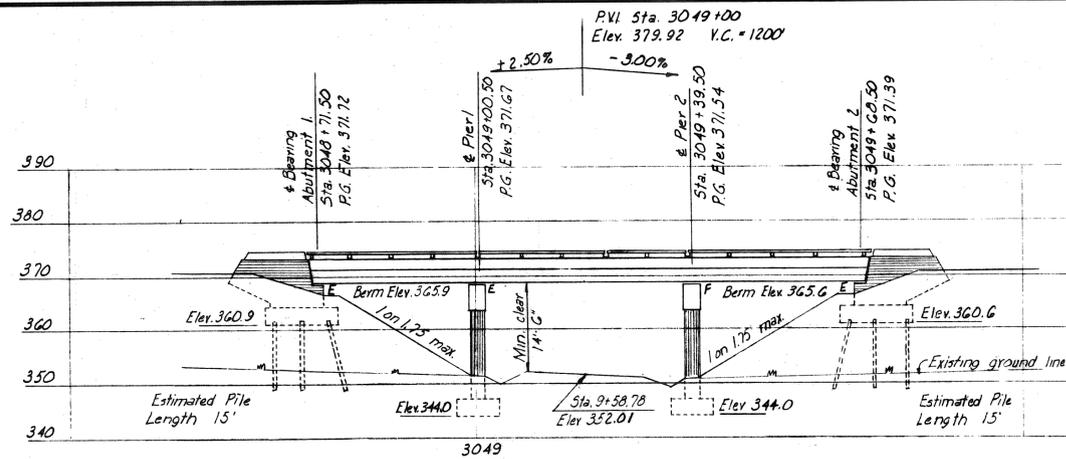


GENERAL NOTES

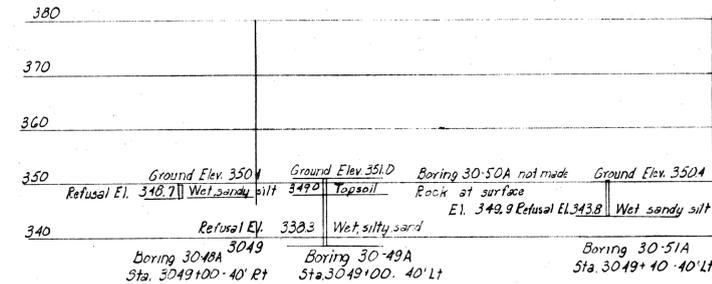
Design Specifications: AA.5.H.O. (1953) with minor modifications
 Design Live Load: H 20 S16
 Maximum Pile Load: Abutments - 35.4 Ton/pile
 Maximum Base Pressure: Piers 2.9 Ton/sq ft.



