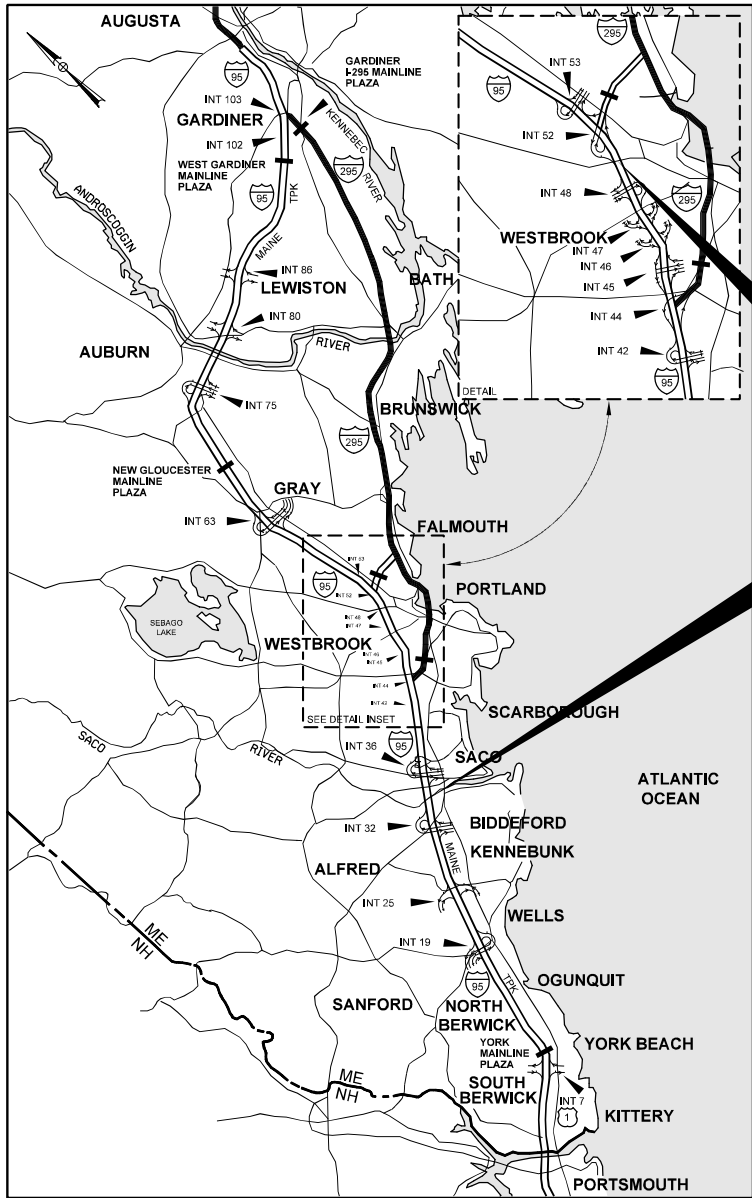


Date: 8/6/2025

Filename: 001_Title_Sheet.dgn



LOCATION MAP



THE GOLD STAR
MEMORIAL HIGHWAY

MAINE TURNPIKE AUTHORITY

MICHAEL J. CIANCHETTE, FALMOUTH, CHAIRMAN
JANE L. LINCOLN, FARMINGDALE, VICE-CHAIR
EMILY N. BECKER, POLAND, MEMBER
NINA A. FISHER, FARMINGDALE, MEMBER
ANDREW J. MCLEAN, GORHAM, MEMBER
THOMAS J. ZUKE, SACO, MEMBER
DALE F. DOUGHTY, MAINEDOT EX-OFFICIO

ANDRE J. BRIERE, COLONEL, USAF (ret.), EXECUTIVE DIRECTOR

CONTRACT 2025.17 BRIDGE REPAIRS SACO RIVER OVERPASS SOUTHBOUND (MM 33.0) FOREST AVENUE OVERPASS (MM 50.0)

INDEX OF SHEETS

SHEET NO.	DESCRIPTION
1	TITLE SHEET
2	GENERAL NOTES & QUANTITIES
3	GUARDRAIL DETAIL
<u>SACO RIVER OVERPASS</u>	
4-8	MAINTENANCE OF TRAFFIC
9	CURB REPAIR PLAN & ELEVATION
10	CURB REPAIR DETAILS
11	REINFORCING SCHEDULE
12	EXISTING RAILING STANDARD DETAILS
13	AS-BUILTS (SACO RIVER BRIDGE)
<u>FOREST AVENUE OVERPASS</u>	
14-16	FOREST AVENUE MOT
17	FOREST AVENUE OVERPASS DECK REPAIR PLAN
18	FOREST AVENUE OVERPASS DECK REPAIR DETAILS

APPROVED: MAINE TURNPIKE AUTHORITY

Stephen R. Tartre
STEPHEN R. TARTRE, P.E., - CHIEF ENGINEER/DIRECTOR OF ENGINEERING

7/30/25
DATE



Timothy R. Cote
TIM COTE P.E.
VICE PRESIDENT | PROJECT DIRECTOR

7/25/2025
DATE

CONTRACT 2025.17

Date:8/5/2025

GENERAL NOTES

1. ALL DETAILS SHALL BE IN CONFORMANCE WITH MAINE DEPARTMENT OF TRANSPORTATION (MaineDOT) STANDARD DETAILS HIGHWAYS AND BRIDGES LATEST REVISION AND MaineDEP BEST MANAGEMENT PRACTICES FOR EROSION AND SEDIMENT CONTROL LATEST REVISION UNLESS OTHERWISE INCLUDED IN THESE PLANS.
2. WASTE MATERIALS SHALL BE DISPOSED OF OFF THE PROJECT SITE AND IN ACCORDANCE WITH ALL ENVIRONMENTAL REGULATIONS.
3. MATERIALS AND EQUIPMENT SHALL NOT BE STORED UNDER OR IN CLOSE PROXIMITY TO HIGHWAY STRUCTURES UNLESS THE CONTRACTOR RECEIVES WRITTEN PERMISSION FROM THE AUTHORITY.
4. COPIES OF THE AS-BUILT PLANS ARE POSTED ON THE MAINE TURNPIKE AUTHORITY WEBSITE AT WWW.MAINETURNPIKE.COM/PROJECTS/CONSRUCTION-CONTRACTS.THE COMPLETENESS AND ACCURACY OF THESE PLANS IS NOT GUARANTEED.
5. CHAMFER ALL EXPOSED NEW CONCRETE EDGES ¾"UNLESS OTHERWISE NOTED.
6. NO EXISTING DRAINAGE SHALL BE ABANDONED,REMOVED OR PLUGGED WITHOUT APPROVAL OF THE RESIDENT.
7. SPEED ZONE DATA COLLECTION DEVICES ARE REQUIRED AT SACO RIVER MAINLINE OVERPASS AND FOREST AVENUE MAINLINE OVERPASS.CONTRACTOR SHALL COORDINATE WITH THE AUTHORITY AND RESIDENT ON PLACEMENT OF DEVICES PRIOR TO LAYING OUT MAINTENANCE OF TRAFFIC CONTROL DEVICES.

UTILITY NOTES

1. EXISTING UTILITIES ON THESE PLANS WERE COMPILED FROM EXISTING PLANS AND VARIOUS OTHER SOURCES. LOCATIONS ARE NOT GUARANTEED TO BE ACCURATE NOR IS IT GUARANTEED THAT ALL UTILITIES ARE SHOWN. NO SEPARATE OR ADDITIONAL COMPENSATION WILL BE ALLOWED TO THE CONTRACTOR DUE TO ANY VARIANCE BETWEEN THE DATA SHOWN ON THE PLANS AND THE ACTUAL FIELD CONDITIONS ENCOUNTERED.THE CONTRACTOR IS REQUIRED TO CALL DIG SAFE AT 1-888-344-7233 AT LEAST 72 HOURS PRIOR TO THE START OF WORK.ALL PROPOSED SIGN AND EXCAVATION LOCATIONS SHALL BE MARKED AT THE NOTIFICATION TIME.THE RESIDENT ENGINEER SHALL BE PROVIDED AN ELECTRONIC COPY OF ALL DIG SAFE TICKETS WITHIN 24 HOURS OF THEIR RELEASE FOR PROJECT NOTIFICATIONS AND 3RD PARTY UTILITY LOCATOR COORDINATION.
2. THE CONTRACTOR SHALL NOTIFY ALL NON-MEMBERS THROUGH WWW.OKTODIG.COM OR AS OTHERWISE REQUIRED BY THE MAINE PUBLIC UTILITIES COMMISSION. ALL PROPOSED SIGN AND EXCAVATION LOCATIONS SHALL BE MARKED AT THE NOTIFICATION TIME. THE RESIDENT ENGINEER SHALL BE PROVIDED AN ELECTRONIC COPY OF ALL NON-MEMBER NOTIFICATIONS WITHIN 24 HOURS OF THEIR RELEASE
3. THE CONTRACTOR SHALL NOTIFY THE RESIDENT 10 CALENDAR DAYS PRIOR TO SUBMITTING ANY UTILITY LOCATE REQUESTS AS NOTED ABOVE SO THAT THE RESIDENT CAN ARRANGE FOR MAINE TURNPIKE UNDERGROUND UTILITY LOCATION. ALL PROPOSED SIGN AND EXCAVATION LOCATIONS SHALL BE MARKED AT THE NOTIFICATION TIME.NO EXCAVATION SHALL BE PERMITTED UNTIL THE AUTHORITY HAS LOCATED AND MARKED ITS UNDERGROUND UTILITIES.
4. FOLLOWING THE COMPLETION OF THE INITIAL UTILITY LOCATE THE CONTRACTOR WILL GPS LOCATE ALL UTILITIES WITHIN THE PROJECT LIMITS AND PROVIDE A COPY OF THE DIG SAFE RECORDS TO THE AUTHORITY. THE CONTRACTOR,ACTING AS THE AUTHORITY'S THIRD-PARTY LOCATOR,SHALL BE RESPONSIBLE FOR REMARKING ALL MAINE TURNPIKE FACILITIES WHEN A DIG SAFE UTILITY IS CALLED FOR THE PROJECT. THIS WORK SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.

EROSION CONTROL

1. ADDITIONAL MEASURES MAY BE PROPOSED BY THE CONTRACTOR DUE TO SITE OR WEATHER CONDITIONS.THE RESIDENT MAY DIRECT THE CONTRACTOR TO IMPLEMENT ADDITIONAL MEASURES, ANY ADDITIONAL MEASURES APPROVED BY THE RESIDENT WILL BE MEASURED FOR PAYMENT.
2. ALL TEMPORARY EROSION CONTROL DEVICES SHALL BE INSTALLED IN ACCORDANCE WITH THE MAINE DEPARTMENT OF TRANSPORTATION BEST MANAGEMENT PRACTICES.
3. A SILT SOCK OR SIMILAR SEDIMENT BARRIER SHALL BE INSTALLED AND MAINTAINED AROUND EACH BRIDGE SCUPPER.

MATERIALS

CONCRETE

ALL CONCRETE MEASURED FOR PAYMENT UNDER SECTION 502 SHALL BE CLASS AAA

REINFORCING STEEL

AASHTO M31,GRADE 60. ALL REINFORCING SHALL BE EPOXY-COATED.

ESTIMATED QUANTITIES					
ITEM NO.	ITEM DESCRIPTION	UNIT	SACO QUANTITY	FOREST QUANTITY	TOTAL QUANTITY
202.17	REMOVING EXISTING STRUCTURAL CONCRETE (48 CY)	LS	1		1
202.202	REMOVING PAVEMENT SURFACE - MAINLINE	SY		1900	1900
202.206	REMOVING RUMBLE STRIPS	LF	1800		1800
403.209	HOT MIX ASPHALT, 9.5 MM (SIDEWALKS, DRIVES & INCIDENTALS)	TON		8	8
403.2101	HOT MIX ASPHALT, 9.5 MM NOMINAL MAXIMUM SIZE (POLYMER MODIFIED)	TON		170	170
409.15	BITUMINOUS TACK COAT	GAL		100	100
502.49	STRUCTURAL CONCRETE CURBS AND SIDEWALKS (48 CY)	LS	1		1
503.14	EPOXY-COATED REINFORCING STEEL, FABRICATED AND DELIVERED	LB	7300		7300
503.15	EPOXY-COATED REINFORCING STEEL, PLACING	LB	7300		7300
507.17	ALUMINUM BRIDGE RAILING REPAIR AND RESET (1250 LF RESET, 30 LF REPAIR)	LS	1		1
508.14	HIGH PERFORMANCE WATERPROOFING MEMBRANE (92 SF)	LS		1	1
515.202	CLEAR PROTECTIVE COATING FOR CONCRETE SURFACES	SY	900		900
518.60	REPAIR OF VERTICAL SURFACES < 8 INCHES	SF	50		50
518.70	REPAIR OF OVERHEAD SURFACES <8 INCHES	SF		60	60
518.80	PARTIAL DEPTH CONCRETE DECK REPAIRS	SF		270	270
518.81	FULL DEPTH CONCRETE DECK REPAIRS	SF		60	60
520.23	ASPHALTIC PLUG JOINT	LF		90	90
526.306	TEMPORARY CONCRETE BARRIER,TYPE 1- SUPPLIED BY AUTHORITY (2050 LF)	LS	1		1
527.341	WORK ZONE CRASH CUSHIONS, TL-3	UN	1		1
606.24	GUARDRAIL TYPE 3D - SINGLE RAIL	LF	300		300
627.73	TEMPORARY 6 INCH PAVEMENT MARKING TAPE	LF	10500		10500
627.731	TEMPORARY 6 INCH BLACK PAVEMENT MARKING TAPE	LF	5050		5050
627.77	REMOVING EXISTING PAVEMENT MARKING	SF	7800		7800
627.78	TEMPORARY 6" PAVEMENT MARKING LINE, WHITE OR YELLOW	LF	10500		10500
629.05	HAND LABOR, STRAIGHT TIME	HR	10	10	20
631.10	AIR COMPRESSOR (INCLUDING OPERATOR)	HR	10	10	20
631.11	AIR TOOL (INCLUDING OPERATOR)	HR	10	10	20
631.172	TRUCK - LARGE (INCLUDING OPERATOR)	HR	10	10	20
631.36	FOREMAN	HR	10	10	20
652.30	FLASHING ARROW	EA	1	2	3
652.33	DRUM	EA	34	200	234
652.34	CONE	EA	34	200	234
652.35	CONSTRUCTION SIGNS	SF	530	880	1410
652.36	MAINTENANCE OF TRAFFIC CONTROL DEVICES - FOREST AVE. OVERPASS	CD		25	25
652.361	MAINTENANCE OF TRAFFIC CONTROL DEVICES - SACO RIVER OVERPASS	LS	1		1
652.38	FLAGGERS	HR		120	120
652.41	PORTABLE CHANGEABLE MESSGE SIGN	EA	1	2	3
652.451	TRUCK MOUNTED ATTENUATOR	EA		4	4
659.10	MOBILIZATION	LS	1	1	2

