plan.dgn



- NOTES:**
- 1. EROSION AND SEDIMENT CONTROL DEVICES REQUIRED FOR THE CONTRACTOR'S ACCESS LOCATIONS AND STORAGE AREAS WILL NOT BE MEASURED FOR PAYMENT.
 - 2. SITE ACCESS IS SOLELY FOR FOOT TRAFFIC AND WILL BE STABILIZED AT THE COMPLETION OF THE PROJECT. COST INCIDENTAL TO THE CONTRACT.
 - 3. CONTRACT LIMIT OF DISTURBANCE FOR NO-NAME RIVER CULVERT IS 0.02 ACRES. CONTRACT LIMIT OF DISTURBANCE AREA FOR ACCESS AND STORAGE FOR NO-NAME RIVER CULVERT IS 0.02 ACRES.
 - 4. SEE SPECIFICATIONS FOR CONTRACTOR'S SUBMITTAL REQUIREMENTS IF INCREASING THE LIMITS OF DISTURBANCE ABOVE THE PROJECT ESTIMATED QUANTITIES.
 - 5. WETLANDS SHOWN ARE CONSIDERED TO BE "WETLANDS OF SPECIAL SIGNIFICANCE".
 - 6. CLOSING OF THE BARRELS SHALL BE DONE IN STAGES TO ALLOW FOR FISH PASSAGE. AT NO TIME WILL TWO BARRELS BE CLOSED AT THE SAME TIME.

Scale: 10 0 10 20				
No.	Revision	By	Date	

Designed by: TYLIN INTERNATIONAL					
CONSULTANT PROJECT MANAGER: Norman L. Baker					
	By	Date		By	Date
Designed	DSM	3/14	Checked	SBK	3/14
Drawn	JD	3/14	In Charge of	NLB	3/14

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**THE GOLD STAR
MEMORIAL HIGHWAY**

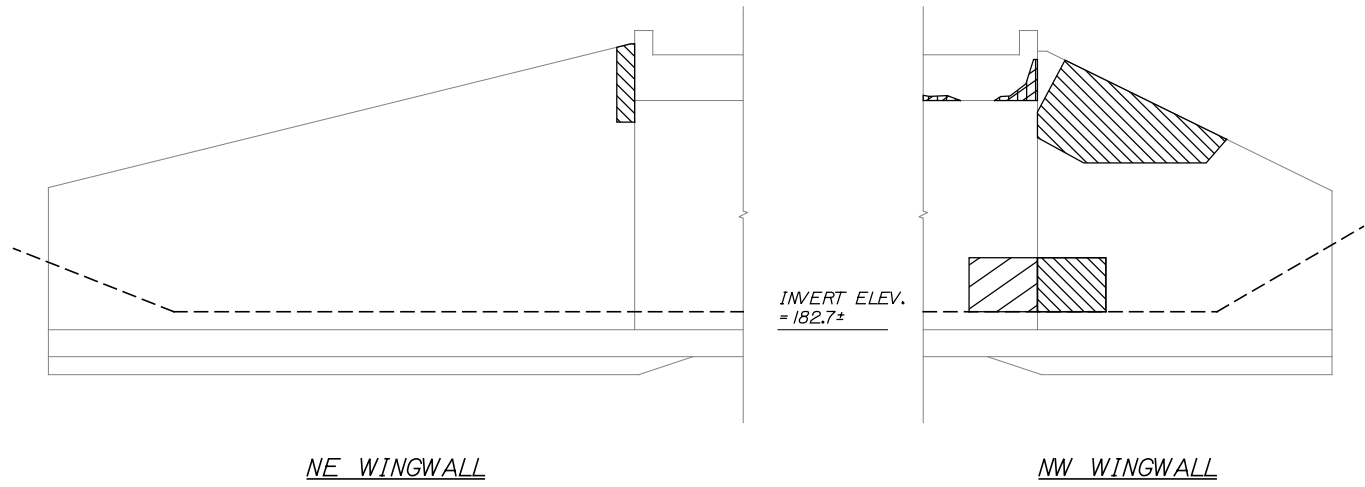
MTA PROJECT MANAGER: Ralph C. Norwood IV

**BRIDGE AND CULVERT REPAIRS
4 LOCATIONS (MILE F1.7 TO MILE 84.3)
NO-NAME RIVER CULVERT - MILE 82.5
GENERAL SITE PLAN**

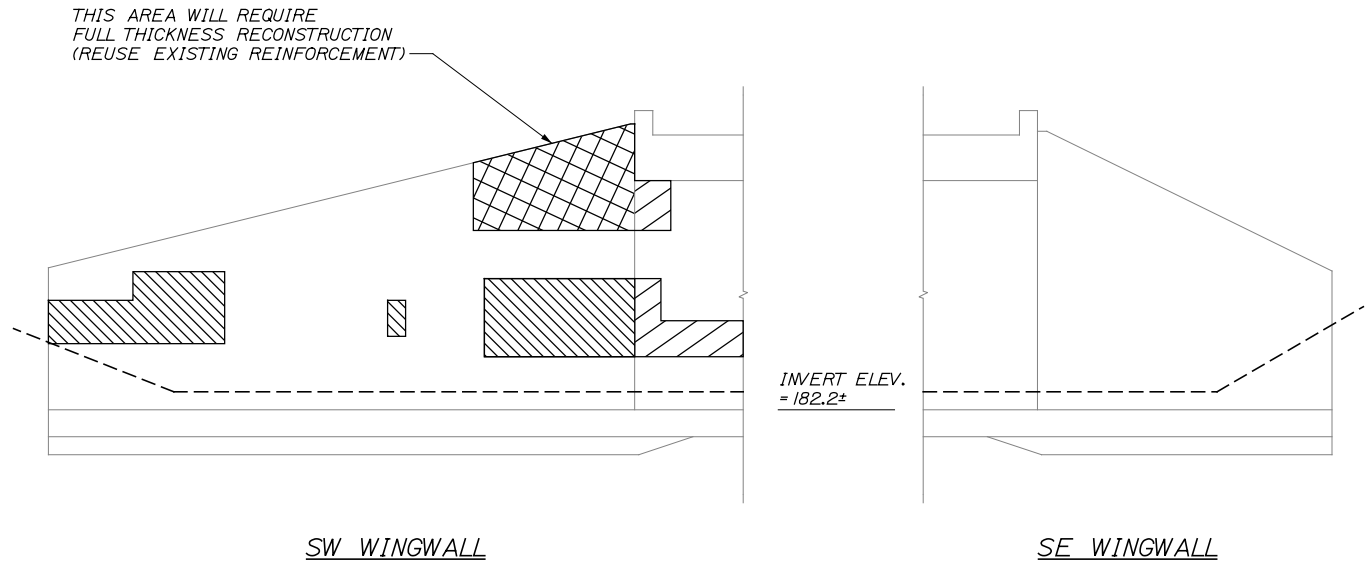
SHEET NUMBER: S-15
CONTRACT: 2014.15
30 OF 52

Date: 3/24/2014

Filename: ...MSTA\031_82.5_Sub_Repair_1.dgn



WINGWALL ELEVATION (NORTH END)



WINGWALL ELEVATION (SOUTH END)

NO-NAME RIVER - MM 82.5 WINGWALLS ESTIMATED QUANTITY OF REPAIRS					
	NE WING	NW WING	SW WING	SE WING	TOTAL
ABUTMENT REPAIRS	10 S.F.	60 S.F.	100 S.F.	0 S.F.	170 S.F.
REPAIR OF VERTICAL SURFACES > 7.9 INCHES	0 C.Y.	0 C.Y.	3 C.Y.	0 C.Y.	3 C.Y.

NO-NAME RIVER - MM 82.5 CULVERT BARRELS ESTIMATED QUANTITY OF REPAIRS				
	W. BARREL	MID. BARREL	E. BARREL	TOTAL
CULVERT SURFACE PATCH/REPAIR	65 S.F.	70 S.F.	65 S.F.	200 S.F.
FASCIA AND OVERHANG REPAIRS	15 S.F.	15 S.F.	15 S.F.	45 S.F.
CULVERT WALL CRACK REPAIRS	10 L.F.	10 L.F.	10 L.F.	30 L.F.

NOTES:

- FOR CONCRETE REPAIR DETAIL SEE SHEET "MM F1.7 - SUBSTRUCTURE REPAIR - 1 OF 3"
- FOR PROCEDURAL NOTES SEE SHEET "GENERAL NOTES"
- CULVERT BARRELS ARE NOT DEPICTED. ESTIMATED REPAIR QUANTITIES ARE LISTED IN THE TABLE ABOVE.
- ALL OVERHEAD CONCRETE REPAIRS INSIDE THE BARRELS SHALL BE PAID FOR UNDER ITEM "FASCIA AND OVERHANG REPAIRS".
- ALL CULVERT WALL REPAIRS INSIDE THE BARRELS SHALL BE PAID FOR UNDER ITEM "CULVERT SURFACE PATCH/REPAIR"
- WINGWALL REPAIRS LESS THAN 7.9 INCHES IN DEPTH SHALL BE PAID FOR UNDER ITEM "ABUTMENT REPAIRS"

LEGEND:

- LIMIT OF WINGWALL FULL THICKNESS RECONSTRUCTION (ITEM "REPAIR OF VERTICAL SURFACES > 7.9 INCHES")
- LIMIT OF WINGWALL CONCRETE SURFACE PATCH/REPAIR (ITEM "ABUTMENT REPAIRS")
- LIMIT OF CULVERT BARREL CONCRETE SURFACE PATCH/REPAIR (ITEM "CULVERT SURFACE PATCH REPAIR")
- LIMIT OF CULVERT BARREL FASCIA PATCH/REPAIR (ITEM "FASCIA AND OVERHANG REPAIRS")
- LIMIT OF CULVERT WALL CRACK REPAIRS
- EXISTING CONCRETE
- EXISTING AND PROPOSED GROUND

