

MAINE TURNPIKE AUTHORITY

ADDENDUM NO. 2

CONTRACT 2026.02

PAVEMENT REHABILITATION AND
SAFETY IMPROVEMENTS
MM 27.4 TO MM 30.3

QUESTIONS

- Question 1: Will the Authority be providing lump sum price for Item Number 401.03 Balance Mix Design Data Collection?
Response: Yes. See the updated P-Sheets included in this addendum with a \$20,000 price for this item.
- Question 2: Does the Authority anticipate paving the 12' outside shoulders (Right) independently?
Response: Yes. The contractor may pave the outside travel lane and shoulder at the same time if desired. See updates to plans and special provisions.
- Question 3: Will the Authority be providing traffic control for the added work at exit 36 on & off ramps southbound?
Response: No, this shall be done by the Contractor. Please refer to the “GENERAL” section of this Addendum No. 2.
- Question 4: Will both exit 36 southbound ramps be allowed to be closed simultaneously?
Response: No, Exit 36 SB Off Ramp is not required to be closed for the work. The Exit 36 Southbound On Ramp and the Exit 35 Southbound Off Ramp are anticipated to be closed simultaneously for work associated with the rumble strips.
- Question 5: Special Provision Section 403 (SP-52) contract states that "Lane 3 and the twelve foot shoulder shall be pulled as one." Given that placing a full 12-foot shoulder in a single pull while paving the adjacent lane is not feasible with standard paving equipment, could the Authority clarify the intent of this requirement? Specifically, is this intended to mandate same-pass paving, or will phased paving using multiple passes be acceptable?
Response: The contractor will not be required to pave Lane 3 and the outside shoulder in one pull. The outside shoulder may be paved separately or at the same time. The contractor will be required to pave Lane 1 and the median shoulder in one pull.

GENERAL

- The GENERAL section on page 1 of 14, Addendum No. 1 shall be amended to correct the typo for the Off Ramp number. Replace the entire paragraph with the following, “Additional work that consists of installing Rumble Strips – Centerline and Grooved Pavement Strips at Exit 36 is being amended to the contract. Work shall be completed with night ramp closures of Exit 36 SB On Ramp and Exit 35 SB Off Ramp in accordance with Special Provision 652. Maintenance of Traffic will be paid for under 652.361.”

SPECIFICATIONS

- Proposal Sheet P-2 shall be replaced to include the lump sum price of \$20,000 for item 401.03 – Balanced Mix Design Data Collection.
- Proposal Sheet P-5 shall be replaced to reflect a change in the quantity of items 627.73 – Temporary Pavement Marking Tape and 627.75 – White or Yellow Pavement & Curb Marking.
- Special Provision Sheet SP-1 shall be replaced with an updated date for when work will be allowed around the Fourth of July holiday.
- Special Provision Sheet SP-13 shall be replaced to include language for what shall happen if shallow shoulder pavement is encountered.
- Special Provision Sheet SP-52 shall be replaced to revise the third paragraph updating wording for shoulder paving.

PLANS

- Plan Sheet 2 Estimate Quantities & General Notes shall be replaced.
- Plan Sheet 4 Typical Sections shall be replaced to update wording about shoulder paving.
- Plan Sheet 8 Pavement Marking Details shall be replaced to update the length of temporary tape used to mark the outside lane edges.
- Plan Sheet 9 Catch Basin and Grooving Details shall be replaced to update the location and length of paint used to mark catch basin locations.

ATTACHMENTS

- This document – Addendum No. 2 (3 Pages)
- Proposal Sheet P-2 (Revised 1/13/2026) (1 Pages)
- Proposal Sheet P-5 (Revised 1/9/2026) (1 Pages)
- Special Provision Sheet SP-1 (Revised 1/9/2026) (1 Page)
- Special Provision Sheet SP-13 (Revised 1/9/2026) (1 Page)
- Special Provision Sheet SP-52 (Revised 1/13/2026) (1 Page)
- Plan Sheet 2 Estimated Quantities & General Notes (Revised 1/9/2026) (1 Page)
- Plan Sheet 4 Typical Sections (Revised 1/13/2026) (1 Page)
- Plan Sheet 8 Pavement Marking Details (Revised 1/9/2026) (1 Page)
- Plan Sheet 9 Catch Basin and Grooving Details (Revised 1/9/2026) (1 Page)

Notes: The above items shall be considered as part of the bid submittal.

The total number of pages included in this addendum is twelve (12) pages.

All bidders are requested to acknowledge the receipt of the Addendum No. 2 by signing below and email this sheet to Nathaniel Carll, Purchasing Department, Maine Turnpike Authority at ncarll@maineturnpike.com . Bidders are also required to acknowledge receipt of this Addendum No. 2 on Page P-7 of the bid package.

Business Name

Print Name and Title

Signature

Date

Very truly yours,

MAINE TURNPIKE AUTHORITY

Nathaniel Carll

Purchasing Department

Maine Turnpike Authority

SCHEDULE OF BID PRICES**CONTRACT NO. 2026.02****Pavement Rehabilitation and Safety Improvements
MM 27.4 to MM 30.3**

Item No.	Item Description	Units	Approx. Quantities	Unit Prices in Numbers		Bid Amount in Numbers	
				Dollars	Cents	Dollars	Cents
202.202	REMOVING PAVEMENT SURFACE	Square Yard	182,900				
202.205	RUMBLE STRIPS	Each	55,200				
202.206	RUMBLE STRIPS - CENTERLINE	Linear Foot	200				
202.220	GROOVED PAVEMENT STRIPS	Linear Foot	225				
203.20	COMMON EXCAVATION	Cubic Yard	10				
211.50	MEDIAN RESTORATION	Linear Foot	14,200				
304.14	AGGREGATE BASE COURSE GRAVEL, TYPE A	Cubic Yard	5				
401.03	BALANCED MIX DESIGN DATA COLLECTION	Lump Sum	1	\$20,000		\$20,000	
401.10	HOT MIX ASPHALT CONTINUOUS THERMAL PROFILING	Lump Sum	1				
403.2081	HOT MIX ASPHALT - 12.5 MM (POLYMER MODIFIED)	Ton	18,600				

