

MAINE TURNPIKE AUTHORITY

ADDENDUM NO. 2

CONTRACT 2026.02

PAVEMENT REHABILITATION AND
SAFETY IMPROVEMENTS
MM 27.4 TO MM 30.3

QUESTIONS

- Question 1: Will the Authority be providing lump sum price for Item Number 401.03 Balance Mix Design Data Collection?
Response: Yes. See the updated P-Sheets included in this addendum with a \$20,000 price for this item.
- Question 2: Does the Authority anticipate paving the 12' outside shoulders (Right) independently?
Response: Yes. The contractor may pave the outside travel lane and shoulder at the same time if desired. See updates to plans and special provisions.
- Question 3: Will the Authority be providing traffic control for the added work at exit 36 on & off ramps southbound?
Response: No, this shall be done by the Contractor. Please refer to the “GENERAL” section of this Addendum No. 2.
- Question 4: Will both exit 36 southbound ramps be allowed to be closed simultaneously?
Response: No, Exit 36 SB Off Ramp is not required to be closed for the work. The Exit 36 Southbound On Ramp and the Exit 35 Southbound Off Ramp are anticipated to be closed simultaneously for work associated with the rumble strips.
- Question 5: Special Provision Section 403 (SP-52) contract states that "Lane 3 and the twelve foot shoulder shall be pulled as one." Given that placing a full 12-foot shoulder in a single pull while paving the adjacent lane is not feasible with standard paving equipment, could the Authority clarify the intent of this requirement? Specifically, is this intended to mandate same-pass paving, or will phased paving using multiple passes be acceptable?
Response: The contractor will not be required to pave Lane 3 and the outside shoulder in one pull. The outside shoulder may be paved separately or at the same time. The contractor will be required to pave Lane 1 and the median shoulder in one pull.

GENERAL

- The GENERAL section on page 1 of 14, Addendum No. 1 shall be amended to correct the typo for the Off Ramp number. Replace the entire paragraph with the following, “Additional work that consists of installing Rumble Strips – Centerline and Grooved Pavement Strips at Exit 36 is being amended to the contract. Work shall be completed with night ramp closures of Exit 36 SB On Ramp and Exit 35 SB Off Ramp in accordance with Special Provision 652. Maintenance of Traffic will be paid for under 652.361.”

SPECIFICATIONS

- Proposal Sheet P-2 shall be replaced to include the lump sum price of \$20,000 for item 401.03 – Balanced Mix Design Data Collection.
- Proposal Sheet P-5 shall be replaced to reflect a change in the quantity of items 627.73 – Temporary Pavement Marking Tape and 627.75 – White or Yellow Pavement & Curb Marking.
- Special Provision Sheet SP-1 shall be replaced with an updated date for when work will be allowed around the Fourth of July holiday.
- Special Provision Sheet SP-13 shall be replaced to include language for what shall happen if shallow shoulder pavement is encountered.
- Special Provision Sheet SP-52 shall be replaced to revise the third paragraph updating wording for shoulder paving.

PLANS

- Plan Sheet 2 Estimate Quantities & General Notes shall be replaced.
- Plan Sheet 4 Typical Sections shall be replaced to update wording about shoulder paving.
- Plan Sheet 8 Pavement Marking Details shall be replaced to update the length of temporary tape used to mark the outside lane edges.
- Plan Sheet 9 Catch Basin and Grooving Details shall be replaced to update the location and length of paint used to mark catch basin locations.

ATTACHMENTS

- This document – Addendum No. 2 (3 Pages)
- Proposal Sheet P-2 (Revised 1/13/2026) (1 Pages)
- Proposal Sheet P-5 (Revised 1/9/2026) (1 Pages)
- Special Provision Sheet SP-1 (Revised 1/9/2026) (1 Page)
- Special Provision Sheet SP-13 (Revised 1/9/2026) (1 Page)
- Special Provision Sheet SP-52 (Revised 1/13/2026) (1 Page)
- Plan Sheet 2 Estimated Quantities & General Notes (Revised 1/9/2026) (1 Page)
- Plan Sheet 4 Typical Sections (Revised 1/13/2026) (1 Page)
- Plan Sheet 8 Pavement Marking Details (Revised 1/9/2026) (1 Page)
- Plan Sheet 9 Catch Basin and Grooving Details (Revised 1/9/2026) (1 Page)

Notes: The above items shall be considered as part of the bid submittal.

The total number of pages included in this addendum is twelve (12) pages.

All bidders are requested to acknowledge the receipt of the Addendum No. 2 by signing below and email this sheet to Nathaniel Carll, Purchasing Department, Maine Turnpike Authority at nearll@maineturnpike.com . Bidders are also required to acknowledge receipt of this Addendum No. 2 on Page P-7 of the bid package.

Business Name

Print Name and Title

Signature

Date

Very truly yours,

MAINE TURNPIKE AUTHORITY

Nathaniel Carll

Purchasing Department

Maine Turnpike Authority

SCHEDULE OF BID PRICES
CONTRACT NO. 2026.02

Pavement Rehabilitation and Safety Improvements
MM 27.4 to MM 30.3

Item No.	Item Description	Units	Approx. Quantities	Unit Prices in Numbers		Bid Amount in Numbers	
				Dollars	Cents	Dollars	Cents
202.202	REMOVING PAVEMENT SURFACE	Square Yard	182,900				
202.205	RUMBLE STRIPS	Each	55,200				
202.206	RUMBLE STRIPS - CENTERLINE	Linear Foot	200				
202.220	GROOVED PAVEMENT STRIPS	Linear Foot	225				
203.20	COMMON EXCAVATION	Cubic Yard	10				
211.50	MEDIAN RESTORATION	Linear Foot	14,200				
304.14	AGGREGATE BASE COURSE GRAVEL, TYPE A	Cubic Yard	5				
401.03	BALANCED MIX DESIGN DATA COLLECTION	Lump Sum	1	\$20,000		\$20,000	
401.10	HOT MIX ASPHALT CONTINUOUS THERMAL PROFILING	Lump Sum	1				
403.2081	HOT MIX ASPHALT - 12.5 MM (POLYMER MODIFIED)	Ton	18,600				