Scale:			
NOT TO SCALE			
No.	Revision	By	Date

Designed by:					
TYLIN INTERNATIONAL					
CONSULTANT PROJECT MANAGER: Norman L. Baker					
	By	Date		By	Date
Designed	DSM	3/14	Checked	SBK	3/14
Drawn	JD	3/14	In Charge of	NLB	3/14

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THE GOLD STAR
MEMORIAL HIGHWAY

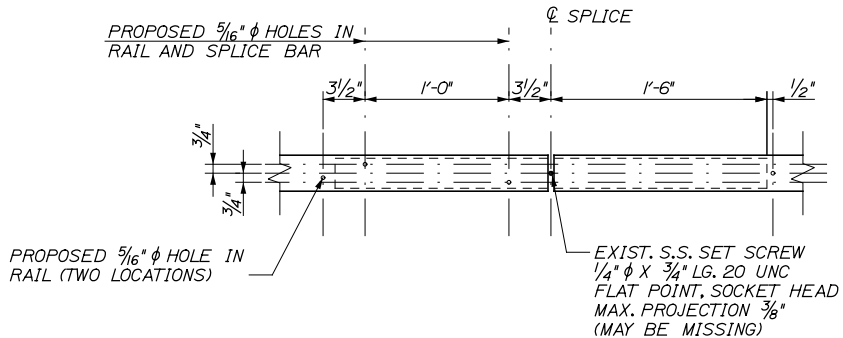
MTA PROJECT MANAGER: Ralph C. Norwood IV

BRIDGE AND CULVERT REPAIRS
4 LOCATIONS (MILE F1.7 TO MILE 84.3)
NO-NAME RIVER CULVERT - MILE 82.5
WINGWALL REPAIRS

SHEET NUMBER: S-16
CONTRACT: 2014.15
31 OF 52

Date: 3/24/2014

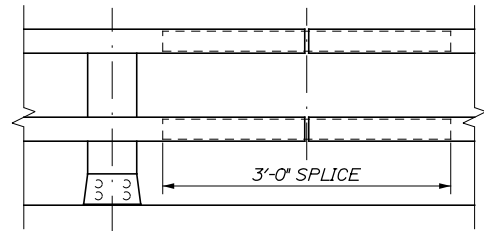
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SPLICE MODIFICATION DETAIL
(ELEVATION VIEW FROM BACK SIDE OR RAIL)
1 1/2" = 1'-0"

SPLICE NOTES:

1. RE-CENTER EXISTING SPLICE BAR AND DRILL TWO 5/16" ϕ HOLES AND INSTALL TWO 3/8" ϕ X 1", TYPE F, HEX, WASHER HEAD TAPPING SCREWS (STAINLESS). DRILL TWO ADDITIONAL 5/16" ϕ HOLES IN RAIL FOR SPLICE LOCATION VERIFICATION.
2. ANY REQUIRED REPLACEMENT SPLICE BARS SHALL BE SUPPLIED BY THE AUTHORITY.



RAILING - ELEVATION
N.T.S.

Scale:			
AS NOTED			
No.	Revision	By	Date

Designed by:					
TY·LIN INTERNATIONAL					
CONSULTANT PROJECT MANAGER: Norman L. Baker					
	By	Date		By	Date
Designed	DSM	3/14	Checked	DSM	3/14
Drawn	JD	3/14	In Charge of	NLB	3/14

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**THE GOLD STAR
MEMORIAL HIGHWAY**

MTA PROJECT MANAGER: Ralph C. Norwood IV

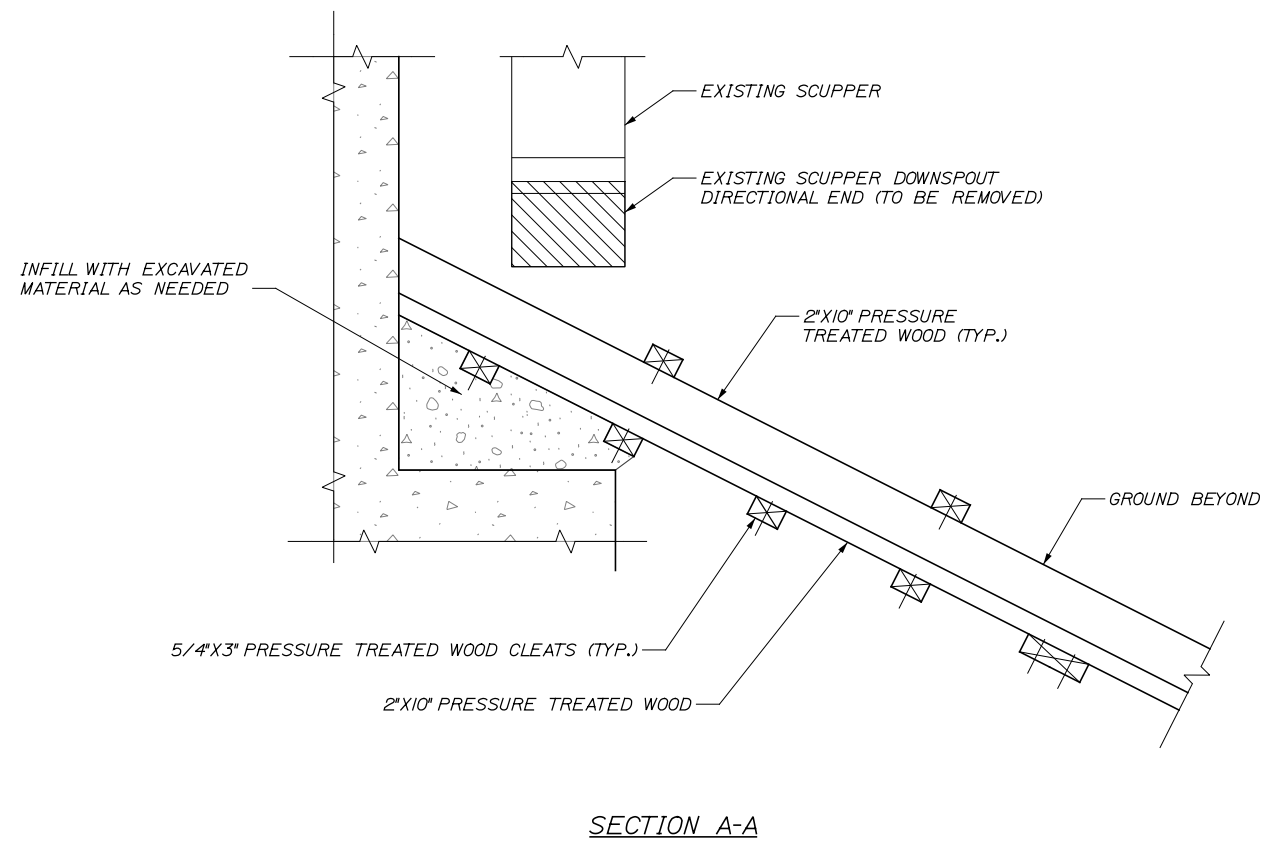
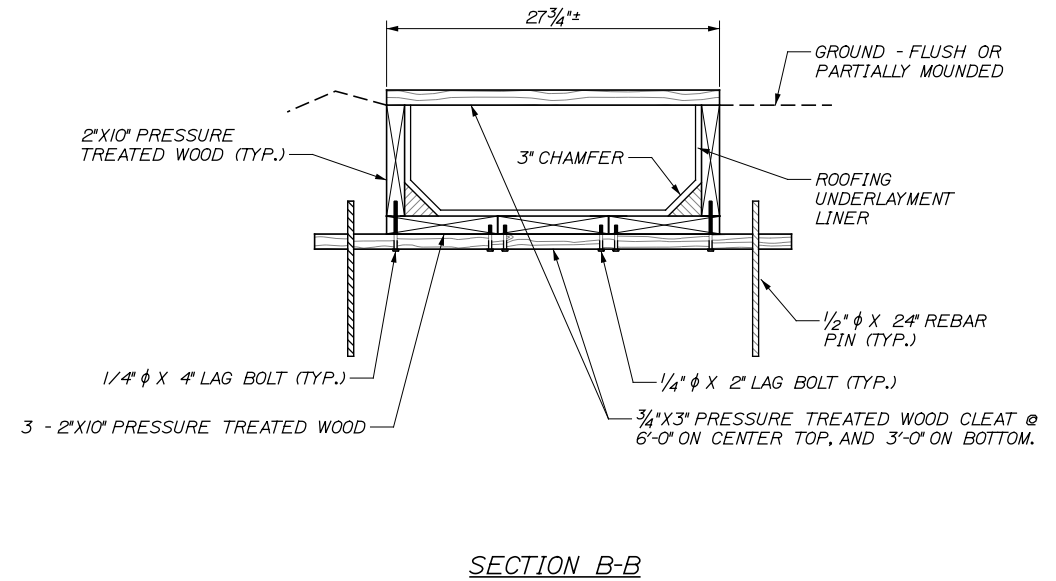
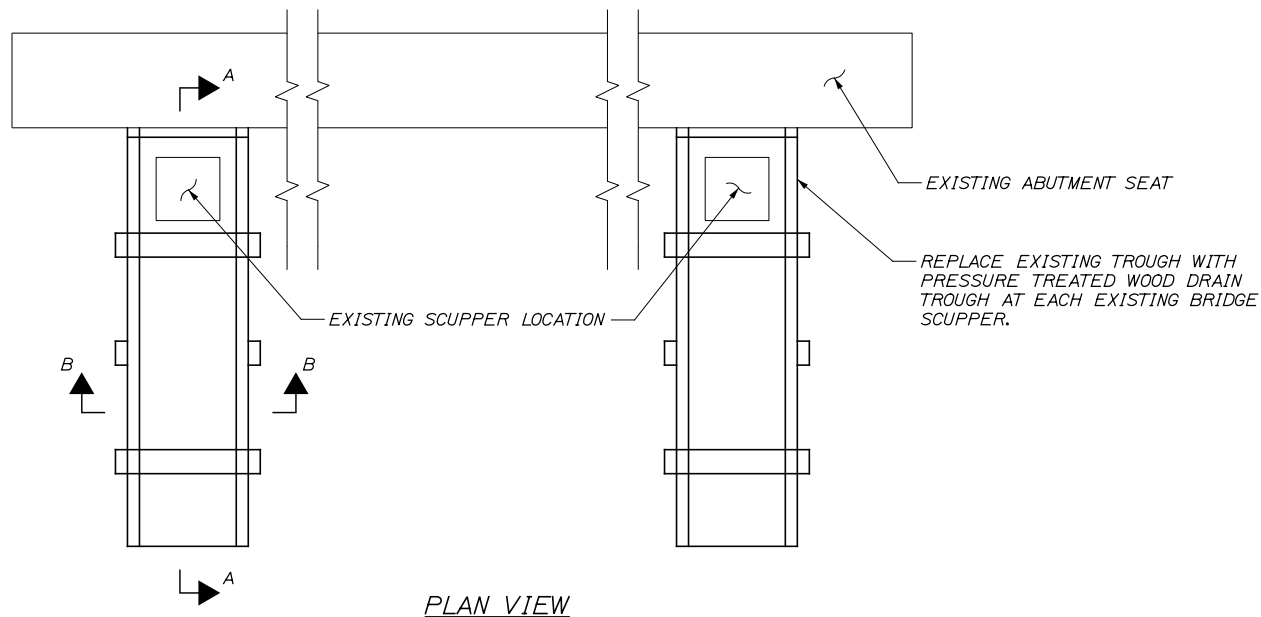
**BRIDGE AND CULVERT REPAIRS
4 LOCATIONS (MILE F1.7 TO MILE 84.3)**