ELEVATION

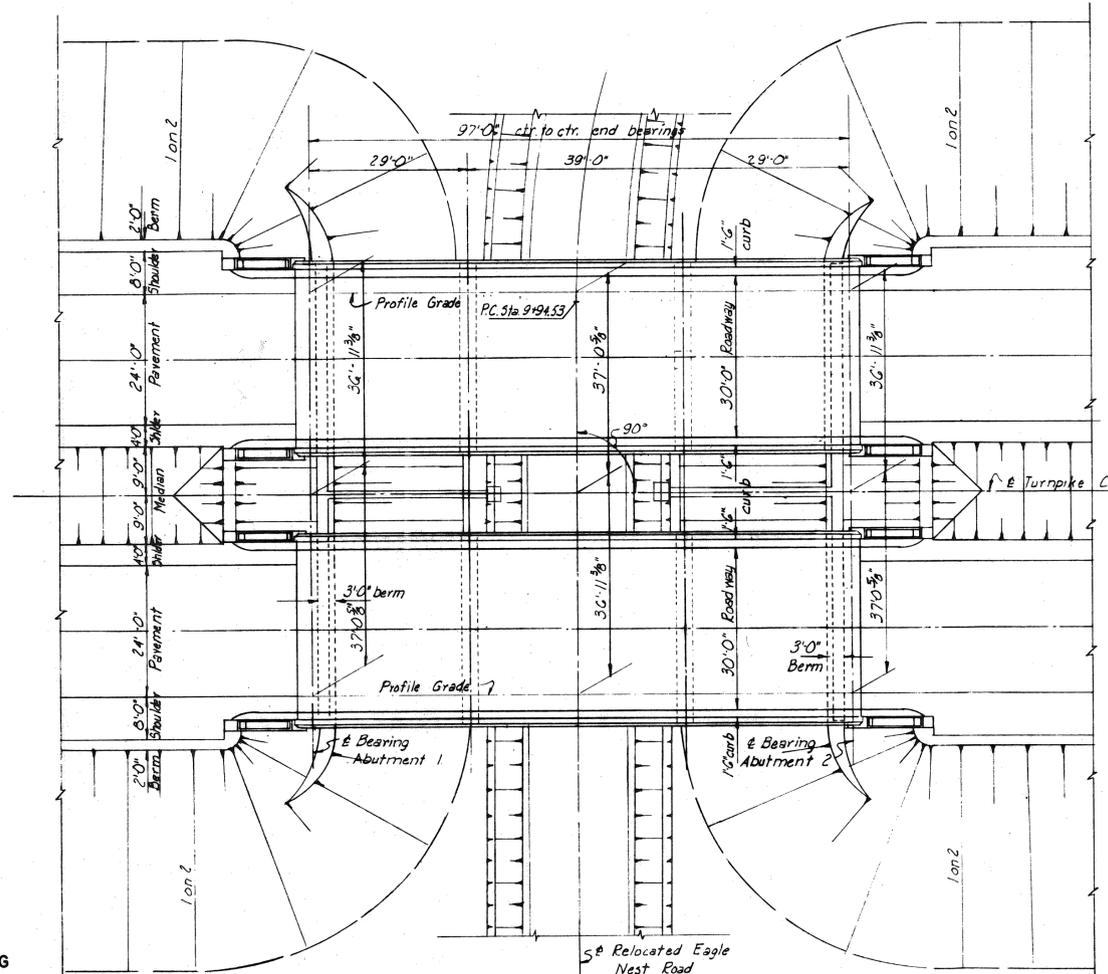
Turnpike Curve Data
 P.I. Sta. 3067+44.07
 $\Delta = 20^{\circ}00'$ Lt.
 $D = 0^{\circ}30'$
 $R = 11459.16$
 $T = 2020.56$
 $L = 1000'$
 $E = 176.78$

Relocated Eagle Nest Road Curve Data
 P.I. Sta. 11+28.44
 $\Delta = 31^{\circ}20'00''$ Rt.
 $D = 12^{\circ}00'$
 $R = 477.47'$
 $T = 133.91'$
 $L = 261.11$
 $E = 18.42$