Filename: 002_Quantities and General Notes.dgn

Filename: 002_Quantities and Materials

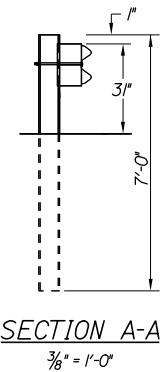
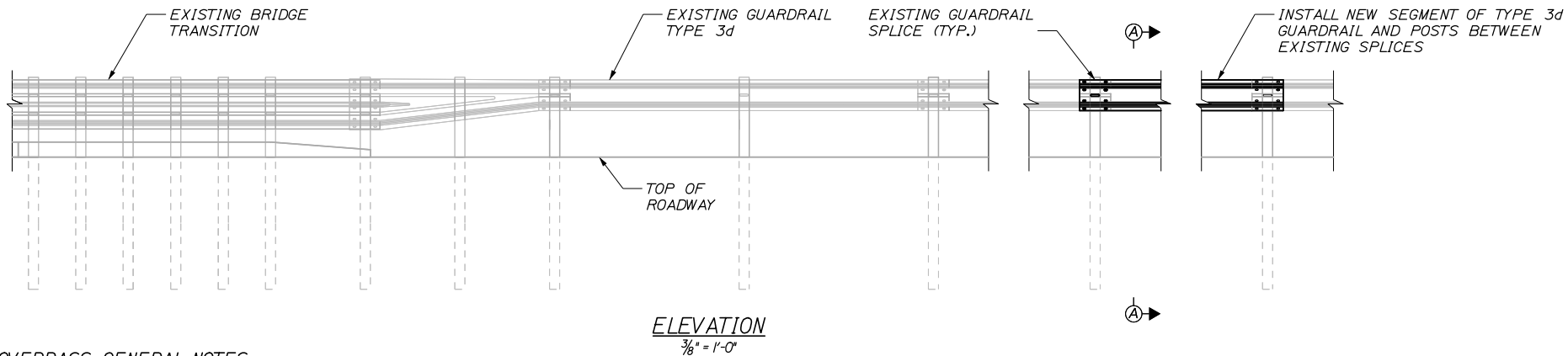
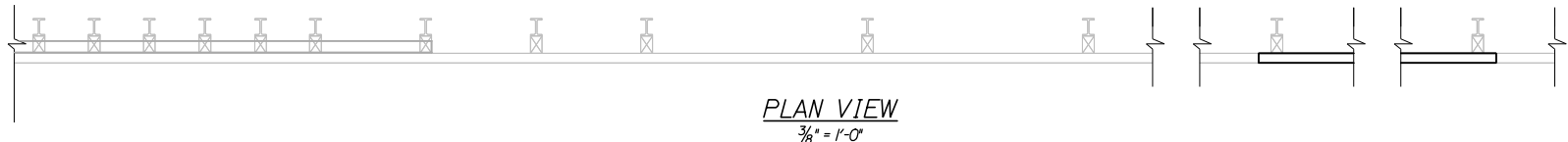
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No.	Revision	By	Date	CONSULTANT PROJECT MANAGER: Timothy R. Cote, P.E.								GENERAL NOTES AND QUANTITIES			
					By	Date		By	Date	SHEET NUMBER: GN-01					
				Designed	BRG	07\25	Checked	JDW	07\25	CONTRACT:2025.17					
				Drawn	ERB	07\25	In Charge of	TRC	07\25	2 OF 18					

Date: 8/5/2025

Filename: 003_Guardrail Details.dgn

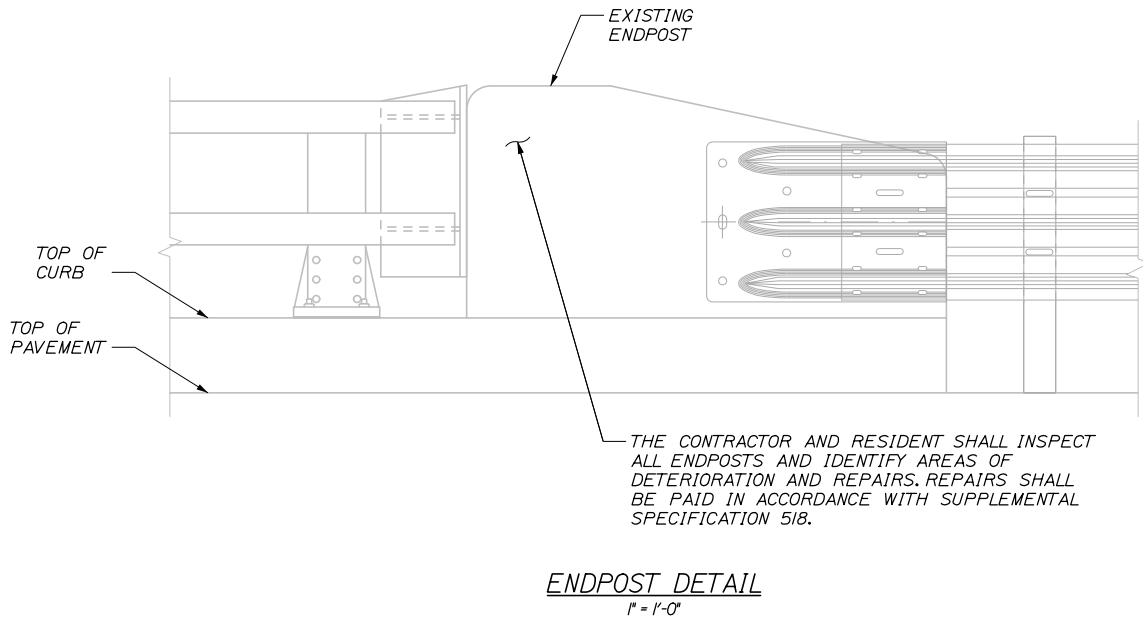
GUARDRAIL NOTES:

1. RAIL ELEMENT SHALL MEET ALL REQUIREMENTS OF AASHTO M-180 EXCEPT AS MODIFIED ON THE PLANS.
2. A GAP (APPROXIMATELY 100') IN THE EXISTING GUARDRAIL EXISTS ON BOTH THE OUTSIDE AND MEDIAN SIDE APPROACHES TO ALLOW LAP IN TEMPORARY BARRIER BEHIND THE GUARDRAIL. GUARDRAIL SHALL BE INSTALLED TO CONNECT THE GAP BETWEEN EXISTING SPLICES ON THE OUTSIDE AND MEDIAN SIDE. GUARDRAIL SHALL BE INSTALLED BEFORE REMOVING BARRIER FROM MOT PHASE.



SACO RIVER OVERPASS GENERAL NOTES:

1. LINE STRIPING FOR MAINTENANCE OF TRAFFIC SHALL BE COMPLETED WITH EITHER TEMPORARY TAPE OR TEMPORARY PAINT AS DIRECTED BY THE RESIDENT.
2. PAVEMENT MARKINGS SHALL BE LAID OUT BY CONTRACTOR FOR FINAL STRIPING. FINAL STRIPING TO BE COMPLETED BY THE AUTHORITY.



Scale: AS NOTED			
No.	Revision	By	Date

Designed by: HNTB					
CONSULTANT PROJECT MANAGER: Timothy R. Cote, P.E.					
	By	Date		By	Date
Designed	BRG	07\25	Checked	PDB	07\25
Drawn	ERB	07\25	In Charge of	TRC	07\25

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THE GOLD STAR
MEMORIAL HIGHWAY

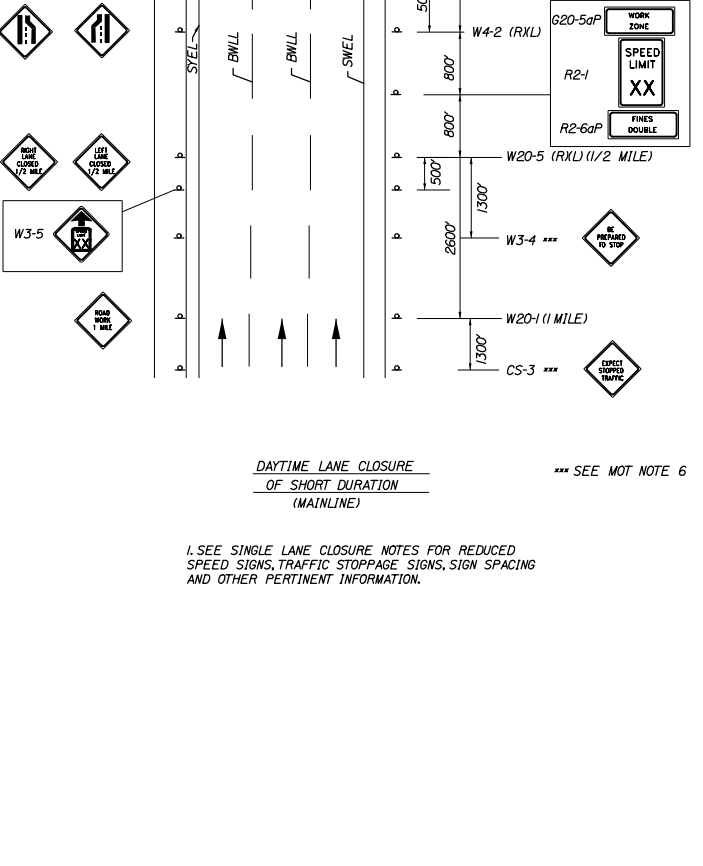
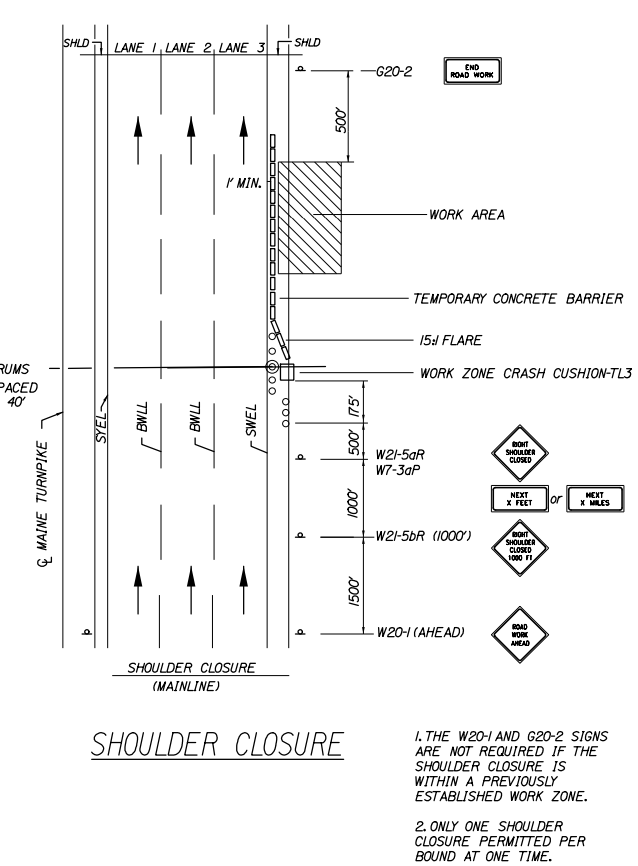
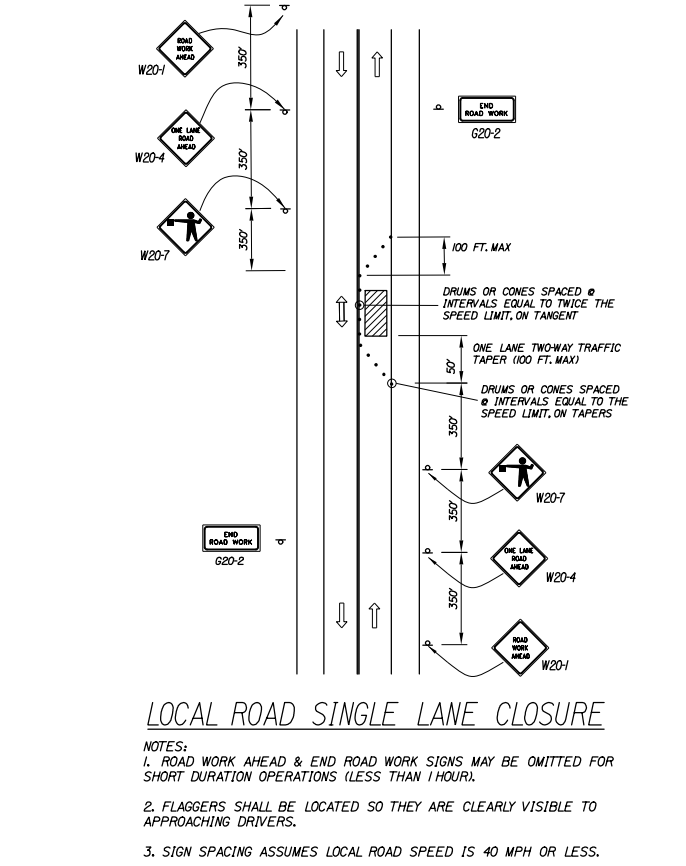
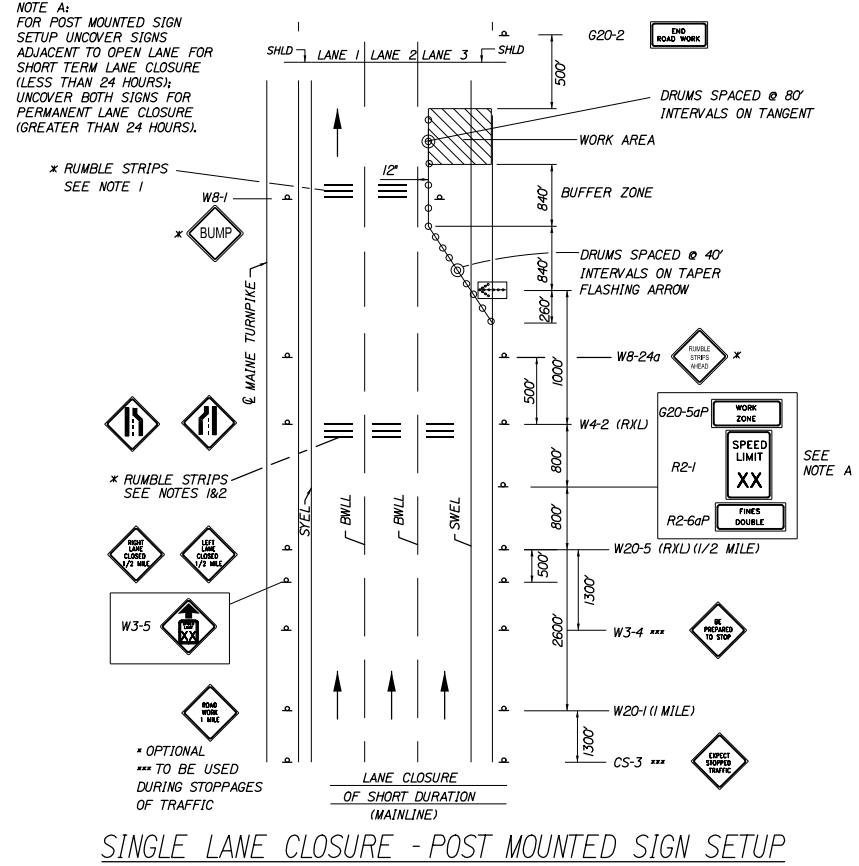
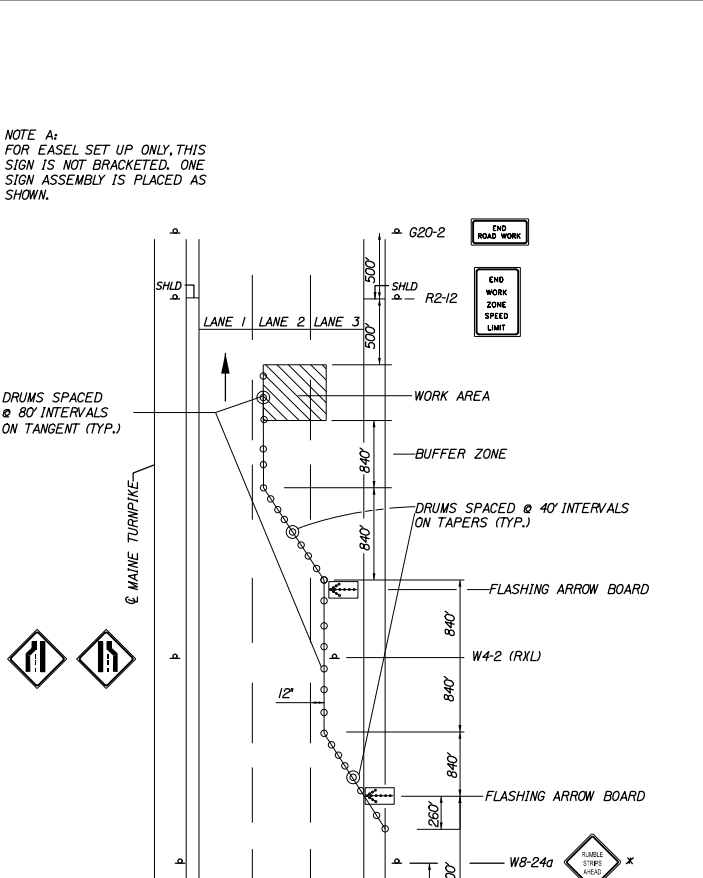
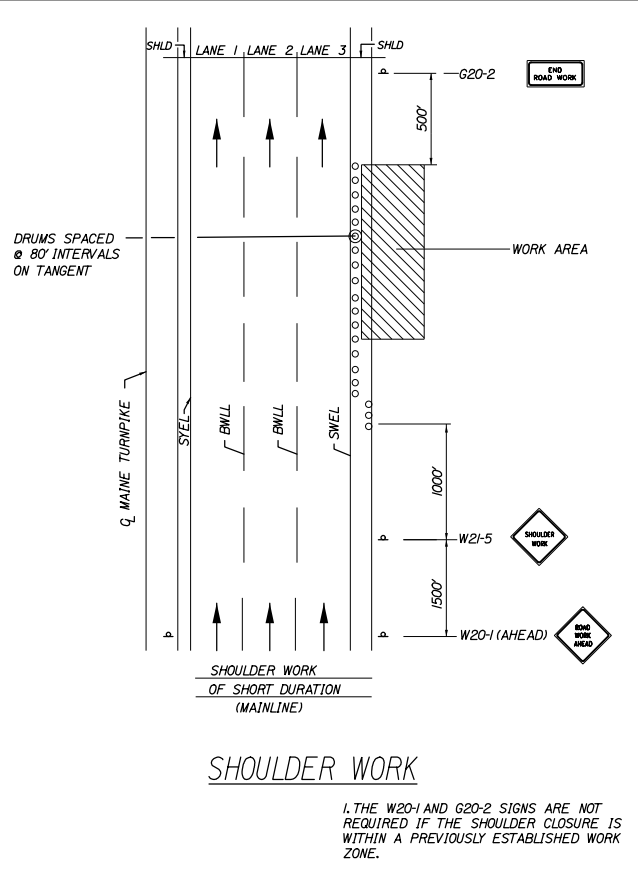
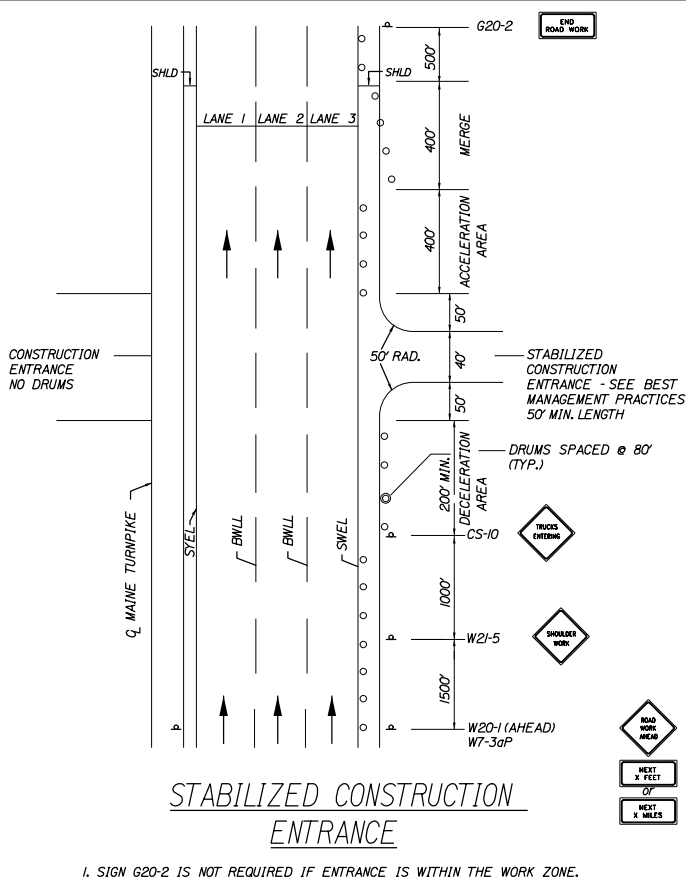
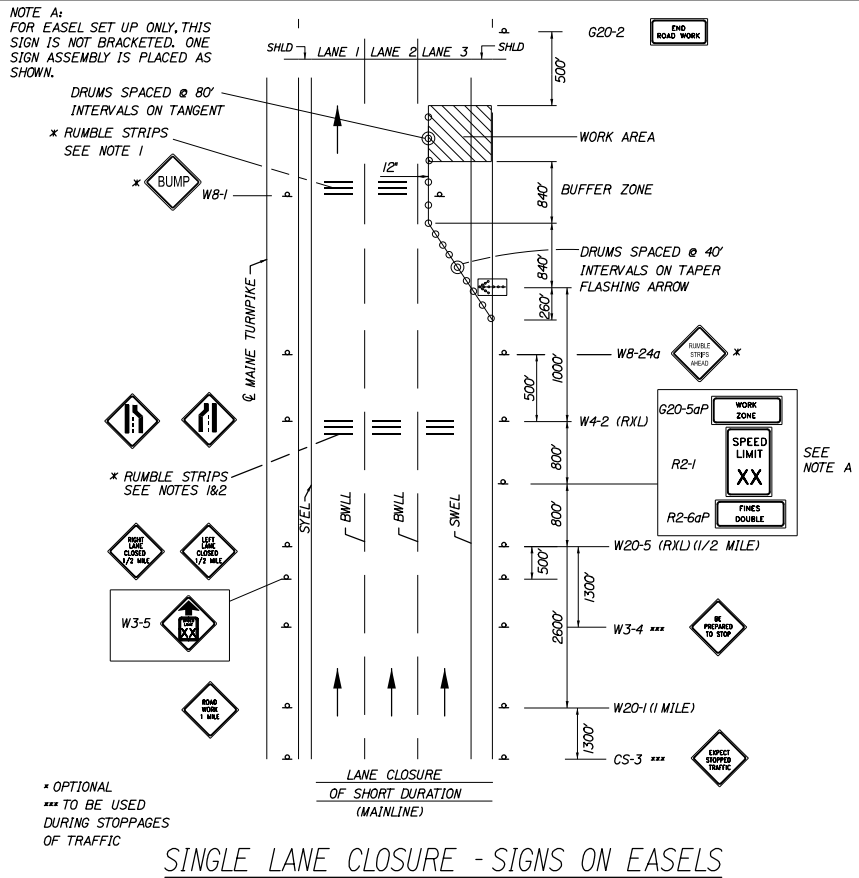
MTA PROJECT MANAGER: Lauren Fleming, P.E.