MISCELLANEOUS DETAILS

SHEET NUMBER: S-17
CONTRACT: 2014.15
32 OF 52

Date: 3/24/2014

Filename: ...MSTA\033_Trough_Details.dgn



NOTES:

1. WOOD DRAIN TROUGH AND ROOFING UNDERLAYMENT LINER SHALL BE PAID UNDER ITEM 603.91, PRESSURE TREATED WOOD DRAIN TROUGH.
2. REMOVAL OF EXISTING TROUGHS AND SCUPPER DOWNSPOUT ENDS, EXCAVATION, AND BACKFILL TO INSTALL PRESSURE TREATED WOOD DRAIN TROUGH SHALL BE INCIDENTAL TO ITEM 603.91.
3. EXISTING TROUGHS ARE CONCRETE AT W. FALMOUTH RD., BITUMINOUS AT CENTER RD., AND CONCRETE AT LISBON RD.
4. REMOVE EXISTING DIRECTIONAL SCUPPER DOWNSPOUT ENDS AS DIRECTED BY THE RESIDENT.
5. MECHANICALLY ATTACH WOOD DOWNSPOUT TO ABUTMENT WITH TWO 3/8" Ø X 6" BOLTS INTO DROP IN ANCHORS.
6. BUTT SPLICE 2"X10" PRESSURE TREATED WOOD MEMBERS AT CLEATS (PROVIDE 5/4"X8" CLEATS AT BUTT SPLICES); OR USE 2"X10"X16" PRESSURE TREATED WOOD SPLICE BOARDS. SPIKE WITH 10-P GALVANIZED NAILS OR SCREWS.
7. INSTALL ROOFING UNDERLAYMENT (ICE AND WATER SHIELD) LINER AND SECURE WITH GALVANIZED ROOFING NAILS PRIOR TO INSTALLING TOP CLEATS.

Scale:			
NOT TO SCALE			
No.	Revision	By	Date

Designed by:					
T.Y. LIN INTERNATIONAL					
CONSULTANT PROJECT MANAGER: Norman L. Baker					
	By	Date		By	Date
Designed	DSM	3/14	Checked	DSM	3/14
Drawn	JD	3/14	In Charge of	NLB	3/14

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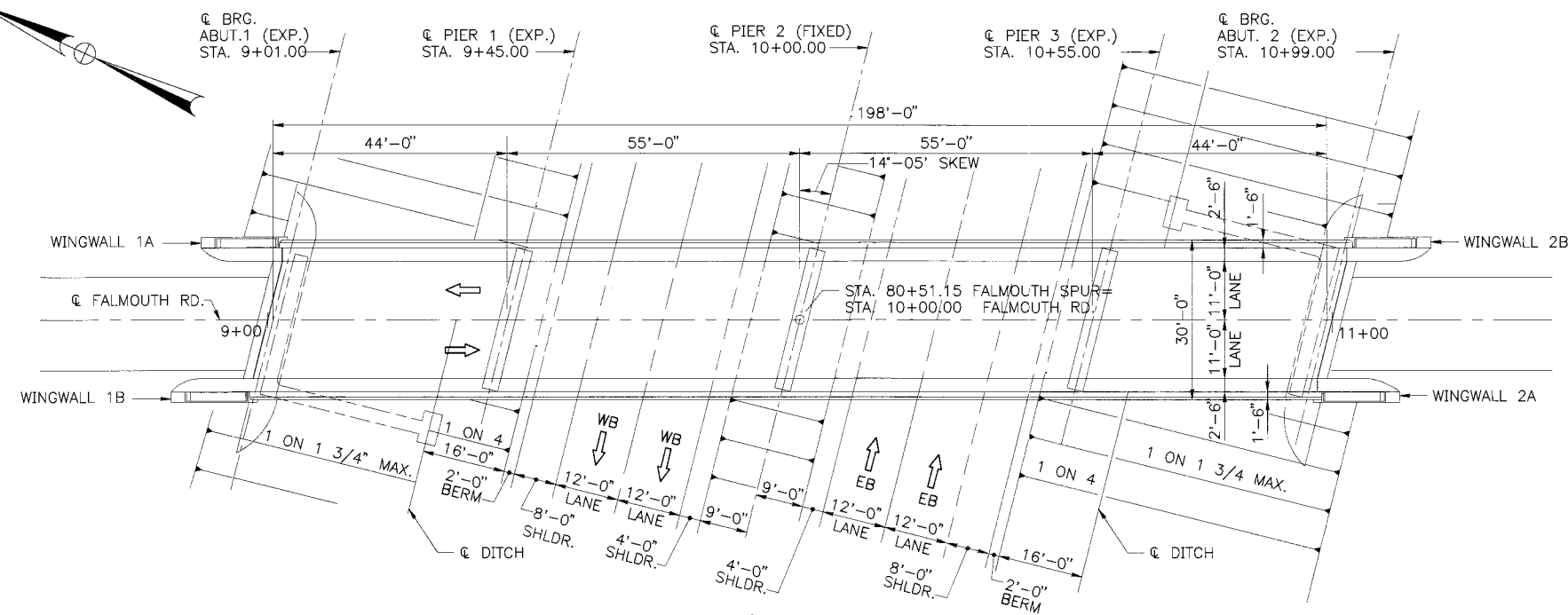


**THE GOLD STAR
MEMORIAL HIGHWAY**

MTA PROJECT MANAGER: Ralph C. Norwood IV

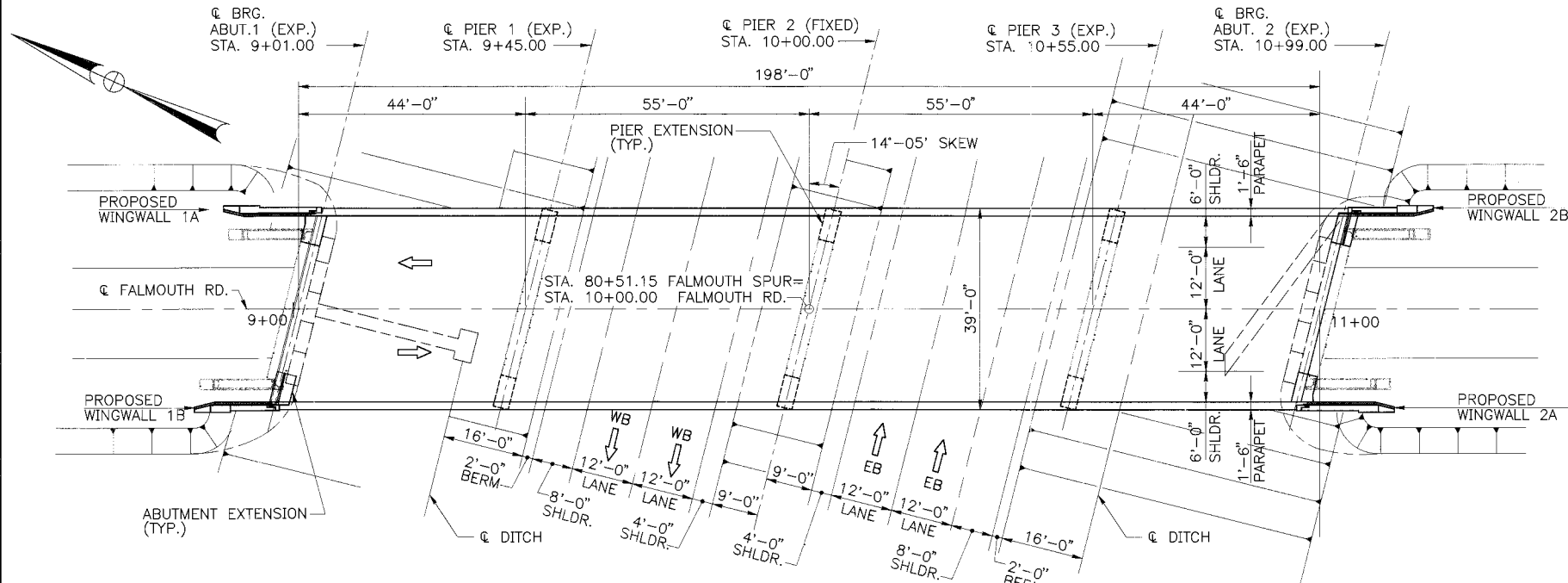
BRIDGE AND CULVERT REPAIRS 4 LOCATIONS (MILE F1.7 TO MILE 84.3) PRESSURE TREATED WOOD DRAIN TROUGH DETAILS	
SHEET NUMBER: S-18	CONTRACT: 2014.15
33 OF 52	