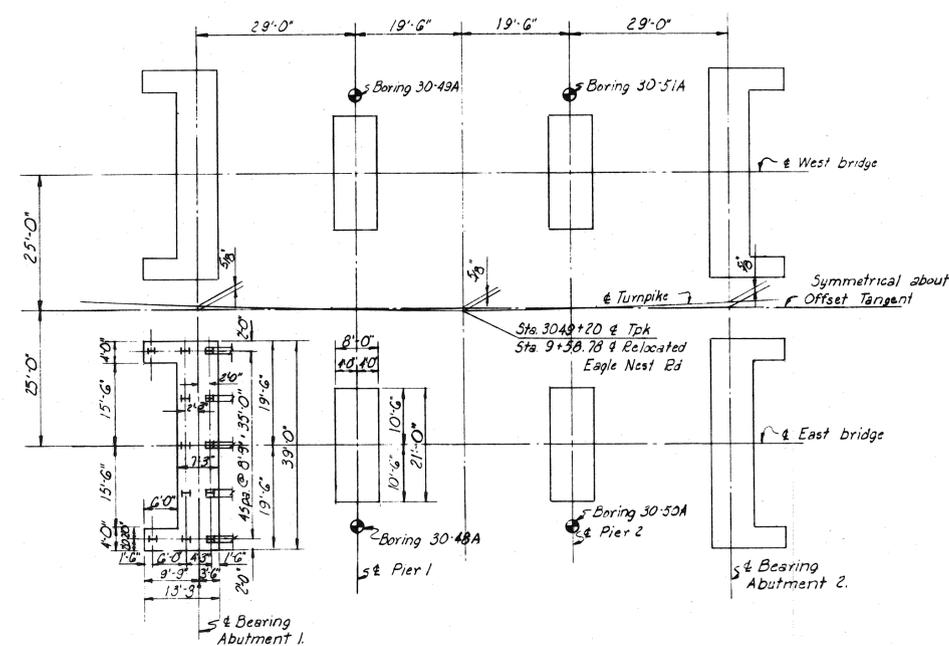


BORING DATA

REFERENCES					
Drawing Number	Title	Substruct. contractor	Steel / Pier / Deck	Steel / Pier / Deck	Superstructure Floor / Pier / Deck
SD-1A	Standard Abut Details	✓	✓	✓	✓
SD-2	Standard Pier Details	✓	✓	✓	✓
SD-3	Abutment Drainage Details	✓	✓	✓	✓
SD-4	Standard Pile Details	✓	✓	✓	✓
SD-5	Standard Handrail, Bearing Devices and Misc Details	✓	✓	✓	✓
SD-6	Standard Diaphragm Details	✓	✓	✓	✓
SD-1A	Type "X" and "Y" Expansion Joints	✓	✓	✓	✓
SD-17	Standard Bridge Floor Cross Section, 28'-0" & 30'-0" Roadways	✓	✓	✓	✓

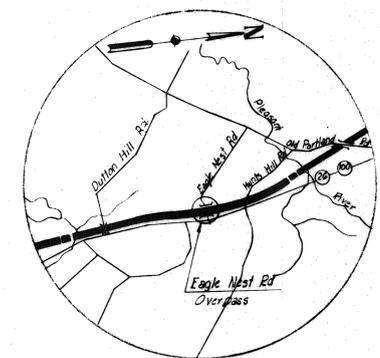


PLAN



FOOTING PLAN

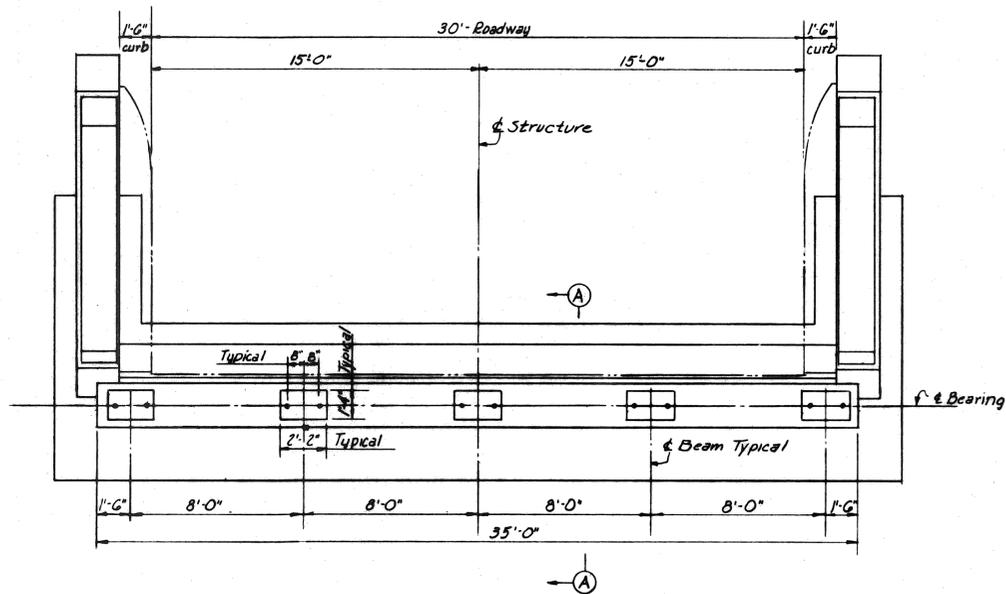
Note: Piles shown battered shall be battered 3/1ft.
 All piles shall be 10BP42
 All pier footings are identical
 Abutment 2 footings are the same as Abutment 1 footings by rotation.