BRIDGE REPAIRS
SACO RIVER OVERPASS
GUARDRAIL DETAILS

SHEET NUMBER: TYP-01
CONTRACT: 2025.17
3 OF 18

Date:8/5/2025


Filename: 004_MTA MOT Standard Details.dgn



Scale:			
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No.	Revision	By	Date

Designed by:			
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CONSULTANT PROJECT MANAGER: Timothy R. Cote, P.E.			
	By	Date	
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Drawn	ERB	07\25	In Charge of TRC 07\25

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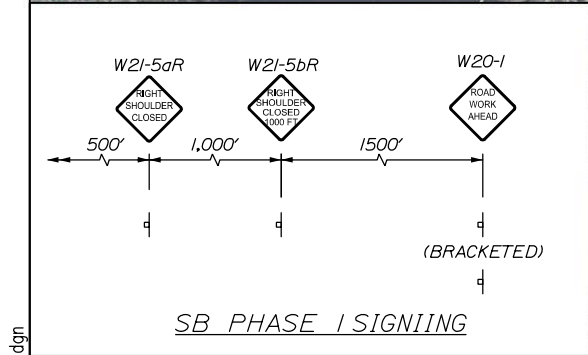
THE GOLD STAR
MEMORIAL HIGHWAY

MTA PROJECT MANAGER: Lauren Fleming, P.E.

BRIDGE REPAIRS
SACO RIVER OVERPASS
MAINTENANCE OF TRAFFIC
STANDARD DETAILS

SHEET NUMBER: MOT-01
CONTRACT:2025.17
4 OF 18


Date: 8/5/2025



Scale: 50 0 50 100 Scale of Feet			
No.	Revision	By	Date

Designed by: HNTB					
CONSULTANT PROJECT MANAGER: Timothy R. Cote, P.E.					
	By	Date		By	Date
Designed	PDB	07\25	Checked	DAM	07\25
Drawn	ERB	07\25	In Charge of	TRC	07\25

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**THE GOLD STAR
MEMORIAL HIGHWAY**

MTA PROJECT MANAGER: Lauren Fleming, P.E.

BRIDGE REPAIRS
SACO RIVER OVERPASS
MAINTENANCE OF TRAFFIC
PHASE 1

SHEET NUMBER: MOT-02
5 OF 18

Filename: 005_MOT_Phase 1.dgn




File: 006_MOT_Phase 2.dgn

Scale: 50 0 50 100 Scale of Feet			
No.	Revision	By	Date

Designed by:					
HNTB					
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	By	Date		By	Date
Designed	PDB	07\25	Checked	DAM	07\25
Drawn	ERB	07\25	In Charge of	TRC	07\25

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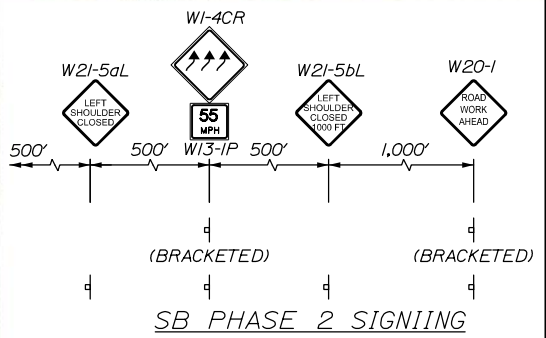
**THE GOLD STAR
MEMORIAL HIGHWAY**

MTA PROJECT MANAGER: Lauren Fleming, P.E.

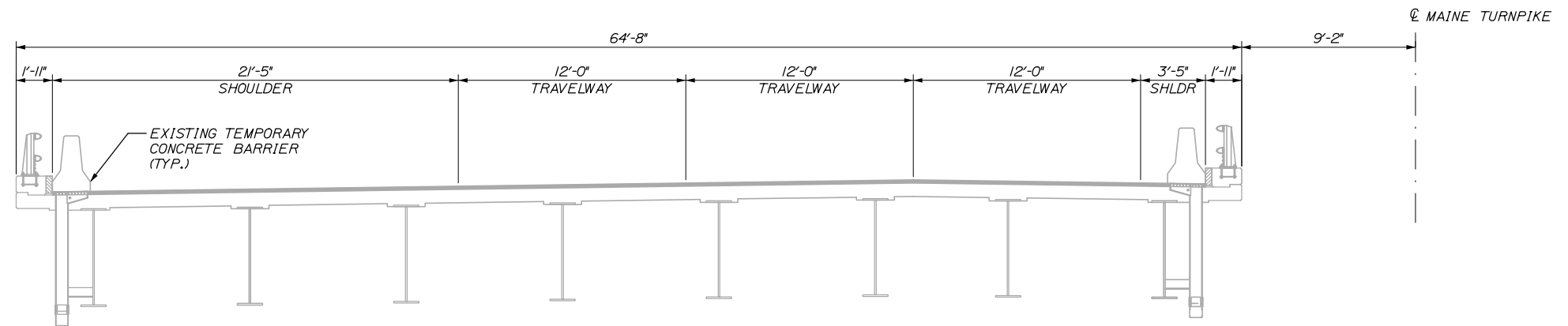
**BRIDGE REPAIRS
SACO RIVER OVERPASS
MAINTENANCE OF TRAFFIC
PHASE 2**

SHEET NUMBER: MOT-03
6 OF 18

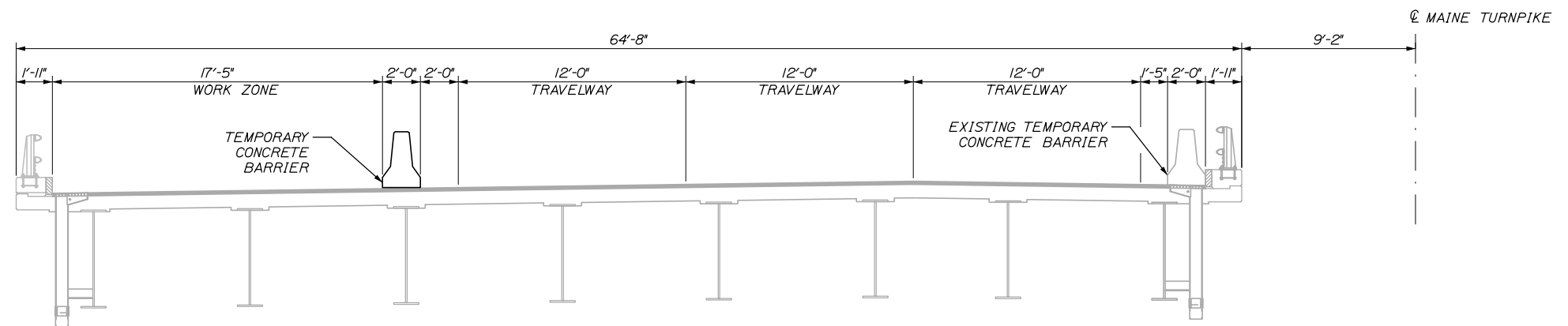
CONTRACT: 2025.17



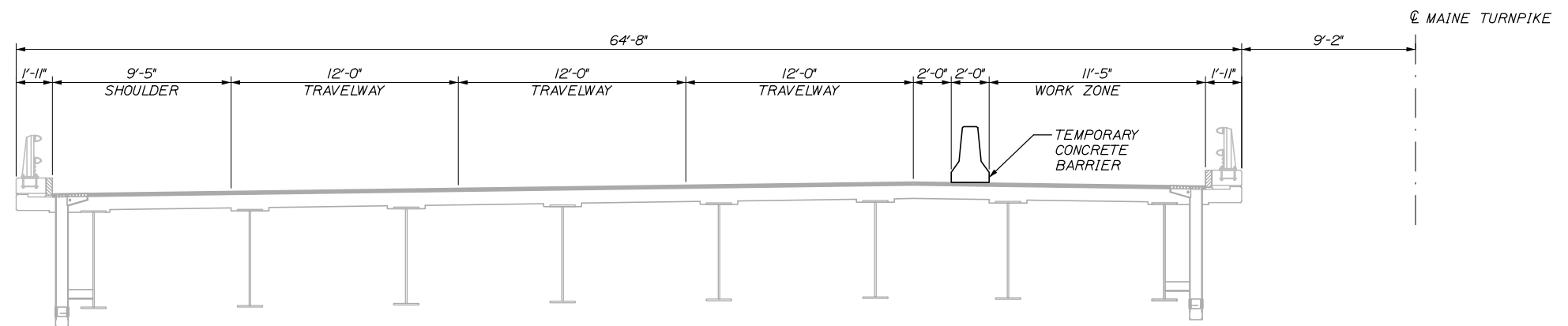
Date:8/5/2025



TRANSVERSE SECTION (SB) - EXISTING CONDITION



TRANSVERSE SECTION (SB) - PHASE 1



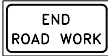






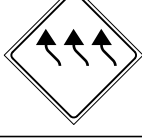

TRANSVERSE SECTION (SB) - PHASE 2





Filename: 007_MOT_Typical.dwg

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No.	Revision	By	Date	CONSULTANT PROJECT MANAGER: Timothy R. Cote, P.E.																	
					By	Date		By	Date	MTA PROJECT MANAGER: Lauren Fleming, P.E.				CONTRACT:2025.17				SHEET NUMBER: MOT-04 7 OF 18			
				Designed	PDB	07\25	Checked	BRG	07\25												
				Drawn	ERB	07\25	In Charge of	TRC	07\25												