(MELPK) BDR-01



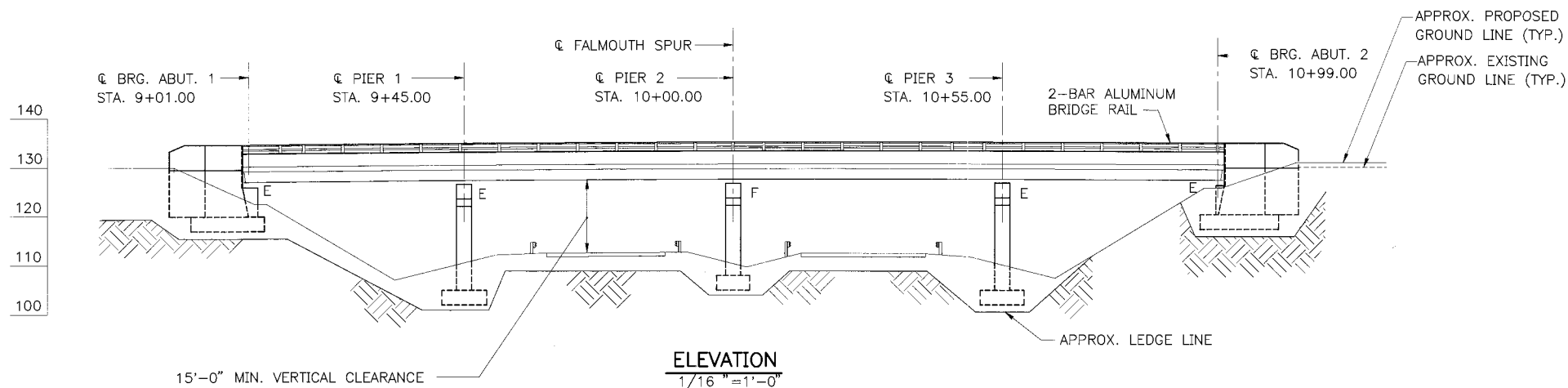
EXISTING PLAN

1/16" = 1'-0"



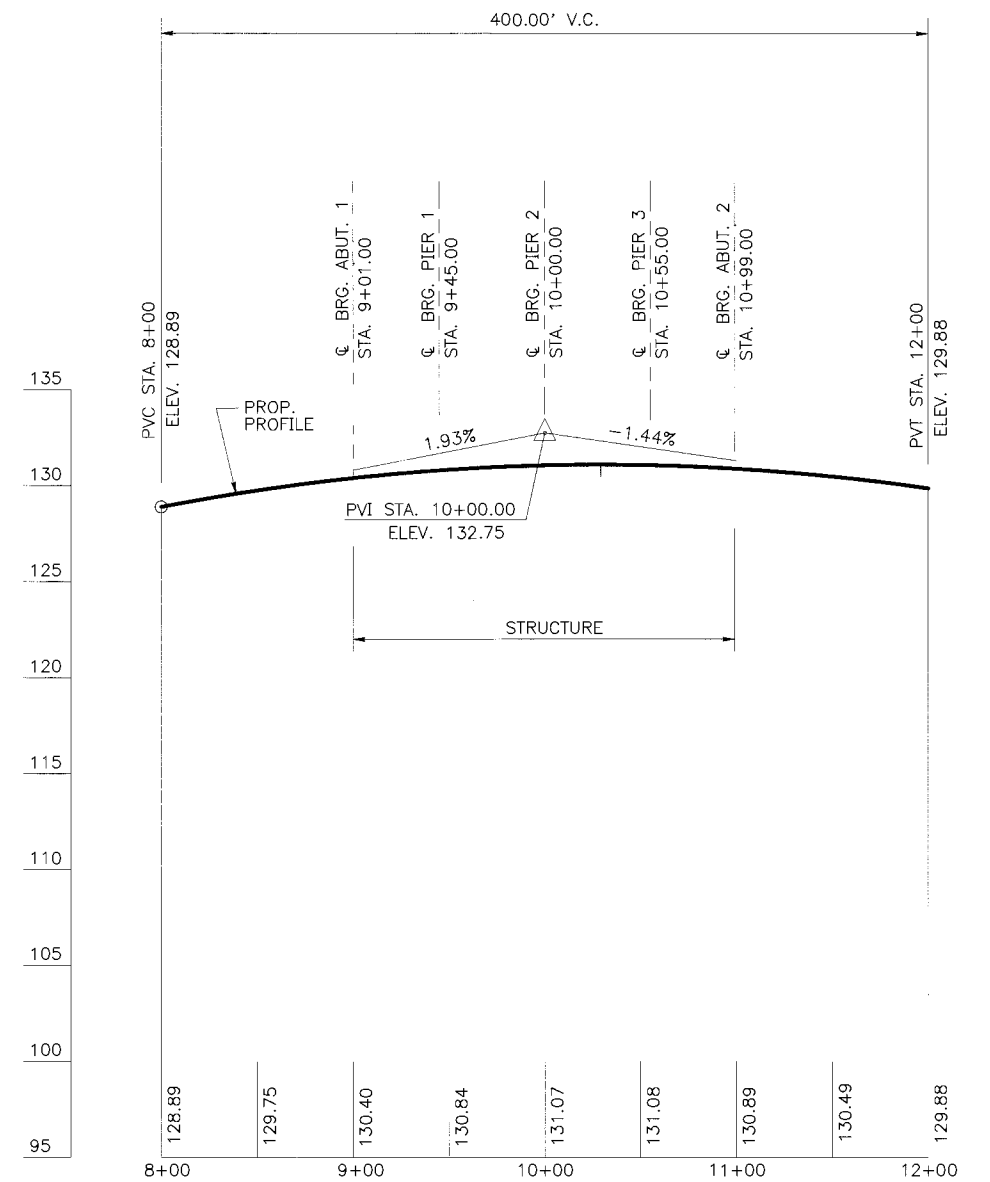
PROPOSED PLAN

1/16" = 1'-0"



ELEVATION

1/16" = 1'-0"



PROPOSED PROFILE

HORIZ. 1" = 50'
VERT. 1" = 5'

M:\B09009\W 958\04\DECK FR-02 01/21/97 11:02

Maine Turnpike Authority
Maine Turnpike

FALMOUTH ROAD UNDERPASS
GENERAL PLAN
AND ELEVATION

Contract 97.9

SHEET NUMBER: 34 OF 52

Sheet No. FR-2

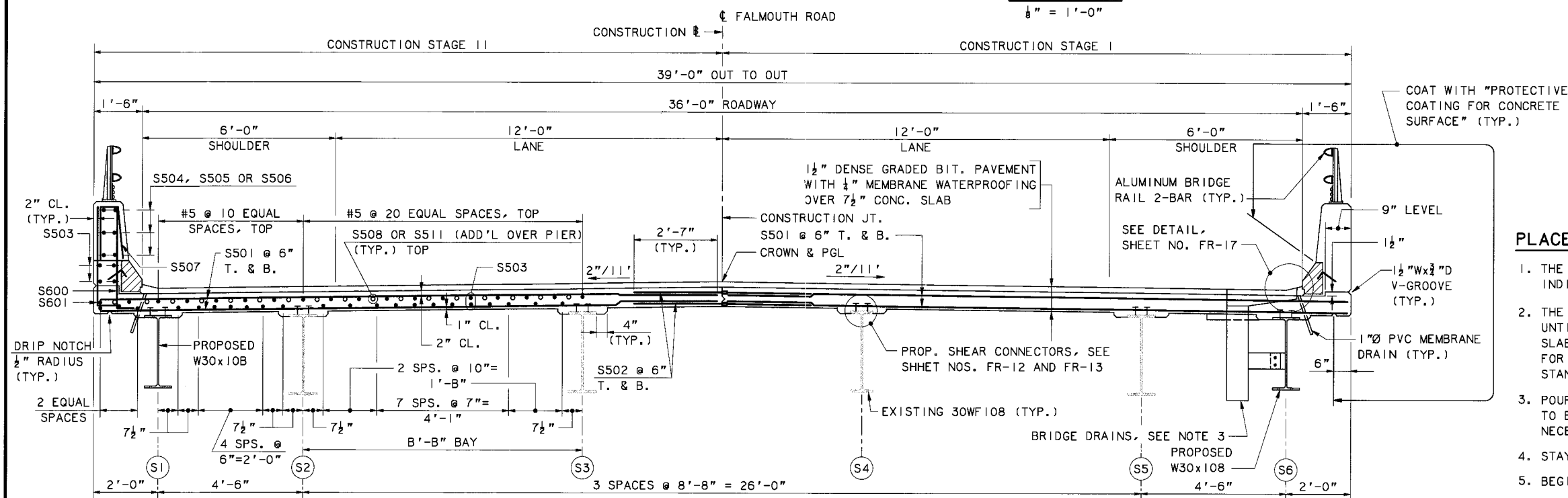
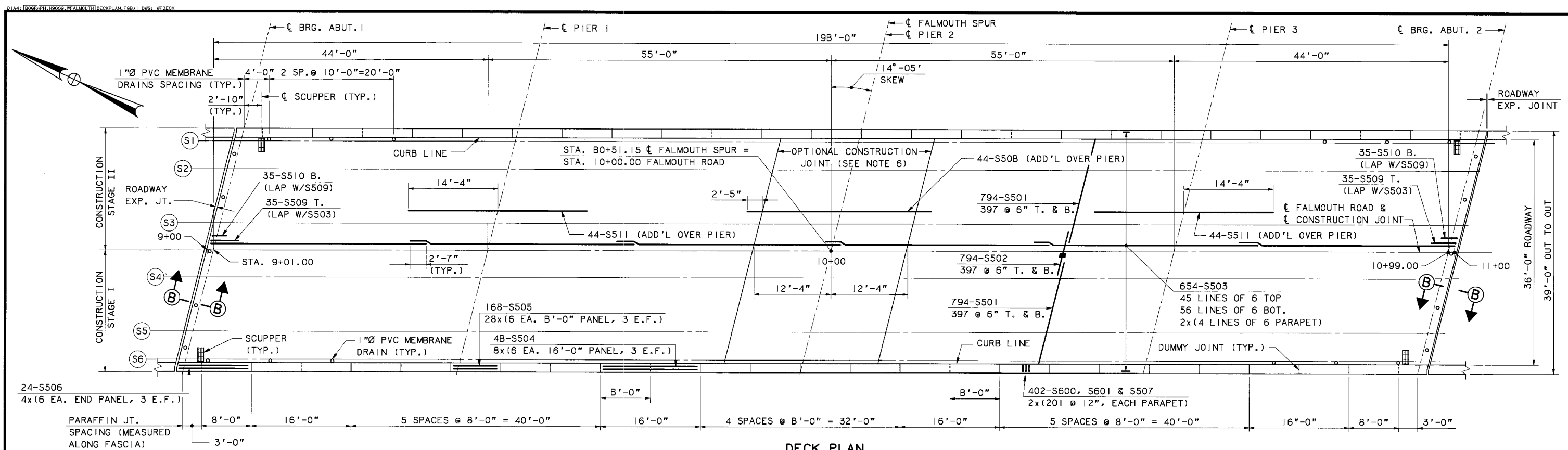
No.	Revision	By	Date	In Charge Of	RAL
		Designed	AD	1/97	
		Drawn	CSL	1/97	
		Checked	JMH	1/97	