CARRIED FORWARD:	
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Item No.	Item Description	Units	Approx. Quantities	Unit Prices in Numbers		Bid Amount in Numbers	
				Dollars	Cents	Dollars	Cents
BROUGHT FORWARD:							
613.319	EROSION CONTROL BLANKET	Square Yard	12,600				
618.14	SEEDING METHOD NUMBER 2	Unit	260				
619.12	MULCH	Unit	260				
619.1202	TEMPORARY MULCH	Lump Sum	1				
620.58	EROSION CONTROL GEOTEXTILE	Square Yard	20				
627.73	TEMPORARY PAVEMENT MARKING TAPE	Linear Foot	10,200				
627.75	WHITE OR YELLOW PAVEMENT & CURB MARKING	Square Foot	20				
627.78	TEMPORARY PAVEMENT MARKING LINE, WHITE OR YELLOW	Linear Foot	65,900				
627.94	PAVEMENT MARKING TAPE	Linear Foot	1,200				
629.05	HAND LABOR, STRAIGHT TIME	Hour	40				
631.12	ALL PURPOSE EXCAVATOR (INCLUDING OPERATOR)	Hour	25				
631.172	TRUCK - LARGE (INCLUDING OPERATOR)	Hour	25				
CARRIED FORWARD:							

MAINE TURNPIKE AUTHORITYSPECIFICATIONSPART II - SPECIAL PROVISIONS

All work shall be governed by the Maine Department of Transportation Standard Specifications, Revision of November 2014, except for that work which applies to sections of the Maine Department of Transportation Standard Specifications which are amended by the Maine Turnpike Supplemental Specifications and the following modifications, additions and deletions.

General Description of Work

The pavement rehabilitation work consists of milling and filling three travel lanes and the median and outside shoulders for both northbound and southbound approaches. There are short sections where the shoulder pavement will be reconstructed. Excess material in the median will be removed and catch basins shall be cleaned to restore proper drainage. Noted catch basin tops will be replaced, and concrete aprons will be installed around the basins where required. Guardrail height will be adjusted as necessary, with damaged sections of guardrail being replaced where it is needed. Pavement markings, maintenance of traffic and all other work incidental thereto will be completed in accordance with the Plans and Specifications.

Plans

The drawings included in these Contract Documents, and referred to as the Plans, show the general character of the work to be done under this Contract. They bear the general title “Maine Turnpike – Contract 2026.02 –Pavement Rehabilitation and Safety Improvements MM 27.4 to 30.3”. The right is reserved by the Resident to make such minor corrections or alterations in the Plans as he deems necessary without change in the unit prices on the Schedule of Prices of the Proposal.

101.2 DefinitionHolidays

The following is added after Memorial Day in the Supplemental Specifications:

Juneteenth Day 2026 (June 19, 2026)	6:00 a.m. Friday to 6:00 p.m. Friday
Independence Day 2026 (Fourth of July)	12:01 p.m. preceding Thursday to 5:00 p.m. the following Monday.

All surplus pavement grindings, except for the amount specified above, shall be disposed of by the Contractor off the turnpike right-of-way. All grindings shall be disposed of in accordance with the Maine Department of Environmental Protection Solid Waste Management Requirements.

In the event of encountering shoulder gravels during milling, the contractor shall reset and plunge cut fully through pavement into shoulder gravels by 1-2 inches as directed by the Resident. Contractor will be paid double the unit price of Item 202.202 - Removing Pavement Surface, for the area requiring the deeper plunge cut. Type A gravels to restore grade shall be paid under 304.14 – Aggregate Base Course – Type A. No additional payment will be made for this work.

202.07 Method of Measurement

The removal of existing bituminous concrete pavement – mainline will be measured by the square yard of material removed to the required depth.

The following sentences are added:

Transporting and stockpiling of the pavement grindings at the maintenance facilities will not be measured separately for payment, but shall be incidental to the Removing Pavement Surface items.

Installation of temporary bituminous ramps will not be measured separately for payment, but shall be incidental to the Contract.

Removal of temporary bituminous ramps will not be measured separately for payment, but shall be incidental to the Contract.

Installation of and removal of longitudinal safety wedges will not be measured separately for payment, but shall be incidental to the Contract.

Shoulder area requiring a deeper plunge cut to remove thin remaining existing pavement will be measured by the square yard of additional material removed. In effect, the Contractor will be paid double the unit price of Item 202.202 - Removing Pavement Surface, for the area requiring the deeper plunge cut; once for typical depth milling and once for the deeper depth removal. Type A gravels to restore grade shall be measured and paid under 304.14 – Aggregate Base Course – Type A. No additional payment will be made for this work.

202.08 Basis of Payment

Removing Pavement Surface – Mainline will be paid for at unit price per square yard which price shall be full compensation for removing and disposing of the bituminous and gravel materials.

Payment will be made under:

Pay Item

Pay Unit

202.202 Removing Pavement Surface – Mainline
SP - 13

Square Yard

The sand and loose debris adjacent to the median guardrail shall be removed and disposed of by the Contractor off of Turnpike property.

The forty-five degree pavement safety edge needed between adjacent lanes and or shoulders shall be incidental to the 202 pay items.

Lane 1 and the median shoulder shall be pulled as one. Lane 3 and the outside shoulder may be pulled as one.

403.04 Method of Measurement

The construction and removal of temporary ramps on sand joints, and maintaining the ramps will not be measured separately for payment, but shall be incidental to Items 403.

The removal of sand and loose debris will not be measured separately for payment, but shall be incidental to paving items.

Hot Mix Asphalt, 12.5 mm (Polymer/Latex Modified) pavement with (up to) 15% RAP, placed as a wearing surface will be measured under Item 403.2081 Hot Mix Asphalt, 12.5 mm (Polymer/Latex Modified).

403.05 Basis of Payment

Hot Mix Asphalt, 12.5 mm (Polymer/Latex Modified) pavement, placed as a wearing surface will be paid under Item 403.2081 Hot Mix Asphalt, 12.5 mm (Polymer/Latex Modified).