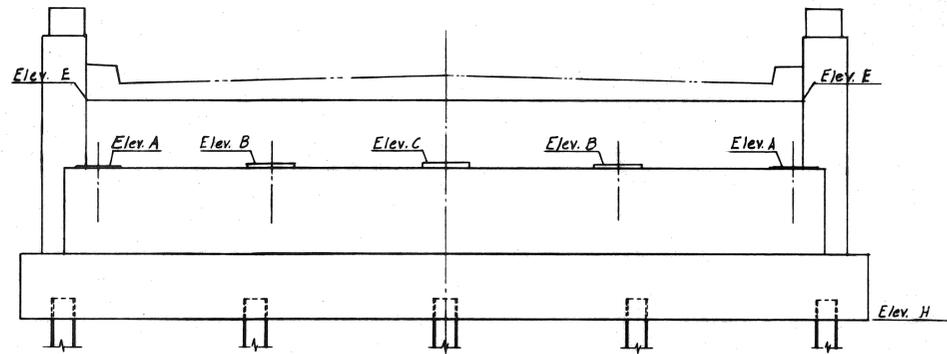
VICINITY MAP
 Scale: 1" = 1 Mile

DRAWING					
35.01.03					
BY	DATE				
MADE	B.K.	4-7-54			
TRACED					
CHECKED	H.J.G.	4-8-54	1	As-Built	HGH 1/1956
IN CHARGE OF	IDSJ		No.	REVISION	BY DATE

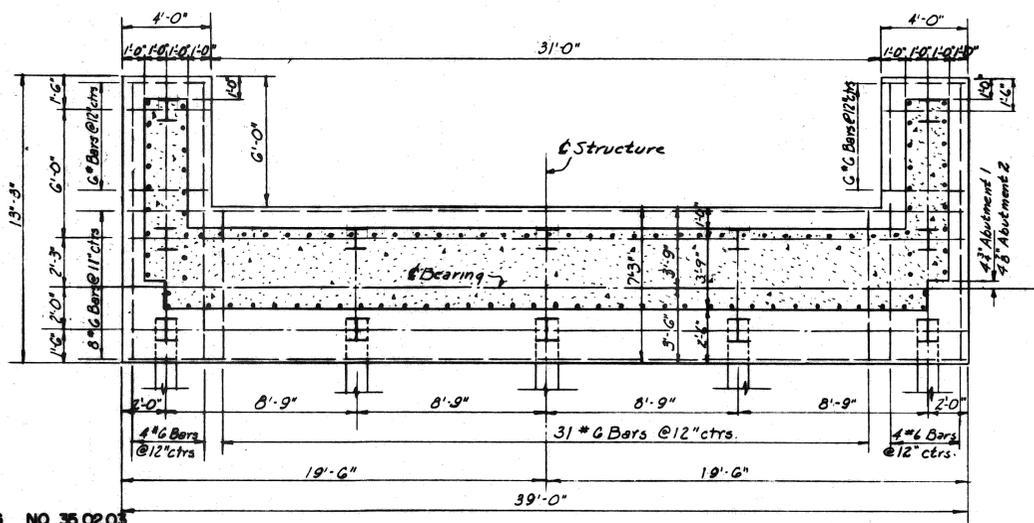
MAINE TURNPIKE AUTHORITY MAINE TURNPIKE SECTION 2 — PORTLAND TO AUGUSTA		
STRUCTURE NO. 35	TURNPIKE OVER	
EAGLE NEST ROAD STA. 3049 + 20		
GENERAL PLAN AND ELEVATION		
HOWARD, NEEDLES, TAMMEN & BERGDOFF		SCALE: 1/16" = 1'-0" except as shown
CONSULTING ENGINEERS		CONTRACT NO.
NEW YORK	KANSAS CITY	SHEET NO. 164 OF 382