HNTB

HOWARD NEEDLES TAMMEN & BERGENDOFF, INC.
ARCHITECTS ENGINEERS PLANNERS

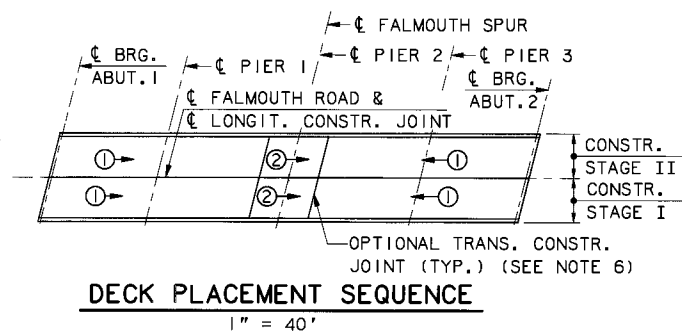
By

Date



SUPERSTRUCTURE NOTES


1. ADJUST REINFORCING STEEL TO FIT AROUND THE DRAINS IN A MANNER APPROVED BY THE ENGINEER. DO NOT CUT TRANSVERSE REINFORCING BARS. CUT LONGITUDINAL REINFORCING STEEL IN THE FIELD TO SUIT, THEN EPOXY COAT CUT ENDS.
2. FOR STEEL REINFORCING SCHEDULE, SEE SHEET NO. FR-25.
3. FOR SCUPPER AND DRAIN DETAILS, SEE SHEET NO. FR-16.
4. FOR 2-BAR ALUMINUM BRIDGE RAIL DETAILS, SEE SHEET NO. FR-20.
5. FOR SLAB DETAILS, SEE SHEET NOS. FR-16 AND FR-17.
6. FOR ROADWAY EXPANSION JOINT DETAILS, SEE SHEET NOS. FR-1B AND FR-19.
7. THE CONCRETE DECK SHALL BE GIVEN A SMOOTH BULL FLOAT OR WOOD FLOAT FINISH.
8. FOR SECTION B-B, SEE SHEET NO. FR-19.
9. CURB JOINTS SHALL LINE UP WITH PARAFFIN AND DUMMY JOINTS.
10. ONLY DOWEL BAR SPLICERS SHALL BE USED WITHIN CONCRETE DECK SLAB FOR STAGE I CONSTRUCTION AT THE LONGITUDINAL CONSTRUCTION JOINT. DOWEL-IN BARS SHALL THEN BE USED WITHIN CONCRETE DECK SLAB FOR STAGE II CONSTRUCTION.
11. FOR RAIL POST SPACING, SEE SHEET NO. FR-17.

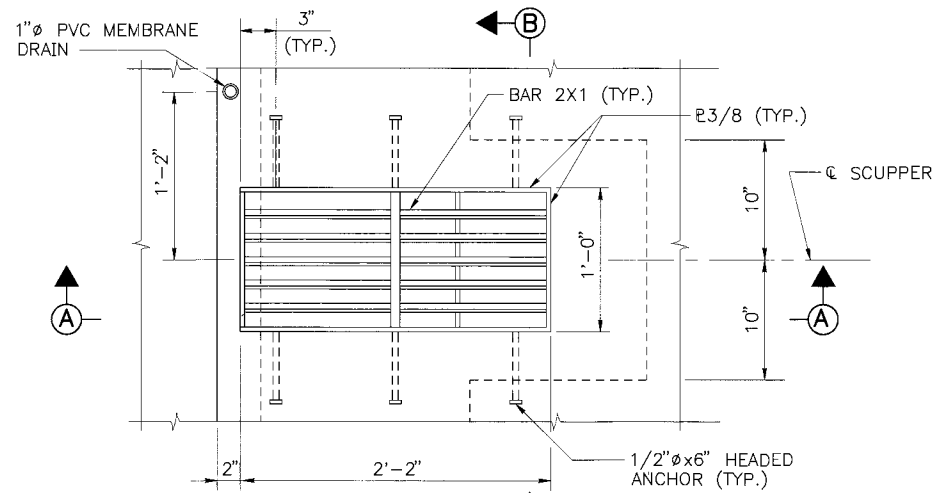


PLACEMENT NOTES

1. THE NUMBERS IN CIRCLES INDICATE PLACING SEQUENCE, THE ARROWS INDICATE DIRECTION OF PLACEMENT.
2. THE FORMWORK FOR THE CONSTRUCTION JOINTS SHALL REMAIN IN PLACE UNTIL A MINIMUM OF 48 HOURS HAS ELAPSED AFTER PLACEMENT OF THE SLAB. AFTER WHICH, REMOVAL OF FORMWORK MEETING THE REQUIREMENTS FOR FORM REMOVAL OF SECTION 502 (STRUCTURAL CONCRETE) OF THE STANDARD SPECIFICATIONS, MAY PROCEED.
3. POURS DESIGNATED BY THE SAME NUMBER DO NOT NECESSARILY HAVE TO BE POURED THE SAME DAY. A WAITING PERIOD OF 72 HOURS IS NECESSARY BETWEEN ADJACENT POURS.
4. STAY IN PLACE FORMS ARE NOT ALLOWED TO BE USED.
5. BEGIN PLACEMENT AT THE LOW END OF THE BLOCK.
6. THE OPTIONAL TRANSVERSE CONSTRUCTION JOINT MAY BE OMITTED PROVIDED THAT THE CONCRETE OF THE ENTIRE DECK SLAB REMAINS PLASTIC UNTIL THE COMPLETION OF THE LAST PLACEMENT AND ALL OTHER SPECIFICATIONS FOLLOWED.

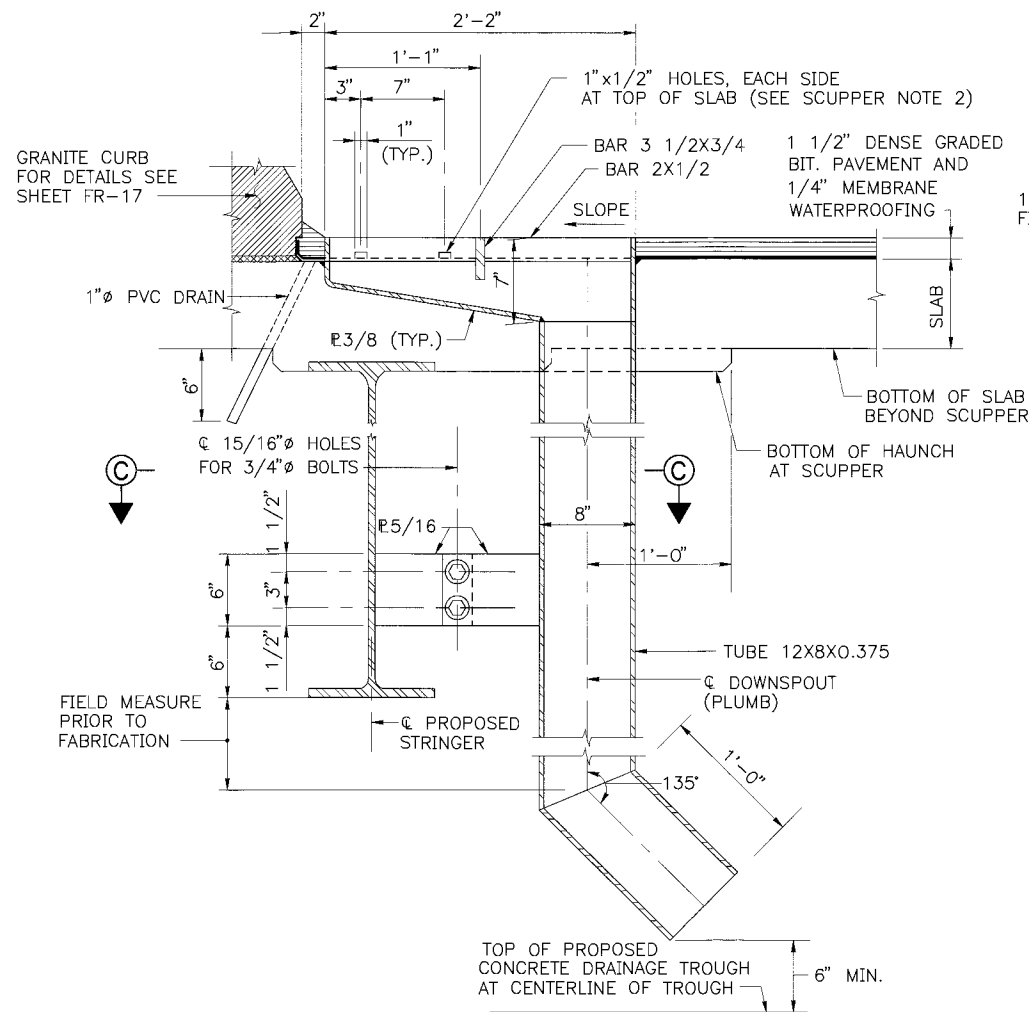
					By: Date
				Designed	AD 1/9
				Drawn	LS 1/9
				Checked	JMH 1/9
No.	Revision	By:	Date:	In charge of:	RAI