CARRIED FORWARD:		
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Item No.	Item Description	Units	Approx. Quantities	Unit Prices in Numbers		Bid Amount in Numbers	
				Dollars	Cents	Dollars	Cents
BROUGHT FORWARD:							
613.319	EROSION CONTROL BLANKET	Square Yard	12,600				
618.14	SEEDING METHOD NUMBER 2	Unit	260				
619.12	MULCH	Unit	260				
619.1202	TEMPORARY MULCH	Lump Sum	1				
620.58	EROSION CONTROL GEOTEXTILE	Square Yard	20				
627.73	TEMPORARY PAVEMENT MARKING TAPE	Linear Foot	10,200				
627.75	WHITE OR YELLOW PAVEMENT & CURB MARKING	Square Foot	20				
627.78	TEMPORARY PAVEMENT MARKING LINE, WHITE OR YELLOW	Linear Foot	65,900				
627.94	PAVEMENT MARKING TAPE	Linear Foot	1,200				
629.05	HAND LABOR, STRAIGHT TIME	Hour	40				
631.12	ALL PURPOSE EXCAVATOR (INCLUDING OPERATOR)	Hour	25				
631.172	TRUCK - LARGE (INCLUDING OPERATOR)	Hour	25				
CARRIED FORWARD:							

MAINE TURNPIKE AUTHORITYSPECIFICATIONSPART II - SPECIAL PROVISIONS

All work shall be governed by the Maine Department of Transportation Standard Specifications, Revision of November 2014, except for that work which applies to sections of the Maine Department of Transportation Standard Specifications which are amended by the Maine Turnpike Supplemental Specifications and the following modifications, additions and deletions.

General Description of Work

The pavement rehabilitation work consists of milling and filling three travel lanes and the median and outside shoulders for both northbound and southbound approaches. There are short sections where the shoulder pavement will be reconstructed. Excess material in the median will be removed and catch basins shall be cleaned to restore proper drainage. Noted catch basin tops will be replaced, and concrete aprons will be installed around the basins where required. Guardrail height will be adjusted as necessary, with damaged sections of guardrail being replaced where it is needed. Pavement markings, maintenance of traffic and all other work incidental thereto will be completed in accordance with the Plans and Specifications.

Plans

The drawings included in these Contract Documents, and referred to as the Plans, show the general character of the work to be done under this Contract. They bear the general title "Maine Turnpike – Contract 2026.02 –Pavement Rehabilitation and Safety Improvements MM 27.4 to 30.3". The right is reserved by the Resident to make such minor corrections or alterations in the Plans as he deems necessary without change in the unit prices on the Schedule of Prices of the Proposal.

101.2 DefinitionHolidays

The following is added after Memorial Day in the Supplemental Specifications:

Juneteenth Day 2026 (June 19, 2026)	6:00 a.m. Friday to 6:00 p.m. Friday
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Independence Day 2026 (Fourth of July)	12:01 p.m. preceding Thursday to 5:00 p.m. the following Monday.
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All surplus pavement grindings, except for the amount specified above, shall be disposed of by the Contractor off the turnpike right-of-way. All grindings shall be disposed of in accordance with the Maine Department of Environmental Protection Solid Waste Management Requirements.

In the event of encountering shoulder gravels during milling, the contractor shall reset and plunge cut fully through pavement into shoulder gravels by 1-2 inches as directed by the Resident Contractor will be paid double the unit price of Item 202.202 - Removing Pavement Surface, for the area requiring the deeper plunge cut. Type A gravels to restore grade shall be paid under 304.14 – Aggregate Base Course – Type A. No additional payment will be made for this work.

202.07 Method of Measurement

The removal of existing bituminous concrete pavement – mainline will be measured by the square yard of material removed to the required depth.

The following sentences are added:

Transporting and stockpiling of the pavement grindings at the maintenance facilities will not be measured separately for payment, but shall be incidental to the Removing Pavement Surface items.

Installation of temporary bituminous ramps will not be measured separately for payment, but shall be incidental to the Contract.

Removal of temporary bituminous ramps will not be measured separately for payment, but shall be incidental to the Contract.

Installation of and removal of longitudinal safety wedges will not be measured separately for payment, but shall be incidental to the Contract.

Shoulder area requiring a deeper plunge cut to remove thin remaining existing pavement will be measured by the square yard of additional material removed. In effect, the Contractor will be paid double the unit price of Item 202.202 - Removing Pavement Surface, for the area requiring the deeper plunge cut; once for typical depth milling and once for the deeper depth removal. Type A gravels to restore grade shall be measured and paid under 304.14 – Aggregate Base Course – Type A. No additional payment will be made for this work.

202.08 Basis of Payment

Removing Pavement Surface – Mainline will be paid for at unit price per square yard which price shall be full compensation for removing and disposing of the bituminous and gravel materials.

Payment will be made under:

<u>Pay Item</u>		<u>Pay Unit</u>
202.202	Removing Pavement Surface – Mainline	Square Yard

SP - 13

The sand and loose debris adjacent to the median guardrail shall be removed and disposed of by the Contractor off of Turnpike property.

The forty-five degree pavement safety edge needed between adjacent lanes and or shoulders shall be incidental to the 202 pay items.

Lane 1 and the median shoulder shall be pulled as one. Lane 3 and the outside shoulder may be pulled as one.

403.04 Method of Measurement

The construction and removal of temporary ramps on sand joints, and maintaining the ramps will not be measured separately for payment, but shall be incidental to Items 403.

The removal of sand and loose debris will not be measured separately for payment, but shall be incidental to paving items.