PLAN

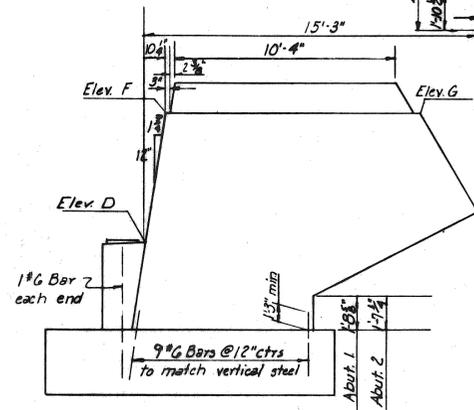


ELEVATION

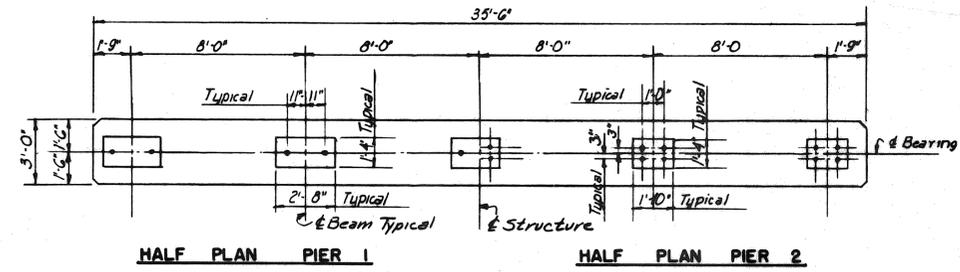


FOOTING PLAN
ABUTMENTS

ELEVATIONS	
Abut. 1	Abut. 2
A 368.10	367.77
B 368.15	367.82
C 368.23	367.90
D 368.02	367.69
E 370.94	370.60
F 373.86	373.52
G 373.87	373.45
H 360.90	360.60