The following pay items are added:

<u>Pay Item</u>	<u>Pay Unit</u>	
403.2081	Hot Mix Asphalt, 12.5 mm (Polymer/Latex Modified)	TON
403.2084	Hot Mix Asphalt, 12.5 mm (incidentals)	TON

Date:1/12/2026

Filename: ... \MSTA 002_general_notes.dgn

ESTIMATED QUANTITIES			
ITEM NO.	ITEM	UNIT	TOTAL QUANTITY
202.202	REMOVING PAVEMENT SURFACE	SY	182900
202.205	RUMBLE STRIPS	EA	55200
202.206	RUMBLE STRIPS - CENTERLINE	LF	200
202.220	GROOVED PAVEMENT STRIPS	LF	225
203.20	COMMON EXCAVATION	CY	10
211.50	MEDIAN RESTORATION	LF	14200
304.14	AGGREGATE BASE COURSE GRAVEL, TYPE A	CY	5
401.03	BALANCED MIX DESIGN DATA COLLECTION	LS	1
401.10	HOT MIX ASPHALT CONTINUOUS THERMAL PROFILING	LS	1
403.2081	HOT MIX ASPHALT - 12.5 MM (POLYMER MODIFIED)	T	18600
403.2084	HOT MIX ASPHALT - 12.5 MM (INCIDENTALS)	T	100
403.211	HOT MIX ASPHALT (SHIMMING)	T	100
403.213	HOT MIX ASPHALT - 12.5 MM (BASE)	T	20
409.152	BITUMINOUS TACK COAT TRACKLESS, APPLIED	G	11100
419.30	SAW CUTTING BITUMINOUS PAVEMENT	LF	600
424.3231	ASPHALT RUBBER MASTIC CRACK SEALER	LB	9800
424.324	ASPHALT RUBBER MASTIC CRACK SEALER WITH AGGREGATE	LB	2500
427.09	CRACK REPAIR	LF	13800
459.06	BITUMINOUS CONCRETE WATERWAY, TYPE I	EA	5
459.061	BITUMINOUS CONCRETE WATERWAY, TYPE II	EA	5
470.08	BERM DROP OFF CORRECTION - GRINDINGS	LF	19300
470.081	BERM CORRECTION	LF	6600
604.182	CLEAN EXISTING CATCH BASIN AND MANHOLE	EA	26
604.184	REBUILDING CATCH BASIN TYPE II	EA	20
604.185	REBUILDING CATCH BASIN TYPE IV	EA	5
606.1307	31" W-BEAM GUARDRAIL - MID-WAY SPLICE FLARED TERMINAL	EA	1
606.178	GUARDRAIL BEAM	LF	200
606.352	REFLECTORIZED BEAM GUARDRAIL DELINEATORS	EA	350
606.3621	GUARDRAIL ADJUST, SINGLE RAIL	LF	1000
606.3622	GUARDRAIL ADJUST, DOUBLE RAIL	LF	2300
606.47	SINGLE WOOD POST	EA	1
606.471	SINGLE OFFSET BLOCK - W-BEAM	EA	15
606.48	SINGLE GALVANIZED STEEL POST	EA	10
610.08	PLAIN RIPRAP	CY	6
613.319	EROSION CONTROL BLANKET	SY	12600
618.14	SEEDING METHOD NUMBER 2	UN	260
619.12	MULCH	UN	260
619.1202	TEMPORARY MULCH	LS	1
620.58	EROSION CONTROL GEOTEXTILE	SY	20
627.73	TEMPORARY PAVEMENT MARKING TAPE	LF	10200
627.75	WHITE OR YELLOW PAVEMENT & CURB MARKING	SF	20
627.78	TEMPORARY PAVEMENT MARKING LINE, WHITE OR YELLOW	LF	65900
627.94	PAVEMENT MARKING TAPE	LF	1200
629.05	HAND LABOR, STRAIGHT TIME	HR	40
631.12	ALL PURPOSE EXCAVATOR (INCLUDING OPERATOR)	HR	25
631.172	TRUCK - LARGE (INCLUDING OPERATOR)	HR	25
631.32	CULVERT CLEANER (INCLUDING OPERATOR)	HR	10
631.36	FOREMAN	HR	25
652.30	FLASHING ARROW BOARD	EA	4
652.35	CONSTRUCTION SIGNS	SF	3000
652.361	MAINTENANCE OF TRAFFIC CONTROL DEVICES	LS	1
652.410	PORTABLE - CHANGEABLE MESSAGE SIGN	EA	4
652.4502	TRUCK MOUNTED ATTENUATOR	EA	2
652.4503	TRUCK MOUNTED ATTENUATOR	WK	8
656.50	BALED HAY, IN-PLACE	EA	50
659.10	MOBILIZATION	LS	1
661.10	TOLL STIPEND	LS	1

CONSTRUCTION NOTES:

459.06 BITUMINOUS CONCRETE WATERWAY, TYPE I
STATION LOCATION QUANTITY
TO BE USED AT THE RESIDENT ENGINEER'S APPROVAL

459.061 BITUMINOUS CONCRETE WATERWAY, TYPE II
STATION LOCATION QUANTITY
TO BE USED AT THE RESIDENT ENGINEER'S APPROVAL

470.08 BERM DROPOFF CORRECTION - GRINDINGS			
STATION	STATION	DIRECTION	LENGTH (FT)
1310+25.00	1313+90.00	NB	365
1318+00.00	1331+73.00	NB	1373
1370+10.00	1378+13.00	NB	803
1381+50.00	1413+06.00	NB	3156
1421+29.00	1446+48.00	NB	2519
1455+48.00	1461+63.00	NB	615
1310+25.00	1331+00.00	SB	2075
1355+50.00	1357+50.00	SB	200
1383+90.00	1405+00.00	SB	2110
1418+91.00	1447+00.00	SB	2809

470.081 BERM CORRECTION	
DIRECTION	LENGTH (FT)
NB Length of Guardrail	3518
SB Length of Guardrail	3075

604.184 REBUILDING CATCH BASIN - TYPE II		
STATION	LOCATION	QUANTITY
1336+10.00	Shoulders	4
1355+45.00	NB Outside Shoulder	1
1357+15.00	NB Outside Shoulder	1
1370+00.00	Shoulders	4
1382+00.00	Shoulders	4
1417+50.00	Shoulders	4
1450+00.00	Shoulders	2