Filename: 007_MOT_Typical.dgn

Filename: 008_MOT - Sign Summary.dgn

IDENTIFI- CATION NUMBER	SIZE OF SIGN		TEXT	TEXT DIMENSIONS (INCHES)			NUMBER OF SIGNS REQUIRED	COLOR		BORDER RADIUS	AREA IN SQUARE FEET
	WIDTH	HEIGHT		LETTER HEIGHT	VERTICAL SPACING	ARROW RTE. MKR.		BACK- GROUND	LEGEND BORDER		
G20-2	48"	24"		TEXT DIMENSIONS SHALL CONFORM TO "2004 EDITION - STANDARD HIGHWAY SIGNS - 2012 SUPPLEMENT"			1	COLORS SHALL CONFORM TO "2004 EDITION -STANDARD HIGHWAY SIGNS -2012 SUPPLEMENT"			8.00 (8)
W2I-5aR	48"	48"					1				16.00 (16)
W2I-5aL	48"	48"					1				16.00 (16)
W2I-5bR	48"	48"					1				16.00 (16)
W2I-5bL	48"	48"					1				16.00 (16)
W20-1 (AHEAD)	48"	48"					2				16.00 (32)
W1-4cR	48"	48"					2				16.00 (32)
W1-4cL	48"	48"					2				16.00 (32)
W13-1P (60)	30"	30"					4				6.25 (25)

IDENTIFI- CATION NUMBER	SIZE OF SIGN		TEXT	TEXT DIMENSIONS (INCHES)			NUMBER OF SIGNS REQUIRED	COLOR		BORDER RADIUS	AREA IN SQUARE FEET
	WIDTH	HEIGHT		LETTER HEIGHT	VERTICAL SPACING	ARROW RTE. MKR.		BACK- GROUND	LEGEND BORDER		
G20-5aP	48"	24"		TEXT DIMENSIONS SHALL CONFORM TO "2004 EDITION - STANDARD HIGHWAY SIGNS - 2012 SUPPLEMENT"			2	COLORS SHALL CONFORM TO "2004 EDITION -STANDARD HIGHWAY SIGNS -2012 SUPPLEMENT"			8.00 (16)
R2-1	48"	60"					2				20.00 (40)
R2-6aP	48"	24"					2				8.00 (16)
W3-5	48"	48"					2				16.00 (32)

Scale:			
NOT TO SCALE			
No.	Revision	By	Date

Designed by:			
			
CONSULTANT PROJECT MANAGER: Timothy R. Cote, P.E.			
	By	Date	
Designed	PDB	07\25	Checked DAM 07\25
Drawn	PDB	07\25	In Charge of TRC 07\25

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THE GOLD STAR
MEMORIAL HIGHWAY

MTA PROJECT MANAGER: Lauren Fleming, P.E.

BRIDGE REPAIRS
SACO RIVER OVERPASS
MAINTENANCE OF TRAFFIC
SIGN SUMMARY 1 OF 1

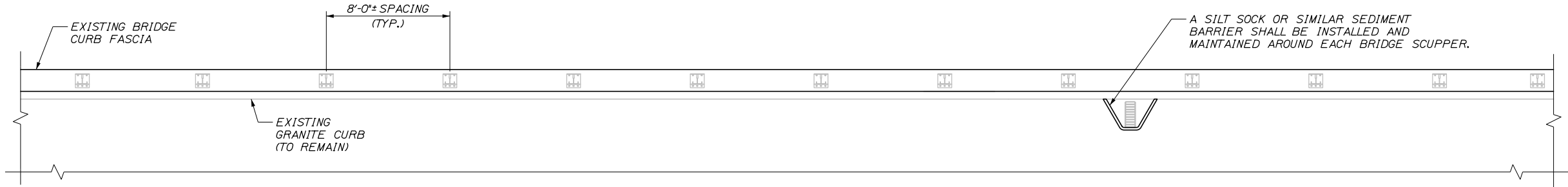
CONTRACT:2025.17

SHEET NUMBER: MOT-05

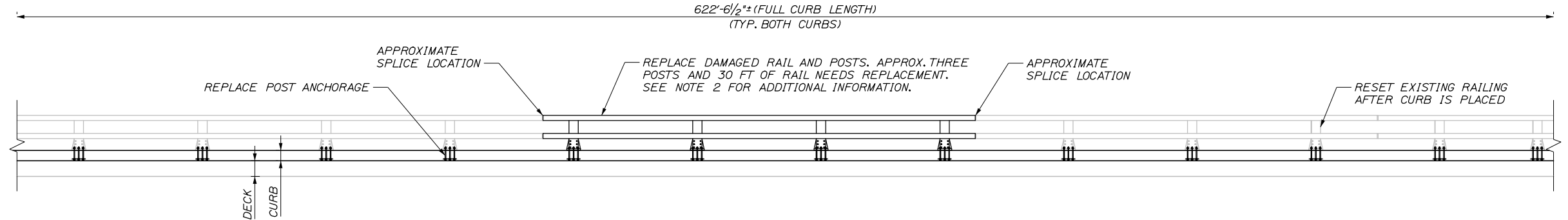
8 OF 18

Date:8/5/2025

Filename: 009_RailPlan and Elevation.dgn



BRIDGE RAIL AND CURB - PLAN
SCALE = 1/4" = 1'-0"



BRIDGE RAIL AND CURB - ELEVATION VIEW
(EXISTING GRANITE CURB NOT SHOWN FOR CLARITY)
SCALE = 1/4" = 1'-0"

GENERAL NOTES:

1. ALL PROPOSED REINFORCING STEEL SHALL BE EPOXY COATED AASHTO M31 GRADE 60, AND SHALL MEET THE REQUIREMENTS OF MAINEDOT STANDARD SPECIFICATION SECTION 503. ANCHORING MATERIAL SHALL BE KELKEN KELIGROUT OR AN APPROVED EQUAL. EMBEDMENT DEPTH SHALL BE SUFFICIENT TO DEVELOP THE FULL TENSILE STRENGTH OF THE BAR.
2. CURB REPAIRS SHALL BE IN ACCORDANCE WITH SECTION 518 OF THE MAINE TURNPIKE AUTHORITY SUPPLEMENTAL SPECIFICATIONS, 2016. THE CONCRETE USED FOR CURB REPAIRS SHALL BE CLASS AAA IN ACCORDANCE WITH MAINE TURNPIKE AUTHORITY SUPPLEMENTAL SPECIFICATION 502.
3. PRIOR TO PLACEMENT OF THE CONCRETE, ALL BONDING SURFACES SHALL BE CLEANED OF DEBRIS AND COATED WITH AN EPOXY BONDING COMPOUND. THE BONDING COMPOUND SHALL BE ATC ULTRABOND I, OR AN APPROVED EQUAL.
4. BRIDGE RAIL AND POST INSTALLATION SHALL BE IN ACCORDANCE WITH SECTION 507 OF THE MAINEDOT STANDARD SPECIFICATIONS.
5. CLEAR PROTECTIVE COATING FOR CONCRETE SURFACES SHALL BE APPLIED TO THE FOLLOWING AREAS:
 - EXPOSED SURFACES OF THE CURBS AND ENDPOSTS
 - VERTICAL FACE OF THE DECK FASCIA
 - UNDERSIDE OF DECK OVERHANG TO GIRDER TOP FLANGE
6. ALL ENDPOSTS SHALL BE INSPECTED BY THE CONTRACTOR AND THE RESIDENT FOR CONCRETE DETERIORATION AND REPAIR. REPAIRS SHALL BE AS DIRECTED BY THE RESIDENT IN ACCORDANCE WITH SUPPLEMENTAL SPECIFICATION 518. END POST REPAIRS SHALL BE PAID UNDER ITEM 518.60 REPAIR OF VERTICAL SURFACES < 8 INCHES. CONTROL DEVICES.

GENERAL NOTES (CONT.):


7. LOOSE CONCRETE ON THE UNDERSIDE OF DECK OVERHANG SHALL BE REMOVED IN THE FOLLOWING LOCATIONS:
 - SPAN 2 OUTSIDE OVERHANG - APPROX. 4' LONG
 - SPAN 3 OUTSIDE OVERHANG - APPROX. 4' LONG
 - SPAN 5 OUTSIDE OVERHANG - APPROX. 8' LONGAREAS ARE APPROXIMATE AND SHALL BE VERIFIED BY THE RESIDENT PRIOR TO REMOVING CONCRETE. CLEAR PROTECTIVE COATING ON THE UNDERSIDE OF OVERHANGS SHALL BE APPLIED AFTER LOOSE CONCRETE IS REMOVED. SPAN 1 IS THE SOUTHERN MOST SPAN, SPAN 6 IS THE NORTHERN MOST SPAN. PAYMENT FOR REMOVAL OF LOOSE CONCRETE SHALL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCIDENTAL TO RELATED CONTRACT ITEMS. SEE SHEET S-03 FOR REPRESENTATIVE PHOTOS.
8. CONTRACTOR SHALL PROVIDE NECESSARY SHIELDING TO PROTECT REMOVED CONCRETE AND NEWLY PLACED CONCRETE FROM ENTERING THE RIVER. PAYMENT SHALL BE INCIDENTAL TO RELATED CONTRACT ITEMS.
9. SAND AND DEBRIS SHALL BE COMPLETELY REMOVED FROM BRIDGE FOLLOWING EACH PHASE OF CONSTRUCTION. PAYMENT SHALL BE INCIDENTAL TO CONTRACT ITEMS.
10. THE ESTIMATED QUANTITY OF TEMPORARY CONCRETE BARRIER, TYPE I SUPPLIED BY THE AUTHORITY, INCLUDES EXISTING TEMPORARY CONCRETE BARRIER ON SITE. IT IS ESTIMATED THAT 1,800 LINEAR FEET OF TEMPORARY BARRIER IS CURRENTLY ON SITE. SEE SPECIAL PROVISION 526 FOR ADDITIONAL INFORMATION.
11. SPEED ZONE DATA COLLECTION DEVICES ARE REQUIRED AT SACO RIVER MAINLINE OVERPASS AND FOREST AVENUE MAINLINE OVERPASS. CONTRACTOR SHALL COORDINATE WITH THE AUTHORITY AND RESIDENT ON PLACEMENT OF DEVICES PRIOR TO LAYING OUT MAINTENANCE OF TRAFFIC CONTROL DEVICES.

BRIDGE RAILING REPAIR NOTES:

1. THE WORK UNDER PAY ITEM 507.17 GENERALLY INCLUDES REMOVING, STACKING AND RESETTING THE EXISTING ALUMINUM BRIDGE RAILING. ALL ANCHORAGES SHALL BE REPLACED.
2. REPLACEMENT RAIL BARS, SPLICES, AND POSTS WILL BE PROVIDED BY THE AUTHORITY. THE CONTRACTOR SHALL FURNISH ALL HARDWARE, ANCHOR RODS, ANCHOR TEMPLATES AND PREFORMED PADS REQUIRED FOR THE INSTALLATION OF THE RAILING COMPONENTS. ALL RAILING COMPONENTS PROVIDED BY THE CONTRACTOR SHALL BE IN ACCORDANCE WITH THE STANDARD RAILING DETAILS INCLUDED IN THIS PLAN SET. THE CONTRACTOR SHALL FIELD VERIFY THE DIMENSIONS FOR ALL COMPONENTS PROVIDED BY THE AUTHORITY TO ENSURE CONTRACTOR FURNISHED COMPONENTS WORK WITH THE AUTHORITY SUPPLIED COMPONENTS. REPLACEMENT RAIL SECTIONS SHALL BE SPLICE TO SPLICE.
3. ANCHOR BOLTS SHALL CONFORM TO ASTM F1554 GRADE 105 OR AN APPROVED EQUAL. ANCHOR SPACER PLATES SHALL CONFORM TO SECTION 713.01 OF THE MAINEDOT STANDARD SPECIFICATIONS.
4. ALL ANCHOR BOLTS, NUTS AND WASHERS SHALL BE HOT DIP GALVANIZED AFTER FABRICATION. HARDWARE FOR FASTENING RAIL BARS, CLAMP BARS, SET SCREWS, AND OTHER COMPONENTS SHALL BE STAINLESS STEEL CONFORMING TO AASTM F593, ALLOY GROUP 1, CONDITION CW.
5. PREFORMED PADS SHALL CONFORM TO SECTION 713.03 OF THE MAINEDOT STANDARD SPECIFICATIONS.

Scale: AS NOTED				Designed by: HNTB			
No.	Revision	By	Date	CONSULTANT PROJECT MANAGER: Timothy R. Cote, P.E.			
				By	Date	By	Date
				Designed	BRG 07\25	Checked	JDW 07\25
				Drawn	ERB 07\25	In Charge of	TRC 07\25

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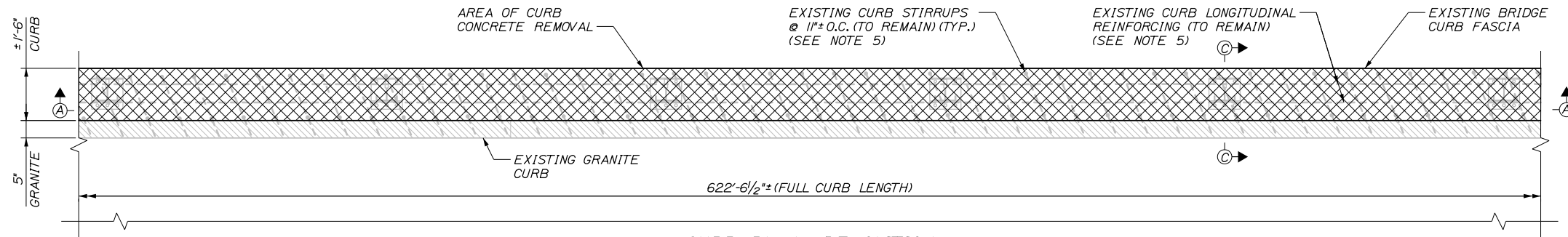
**THE GOLD STAR
MEMORIAL HIGHWAY**

MTA PROJECT MANAGER: Lauren Fleming, P.E.