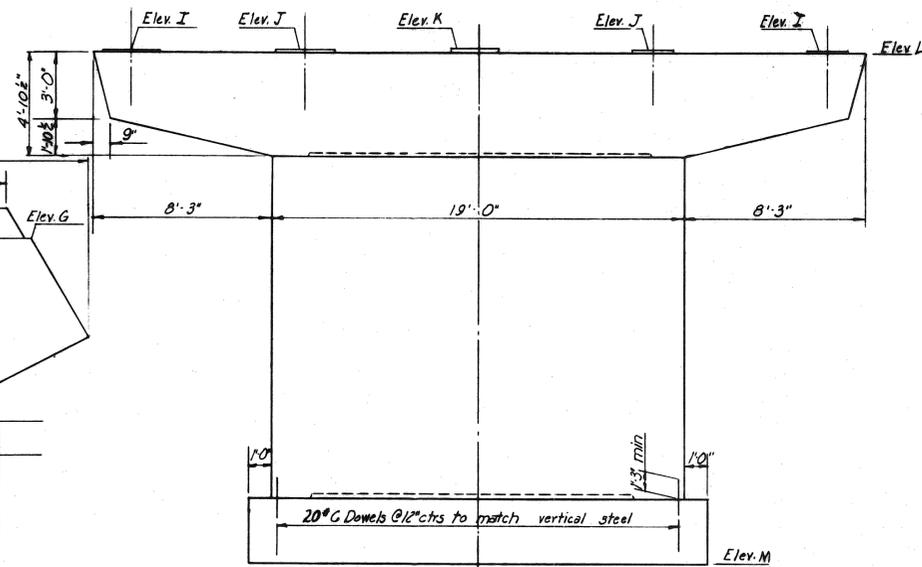


END ELEVATION

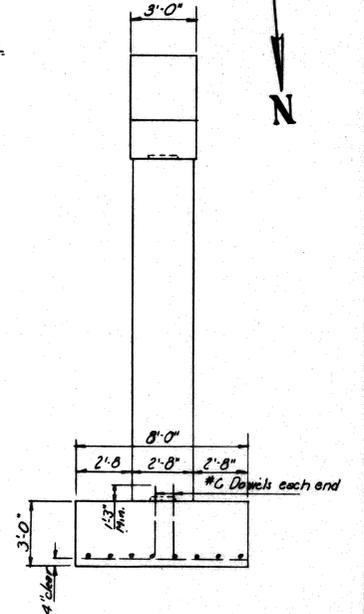


HALF PLAN PIER 1

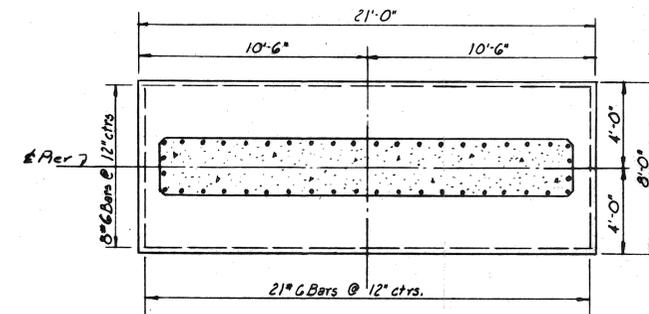
HALF PLAN PIER 2



ELEVATION

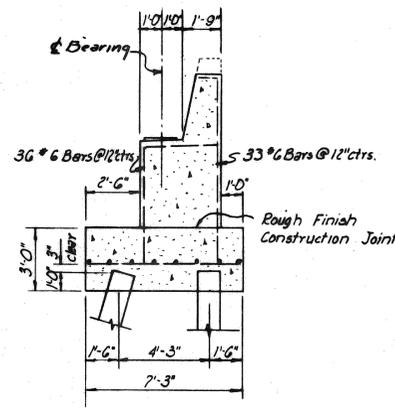


END ELEVATION



FOOTING PLAN

ELEVATION	
Pier 1	Pier 2
I 367.88	367.91
J 367.90	367.93
K 367.98	368.02
L 367.86	367.89
M 344.00	344.00



SECTION A-A

Note: Piles shall be 10BP42
Battered piles shall be
battered 3"/ft.

DRAWING NO. 35.02.03

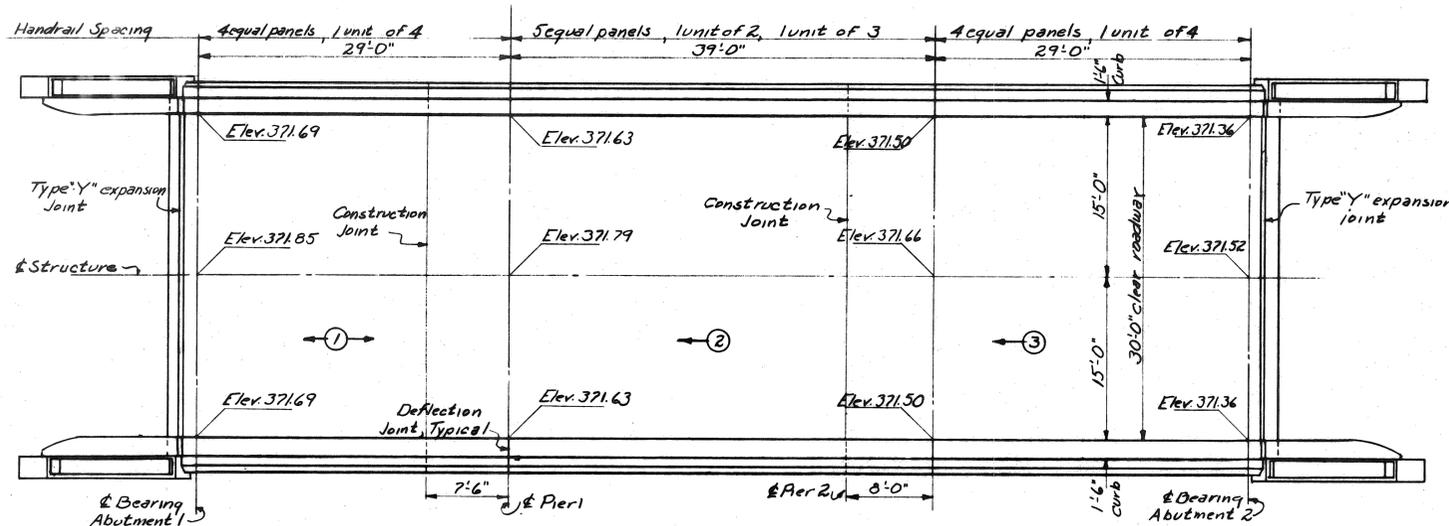
MADE	BY	DATE			
TRACED	B.K.	1-1-54			
CHECKED	R.S.G.	4-25-54	As-Built	NON-HRS	
IN CHARGE OF	I.D.S.K.		No.	REVISION	BY DATE