Hot Mix Asphalt, 12.5 mm (Polymer/Latex Modified) pavement with (up to) 15% RAP, placed as a wearing surface will be measured under Item 403.2081 Hot Mix Asphalt, 12.5 mm (Polymer/Latex Modified).

403.05 Basis of Payment

Hot Mix Asphalt, 12.5 mm (Polymer/Latex Modified) pavement, placed as a wearing surface will be paid under Item 403.2081 Hot Mix Asphalt, 12.5 mm (Polymer/Latex Modified).

The following pay items are added:

<u>Pay Item</u>	<u>Pay Unit</u>	
403.2081	Hot Mix Asphalt, 12.5 mm (Polymer/Latex Modified)	TON
403.2084	Hot Mix Asphalt, 12.5 mm (incidentals)	TON

ESTIMATED QUANTITIES			
ITEM NO.	ITEM	UNIT	TOTAL QUANTITY
202.202	REMOVING PAVEMENT SURFACE	SY	182900
202.205	RUMBLE STRIPS	EA	55200
202.206	RUMBLE STRIPS - CENTERLINE	LF	200
202.220	GROOVED PAVEMENT STRIPS	LF	225
203.20	COMMON EXCAVATION	CY	70
211.50	MEDIAN RESTORATION	LF	14200
304.14	AGGREGATE BASE COURSE GRAVEL, TYPE A	CY	5
401.03	BALANCED MIX DESIGN DATA COLLECTION	LS	1
401.10	HOT MIX ASPHALT CONTINUOUS THERMAL PROFILING	LS	1
403.208I	HOT MIX ASPHALT - 12.5 MM (POLYMER MODIFIED)	T	18600
403.2084	HOT MIX ASPHALT - 12.5 MM (INCIDENTALS)	T	100
403.211	HOT MIX ASPHALT (SHIMMING)	T	100
403.213	HOT MIX ASPHALT - 12.5 MM (BASE)	T	20
409.152	BITUMINOUS TACK COAT TRACKLESS, APPLIED	G	1100
419.30	SAW CUTTING BITUMINOUS PAVEMENT	LF	600
424.323I	ASPHALT RUBBER MASTIC CRACK SEALER	LB	9800
424.324	ASPHALT RUBBER MASTIC CRACK SEALER WITH AGGREGATE	LB	2500
427.09	CRACK REPAIR	LF	13800
459.06	BITUMINOUS CONCRETE WATERWAY, TYPE I	EA	5
459.06I	BITUMINOUS CONCRETE WATERWAY, TYPE II	EA	5
470.08	BERM DROP OFF CORRECTION - GRINDINGS	LF	19300
470.08I	BERM CORRECTION	LF	6600
604.182	CLEAN EXISTING CATCH BASIN AND MANHOLE	EA	26
604.184	REBUILDING CATCH BASIN TYPE II	EA	20
604.185	REBUILDING CATCH BASIN TYPE IV	EA	5
606.1307	31" W-BEAM GUARDRAIL - MID-WAY SPLICING	EA	1
606.178	GUARDRAIL BEAM	STATION	STATION DIRECTION LENGTH (FT)
1310-25.00	1313-90.00	NB	365
1318-00.00	1331-73.00	NB	1373
1370-10.00	1378-13.00	NB	803
1381-50.00	1413-06.00	NB	3156
1421-29.00	1446-48.00	NB	2519
1455-48.00	1461-63.00	NB	615
1310-25.00	1331-00.00	SB	2075
1355-50.00	1357-50.00	SB	200
1383-90.00	1405-00.00	SB	2110
1418-91.00	1447-00.00	SB	2809
404.184	REBUILDING CATCH BASIN - TYPE II	STATION	LOCATION QUANTITY
1336-10.00	Shoulders	4	
1355-45.00	NB Outside Shoulder	1	
1357-15.00	NB Outside Shoulder	1	
1370-00.00	Shoulders	4	
1382-00.00	Shoulders	4	
1417-50.00	Shoulders	4	
1450-00.00	Shoulders	2	
604.185	REBUILDING CATCH BASIN - TYPE IV	STATION	LOCATION QUANTITY
1312-05.00	Median	1	
1318-10.00	Median	1	
1323-05.00	Median	1	
1326-45.00	Median	1	
1333-10.00	Median	1	
1336-10.00	Median	1	
1339-05.00	Median	1	
1344-05.00	Median	1	
1349-05.00	Median	1	
1354-05.00	Median	1	
1366-05.00	Median	1	
1370-00.00	Median	1	
1382-00.00	Median	1	
1383-50.00	Median	1	
1390-00.00	Median	1	
1396-00.00	Median	1	
1408-00.00	Median	1	
1414-00.00	Median	1	
1417-50.00	Median	1	
1420-00.00	Median	1	
1425-50.00	Median	1	
1432-00.00	Median	1	
1437-50.00	Median	1	
1450-00.00	Median	1	
1454-00.00	Median	1	
1459-00.00	Median	1	
652.30	FLASHING ARROW BOARD	EA	4
652.35	CONSTRUCTION SIGNS	SF	3000
652.36I	MAINTENANCE OF TRAFFIC CONTROL DEVICES	LS	1
652.410	PORTABLE - CHANGEABLE MESSAGE SIGN	EA	4
652.4502	TRUCK MOUNTED ATTENUATOR	EA	2
652.4503	TRUCK MOUNTED ATTENUATOR	WK	8
656.50	BALED HAY, IN-PLACE	EA	50
659.10	MOBILIZATION	LS	1
661.10	TOLL STIPEND	LS	1

CONSTRUCTION NOTES:

459.06 BITUMINOUS CONCRETE WATERWAY, TYPE I

STATION LOCATION QUANTITY
TO BE USED AT THE RESIDENT ENGINEER'S APPROVAL

459.06I BITUMINOUS CONCRETE WATERWAY, TYPE II

STATION LOCATION QUANTITY
TO BE USED AT THE RESIDENT ENGINEER'S APPROVAL

470.08 BER M DROP OFF CORRECTION - GRINDINGS

STATION STATION DIRECTION LENGTH (FT)