604.185 REBUILDING CATCH BASIN - TYPE IV
STATION LOCATION QUANTITY
TO BE USED AT THE RESIDENT ENGINEER'S APPROVAL

604.182 CLEAN EXISTING CB AND MANHOLE		
STATION	LOCATION	QUANTITY
1312+05.00	Median	1
1318+10.00	Median	1
1323+05.00	Median	1
1326+45.00	Median	1
1333+10.00	Median	1
1336+10.00	Median	1
1339+05.00	Median	1
1344+05.00	Median	1
1349+05.00	Median	1
1354+05.00	Median	1
1366+05.00	Median	1
1370+00.00	Median	1
1382+00.00	Median	1
1383+50.00	Median	1
1390+00.00	Median	1
1396+00.00	Median	1
1408+00.00	Median	1
1414+00.00	Median	1
1417+50.00	Median	1
1420+00.00	Median	1
1425+50.00	Median	1
1432+00.00	Median	1
1437+50.00	Median	1
1450+00.00	Median	1
1454+00.00	Median	1
1459+00.00	Median	1

606.1307 31" W-BEAM GUARDRAIL - MID-WAY SPLICE FLARED TERMINAL
STATION STATION DIRECTION LENGTH (FT)
TO BE USED AT THE RESIDENT ENGINEER'S APPROVAL

606.178 GUARDRAIL BEAM			
STATION	STATION	DIRECTION	LENGTH (FT)
1314+25	1314+63	NB	37.50
1315+39	1315+77	NB	37.50
1331+74	1331+99	NB	25.00
1413+40	1413+65	NB	25.00
1462+25	1462+50	SB	25.00
1418+49	1418+61	SB	12.50
1383+30	1383+55	SB	25.00

606.3621 GUARDRAIL ADJUST, SINGLE RAIL			
STATION	STATION	DIRECTION	LENGTH (FT)
1413+06	1413+71	NB	65
1446+48	1447+13	NB	65
1371+42	1372+07	SB	65
1462+21	1462+86	SB	65

606.47 SINGLE WOOD POST
STATION LOCATION QUANTITY
1383+90 SB 1

606.471 SINGLE OFFSET BLOCK - W-BEAM		
STATION	LOCATION	QUANTITY
1315+39	NB	1
1315+52	NB	1
1315+64	NB	1
1315+77	NB	1
1331+74	NB MEDIAN	1
1366+88	NB	1

606.48 SINGLE GALVANIZED STEEL POST		
STATION	LOCATION	QUANTITY
1315+39	NB	1
1315+52	NB	1
1315+64	NB	1
1315+77	NB	1
1331+74	NB MEDIAN	1

DRAINAGE NOTES:

1. NO EXISTING DRAINAGE SHALL BE ABANDONED, REMOVED OR PLUGGED WITHOUT APPROVAL OF THE RESIDENT.
2. PLAIN RIP RAP QUANTITY IS FOR REPAIR ON EXISTING PIPE ENDS AS APPROVED BY THE RESIDENT.

EROSION CONTROL:

1. ADDITIONAL MEASURES MAY BE PROPOSED BY THE CONTRACTOR DUE TO SITE OR WEATHER CONDITIONS. THE RESIDENT MAY DIRECT THE CONTRACTOR TO IMPLEMENT ADDITIONAL MEASURES. ANY ADDITIONAL MEASURES APPROVED BY THE RESIDENT WILL BE MEASURED FOR PAYMENT.
2. ALL TEMPORARY EROSION CONTROL DEVICES SHALL BE INSTALLED IN ACCORDANCE WITH THE MAINE DEPARTMENT OF TRANSPORTATION BEST MANAGEMENT PRACTICES.

GENERAL NOTES:

1. ALL WORK SHALL CONFORM TO THE 2014 MAINE DEPARTMENT OF TRANSPORTATION STANDARD SPECIFICATIONS FOR HIGHWAY AND BRIDGES, EXCEPT AS MODIFIED BY THE MAINE TURNPIKE AUTHORITY'S SUPPLEMENTAL SPECIFICATIONS AND SPECIAL PROVISIONS.
2. EXISTING UTILITIES ON THESE PLANS WERE COMPLIED FROM EXISTING PLANS AND VARIOUS OTHER SOURCES. LOCATIONS ARE NOT GUARANTEED TO BE ACCURATE NOR IS IT GUARANTEED THAT ALL UTILITIES ARE SHOWN. NO SEPARATE OR ADDITIONAL COMPENSATION WILL BE ALLOWED TO THE CONTRACTOR DUE TO ANY VARIANCE BETWEEN THE DATA SHOWN ON THE PLANS AND THE ACTUAL FIELD CONDITIONS ENCOUNTERED. NOT WORK SHALL BE STARTED UNTIL THE OWNERS OF THE VARIOUS UTILITIES ARE NOTIFIED BY THE CONTRACTOR OF THE PROPOSED CONSTRUCTION. THE CONTRACTOR IS ALSO REQUIRED TO CALL DIG SAFE AT 1-888-344-7233 AT LEAST 72 HOURS PRIOR TO THE START OF THE WORK.
3. THE CONTRACTOR SHALL NOTIFY ALL NON-MEMBERS THROUGH WWW.OKtoDIG.COM OR AS OTHERWISE REQUIRED BY THE MAINE PUBLIC UTILITIES COMMISSION. ALL PROPOSED SIGN LOCATIONS AND EXCAVATION LOCATIONS SHALL BE MARKED AT THE NOTIFICATION TIME. NO EXCAVATION SHALL BE PERMITTED UNTIL THE AUTHORITY HAS LOCATED AND MARKED ITS UNDERGROUND UTILITIES. THE RESIDENT ENGINEER SHALL BE PROVIDED AN ELECTRONIC COPY OF ALL DIG SAFE TICKETS WITHIN 24 HOURS OF THEIR RELEASE FOR PROJECT NOTIFICATIONS AND 3RD PARTY UTILITY LOCATER COORDINATION.
4. THE CONTRACTOR SHALL NOTIFY THE RESIDENT 10 CALENDAR DAYS PRIOR TO SUBMITTING A UTILITY LOCATE REQUEST THROUGH DIG SAFE SO THAT THE RESIDENT CAN ARRANGE FOR MAINE TURNPIKE UNDERGROUND UTILITY LOCATION. ALL PROPOSED SIGN LOCATIONS AND EXCAVATION LOCATIONS SHALL BE MARKED AT THE NOTIFICATION TIME.
5. FOLLOWING THE COMPLETION OF THE INITIAL UTILITY LOCATE THE CONTRACTOR WILL GPS LOCATE ALL UTILITIES WITHIN THE PROJECT LIMITS AND PROVIDE A COPY OF THE DIG SAFE RECORDS TO THE AUTHORITY. THE CONTRACTOR, ACTING AS THE AUTHORITY'S THIRD PARTY LOCATOR SHALL BE RESPONSIBLE FOR REMARKING ALL MAINE TURNPIKE FACILITIES WHEN A DIG SAFE UTILITY LOCATE IS CALLED FOR IN THE PROJECT. THIS WORK SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
6. EXCAVATIONS ACCOMPLISHED AS PART OF THIS PROJECT SHALL BE CONSTRUCTED IN ACCORDANCE WITH OSHA SUBPART P OF 29 CFR PART 1926.650-652 (CONSTRUCTION STANDARDS FOR EXCAVATIONS).
7. ALL STANDARD DETAILS SHALL BE IN CONFORMANCE WITH MAINE DEPARTMENT OF TRANSPORTATION (MAINE DOT) STANDARD DETAILS HIGHWAY AND BRIDGES 2020 LATEST REVISIONS AND MAINE DOT BEST MANAGEMENT PRACTICES FOR EROSION AND SEDIMENT CONTROL LATEST REVISION UNLESS OTHERWISE INCLUDED IN THESE PLANS.
8. COMMON BORROW SHALL BE COMPACTED TO 90% OF ITS MAXIMUM DRY DENSITY AS DETERMINED BY THE MODIFIED PROCTOR. GRANULAR BORROW AND AGGREGATE SHALL BE COMPACTED TO 95% OF THEIR MAXIMUM DRY DENSITY AS DETERMINED BY THE MODIFIED PROCTOR.
9. ALL STATIONS ARE APPROXIMATE, LOCATIONS TO BE ADJUSTED OR FINALIZED IN FIELD BY RESIDENT.