Maine Turnpike Authority	
Maine Turnpike	
	FALMOUTH ROAD UNDERPASS
DECK PLAN AND TYPICAL SECTION	
HNTB	HOWARD NEEDLES TAMMEN & BERGENDOFF, INC. ARCHITECTS ENGINEERS PLANNERS
Contract 97.9	Sheet No. FR-15
SHEET NUMBER: 35 OF 52	



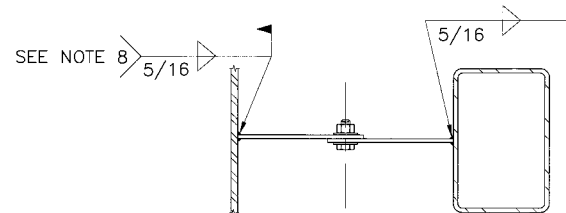
PLAN - SCUPPER

1 1/2" = 1'-0"



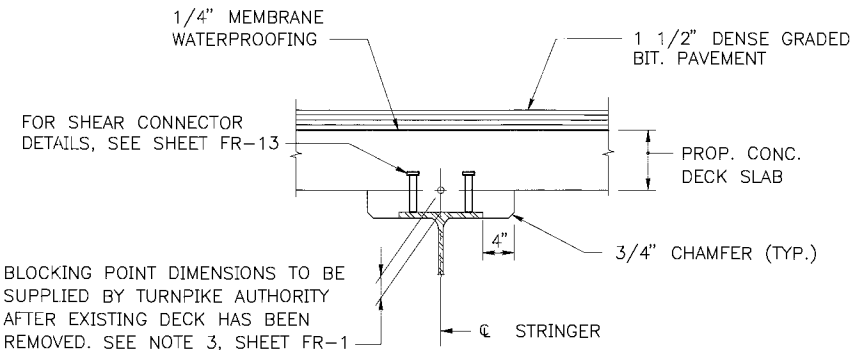
SECTION A-A

1 1/2" = 1'-0"



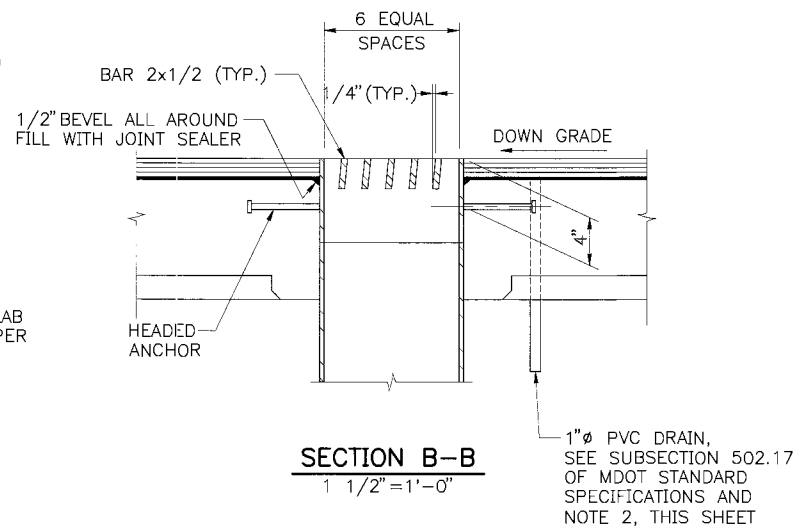
SECTION C-C

1 1/2" = 1'-0"



BLOCKING POINT DETAIL

1" = 1'-0"

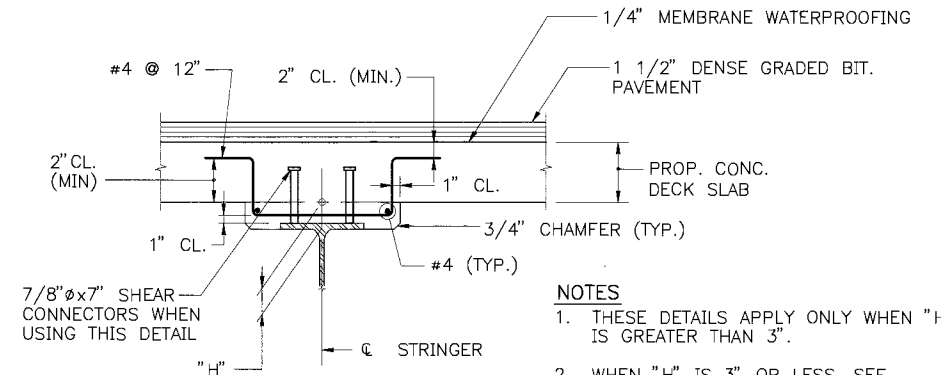


SECTION B-B

1 1/2" = 1'-0"

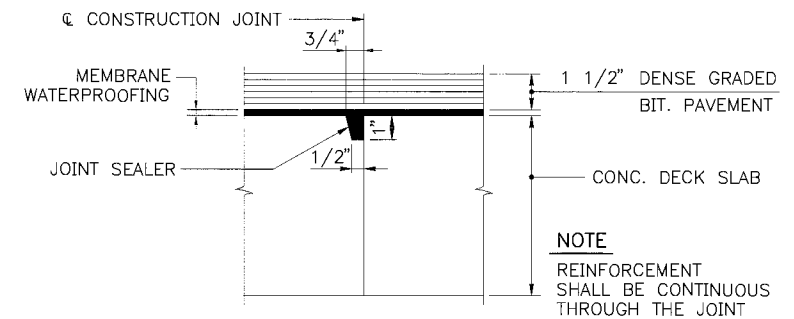
SCUPPER NOTES

1. ALL WELDS SHALL BE CONTINUOUS 1/4" FILLET WELDS EXCEPT AS NOTED.
2. DO NOT COVER DECK DRAINS WITH MEMBRANE WATERPROOFING. DEPRESS DRAINS 1/2" BELOW TOP OF SLAB, PROVIDE 23 GAUGE GALVANIZED SCREENS (1/4" MESH) OVER DRAINS.
3. SCUPPERS TO BE GALVANIZED AFTER FABRICATION. GALVANIZING SHALL CONFORM TO ASTM A153.
4. ALL PLATES SHALL CONFORM TO ASTM A709, GRADE 36.
5. STRUCTURAL TUBES SHALL CONFORM TO ASTM A501.
6. PAYMENT FOR SCUPPERS, PVC DRAINS AND SCREENS SHALL BE INCIDENTAL TO ITEM NO. 502.261.
7. FOR LOCATION OF SCUPPERS AND 1" DRAINS, SEE SHEET NO. FR-15.
8. FIELD REPAIR GALVANIZING WITH ZINC RICH PAINT ACCORDING TO SECTION 506 OF THE STANDARD SPECIFICATIONS. (PAYMENT INCIDENTAL TO ITEM NO. 502.261).



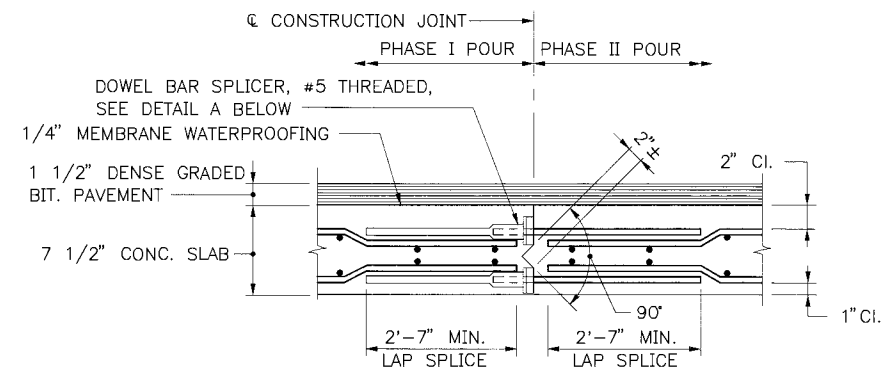
EXTRA DEPTH HAUNCH DETAILS

1" = 1'-0"



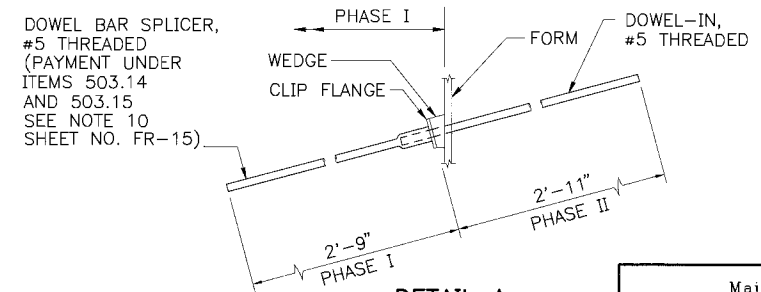
TRANSVERSE CONSTRUCTION JOINT DETAIL

3" = 1'-0"



LONGITUDINAL CONSTRUCTION JOINT DETAIL

NOT TO SCALE

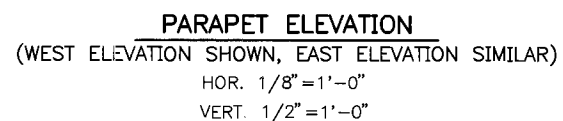
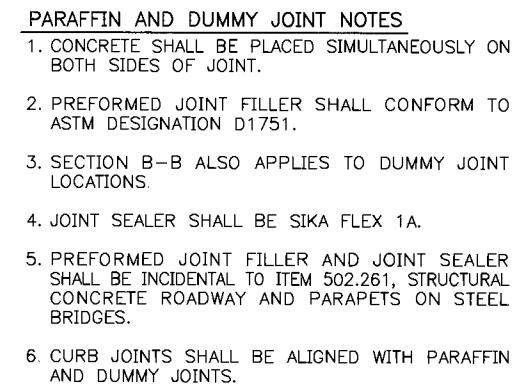