1310-25.00 1313-90.00 NB 365

1318-00.00 1331-73.00 NB 1373

1370-10.00 1378-13.00 NB 803

1381-50.00 1413-06.00 NB 3156

1421-29.00 1446-48.00 NB 2519

1455-48.00 1461-63.00 NB 615

1310-25.00 1331-00.00 SB 2075

1355-50.00 1357-50.00 SB 200

1383-90.00 1405-00.00 SB 2110

1418-91.00 1447-00.00 SB 2809

606.1307 31" W-BEAM GUARDRAIL - MID-WAY SPLICE

FLARED TERMINAL
STATION STATION DIRECTION LENGTH (FT)

TO BE USED AT THE RESIDENT ENGINEER'S APPROVAL

606.47 SINGLE WOOD POST

STATION LOCATION QUANTITY

1383-90 SB 1

606.47I SINGLE OFFSET BLOCK - W-BEAM

STATION LOCATION QUANTITY

1315-39 NB 1

1315-52 NB 1

1315-64 NB 1

1315-77 NB 1

1317-74 NB MEDIAN 1

1366-88 NB 1

606.48 SINGLE GALVANIZED STEEL POST

STATION LOCATION QUANTITY

1315-39 NB 1

1315-52 NB 1

1315-64 NB 1

1315-77 NB 1

1331-74 NB MEDIAN 1

606.362I GUARDRAIL ADJUST, SINGLE RAIL

STATION STATION DIRECTION LENGTH (FT)

1413-06 1413-71 NB 65

1446-48 1447-13 NB 65

1371-42 1372-07 SB 65

1462-21 1462-86 SB 65

DRAINAGE NOTES:

1. NO EXISTING DRAINAGE SHALL BE ABANDONED, REMOVED OR PLUGGED WITHOUT APPROVAL OF THE RESIDENT.
2. PLAIN RIP RAP QUANTITY IS FOR REPAIR ON EXISTING PIPE ENDS AS APPROVED BY THE RESIDENT.

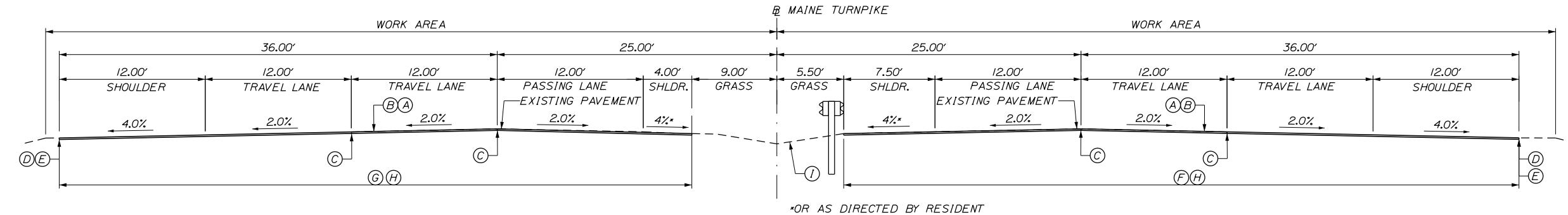
EROSION CONTROL:

1. ADDITIONAL MEASURES MAY BE PROPOSED BY THE CONTRACTOR DUE TO SITE OR WEATHER CONDITIONS. THE RESIDENT MAY DIRECT THE CONTRACTOR TO IMPLEMENT ADDITIONAL MEASURES. ANY ADDITIONAL MEASURES APPROVED BY THE RESIDENT WILL BE MEASURED FOR PAYMENT.
2. ALL TEMPORARY EROSION CONTROL DEVICES SHALL BE INSTALLED IN ACCORDANCE WITH THE MAINE DEPARTMENT OF TRANSPORTATION BEST MANAGEMENT PRACTICES.

GENERAL NOTES:

1. ALL WORK SHALL CONFORM TO THE 2014 MAINE DEPARTMENT OF TRANSPORTATION STANDARD SPECIFICATIONS FOR HIGHWAY AND BRIDGES, EXCEPT AS MODIFIED BY THE MAINE TURNPIKE AUTHORITY'S SUPPLEMENTAL SPECIFICATIONS AND SPECIAL PROVISIONS.
2. EXISTING UTILITIES ON THESE PLANS WERE COMPILED FROM EXISTING PLANS AND VARIOUS OTHER SOURCES. LOCATIONS ARE NOT GUARANTEED TO BE ACCURATE NOR IS IT GUARANTEED THAT ALL UTILITIES ARE SHOWN. NO SEPARATE OR ADDITIONAL COMPENSATION WILL BE ALLOWED TO THE CONTRACTOR DUE TO ANY VARIANCE BETWEEN THE DATA SHOWN ON THE PLANS AND THE ACTUAL FIELD CONDITIONS ENCOUNTERED. NOT WORK SHALL BE STARTED UNTIL THE OWNERS OF THE VARIOUS UTILITIES ARE NOTIFIED BY THE CONTRACTOR OF THE PROPOSED CONSTRUCTION. THE CONTRACTOR IS ALSO REQUIRED TO CALL DIG SAFE AT 1-888-344-7233 AT LEAST 72 HOURS PRIOR TO THE START OF THE WORK.
3. THE CONTRACTOR SHALL NOTIFY ALL NON-MEMBERS THROUGH WWW.OK2DIG.COM OR AS OTHERWISE REQUIRED BY THE MAINE PUBLIC UTILITIES COMMISSION. ALL PROPOSED SIGN LOCATIONS AND EXCAVATION LOCATIONS SHALL BE MARKED AT THE NOTIFICATION TIME. NO EXCAVATION SHALL BE PERMITTED UNTIL THE AUTHORITY HAS LOCATED AND MARKED ITS UNDERGROUND UTILITIES. THE RESIDENT ENGINEER SHALL BE PROVIDED AN ELECTRONIC COPY OF ALL DIG SAFE TICKETS WITHIN 24 HOURS OF THEIR RELEASE FOR PROJECT NOTIFICATIONS AND 3RD PARTY UTILITY LOCATOR COORDINATION.
4. THE CONTRACTOR SHALL NOTIFY THE RESIDENT 10 CALENDAR DAYS PRIOR TO SUBMITTING A UTILITY LOCATE REQUEST THROUGH DIG SAFE SO THAT THE RESIDENT CAN ARRANGE FOR MAINE TURNPIKE UNDERGROUND UTILITY LOCATION. ALL PROPOSED SIGN LOCATIONS AND EXCAVATION LOCATIONS SHALL BE MARKED AT THE NOTIFICATION TIME.
5. FOLLOWING THE COMPLETION OF THE INITIAL UTILITY LOCATE THE CONTRACTOR WILL GPS LOCATE ALL UTILITIES WITHIN THE PROJECT LIMITS AND PROVIDE A COPY OF THE DIG SAFE RECORDS TO THE AUTHORITY. THE CONTRACTOR, ACTING AS THE AUTHORITY'S THIRD PARTY LOCATOR SHALL BE RESPONSIBLE FOR REMARKING ALL MAINE TURNPIKE FACILITIES WHEN A DIG SAFE UTILITY LOCATE IS CALLED FOR IN THE PROJECT. THIS WORK SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
6. EXCAVATIONS ACCOMPLISHED AS PART OF THIS PROJECT SHALL BE CONSTRUCTED IN ACCORDANCE WITH OSHA SUBPART P OF 29 CFR PART 1926.650-652 (CONSTRUCTION STANDARDS FOR EXCAVATIONS).
7. ALL STANDARD DETAILS SHALL BE IN CONFORMANCE WITH MAINE DEPARTMENT OF TRANSPORTATION (MAINE DOT) STANDARD DETAILS HIGHWAY AND BRIDGES 2020 LATEST REVISIONS AND MAINE DOT BEST MANAGEMENT PRACTICES FOR EROSION AND SEDIMENT CONTROL LATEST REVISION UNLESS OTHERWISE INCLUDED IN THESE PLANS.
8. COMMON BORROW SHALL BE COMPACTED TO 90% OF ITS MAXIMUM DRY DENSITY AS DETERMINED BY THE MODIFIED PROCTOR. GRANULAR BORROW AND AGGREGATE SHALL BE COMPACTED TO 95% OF THEIR MAXIMUM DRY DENSITY AS DETERMINED BY THE MODIFIED PROCTOR.
9. ALL STATIONS ARE APPROXIMATE. LOCATIONS TO BE ADJUSTED OR FINALIZED IN FIELD BY RESIDENT.



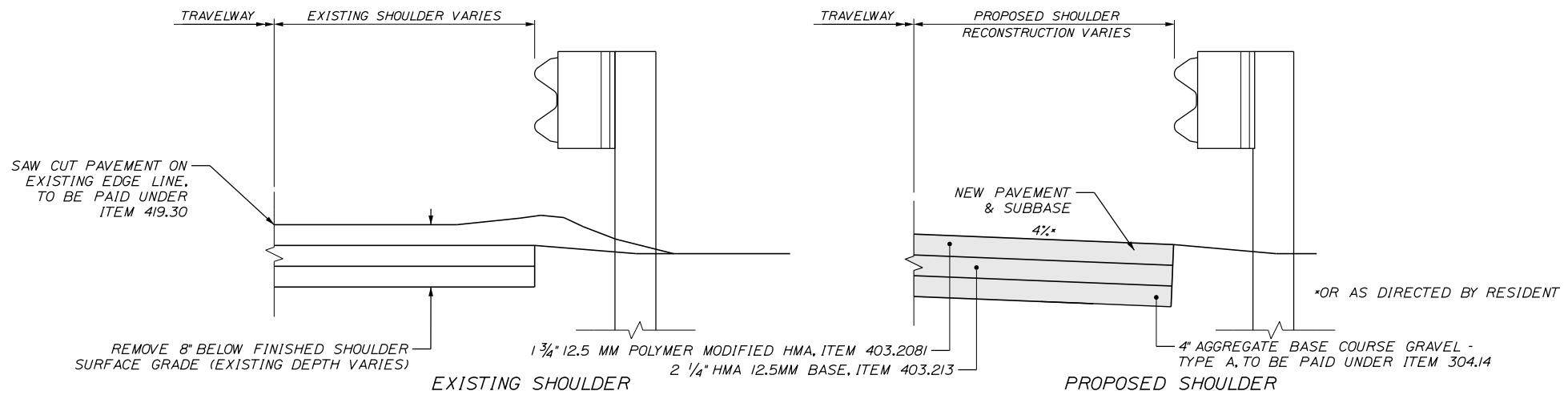


PAVEMENT MILL & FILE

NOT TO SCALE

PAVEMENT LEGEND:

- (A) = *1 3/4" HOT MIX ASPHALT 12.5 mm NOMINAL MAXIMUM SIZE (POLYMER MODIFIED).*
- (B) = *MILLING DEPTH VARIES TO ACHIEVE A 1 3/4" DEPTH AT THE EDGES OF LANE BEING MILLED.*
- (C) = *DOUBLE LAYER OF TACK ON NOTCHED WEDGE*
- (D) = *BERM DROPOFF CORRECTION - GRINDINGS (SEE DETAIL A AND B. SHEET P-2).*
- (E) = *BERM CORRECTION (SEE DETAIL C. SHEET P-2).*
- (F) = *LIMITS OF MILL AND FILL (55'-6").*
- (G) = *LIMITS OF MILL AND FILL (52'-0").*
- (H) = *WHEN MEDIAN GUARDRAIL IS ADJACENT TO THE SHOULDER, MILL TO FACE OF RAIL, OR EDGE OF PAVEMENT, WHICHEVER IS THE LESSER OFFSET*
- (I) = *MEDIAN RESTORATION/REHABILITATION (SEE DETAIL SHEET P-3)*



SHOULDER RECONSTRUCTION

<u>STATION</u>	<u>TO</u>	<u>STATION</u>	<u>DIRECTION</u>
1381+60		1382+40	SB SHOULDER
1118+66		1119+71	NP SHOULDER

NOTES.