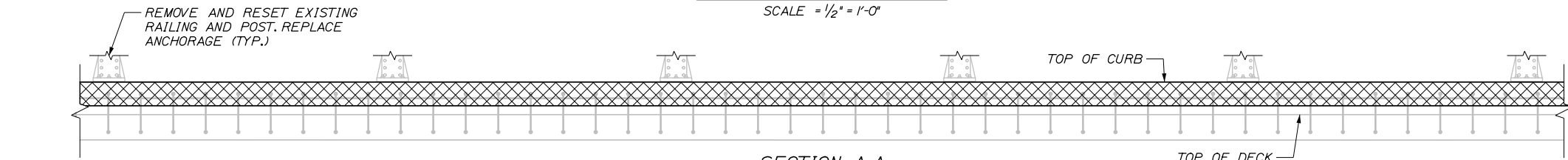
BRIDGE REPAIRS
SACO RIVER OVERPASS
CURB REPAIR
PLAN & ELEVATION

SHEET NUMBER: S-01
CONTRACT: 2025.17
9 OF 18

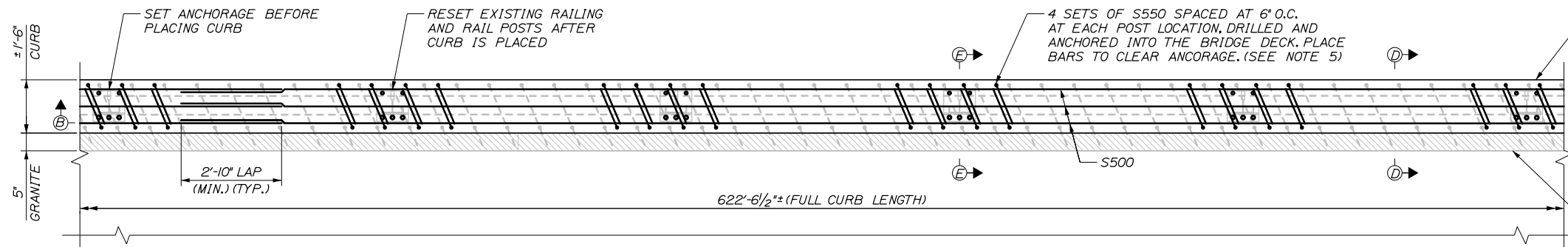
Date: 8/5/2025



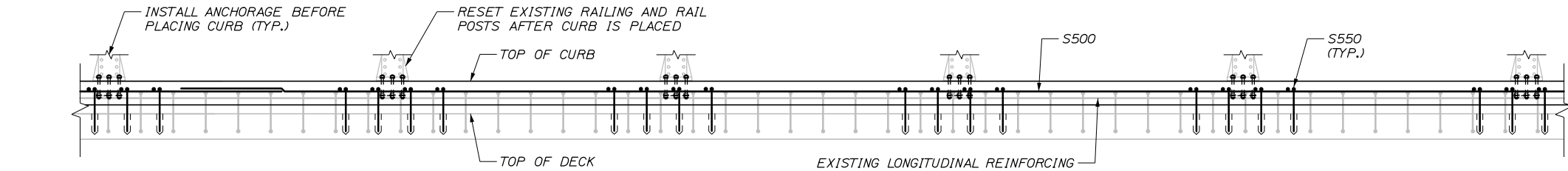
CURB PLAN - DEMOLITION
SCALE = 1/2" = 1'-0"



SECTION A-A
SCALE = 1/2" = 1'-0"



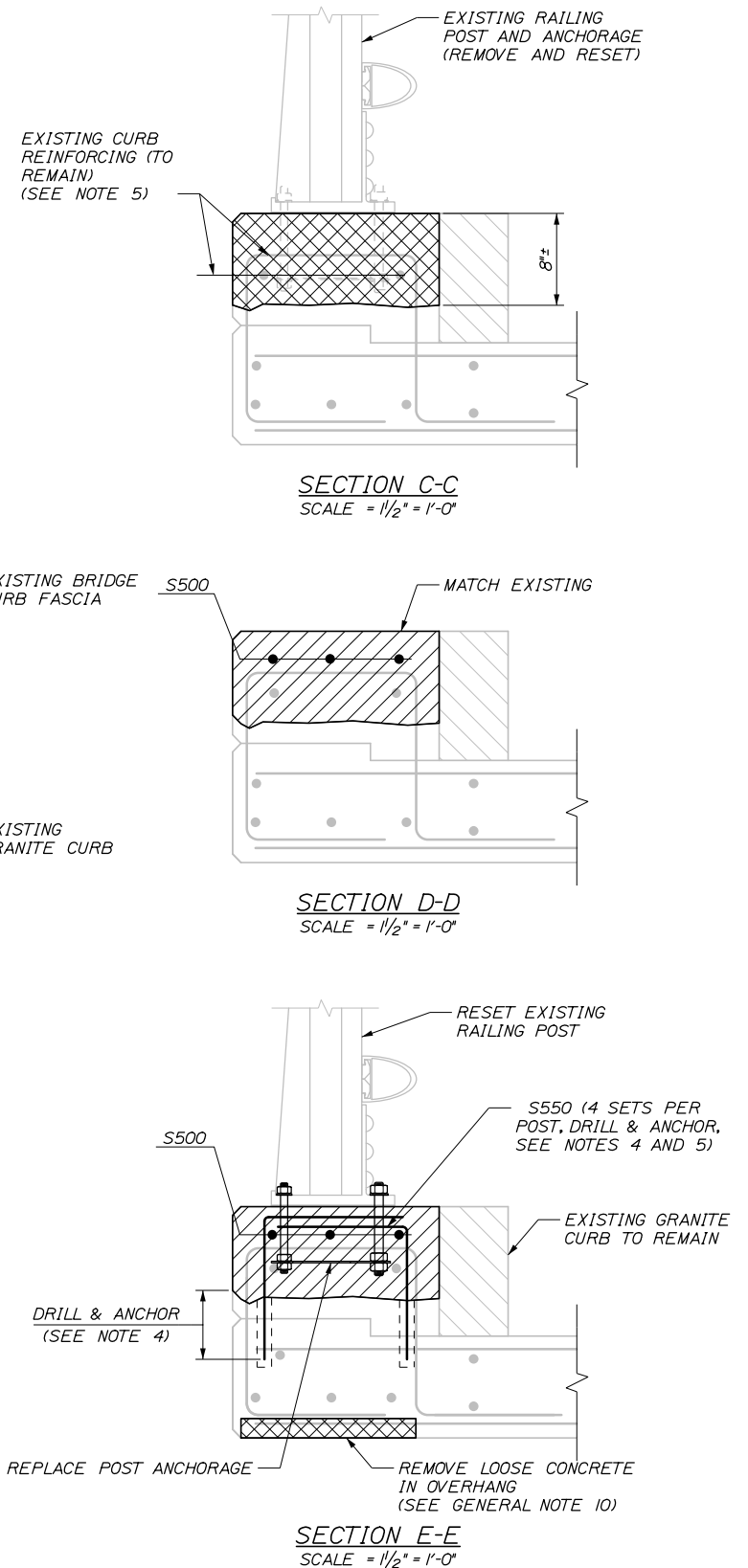
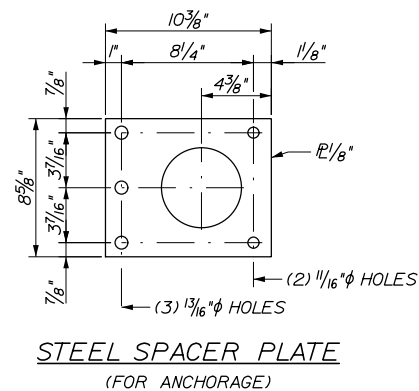
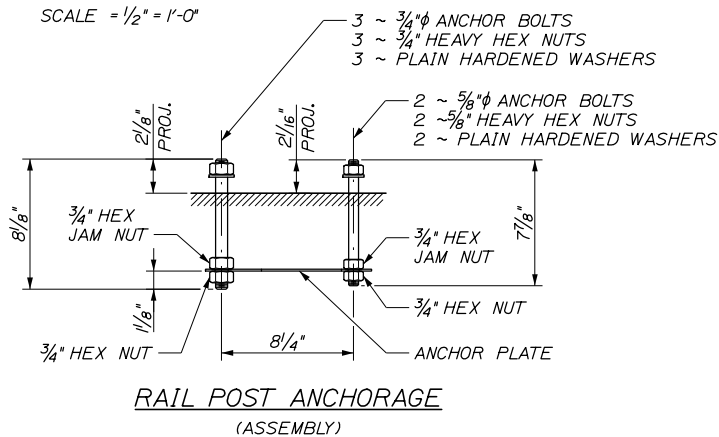
CURB PLAN - CONSTRUCTION
SCALE = 1/2" = 1'-0"




SECTION B-B
SCALE = 1/2" = 1'-0"

NOTES:

1. EXISTING REINFORCING SHOWN IS APPROXIMATE. WHERE EXISTING REINFORCING IS HEAVILY DEFORMED OR DETERIORATED, NEW SUPPLEMENTAL REINFORCING SHALL BE PLACED AS DIRECTED BY THE RESIDENT.
2. EXISTING REINFORCING SHALL BE CLEANED OF SURFACE RUST AND REPAIRED USING A TWO-PART EPOXY REPAIR MATERIAL IN ACCORDANCE WITH STANDARD SPECIFICATION 503 AS DIRECTED BY THE RESIDENT.
3. ALL PROPOSED REINFORCING BARS SHALL BE #5 BARS. CONCRETE COVER SHALL BE 2".
4. PROPOSED HOOK BARS SHALL BE DRILLED AND ANCHORED 6" INTO THE EXISTING CONCRETE. THE CONTRACTOR SHALL PROGRESS THE WORK IN A MANNER THAT PREVENTS DRILLING COMPLETELY THROUGH THE BRIDGE DECK.
5. AT THE CONTRACTOR'S OPTION, EXISTING STIRRUPS AND LONGITUDINAL BARS MAY BE REMOVED DURING DEMOLITION AND REPLACED WITH ADDITIONAL S550 L-BARS TO MATCH EXISTING SPACING (11" O.C.). THE ADDITIONAL REINFORCING FOR THIS OPTION WILL NOT BE MEASURED FOR PAYMENT AND SHALL BE INCIDENTAL TO THE CONTRACT.

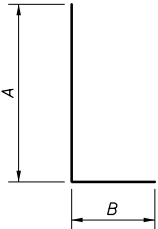


Filename: 010_RailDetails.dgn

Scale: AS NOTED				Designed by: HNTB				HNTB CORPORATION 82 Running Hill Road, Suite 201 South Portland, ME 04106 TEL (207) 774-5155 FAX (207) 228-0909				<div></div> THE GOLD STAR MEMORIAL HIGHWAY				BRIDGE REPAIRS SACO RIVER OVERPASS CURB REPAIR DETAILS			
No.	Revision	By	Date																
				CONSULTANT PROJECT MANAGER: Timothy R. Cote, P.E.															
					By	Date		By	Date										
				Designed	BRG	07\25	Checked	JDW	07\25										
				Drawn	ERB	07\25	In Charge of	TRC	07\25										
CONTRACT: 2025.17																			
SHEET NUMBER: S-02																			
10 OF 18																			

Date:8/5/2025

SACO RIVER BRIDGE (SB)																
MARK	SIZE	QTY	LENGTH		TYPE	A	B	C	D	E	F	G	H	O	R	REMARKS
SUPERSTRUCTURE																
S500	5	102	40'	0"	Straight											Longitudinal Curb Reinforcing
S550	5	1280	2'	3"	118	1'-0"	1'-3"									Hooked Curb Reinforcing



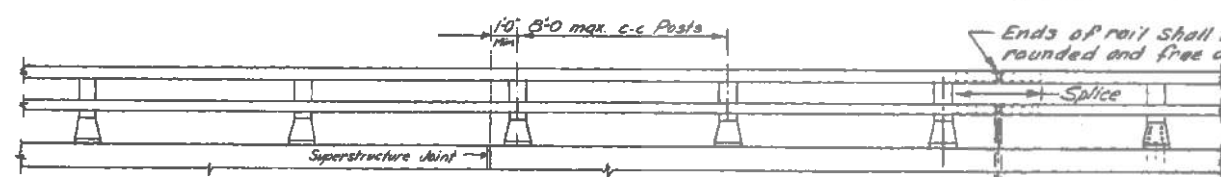
TYPE 118



REPRESENTATIVE PHOTOS OF INCIPIENT SPALLS AND DELAMINATION TO BE REMOVED

Filename: 011_Reinforcing Schedule.dgn

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No.	Revision	By	Date	CONSULTANT PROJECT MANAGER: Timothy R. Cote, P.E.													
					By	Date										By	Date
				Designed	BRG	07\25	Checked									JDW	07\25
				Drawn	ERB	07\25	In Charge of									TRC	07\25
								MTA PROJECT MANAGER: Lauren Fleming, P.E.				CONTRACT:2025.17					
												SHEET NUMBER: S-03 11 OF 18					

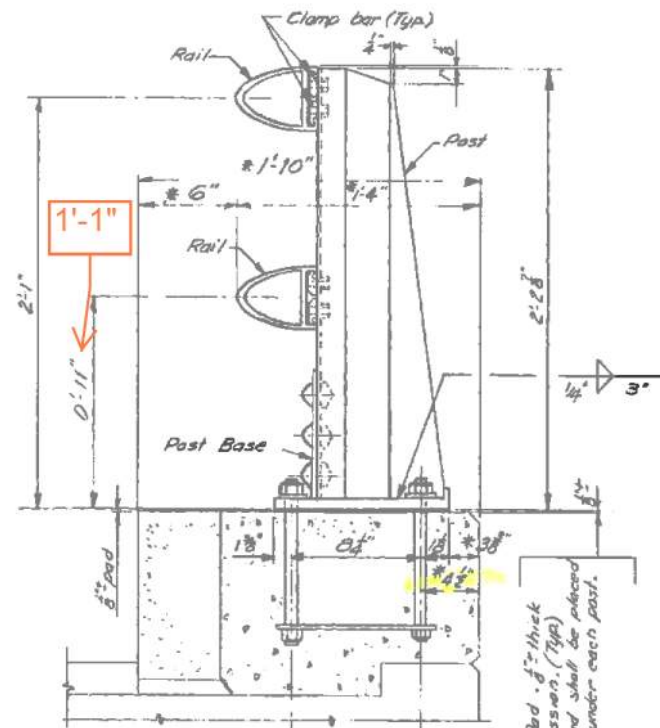


RAILING - ELEVATION

Lengths of rail shall be attached to a minimum of four (4) rail posts wherever possible, and in any case never less than two (2). Rail posts are to be Typ. Joints set normal to grade unless otherwise shown on the Bridge Plans.

2" x 10" F@ Bridge Exp. Joints unless otherwise noted, on Design Drawings.

Top edges of post shall be rounded and free of burrs. Typical all posts.



BRIDGE RAILING

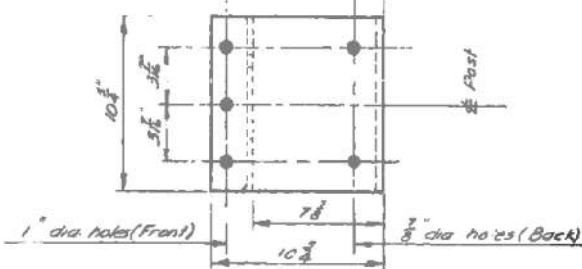
(Assembly)

* Preferable minimum dimensions. For actual dimensions see Bridge Plan.