DETAIL A



NO SCALE

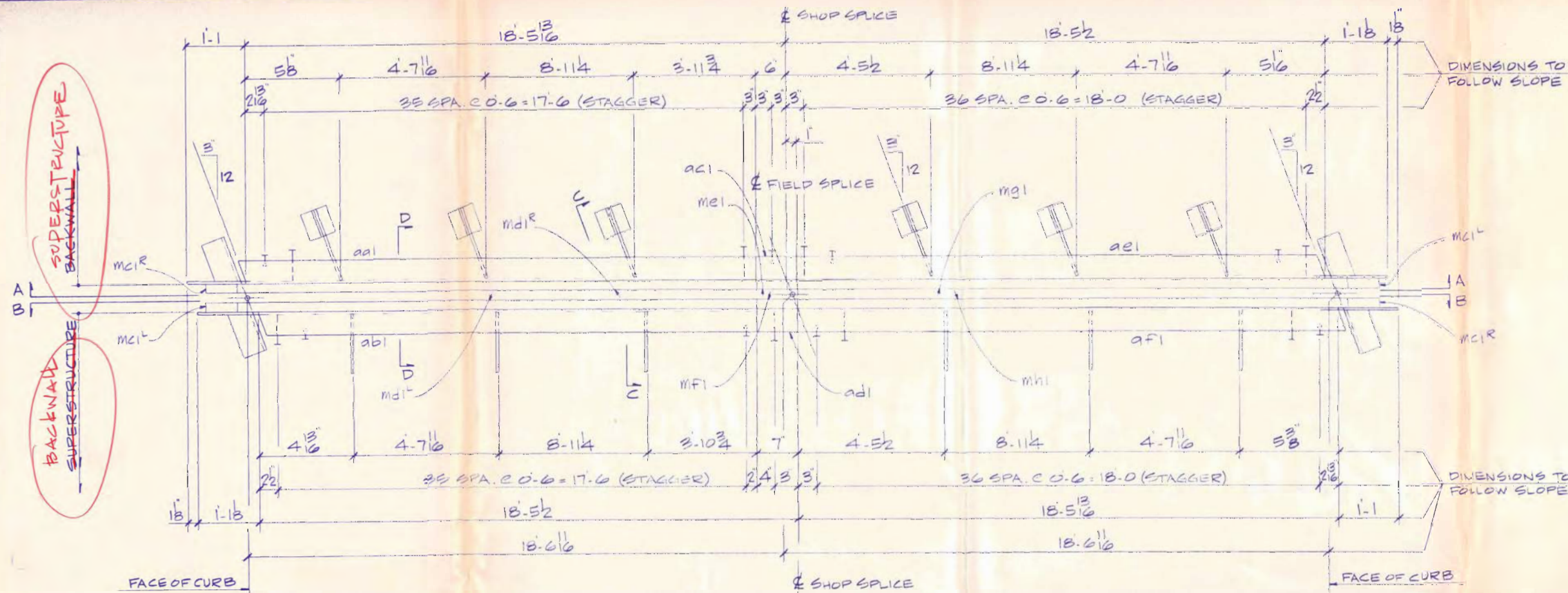
Maine Turnpike Authority	
Maine Turnpike	
FALMOUTH ROAD UNDERPASS	
SLAB DETAILS I	
HOWARD NEEDLES TAMMEN & BERGENDOFF, INC. ARCHITECTS ENGINEERS PLANNERS	
Contract 97.9	Sheet No. FR-16
SHEET NUMBER: 36 OF 52	

No.	Revision	By	Date	In Charge Of
		Designed	AD 1/97	
		Drawn	CSL 1/97	
		Checked	JMH 1/97	
				RAL



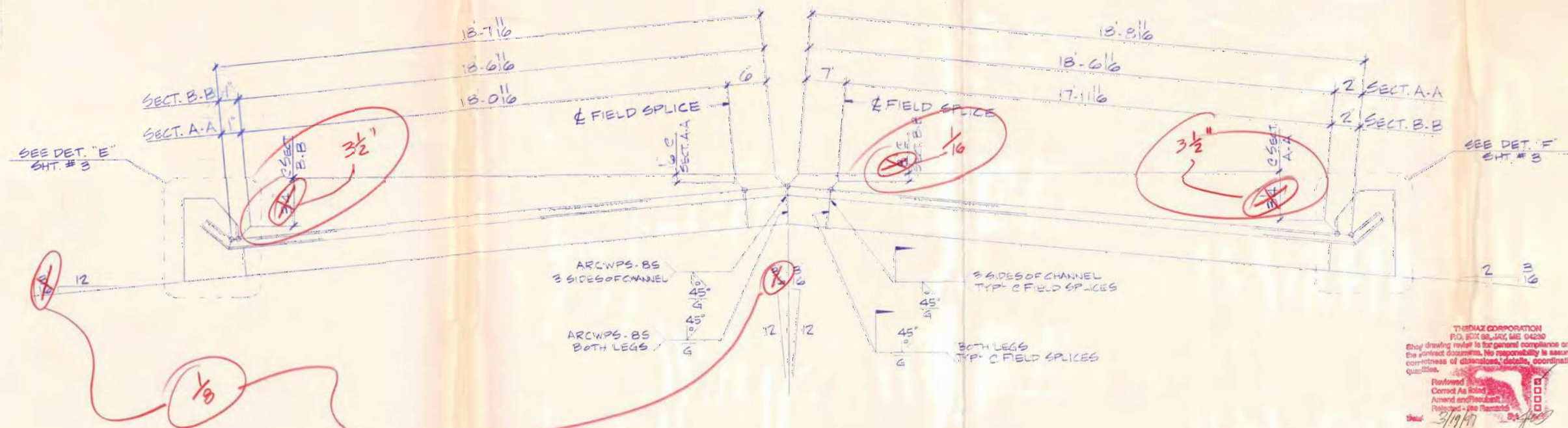
					By	De
				Designed	AD	1/
				Drawn	CSL	1/
				Checked	JMH	1/
No	Revision	By	Date	In Charge of	RAI	

Maine Turnpike Authority Maine Turnpike		FALMOUTH ROAD UNDERPASS SLAB DETAILS II	
			
Contract 97.9		Sheet No. FR-17	
SHEET NUMBER:		37 OF 52	



ONE - EXPANSION DEVICE - MK. A1

ONE - EXPANSION DEVICE - MK. B1



SECT. A-A
SECT. B-B

SHOP NOTE!
WORK THIS DWG. WITH DWG. #1 & 5

BILL OF MATERIALS

SHIPPING MARK	SHOP MARK	QTY	DESCRIPTION
A1		1	EXPANSION DEVICE
aa1		1	LBX4X2X18'-0 11/16 PREP BIE
ab1		1	LBX4X2X17'-10 2/3 PREP BIE
ag1		6	L3/2X3/2X3/8X1'-0
ma1		72	3/8\"X0'-8 H.S.A.
mb1		10	3/8\"X0'-4 H.S.A.
mc1		1	WATSON BOWMAN ACME EDGE CHANNEL X0'-9 BIE PREP
md1		1	d.o. X18'-3 3/8 BIE
mj1		3	WTB8X25X0'-6
mk1		3	3/4\"X22 A325 W/N&2W
mn1		6	3/4\"X2\" A307 W/N&W
mo1		3	2\"X1'-0 THD ROD W/4N&4BW
pa1		3	#2 X10\"X0'-8 3/4 SHP.
pb1		3	#2 X12\"X0'-9 SHP.
pc1		1	#2 X13 3/8 X1'-6 3/8 SHP.
pd1		1	#2 X13 3/4 X1'-6 3/8 SHP.
pe1		1	BENT #2 X8\"X0'-11 7/8 SHP.
pf1		1	BENT #2 X9 1/4 X0'-11 7/8 SHP.
B1		1	EXPANSION DEVICE
ac1		1	LBX4X2X0'-6 PREP 2E BIE
ad1		1	X0'-7 PREP 2E BIE
ae1		1	X18'-5 1/2 BIE
af1		1	LBX4X2X18'-6 11/16 BIE
ag1		6	L
ma1		76	H.S.A.
mb1		10	H.S.A.
mc1		1	EDGE CHANNEL WATSON BOWMAN ACME EDGE CHANNEL X0'-6 PREP 2E BIE
me1		1	X0'-7 PREP 2E BIE
mf1		1	X0'-7 PREP 2E BIE
mg1		1	X18'-10 3/8 BIE
mh1		1	d.o. X18'-9 3/8 BIE
mj1		3	WT
mk1		3	BOLT
mn1		6	BOLT
mo1		3	THD ROD
pa1		3	#2
pb1		3	#2
pc1		1	#2
pd1		1	#2
pe1		1	BENT #2
pf1		1	BENT #2