MAINE TURNPIKE AUTHORITY
MAINE TURNPIKE
 SECTION 2 — PORTLAND TO AUGUSTA

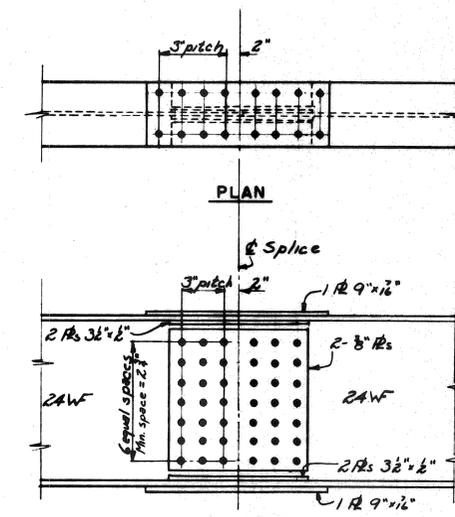
STRUCTURE NO. 35 TURNPIKE OVER
 EAGLE NEST ROAD
 STA. 3049 + 20
 ABUTMENTS AND PIERS

HOWARD, NEEDLES, TAMMEN & BERGENDOFF CONSULTING ENGINEERS
 NEW YORK KANSAS CITY

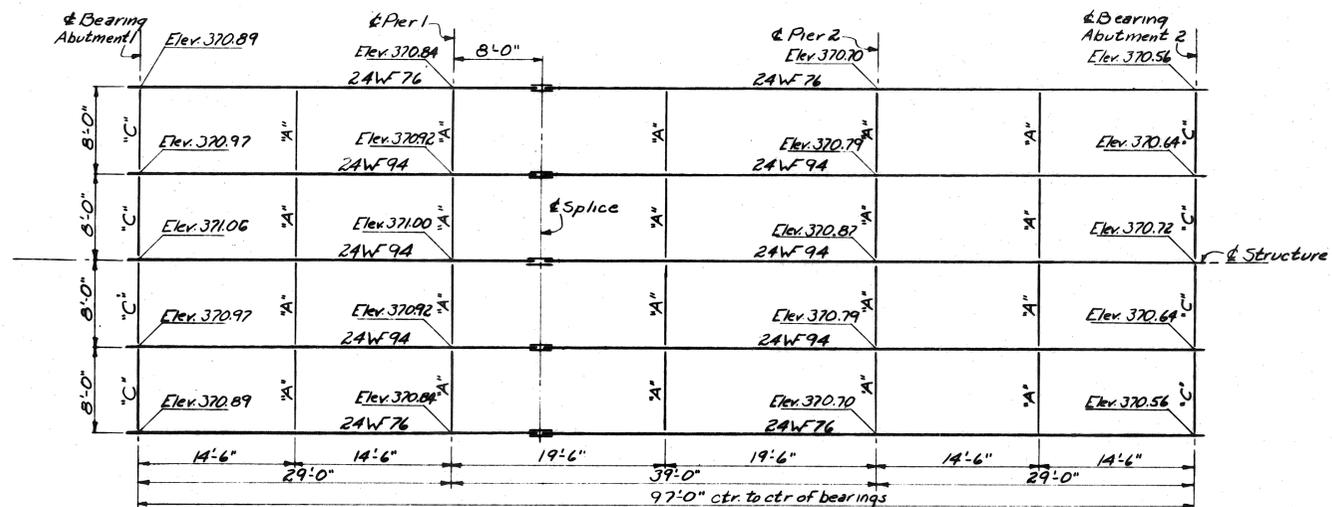
SCALE: 1/4"=1'-0"
 CONTRACT NO. _____
 SHEET NO. 65 OF 302