1 1/4" (highlighted)

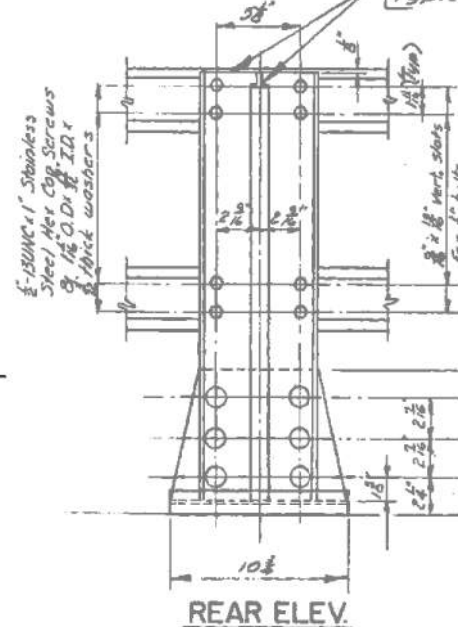
Anchor Bolts

Anchor Bolts

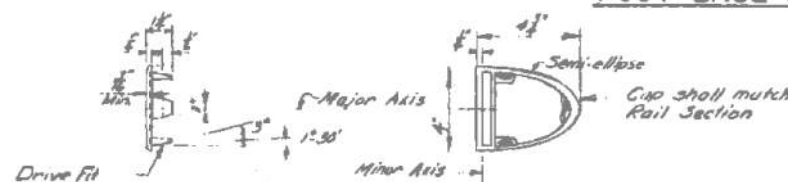


POST BASE

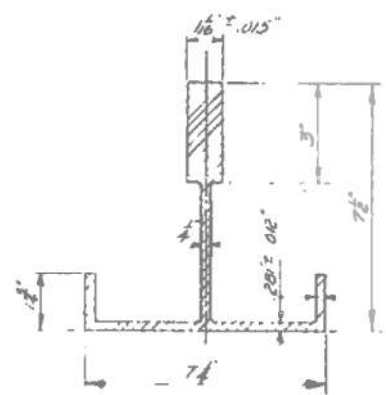
(Bottom View)



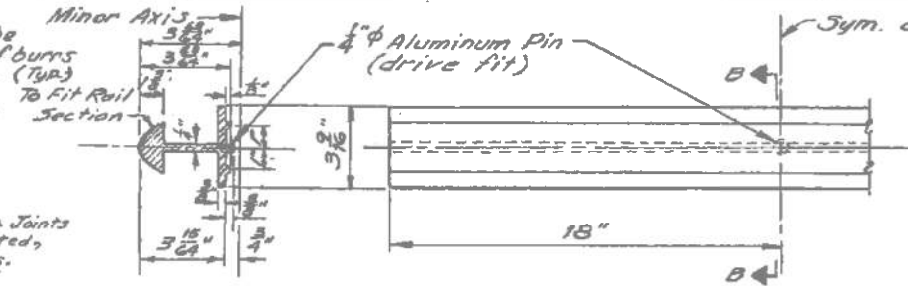
REAR ELEV.



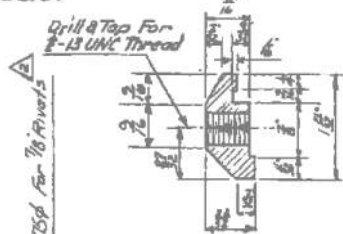
RAIL CAP



POST SECTION



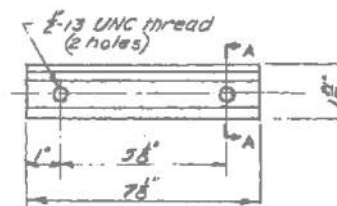
SECTION B-B



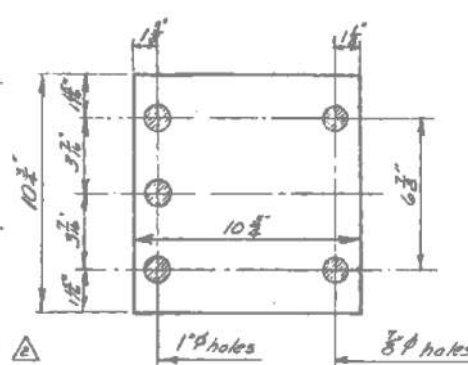
SECTION A-A

POST BASE SECTION

Alternate splice bars may be substituted if approved by the Engineer.



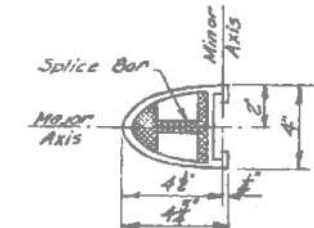
CLAMP BAR



PREFORMED PAD

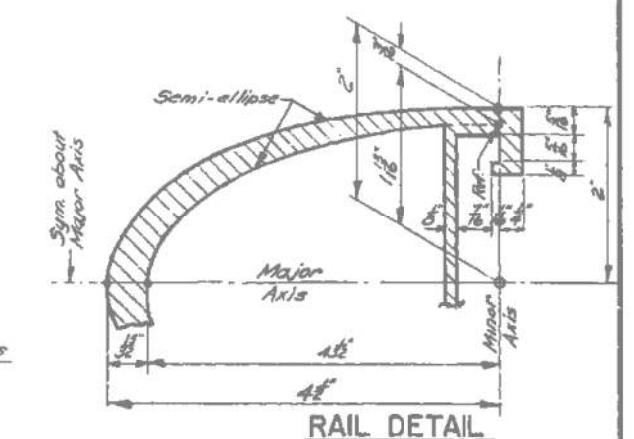
Sym. about & Splice

If there is a conflict between this standard Detail and the Design Drawings, the requirements of the Design Drawings shall be followed.

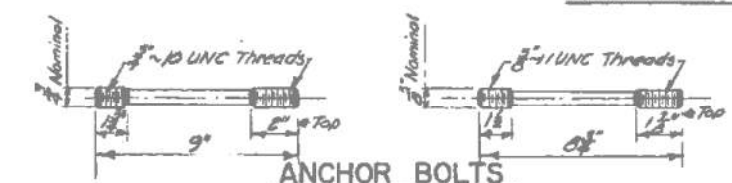


RAIL SECTION

See "Rail Detail"

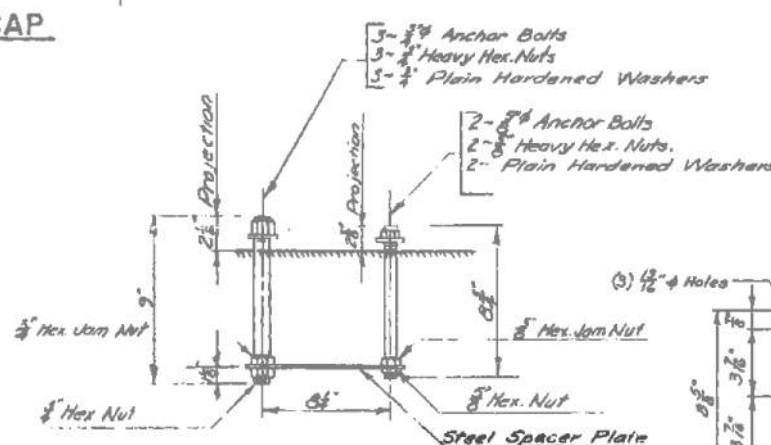


RAIL DETAIL



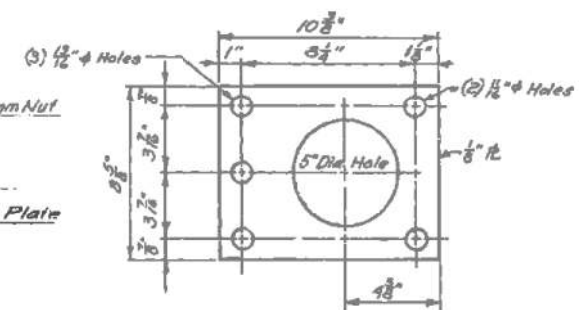
ANCHOR BOLTS

If cut threads are used body diameter shall be not less than nominal diameter.
If rolled threads are used, body diameter shall be not less than pitch diameter of the threads.



RAIL POST ANCHORAGE

(Assembly)

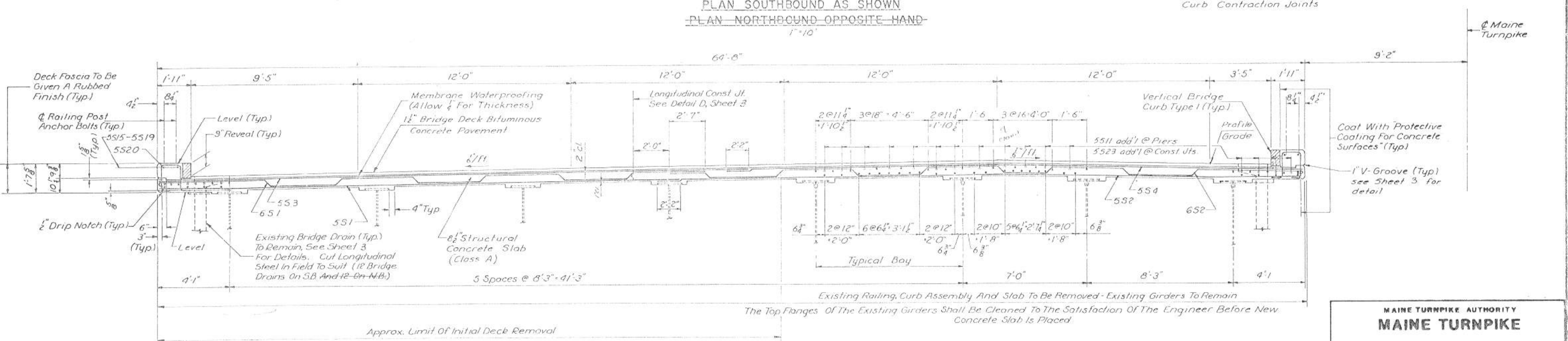
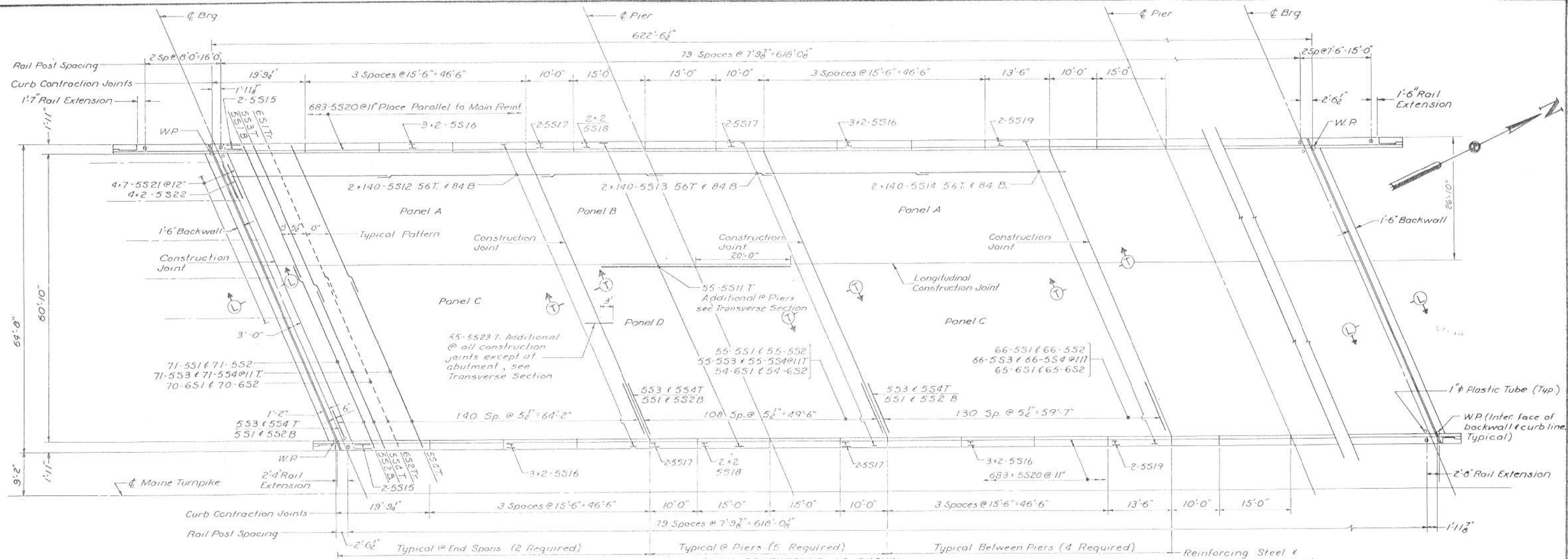


STEEL SPACER PLATE

(For Anchorage)

Added Nuts	10-84
Revised Post Base	8-84
Altered base dimensions	7-83
REVISIONS	
STATE OF MAINE	
DEPARTMENT OF TRANSPORTATION	
STANDARD DETAILS	
(BD 114-81)	
ALUMINUM BRIDGE RAILING	
2-BAR (SEMI-ELLIPSE)	
SHEET 15 OF 20 AUGUSTA, MAINE JUNE 1981	
Newcastle	

DESIGN - DETAILED	BY	DATE
CHECKED	K. Leach	Jan. 1981
REVISIONS		
FIELD CHANGES		
PLANS		



- NOTES**
1. For general superstructure notes, see Sheet 3
 2. For Section L-L, see Sheet 3
 3. For Section T-T, see Sheet 3
 4. All Reinforcement shall be Epoxy Coated.

NO.	REVISION	BY	DATE	IN CHARGE OF
		MADE	ABM 1-88	
		TRACED		
		CHECKED	KAC 1-88	
				J.P.W.

MAINE TURNPIKE AUTHORITY
MAINE TURNPIKE

SACO RIVER BRIDGE
DECK REPLACEMENT

HOWARD, NEEDLES, TAMMEN & BERGENDOFF
 CONSULTING ENGINEERS
 BOSTON

SCALE: AS NOTED
 SHEET NO. 2

Filename: 014_MTA MOT Standard Details - Forest.dgn



1. THE W20-1 AND G20-2 SIGNS ARE NOT REQUIRED IF THE SHOULDER CLOSURE IS WITHIN A PREVIOUSLY ESTABLISHED WORK ZONE.

1. ALL PAVEMENT STRIPING & SIGNING SHALL BE IN ACCORDANCE WITH THE "MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES", U.S.D.O.T., F.H.W.A., LATEST EDITION.
2. THESE PLANS SHOW THE GENERAL CONDITION FOR TURNPIKE MAINLINE TRAFFIC CONTROL DURING CONSTRUCTION, SLIGHT MODIFICATIONS IN CONSTRUCTION PROCEDURE MAY OCCUR AND MAY REQUIRE SOME MINOR ADJUSTMENTS TO BE MADE IN THE FIELD. ALL PROCEDURES MUST BE APPROVED BY THE RESIDENT.
3. THE CONTRACTOR SHALL REMOVE ALL PAVEMENT MARKINGS THAT CONFLICT WITH PROPOSED PAVEMENT MARKINGS IN ACCORDANCE WITH THE SPECIFICATIONS AND MUTCD. PAVEMENT SHALL BE ENDS UNDER ITEM 627.77 - REMOVING PAVEMENT MARKINGS.
4. EXPOSED BARRIER ENDS SHALL BE PROTECTED BY A WORK ZONE CRASH CUSHION. PAYMENT WILL BE UNDER ITEM 527.341 - WORK ZONE CRASH CUSHION - TL-3.
5. SPEED LIMIT, STOP AND YIELD SIGNS SHALL BE A MINIMUM OF 5' ABOVE THE PAVEMENT.