STATION	TO	STATION	DIRECTION	OFFSET	1. OTHER LOCATIONS NB & SB TO BE DETERMINED BY RESIDENT AFTER MILLING OF SHOULDER HAS OCCURRED.
1381-60		1382-40	SB SHOULDER	RT.	
1448-66		1448-71	NB SHOULDER	RT.	

▲

2. FOR AREAS OF SHOULDER RECONSTRUCTION, SURFACE PAVEMENT SHALL BE COMPLETED AT SAME TIME AS OTHER MAIN LINE SHOULDER PAVING.

3. ITEM 403.213 SHALL BE MACHINE PLACED.

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Section

NOT TO SCALE

Designed by

TYLin

No.	Revision	By	Date	T T T T T T T T				
1	ADDENDUM *2	TWA	1/2026	CONSULTANT PROJECT MANAGER: Shawn R. Davis				
				By	Date	Checked	By	Date
				Designed		KSD	12/2025	
				Reviewed		CSR	12/2025	

T.Y. Lin International
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Falmouth, Maine 04105
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FAX: (207) 781-4753



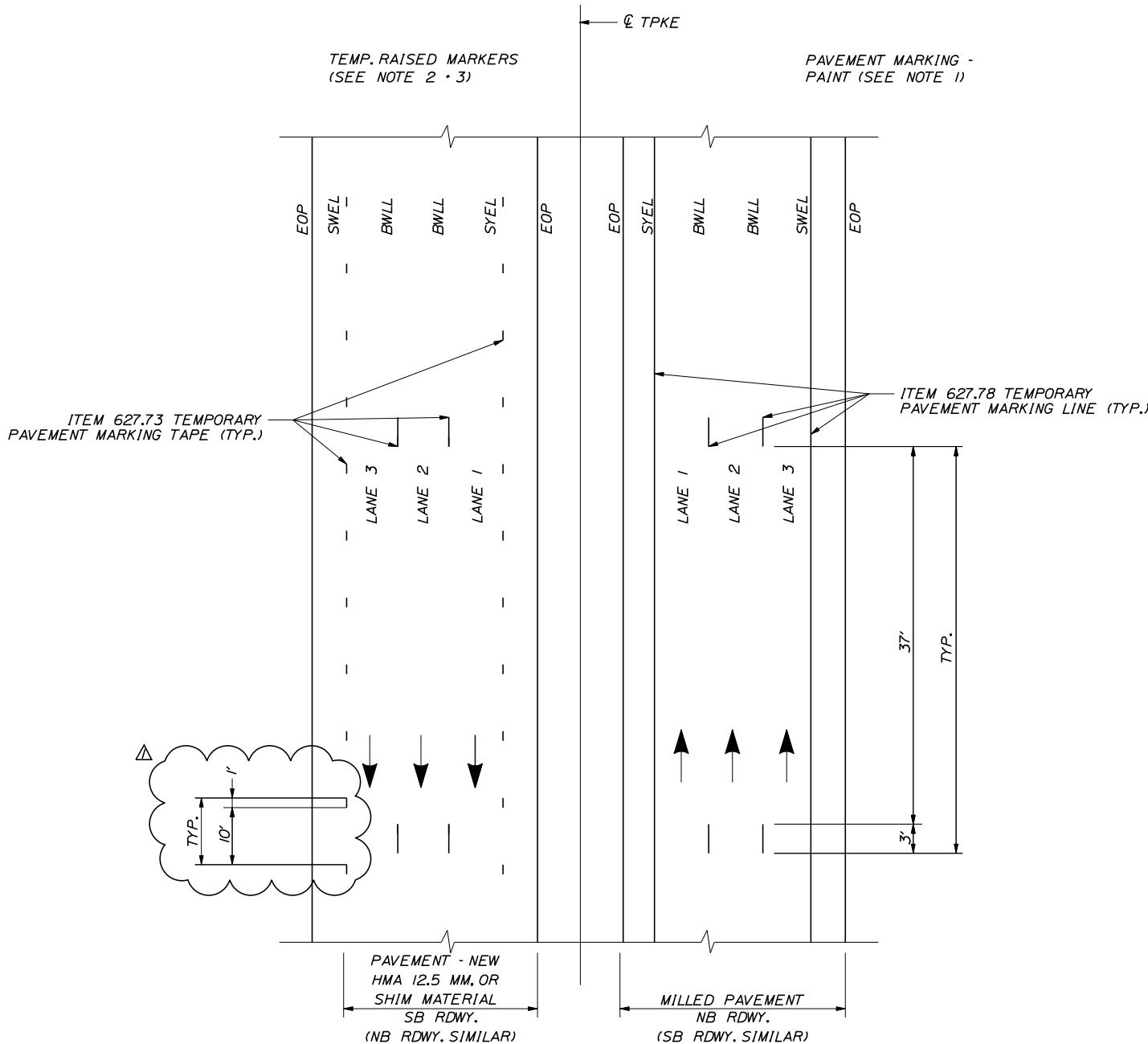
THE GOLD STAR MEMORIAL HIGHWAY

PAVEMENT REHABILITATION AND SAFETY IMPROVEMENTS

TYPICAL SECTIONS

SHEET NUMBER: P

4 OF 18



TEMP. PAVEMENT MARKINGS
3 LANE ROADWAY

NOTES:

1. ON MILLED PAVEMENT TEMPORARY PAVEMENT MARKINGS SHALL BE 6" SOLID WHITE, BROKEN WHITE, AND SOLID YELLOW PAINT LINES.
2. ON NEW HMA 12.5 MM OR SHIM MATERIAL SWEL AND SYEL SHALL BE TEMPORARY PAVEMENT TAPE AND OFFSET FROM FINAL STRIPING LOCATION.
3. ON NEW HMA 12.5 MM OR SHIM MATERIAL BWLL SHALL BE PAVEMENT MARKING TAPE. PIECES SHALL BE 3' PLACED AT 40'-0" O.C. AND OFFSET FROM FINAL STRIPING LOCATION.
4. FINAL PAINTED PAVEMENT MARKINGS WILL BE LAID OUT BY CONTRACTOR AND INSTALLED BY MTA. RESIDENT SHALL BE ALLOWED TO REVIEW LAYOUT.
5. ONCE FINAL PAINTED LINES ARE COMPLETED BY MTA, CONTRACTOR WILL REMOVE ALL TAPE WITHIN 2 DAYS OF FINAL STRIPING.

LEGEND

SYEL = SOLID YELLOW EDGE LINE

SWEL = SOLID WHITE EDGE LINE

BWLL - BROKEN WHITE LINE

EOB - EDGE OF BAVEMENT

NOTES:

1. PAVEMENT MARKINGS SHALL BE 6" WIDE LINES.
2. THE OFFSET DISTANCES SHOWN ARE MEASURED FROM THE CENTER OF THE 6" PAINT LINES.
3. TO BE LAID OUT BY CONTRACTOR AND PAINTED BY MTA

19

Scalable

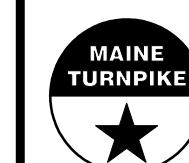
NOT TO SCALE

Designed by

TXLin

No.	Revision	By	Date	
	1	ADDENDUM •2	ETW	1/2026
CONSULTANT PROJECT MANAGER: Shawn R. Davis				
	By	Date	By	Date
	Designed	TWA 6/2025	Checked	KSD 12/2025
	Reviewed	TWA 6/2025	Initials	CRD 12/2025

T.Y. Lin International
12 Northbrook Drive
Building A, Suite One
Falmouth, Maine 04105
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FAX: (207) 781-4753



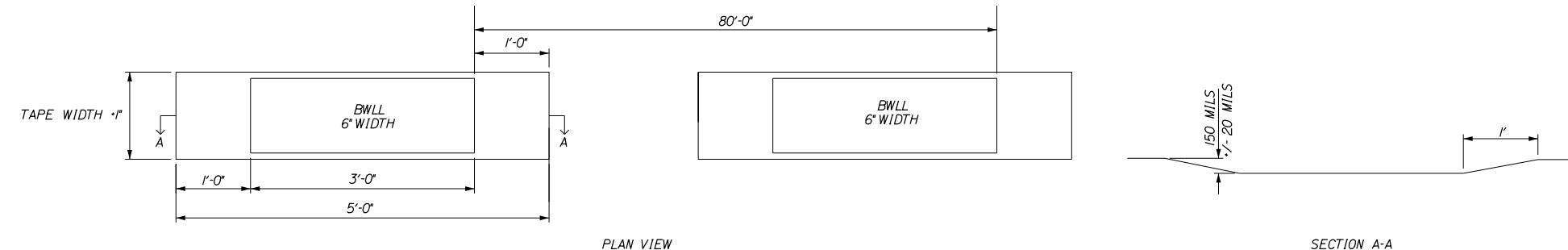
THE GOLD STAR MEMORIAL HIGHWAY

PAVEMENT REHABILITATION AND SAFETY IMPROVEMENTS

PAVEMENT MARKING DETAILS

SHEET NUMBER

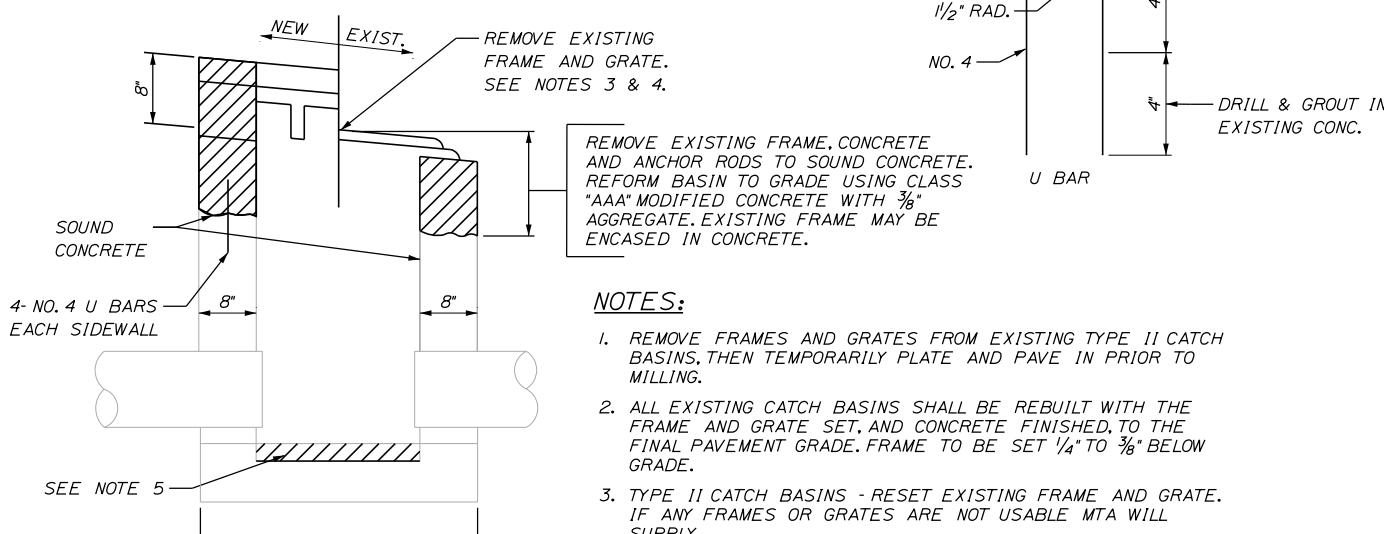
SER. F-3



ITEM 627.94
PERMANENT PAVEMENT
MARKINGS WITH TAPE
GROOVING DETAIL
NTS

NOTES:

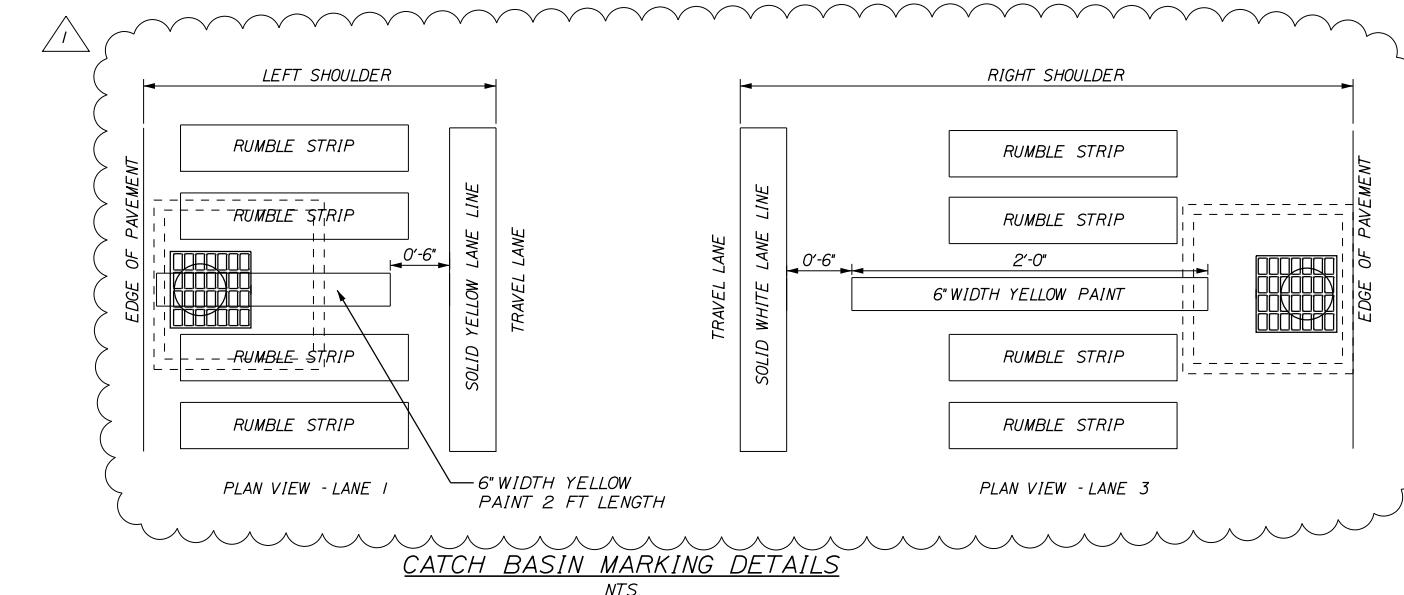
1. ITEM 627.94 PAVEMENT MARKING TAPE FOR DWLL SHALL BE INSTALLED IN AN "IN-AND-OUT" GROOVING PATTERN. SEE DETAIL ON THIS SHEET.
2. ANY CHANGES TO INSTALLATION PROCEDURES THAT VARY FROM SPECIAL PROVISIONS SHALL BE APPROVED BY THE RESIDENT.



NOTES:

1. REMOVE FRAMES AND GRATES FROM EXISTING TYPE II CATCH BASINS, THEN TEMPORARILY PLATE AND PAVE IN PRIOR TO MILLING.
2. ALL EXISTING CATCH BASINS SHALL BE REBUILT WITH THE FRAME AND GRATE SET, AND CONCRETE FINISHED, TO THE FINAL PAVEMENT GRADE. FRAME TO BE SET 1/4" TO 3/8" BELOW GRADE.
3. TYPE II CATCH BASINS - RESET EXISTING FRAME AND GRATE. IF ANY FRAMES OR GRATES ARE NOT USABLE MTA WILL SUPPLY.
4. TYPE IV CATCH BASINS - RESET EXISTING FRAME AND GRATE. IF ANY FRAMES OR GRATES ARE NOT USABLE MTA WILL SUPPLY.
5. REMOVE UNSOUND CONCRETE (2" MIN.) AND REPLACE WITH NEW CONCRETE IF DIRECTED BY RESIDENT.

REBUILD CATCH BASIN TO GRADE - TYPE II & TYPE IV
NOT TO SCALE



CATCH BASIN MARKING NOTES:

1. CATCH BASIN MARKINGS TO BE PLACED ADJACENT TO ALL MEDIAN AND SHOULDER CATCH BASINS.
2. PAYMENT SHALL BE MADE UNDER ITEM 627.75 WHITE OR YELLOW PAVEMENT & CURB MARKING.
3. PAINT COLOR SHALL BE YELLOW.

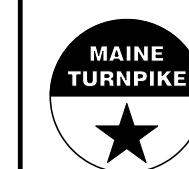
Scale:
NOT TO SCALE

Designed by:

TYLin

No.	Revision	By	Date
1	ADDENDUM *2	TWA	1/2026
CONSULTANT PROJECT MANAGER: Shawn R. Davis			
	By	Date	By
	Designed	TWA	3/2025
	Drawn	TWA	3/2025
		In Charge of	SRD
		Date	12/2025

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**THE GOLD STAR
MEMORIAL HIGHWAY**

MTA PROJECT MANAGER: Lauren Fleming, P.E.

PAVEMENT REHABILITATION
AND SAFETY IMPROVEMENTS
CATCH BASIN
AND GROOVING DETAILS
SHEET NUMBER: P-6

CONTRACT:2026.02

9 OF 18