Scale:			
NOT TO SCALE			
No.	Revision	By	Date
1	ADDENDUM *1	TWA	1/2026
2	ADDENDUM *2	TWA	1/2026

Designed by:			
TYLin			
CONSULTANT PROJECT MANAGER: Shawn R. Davis			
	By	Date	
Designed	TWA	10/2025	Checked
Drawn	MJD	10/2025	In Charge of

T.Y. Lin International			
12 Northbrook Drive			
Building A, Suite One			
Falmouth, Maine 04105			
TEL: (207) 781-4721			
FAX: (207) 781-4753			



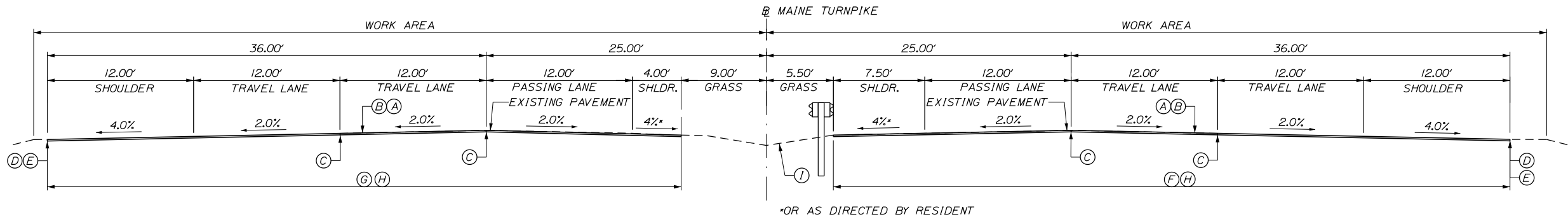
THE GOLD STAR
MEMORIAL HIGHWAY

MTA PROJECT MANAGER: Lauren Fleming, P.E.

PAVEMENT REHABILITATION AND SAFETY IMPROVEMENTS	
ESTIMATED QUANTITIES & GENERAL NOTES	
SHEET NUMBER: C-1	
CONTRACT: 2026.02	2 OF 18

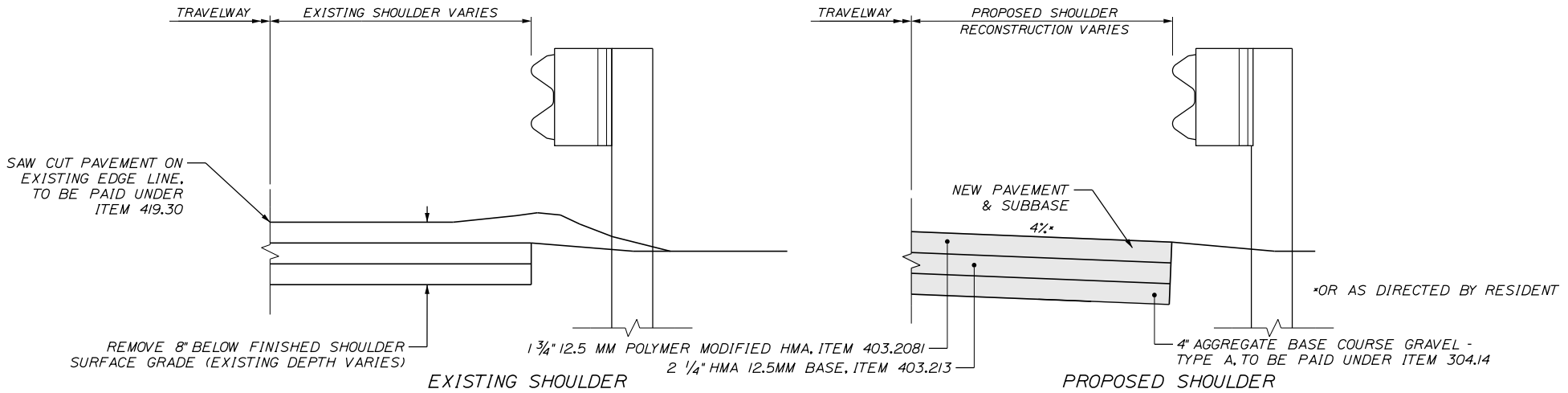
Date:1/13/2026

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PAVEMENT LEGEND:

- (A) = 1 3/4" HOT MIX ASPHALT 12.5 mm NOMINAL MAXIMUM SIZE (POLYMER MODIFIED).
- (B) = MILLING DEPTH VARIES TO ACHIEVE A 1 3/4" DEPTH AT THE EDGES OF LANE BEING MILLED.
- (C) = DOUBLE LAYER OF TACK ON NOTCHED WEDGE
- (D) = BERM DROPOFF CORRECTION - GRINDINGS (SEE DETAIL A AND B, SHEET P-2).
- (E) = BERM CORRECTION (SEE DETAIL C, SHEET P-2).
- (F) = LIMITS OF MILL AND FILL (55'-6").
- (G) = LIMITS OF MILL AND FILL (52'-0").
- (H) = WHEN MEDIAN GUARDRAIL IS ADJACENT TO THE SHOULDER, MILL TO FACE OF RAIL OR EDGE OF PAVEMENT, WHICHEVER IS THE LESSER OFFSET
- (I) = MEDIAN RESTORATION/REHABILITATION (SEE DETAIL, SHEET P-3)



SHOULDER RECONSTRUCTION

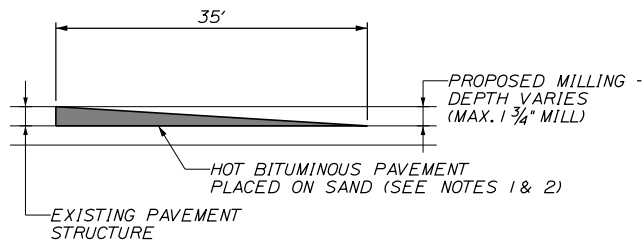
STATION TO	STATION	DIRECTION
1381+60	1382+40	SB SHOULDER
1448+66	1448+71	NB SHOULDER

NOTES:

- OTHER LOCATIONS NB & SB TO BE DETERMINED BY RESIDENT AFTER MILLING OF SHOULDER HAS OCCURRED.
- FOR AREAS OF SHOULDER RECONSTRUCTION, SURFACE PAVEMENT SHALL BE COMPLETED AT SAME TIME AS OTHER MAIN LINE SHOULDER PAVING.
- ITEM 403.213 SHALL BE MACHINE PLACED.

NOTES:

- BITUMINOUS TACK COAT IS REQUIRED BETWEEN ALL LIFTS OF PAVEMENT. BITUMINOUS TACK COAT IS REQUIRED ON ALL EXISTING PAVED OR MILLED SURFACES PRIOR TO PLACING PROPOSED PAVEMENT.
- CROWNS FOR ALL COURSES OF PAVEMENT SHALL BE STRAIGHT.
- PAVEMENT DEPTHS AS SHOWN ON THE PLAN ARE INTENDED TO BE NOMINAL.
- MILLED SURFACES SHALL BE CRACK SEALED PRIOR TO FILLING/OVERLAY USING THE MATERIALS DIRECTED BY THE RESIDENT. PAYMENT FOR CRACK SEALING SHALL BE MADE UNDER ITEM 424.323 OR 424.324.
- EXISTING PAVEMENT SURFACE TO BE OVERLAID SHALL BE CRACK SEALED PRIOR TO OVERLAY USING MATERIALS DIRECTED BY THE RESIDENT. PAYMENT FOR CRACK SEALING SHALL BE MADE UNDER ITEM 424.323 OR 424.324.
- LONGITUDINAL JOINTS SHALL BE CONSTRUCTED AS THE "NOTCHED WEDGE" TYPE.
- A COATING OF HOT RUBBERIZED ASPHALT (ASTM D6690 TYPE IV) SHALL BE APPLIED AT THE TOP SURFACE LIFT TO ALL TRANSVERSE BUTT JOINTS AND LONGITUDINAL JOINTS EXCEPT WHERE THE NOTCHED WEDGE IS USED. INCIDENTAL TO ITEM 403.2081.
- MEDIAN SHOULDERS SHALL BE PAVED IN CONJUNCTION WITH ADJACENT TRAVEL LANE. OUTSIDE SHOULDER MAY BE PAVED INDEPENDENTLY.
- THE EXISTING PAVEMENT UTILIZES A RUBBER GAP GRADED ASPHALT MIXTURE.
- THE CONTRACTOR SHALL MEASURE THE EXISTING CROSS SLOPES FOR LANE 1, 2, 3 AND THE INSIDE AND OUTSIDE SHOULDER EVERY 1/4 MILE AND REPORT TO THE RESIDENT PRIOR TO MILLING. PAVEMENT FOR THIS WORK SHALL BE CONSIDERED INCIDENTAL TO 403 PAY ITEMS.



TEMPORARY BITUMINOUS RAMP
NOT TO SCALE

NOTES:

- HOT MIX ASPHALT FOR TEMPORARY RAMPS WILL NOT BE MEASURED FOR PAYMENT, BUT SHALL BE INCIDENTAL TO ITEM 403.2081.
- REMOVAL OF TEMP. BITUMINOUS RAMPS WILL NOT BE MEASURED FOR PAYMENT, BUT SHALL BE INCIDENTAL TO ITEM 403.2081.

Scale:			
NOT TO SCALE			
No.	Revision	By	Date
1	ADDENDUM #2	TWA	1/2026

Designed by:			
TYLin			
CONSULTANT PROJECT MANAGER: Shawn R. Davis			
	By	Date	
Designed			KSD 12/2025
Drawn		In Charge of	SRD 12/2025

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**THE GOLD STAR
MEMORIAL HIGHWAY**