BWLL = BROKEN WHITE LANE LINE
 SWEL = SOLID WHITE EDGE LINE
 SYEL = SOLID YELLOW EDGE LINE
 TBWLL = TEMPORARY BROKEN WHITE LANE LINE
 TDWEL = TEMPORARY DOTTED WHITE EDGE LINE
 TSWEL = TEMPORARY SOLID WHITE EDGE LINE
 TSWLL = TEMPORARY SOLID WHITE LANE LINE
 TSYCL = TEMPORARY SOLID YELLOW CENTER LINE

1. IF RUMBLE STRIPS ARE USED THEY SHALL BE PLACED IN ONE OF THE FOLLOWING CONFIGURATIONS:
 - * ADJACENT TO THE WORK ZONE (1 UNIT)
 - * UPSTREAM FROM THE TAPER FOR THE WORKZONE (2 UNITS)
 - * BOTH ADJACENT TO THE WORKZONE AND PRIOR TO THE TAPER (3 UNITS)W8-1 SIGNS SHALL BE PLACED ADJACENT TO THE FIRST RUMBLE STRIP AT ANY LOCATION. ONLY ONE SET OF W8-24a SIGNS ARE REQUIRED FOR ANY OF THE ABOVE CONFIGURATIONS.
2. RUMBLE STRIPS MAY BE PLACED UPSTREAM OF THE TAPER BETWEEN THE W3-5 SIGNS AND THE W4-2 SIGNS. IF RUMBLE STRIPS ARE INSTALLED PRIOR TO TAPER, W8-1 SIGNS SHALL BE PLACED ADJACENT TO THE FIRST STRIP AND THE W8-24a SIGNS SHALL BE MOVED TO 400' AFTER THE W20-5 SIGNS.




NOTES:

1. ROAD WORK AHEAD & END ROAD WORK SIGNS MAY BE OMITTED FOR SHORT DURATION OPERATIONS (LESS THAN 1 HOUR).
2. FLAGGERS SHALL BE LOCATED SO THEY ARE CLEARLY VISIBLE TO APPROACHING DRIVERS.
3. SIGN SPACING ASSUMES LOCAL ROAD SPEED IS 40 MPH OR LESS.



1. THE W20-1 AND G20-2 SIGNS ARE NOT REQUIRED IF THE SHOULDER CLOSURE IS WITHIN A PREVIOUSLY ESTABLISHED WORK ZONE.

2. ONLY ONE SHOULDER CLOSURE PERMITTED PER BOUND AT ONE TIME.

Scale:				Designed by:						
NOT TO SCALE										
No.	Revision			By	Date					
								CONSULTANT PROJECT MANAGER: Timothy R. Cote, P.E.		
						By	Date		By	Date
					Designed	PDB	07\25	Checked	DAM	07\25
					Drawn	ERB	07\25	In Charge of	TRC	07\25

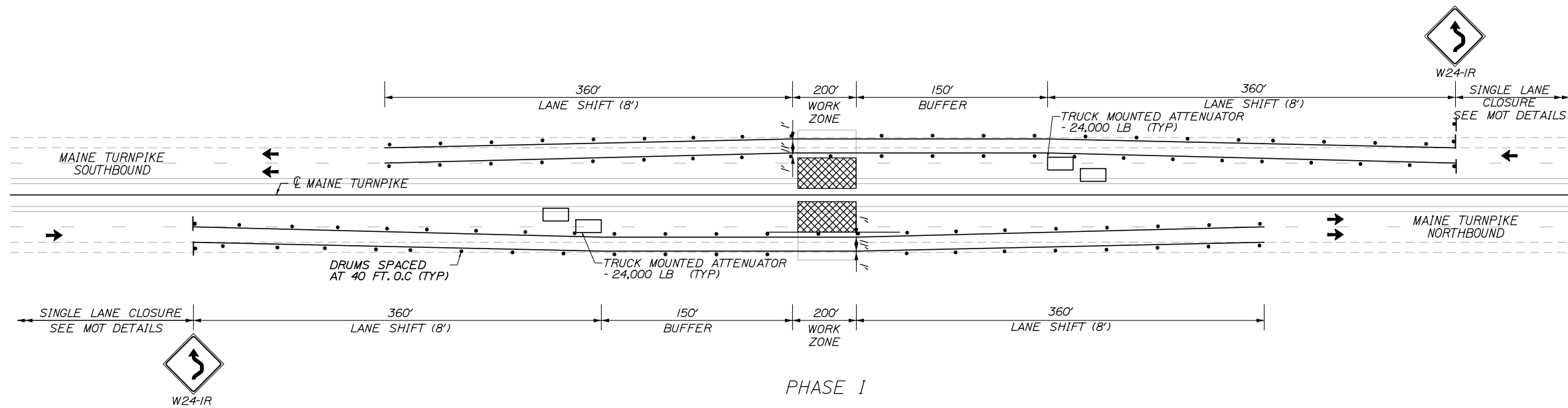


MTA PROJECT MANAGER: Lauren Fleming, P.E.

SHEET NUMBER: MOT-06

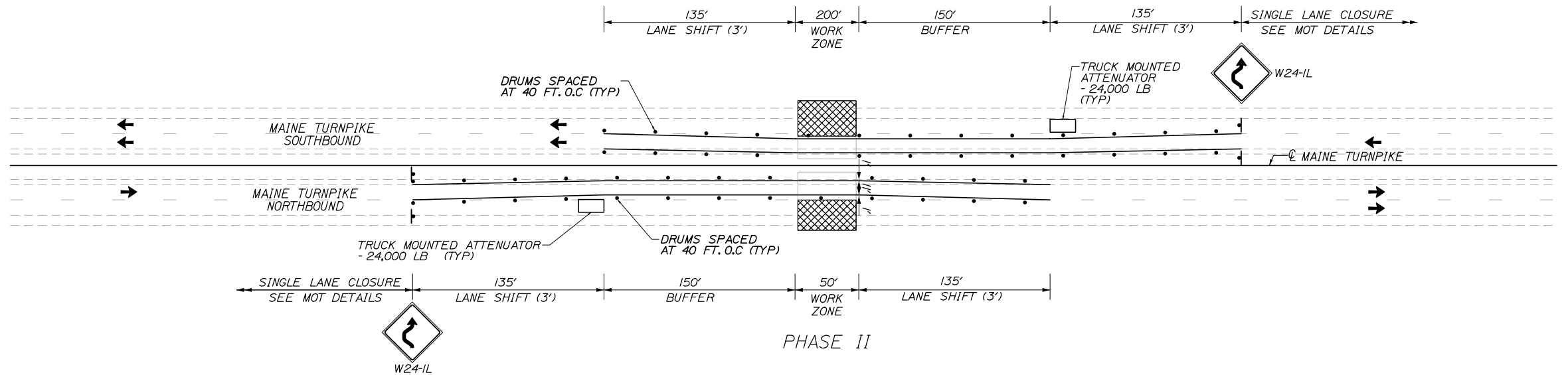
Date: 8/5/2025

Filename: 015_Forest - MOT.dgn



NOTES:


1. LANE CLOSURES SHALL BE SIGNED FOR 45 MPH AND MAY ONLY OCCUR AT NIGHT AND LANES MUST BE REOPENED FOR THE NEXT DAY. SEE SPECIAL PROVISION 652.
2. FOR NIGHT LANE CLOSURES TEMPORARY STRIPING IS NOT REQUIRED.
3. LANE SHIFTS ARE SHOWN FOR INFORMATIONAL PURPOSES. ACTUAL LANE SHIFTS SHALL BE DETERMINED IN THE FIELD BASED ON WIDTH LIMITS OF MILLING, LOCATION AND EXTENT OF DECK REPAIRS, AND WIDTH LIMITS OF PAVEMENT REHABILITATION.
4. CONTRACTOR SHALL SUBMIT MOT LAYOUT FOR RESIDENT REVIEW AND APPROVAL A MINIMUM OF 7 DAYS PRIOR TO SETUP.
5. PORTABLE CHANGEABLE MESSAGE BOARDS SHALL BE SET UP ONE WEEK IN ADVANCE OF PLANNED LANE CLOSURES AND SHALL BE MAINTAINED DURING TEMPORARY CLOSURES.
6. PEDESTRIAN TRAFFIC ON FOREST AVENUE MUST BE MAINTAINED AT ALL TIMES.
7. ONE TRUCK MOUNTED ATTENUATOR MINIMUM PER BOUND IS REQUIRED. A SECOND TRUCK MOUNTED ATTENUATOR MAY BE USED IF NECESSARY.



Scale:			
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HNTB			
CONSULTANT PROJECT MANAGER: Timothy R. Cote, P.E.			
	By	Date	
Designed	PDB	07\25	Checked DAM 07\25
Drawn	ERB	07\25	In Charge of TRC 07\25

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**THE GOLD STAR
MEMORIAL HIGHWAY**

MTA PROJECT MANAGER: Lauren Fleming, P.E.




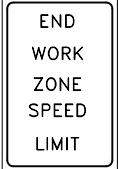





**EMERGENCY REPAIRS
FOREST AVENUE OVERPASS
MAINTENANCE OF TRAFFIC**







SHEET NUMBER: MOT-07

CONTRACT: 2025.17


15 OF 18

Filename: 0016_MOT - Sign Summary - Forest.dgn

IDENTIFI- CATION NUMBER	SIZE OF SIGN		TEXT	TEXT DIMENSIONS (INCHES)			NUMBER OF SIGNS REQUIRED	COLOR		BORDER RADIUS	AREA IN SQUARE FEET
	WIDTH	HEIGHT		LETTER HEIGHT	VERTICAL SPACING	ARROW RTE. MKR.		BACK- GROUND	LEGEND BORDER		
G20-5aP	48"	24"		TEXT DIMENSIONS SHALL CONFORM TO "2004 EDITION - STANDARD HIGHWAY SIGNS - 2012 SUPPLEMENT"			4	COLORS SHALL CONFORM TO "2004 EDITION -STANDARD HIGHWAY SIGNS -2012 SUPPLEMENT"			8.00 (32)
R2-1	48"	60"					4				20.00 (80)
R2-6aP	48"	24"					4				8.00 (32)
R2-12	48"	60"					4				20.00 (80)
W4-2L	48"	48"					4				16.00 (64)
W4-2R	48"	48"					4				16.00 (64)
W24-1L	48"	48"					5				16.00 (80)
W24-1R	48"	48"					6				16.00 (96)
W3-5	48"	48"					4				16.00 (64)

IDENTIFI- CATION NUMBER	SIZE OF SIGN		TEXT	TEXT DIMENSIONS (INCHES)			NUMBER OF SIGNS REQUIRED	COLOR		BORDER RADIUS	AREA IN SQUARE FEET
	WIDTH	HEIGHT		LETTER HEIGHT	VERTICAL SPACING	ARROW RTE. MKR.		BACK- GROUND	LEGEND BORDER		
W20-1 (1 MILE)	48"	48"		TEXT DIMENSIONS SHALL CONFORM TO "2004 EDITION - STANDARD HIGHWAY SIGNS - 2012 SUPPLEMENT"			4	COLORS SHALL CONFORM TO "2004 EDITION -STANDARD HIGHWAY SIGNS -2012 SUPPLEMENT"			16.00 (64)
W20-5L	48"	48"					2				16.00 (32)
W20-5R	48"	48"					4				16.00 (64)
W20-7	48"	48"					2				16.00 (32)
W20-4	48"	48"					2				16.00 (32)
W20-1	48"	48"					2				16.00 (32)

Scale:			
NOT TO SCALE			
No.	Revision	By	Date

Designed by:			
			
CONSULTANT PROJECT MANAGER: Timothy R. Cote, P.E.			
	By	Date	
Designed	PDB	07\25	Checked DAM 07\25
Drawn	PDB	07\25	In Charge of TRC 07\25

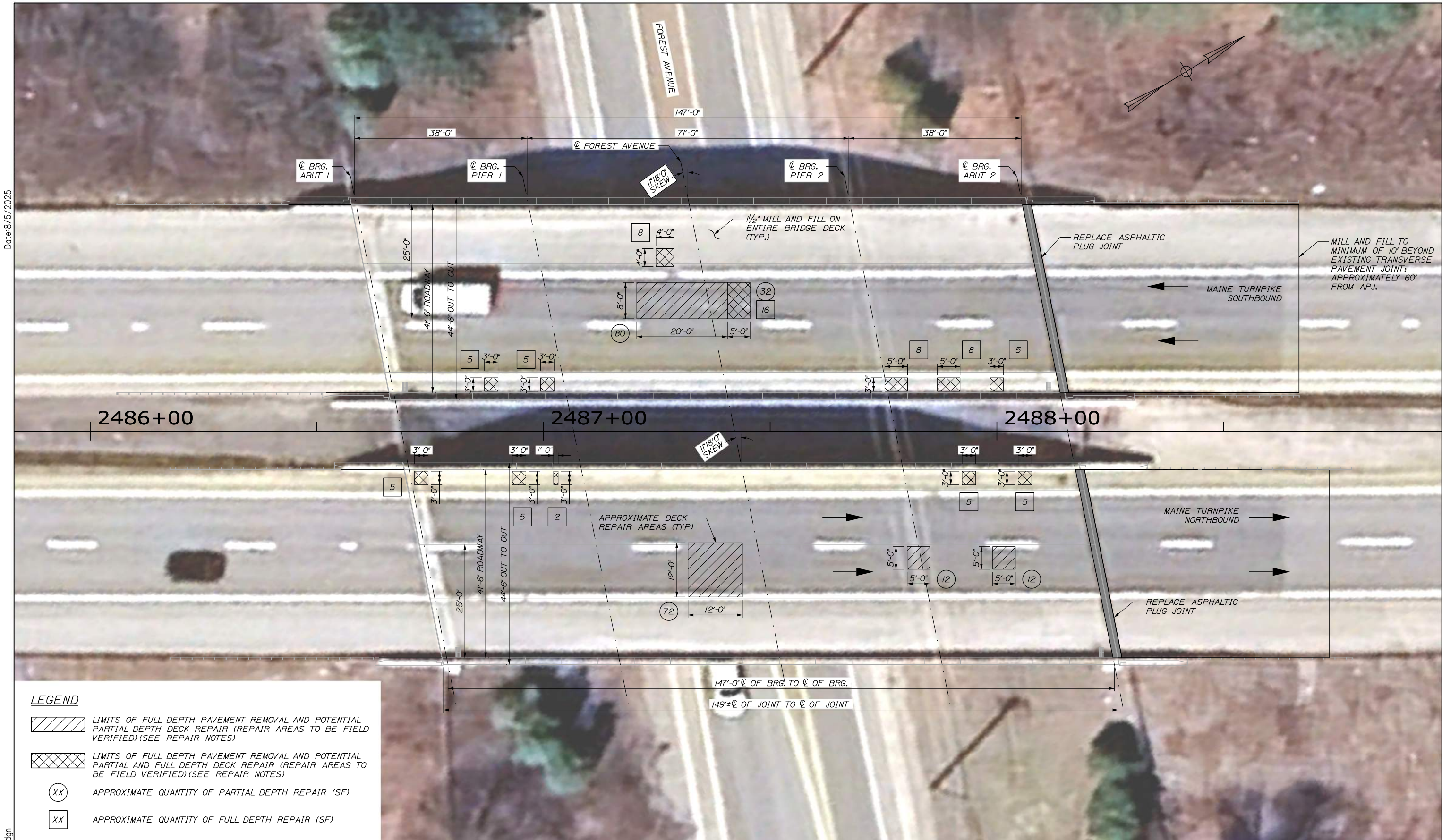
HNTB CORPORATION
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THE GOLD STAR
MEMORIAL HIGHWAY

MTA PROJECT MANAGER: Lauren Fleming, P.E.