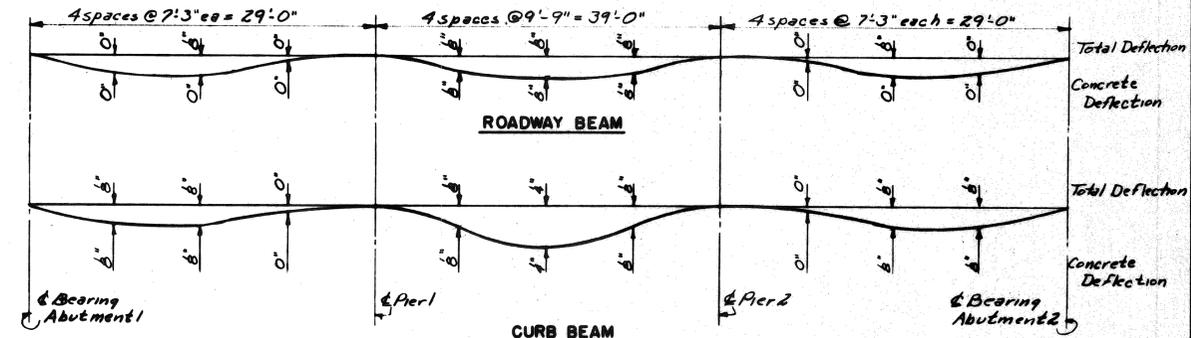
PLAN
Scale: 8"=1'-0"



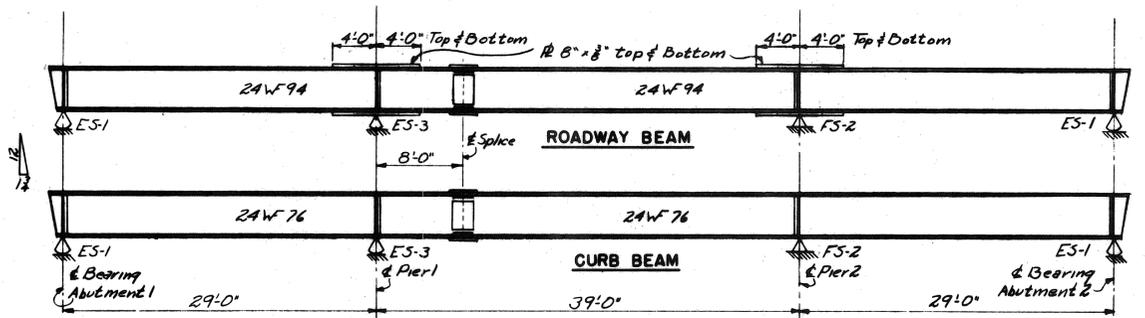
TYPICAL BEAM SPLICE
Scale: 1"=1'-0"



FRAMING PLAN
Scale: 8"=1'-0"



DEAD LOAD DEFLECTION DIAGRAMS
No Scale



BEAM ELEVATIONS
No Scale

Notes:
Elevations shown on floor plan are to top of finished roadway.
Elevations shown on framing plan are to top of beam flanges.
Slab is 6 1/2" non-composite design with 2" bituminous wearing surface as shown on Standard Drawing No. 17.
Sequence and direction of placing concrete are noted thus ->.
Marks "A" and "X" on framing plan represent Standard Diaphragms as shown on Standard Drawing No. 6.
Bents at beam ends are with respect to beam axis.
All bearing stiffeners are 7x4x8 Ls.

DRAWING NO. 35.03.03

BY	DATE			
MADE	RSG	4-5-54		
TRACED				
CHECKED	H.L.G.	4-6-54	1	As-Built
IN CHARGE OF	IDSK	No.	REVISION	BY DATE

MAINE TURNPIKE AUTHORITY	
MAINE TURNPIKE	
SECTION 2 - PORTLAND TO AUGUSTA	
STRUCTURE NO. 35	TURNPIKE OVER
EAGLE NEST ROAD	
STA. 3049+20.00	
SUPERSTRUCTURE	
HOWARD, NEEDLES, TAMMEN & BERGENDOFF	SCALE: As Shown
CONSULTING ENGINEERS	CONTRACT NO.
NEW YORK	KANSAS CITY
SHEET NO. 66 OF 302	