MATL: A36
WELD: PER WELD PROCEDURE
SURFACE PREP: SPG
FINISH: NONE

PROJ. #: 97.9
ITEM #: 320.221



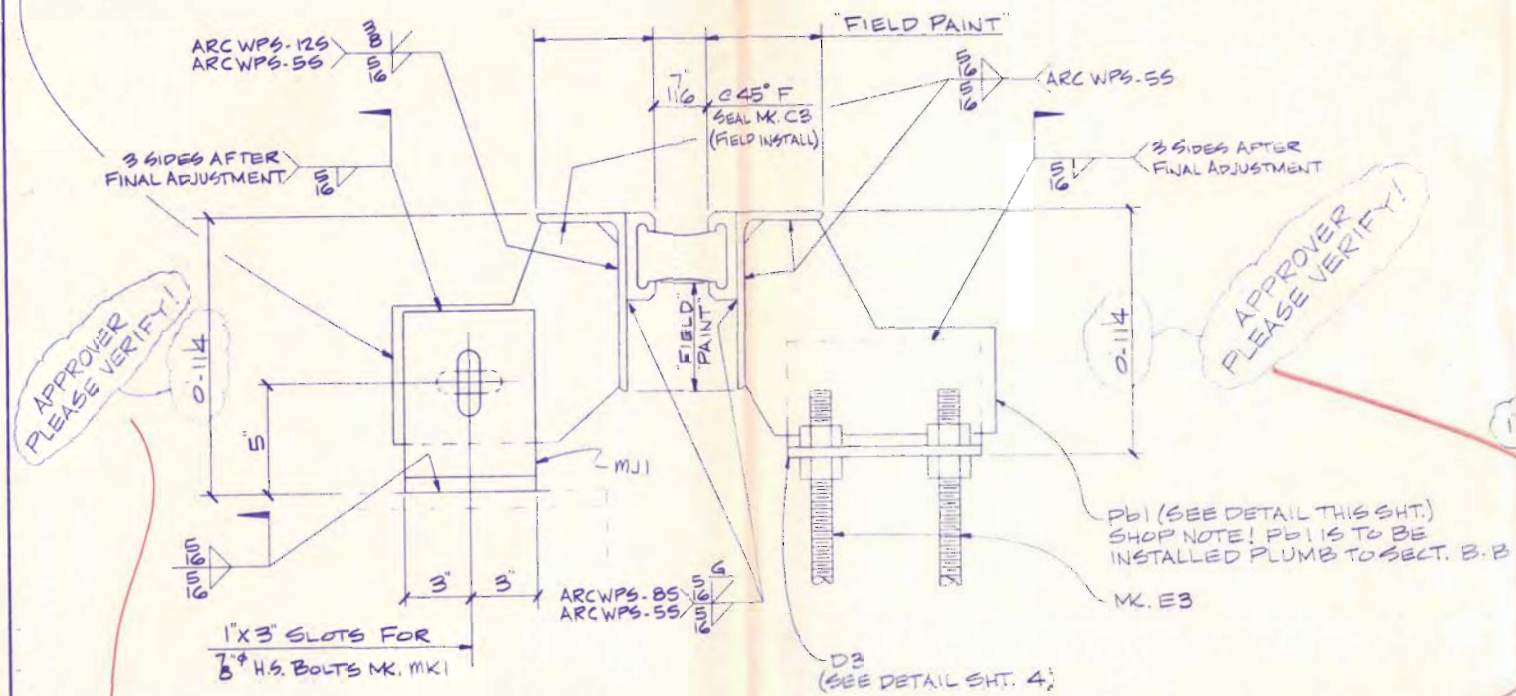
ADVANCED RESOURCES
& CONSTRUCTION

P.O. BOX 120 KINGFIELD, ME 04947
PHONE: (207) 265-2646 • FAX: (207) 265-4054

DESCRIPTION: EXPANSION DEVICE ABUT. ONE
PROJECT: FALMOUTH ROAD UNDERPASS
LOCATION: FALMOUTH, ME.
DESIGNER: ME. TURNPIKE AUTHORITY
CUSTOMER: THE DIAZ CORP.

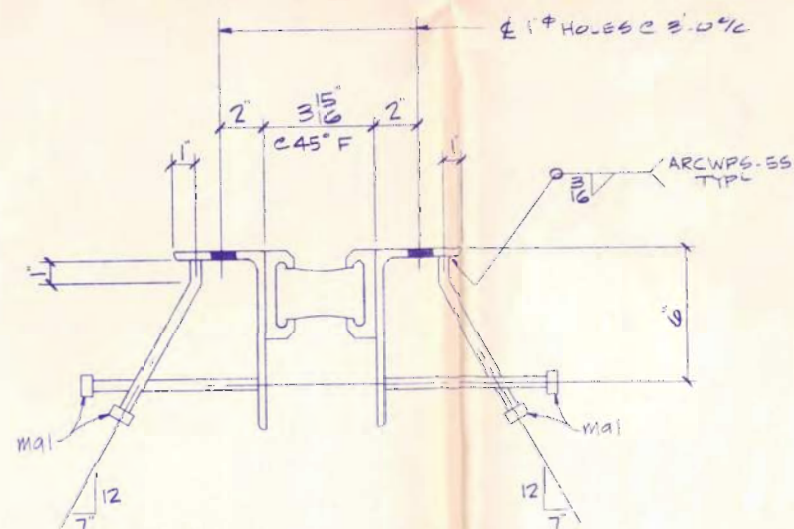
DRAWN BY: SNOWMAN DATE: 3/10/97
CHK'D BY: DATE: 97.9
JOB NUMBER: 97.9
DWG. NO.: 1

PA1 (SEE DETAIL THIS SHT.)
SHOP NOTE! PA1 IS TO BE
INSTALLED PLUMB TO SECT. A-A

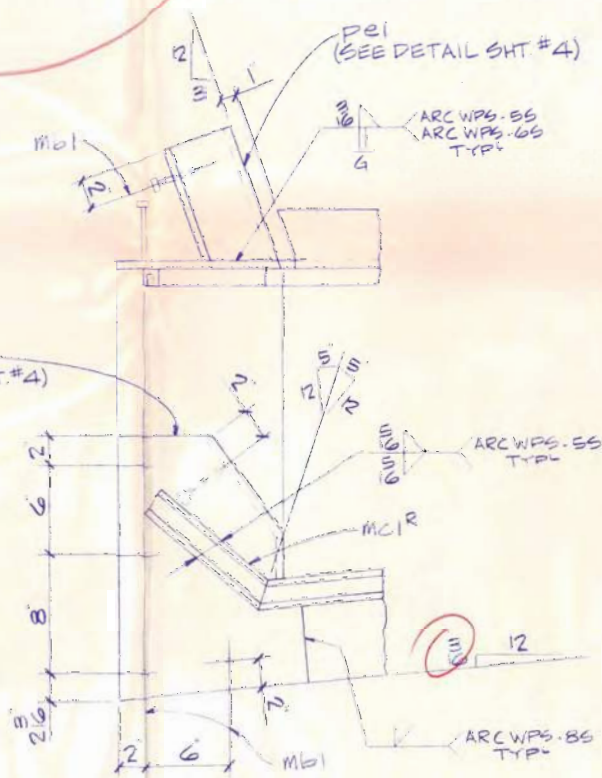


SECT. C-C

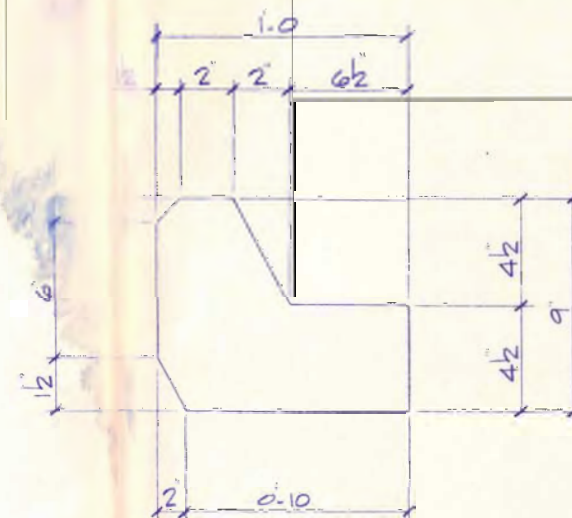
FIELD VERIFY



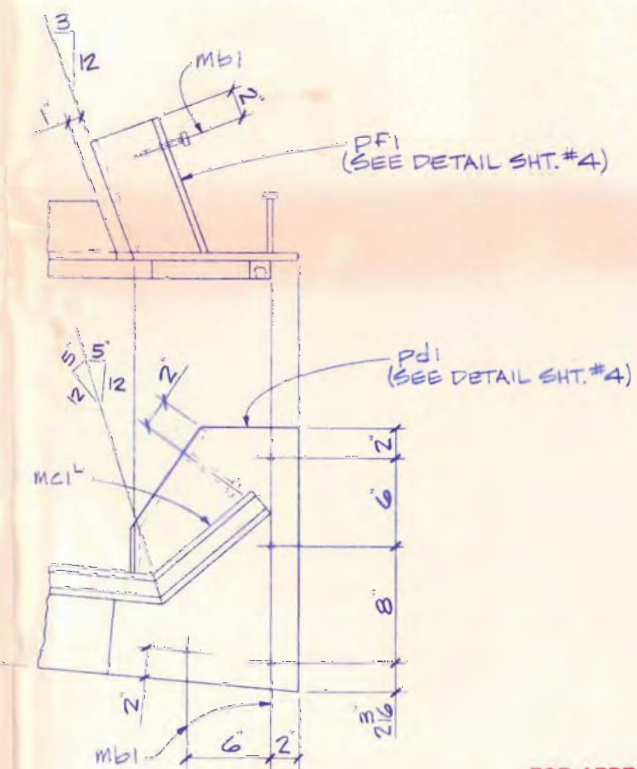
SECT. D-D



DETAIL 'E



Pbl - DETAIL



DETAIL "F"

PROJ.#: 97.9
ITEM#: 520.221

SHOP NOTE!
WORK THIS DWG. WITH DWG. # 1, 2, 3 & 5

SHEET NUMBER: 40 OF 52

THE MAX CORPORATION
P.O. BOX 49, JAY, MS 39230

Shop drawing reflects general compliance only with the contract documents. No responsibility is assumed for correctness of dimensions, details, coordination, or quantities.

Performance Level
Correct As Noted
Amend and Resubmit
Rejected - See Remarks

3/14/94



ADVANCED RESOURCES & CONSTRUCTION

P.O. BOX 120 KINGFIELD, ME 04947
PHONE: (207) 265-2646 • FAX: (207) 265-4054

DESCRIPTION SECTIONS & DETAILS
PROJECT FALMOUTH ROAD UNDERPASS
LOCATION FALMOUTH, ME.
DESIGNER ME. TURNPIKE AUTHORITY
CUSTOMER THE DIAZ CORP

DRAWN BY <u>SNOWMAN</u> DATE <u>3/6/97</u>	JOB NUMBER <u>97.9</u>	DWG. NO. <u>4</u> ✓
CHK'D BY _____ DATE _____		