EMERGENCY REPAIRS FOREST AVENUE OVERPASS MAINTENANCE OF TRAFFIC SIGN SUMMARY 1 OF 1	
CONTRACT:2025.17	SHEET NUMBER: MOT-08 16 OF 18



LEGEND

- LIMITS OF FULL DEPTH PAVEMENT REMOVAL AND POTENTIAL PARTIAL DEPTH DECK REPAIR (REPAIR AREAS TO BE FIELD VERIFIED)(SEE REPAIR NOTES)
- LIMITS OF FULL DEPTH PAVEMENT REMOVAL AND POTENTIAL PARTIAL DEPTH DECK REPAIR (REPAIR AREAS TO BE FIELD VERIFIED)(SEE REPAIR NOTES)
- APPROXIMATE QUANTITY OF PARTIAL DEPTH REPAIR (SF)
- APPROXIMATE QUANTITY OF FULL DEPTH REPAIR (SF)

Scale:

No.	Revision	By	Date

Designed by:					
CONSULTANT PROJECT MANAGER: Timothy R. Cote, P.E.					
	By	Date		By	Date
Designed	PDB	07\25	Checked	DAM	07\25
Drawn	ERB	07\25	In Charge of	TRC	07\25

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THE GOLD STAR
MEMORIAL HIGHWAY

MTA PROJECT MANAGER: Lauren Fleming, P.E.

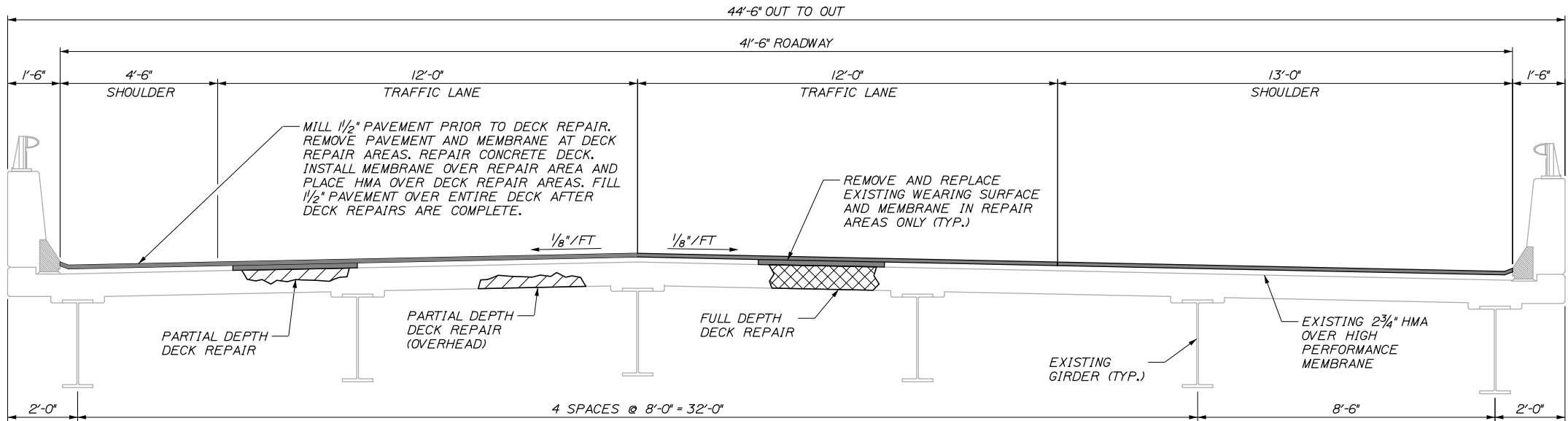
EMERGENCY REPAIRS FOREST AVENUE OVERPASS DECK REPAIR PLAN	
CONTRACT:2025.17	SHEET NUMBER: S-04 17 OF 18

Filename: 017_Forest - Plan.dgn

Date:8/5/2025

Date:8/5/2025

Filename: 018_Forest - Bridge Sections.dgn



TRANSVERSE SECTION

1/2" = 1'-0"
(NB - LOOKING NORTH)
(SB - LOOKING SOUTH)

LEGEND

- PARTIAL DEPTH DECK REPAIRS
FULL DEPTH DECK REPAIRS

GENERAL NOTES:

1. ASSUMED AREAS OF PARTIAL AND FULL DEPTH DECK REPAIRS, AND ESTIMATED QUANTITIES FOR EACH REPAIR AREA, ARE IDENTIFIED IN PLAN VIEW ON THIS SHEET. THE AREAS OF FULL DEPTH PAVEMENT AND MEMBRANE REMOVAL ARE INTENDED TO PROVIDE ACCESS TO THE TOP OF DECK TO SUPPORT INSPECTION AND REPAIR. ACTUAL CONCRETE REPAIR LIMITS WILL BE DETERMINED IN THE FIELD BY THE RESIDENT FOLLOWING THE REMOVAL OF PAVEMENT AND MEMBRANE.
2. FULL DEPTH DECK REPAIR AREAS SHOWN ON THE PLAN ARE BASED ON VISUAL INSPECTION OF BOTH THE WEARING SURFACE AND THE UNDERSIDE OF THE DECK, WHERE SIGNIFICANT DETERIORATION IS OBSERVED ON BOTH SIDES. IF TOP SIDE INSPECTION DOES NOT CONFIRM THE NEED FOR FULL-DEPTH REPAIRS IN THE AREAS NOTED, THE CONTRACTOR SHALL INSTEAD REPAIR ANY SIGNIFICANT CRACKING AND SPALLING FOUND ON THE UNDERSIDE OF THE DECK. THIS WORK WILL BE PAID UNDER PAY ITEM 518.70 REPAIR OF OVERHEAD SURFACES < 8 INCHES.
3. THE ROADWAY SHALL REMAIN CLOSED TO TRAFFIC UNTIL THE NEWLY PLACED CONCRETE REACHES A MINIMUM COMPRESSIVE STRENGTH OF 3,500 PSI. STRENGTH SHALL BE DETERMINED USING THE MANUFACTURER'S STRENGTH GAIN TABLES AND THE AMBIENT TEMPERATURE AT THE TIME OF PLACEMENT. CYLINDER TESTING WILL NOT BE REQUIRED.
4. ESTIMATED QUANTITIES FOR PARTIAL AND FULL DEPTH DECK REPAIRS INCLUDE A 50% CONTINGENCY TO ACCOMMODATE POTENTIAL ADDITIONAL AREAS REQUIRING REPAIR. OVERHEAD DECK REPAIR QUANTITIES ARE ASSUMED AND MAY BE ADJUSTED BASED ON FIELD CONDITIONS.
5. SHIELDING SHALL BE PROVIDED FOR ALL REPAIR LOCATIONS OVER LIVE TRAFFIC. SHIELDING SHALL EXTEND THE FULL BAY WIDTH UNDERNEATH THE REPAIR AND LONGITUDINALLY TO 3 FT BEYOND THE REPAIR.
6. CONTRACTOR SHALL PROVIDE THE NECESSARY SHIELDING TO PROTECT REMOVED CONCRETE AND NEWLY PLACED CONCRETE FROM ENTERING THE TRAVEL LANES AND SIDEWALKS OF FOREST AVENUE. SHIELDING FOR ANY FULL DPETH REPAIR OR UPWARD FACING REPAIR SHALL REMAIN IN PLACE. ALL SHIELDING TO REMAIN IN PLACE SHALL CONSIST ENTIRELY OF DIMENSIONAL LUMBER, AND THE CONTRACTOR SHALL SUBMIT WORKING DRAWINGS WHERE THE SHIELDING IS BRACED VERTICALLY TO THE UNDERSIDE OF THE BRIDGE DECK AND LONGITUDINALLY AGAINST DIAPHRAGM AND CONNECTION PLATES TO ENSURE THE SHIELDING DOES NOT SHIFT OR MOVE OVER TIME. PAYMENT SHALL BE INCIDENTAL TO RELATED CONTRACT ITEMS.

GENERAL NOTES (CONT.):

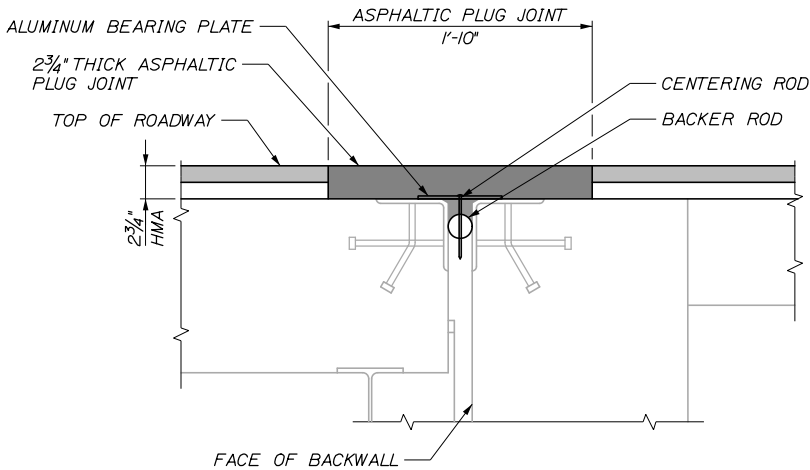
7. SIDEWALKS SHALL REMAIN OPEN DURING CONSTRUCTION UNLESS APPROVED BY THE RESIDENT.
8. ALL WORK SHALL BE ACCOMPLISHED USING THE CONSTRUCTION SEQUENCE PRESENTED IN THE MAINTENANCE OF TRAFFIC DETAILS AND THE SPECIFICATION.
9. PREFORM 1 INCH DEEP SAWCUTS ALONG LIMITS OF REMOVAL.
10. CHIP CONCRETE TO DEPTH SHOWN AND DESCRIBED IN SPECIFICATION 518.
11. THE EXISTING WEARING SURFACE AND MEMBRANE SHALL BE REMOVED IN LOCATIONS IDENTIFIED ON SHEET S-04. THE RESIDENT SHALL IDENTIFY AREAS OF DECK REPAIRS BASED ON THE REQUIREMENTS IN SUPPLEMENTAL SPECIFICATION 518 AND DELINEATE AREAS OF THE REPAIR. THE AREAS OF REPAIR DELINEATED BY THE RESIDENT WILL INCLUDE OBVIOUS LOCATIONS OF DETERIORATION (I.E., EXPOSED REINFORCING, SPALLS, OR NOTABLE DETERIORATION). THIS PROJECT IS NOT INTENDED TO ADDRESS ALL AREAS OF DETERIORATION.
12. AFTER THE RESIDENT HAS DELINEATED THE REPAIR AREAS, THE CONTRACTOR AND RESIDENT SHALL AGREE ON THE REPAIR LIMITS. SHOULD THE REMOVAL AREA LIMITS CHANGE DURING THE REPAIR PROCESS, THE CONTRACTOR SHALL NOTIFY THE RESIDENT. THE RESIDENT AND CONTRACTOR SHALL AGREE ON THE REVISED PAY LIMITS PRIOR TO THE CONTRACTOR CONTINUING THE REMOVALS. WORK SHALL BE INCIDENTAL TO THE SPECIFIED REPAIR ITEMS IN SPECIFICATION 518.
13. PREPARE AND PATCH PARTIAL DEPTH AND FULL DEPTH REPAIR AREAS WITH RAPID SETTING CONCRETE SELECTED FROM THE MAINEDOT QUALIFIED PRODUCTS LIST FOR RAPID SET CONCRETE PATCH MATERIALS. SELECTED PRODUCT SHALL BE APPLICABLE FOR TRAFFIC LOADING AND SUBMITTED FOR APPROVAL TO THE RESIDENT. OVERHEAD REPAIR AREAS SHALL BE COMPLETED WITH A RAPID SETTING REPAIR MATERIAL SPECIFICALLY FORMULATED FOR OVERHEAD LOCATIONS. SEE SPECIFICATIONS FOR MATERIAL MATERIAL PREPERATION, PLACEMENT, AND CURING REQUIREMENTS.
14. ALL REPAIRS, WHETHER ON THE TOP OR BOTTOM SIDE OF THE BRIDGE DECK, SHALL EXTEND TO A MINIMUM OF 1 INCH BEHIND REINFORCING.
15. AREAS OF MEMBRANE REMOVAL SHALL BE REPAIRED WITH HIGH PERFORMANCE WATERPROOFING MEMBRANE APPLIED BY THE CONTRACTOR. AREAS OF NEW MEMBRANE SHALL LAP THE EXISTING MEMBRANE BY AT LEAST 6 INCHES.

GENERAL NOTES (CONT.):

16. INSTALLATION OF THE BASE COURSE OF PAVEMENT OVER THE CONCRETE DECK REPAIRS MAY BE HAND OR MARCHINE PLACED.
17. EXISTING HIGH PERFORMANCE WATERPROOFING MEMBRANE SHALL BE REMOVED APPROXIMATELY 6\" BEYOND THE DECK REPAIR AREA.
18. SEAL DECK SHOULDERS WITH TWO APPLICATIONS OF EMULSIFIED BITUMINOUS SEALER PER MDOT STANDARD SPECIFICATION 403.03 d AFTER COMPLETING ALL DECK PAVING WORK.

APJ NOTES:

1. ASPHALTIC PLUG JOINT MATERIALS SHALL MEET THE REQUIREMENTS OF THE PROJECT SPECIFICATIONS AND SHALL BE INSTALLED IN ACCORDANCE WITH THE MANUFATURER'S RECOMMENDATIONS.
2. ASPHALTIC PLUG JOINTS SHALL BE INSTALLED AFTER THE SECOND LIFT OF PAVEMENT IS INSTALLED. CARE SHALL BE TAKEN BY THE CONTRACTOR TO AVOID DAMAGING THE MEMBRANE DURING JOINT INSTALLATION AS OUTLINED IN SPECIAL PROVISION SECITON 520.
3. THE JOINT AREA SHALL BE CLEANED IN ACCORDANCE WITH THE MANUFACTURER'S RECOMMENDATIONS.
4. SAW CUT IN PAYMENT SHALL BE SEALED WITH EMULSIFIED ASPHALT SEALING COMPOUND CONFORMING TO SPECIFICATION 702.12.
5. 16D GALVANIZED CENTERING NAILS SHALL BE SPACED AT 12\" O.C. MAXIMUM AND PLACED 2\" FROM THE JOINTS IN THE BEARING PLATE.
6. BACKER ROD SHALL BE CLOSED CELL FOAM IN ACCORDANCE WITH THE PROJECT SPECIFICATION. BACKER ROD SHALL BE SIZED AS REQUIRED TO FILL THE OPENING.



ASPHALTIC PLUG JOINT DETAIL

FOREST AVENUE BRIDGE NB & SB

NORTH ABUTMENT PROPOSED WORK

1/2" = 1'-0"

Scale:			
AS NOTED			
No.	Revision	By	Date

Designed by:					
HNTB					
CONSULTANT PROJECT MANAGER: Timothy R. Cote, P.E.					
	By	Date		By	Date
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MEMORIAL HIGHWAY

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EMERGENCY REPAIRS
FOREST AVENUE OVERPASS
DECK REPAIR
SECTIONS

SHEET NUMBER: S-05
CONTRACT: 2025.17
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