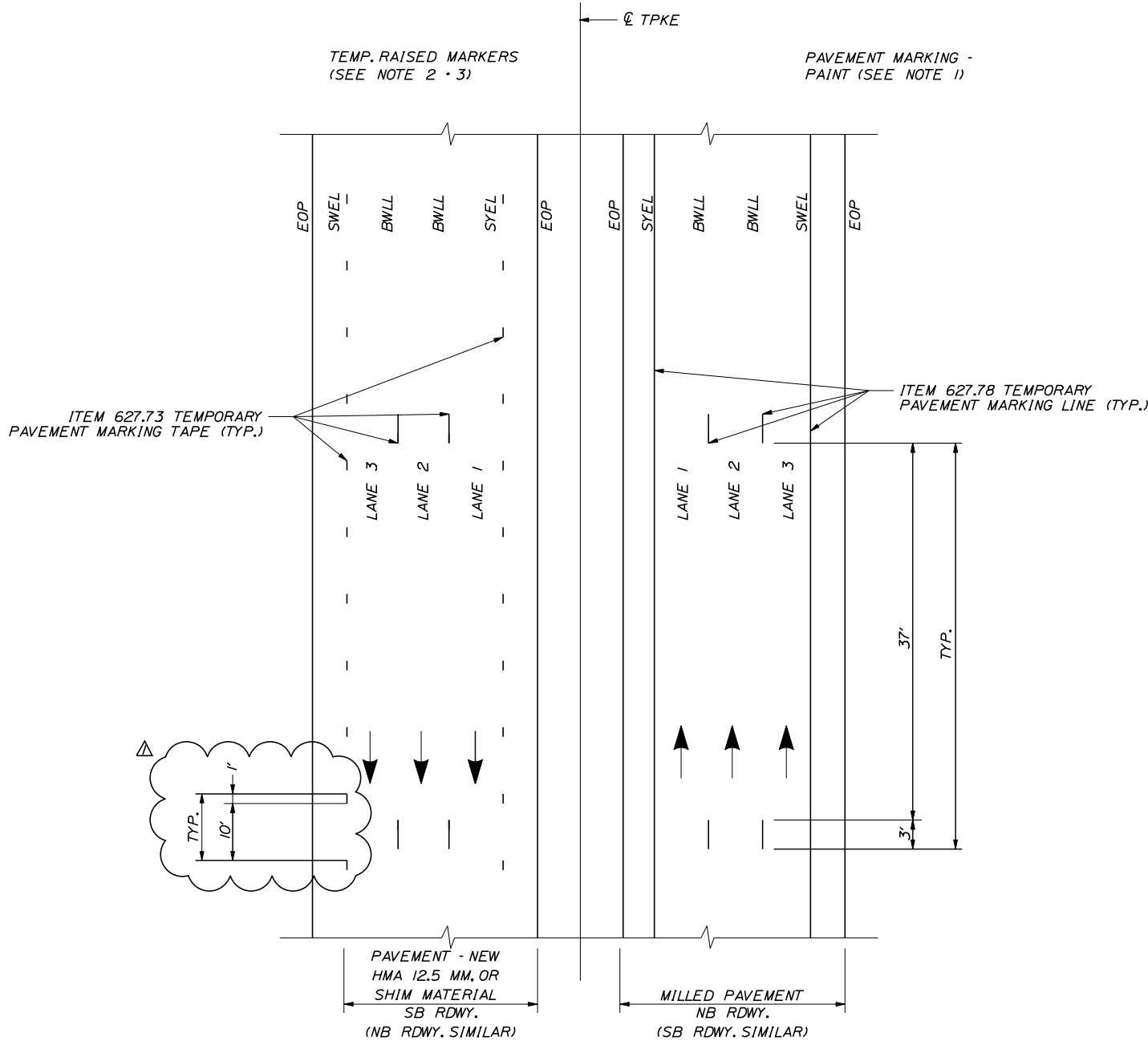
MTA PROJECT MANAGER: Lauren Fleming, P.E.

**PAVEMENT REHABILITATION
AND SAFETY IMPROVEMENTS**

TYPICAL SECTIONS

SHEET NUMBER: P-1

4 OF 18



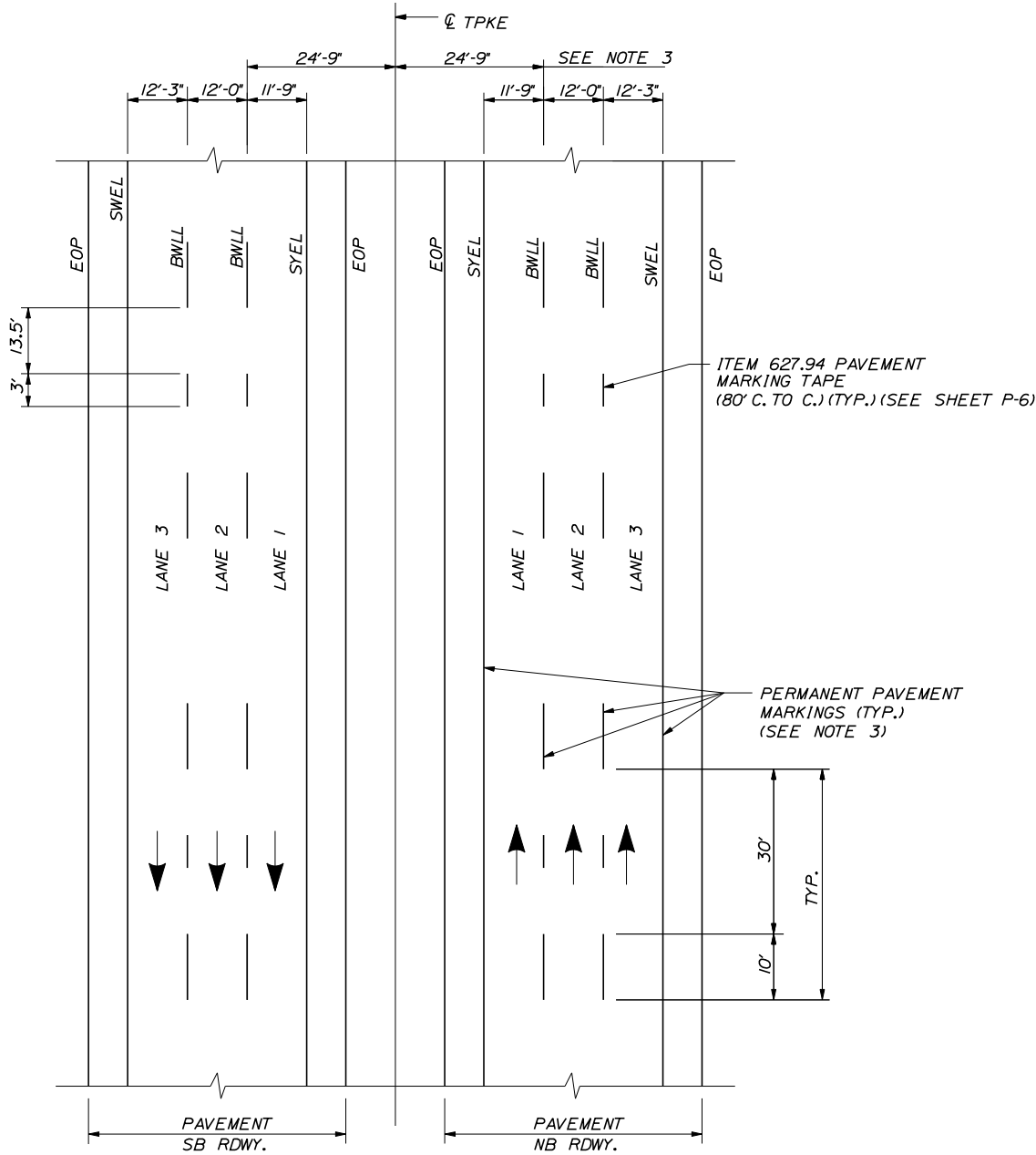
TEMP. PAVEMENT MARKINGS
3 LANE ROADWAY

NOTES:

1. ON MILLED PAVEMENT TEMPORARY PAVEMENT MARKINGS SHALL BE 6" SOLID WHITE, BROKEN WHITE, AND SOLID YELLOW PAINT LINES.
2. ON NEW HMA 12.5 MM OR SHIM MATERIAL SWEL AND SYEL SHALL BE TEMPORARY PAVEMENT TAPE AND OFFSET FROM FINAL STRIPING LOCATION.
3. ON NEW HMA 12.5 MM OR SHIM MATERIAL BWLL SHALL BE PAVEMENT MARKING TAPE. PIECES SHALL BE 3' PLACED AT 40'-0" O.C. AND OFFSET FROM FINAL STRIPING LOCATION.
4. FINAL PAINTED PAVEMENT MARKINGS WILL BE LAID OUT BY CONTRACTOR AND INSTALLED BY MTA. RESIDENT SHALL BE ALLOWED TO REVIEW LAYOUT.
5. ONCE FINAL PAINTED LINES ARE COMPLETED BY MTA, CONTRACTOR WILL REMOVE ALL TAPE WITHIN 2 DAYS OF FINAL STRIPING.

LEGEND

- SYEL = SOLID YELLOW EDGE LINE
SWEL = SOLID WHITE EDGE LINE
BWLL = BROKEN WHITE LANE LINE
EOP = EDGE OF PAVEMENT



PAVEMENT MARKING DETAIL
3 LANE ROADWAY

NOTES:

1. PAVEMENT MARKINGS SHALL BE 6" WIDE LINES.
2. THE OFFSET DISTANCES SHOWN ARE MEASURED FROM THE CENTER OF THE 6" PAINT LINES.
3. TO BE LAID OUT BY CONTRACTOR AND PAINTED BY MTA.

Scale: NOT TO SCALE			
No.	Revision	By	Date
1	ADDENDUM #2	ETW	1/2026

Designed by: TYLin			
CONSULTANT PROJECT MANAGER: Shawn R. Davis			
	By	Date	
Designed	TWA	6/2025	Checked KSD 12/2025
Drawn	TWA	6/2025	In Charge of SRD 12/2025

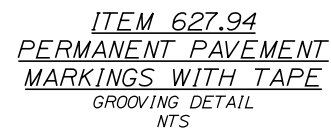
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**THE GOLD STAR
MEMORIAL HIGHWAY**

MTA PROJECT MANAGER: Lauren Fleming, P.E.

PAVEMENT REHABILITATION AND SAFETY IMPROVEMENTS	
PAVEMENT MARKING DETAILS	
SHEET NUMBER: P-5	
CONTRACT: 2026.02	8 OF 18



1. ITEM 627.94 PAVEMENT MARKING TAPE FOR DWLL SHALL BE INSTALLED IN AN "IN-AND-OUT" GROOVING PATTERN. SEE DETAIL ON THIS SHEET.
2. ANY CHANGES TO INSTALLATION PROCEDURES THAT VARY FROM SPECIAL PROVISIONS SHALL BE APPROVED BY THE RESIDENT.

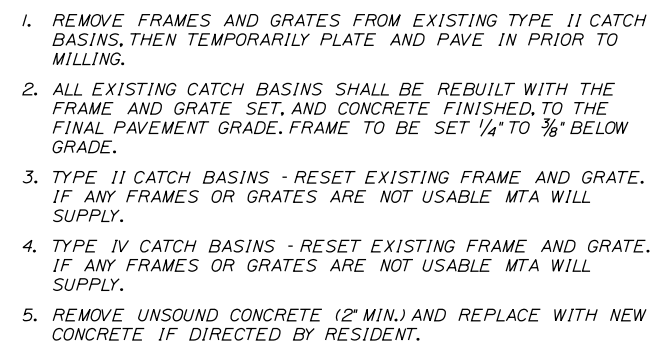


Diagram illustrating Catch Basin Marking Details for Lane 1 and Lane 3.

Left Shoulder: RUMBLE STRIP, RUMBLE STRIP, RUMBLE STRIP, RUMBLE STRIP.

Travel Lane: SOLID YELLOW LANE LINE (Lane 1), SOLID WHITE LANE LINE (Lane 3).

Right Shoulder: RUMBLE STRIP, RUMBLE STRIP, RUMBLE STRIP, RUMBLE STRIP.



Plan View - Lane 1: Shows a 6" WIDTH YELLOW PAINT 2 FT LENGTH marking.

Plan View - Lane 3: Shows a 6" WIDTH YELLOW PAINT 2 FT LENGTH marking.

Dimensions: 0'-6" (Lane 1), 2'-0" (Lane 3).

Labels: EDGE OF PAVEMENT, TRAVEL LANE, CATCH BASIN MARKING DETAILS, NTS.

1. CATCH BASIN MARKINGS TO BE PLACED ADJACENT TO ALL MEDIAN AND SHOULDER CATCH BASINS.
2. PAYMENT SHALL BE MADE UNDER ITEM 627.75 WHITE OR YELLOW PAVEMENT & CURB MARKING.
3. PAINT COLOR SHALL BE YELLOW.

Scale:				Designed by:			
NOT TO SCALE							
No.	Revision	By	Date				
1	ADDENDUM *2	TWA	1/2026				
				CONSULTANT PROJECT MANAGER: Shawn R. Davis			
					By	Date	
				Designed	TWA	3/2025	Checked
				Drawn	TWA	3/2025	In Charge of
					SRD	12/2025	
							
				THE GOLD STAR MEMORIAL HIGHWAY			
				MTA PROJECT MANAGER: Lauren Fleming, P.E.			
				PAVEMENT REHABILITATION AND SAFETY IMPROVEMENTS CATCH BASIN AND GROOVING DETAILS			
				SHEET NUMBER: P-6			
				CONTRACT: 2026.02			
				9 OF 18			