

MAINE TURNPIKE AUTHORITY

ADDENDUM NO. 1

CONTRACT 2025.07

CULVERT REPAIRS

DUNSTAN RIVER CULVERT (MM 40.30)

UNNAMED STREAM CULVERT (MM 72.00)

CULVERT (MM 72.20)

A pre-bid meeting was held on April 1, 2025. The pre-bid meeting agenda, presentation and sign-in sheet are attached to this addendum. **All questions must be submitted by 5:00pm on Monday April 7, 2025, to be considered.**

Questions:

The following are questions asked at the pre-bid meeting or submitted to the Authority in writing. Answers to the questions are noted. Bidders shall utilize this information in preparing their bid.

Question 1: Would you consider GeoKrete Geopolymer an approved cementitious liner in lieu of Slip Lining for this project?

Response: *GeoKrete Geopolymer is not approved as a sliplining material for this project.*

Question 2: Can you clarify what portions of the access ways need to be fully restored?

Response: *Per Special Provision 510, any access ways that meet the criteria outlined in Section 510.06 may remain in place after completion of the work as approved by the Resident. Any temporary access ways constructed that do not meet the criteria outlined in Section 510.06 shall be restored to the preconstruction condition.*

Question 3: For the Dunstan River Culvert, can the headwalls be cast-in-place?

Response: *Per Special Provision 534, the Contractor may construct the headwalls and wingwalls from precast concrete or cast-in-place concrete, at their option.*

Question 4: Are there pictures available of the culvert inlets and outlets?

Response: *Yes. See attached for culvert photos. Note that these photos may not be representative of the current site conditions.*

Question 5: Could I please get some clarification on the special detour item? Just want to confirm that if we choose to leave the access in place if accepted and built to spec, or if we choose to do more of a temporary access, either method will be paid under this item?

Response: *Yes. All work for the construction of the access ways, whether or not they are constructed to be left in-place, shall be paid as Item 510.131 and Item 510.132.*

Question 6: Was wondering if the slip-lining portion would be allowed to be done in the wet at all? Cleaning, bulkheading, grouting and all other in stream work could all be done in the dry.

Response: *Only the installation of the liner pipe may be done in the wet, at the Contractor's option. All work done in the wet shall be done within the required in-water work window at each location.*

Make the following changes to the bid documents:

In the Pre-Bid Agenda, Page 1 Section 3 **DELETE** “at (207) 871-7739 (fax) or.” All questions shall be submitted by email.

Note: The above items shall be considered as part of the bid submittal.

Attachments

- Pre-Bid Agenda (4 pages)
- Pre-Bid Presentation (18 pages)
- Pre-Bid Sign-In Sheet (1 page)
- Project Photos (15 pages)

The total number of pages included with this addendum is forty (40).

All bidders are requested to acknowledge the receipt of the Addendum No. 1 by signing below and emailing this sheet to Nate Carll, Purchasing Department, ncarll@maineturnpike.com. Bidders are also required to acknowledge receipt of this Addendum No. 1 on Page P-3 of the bid package.

Business Name

Print Name and Title

Signature

Date

April 4, 2025

Very truly yours,

MAINE TURNPIKE AUTHORITY

Purchasing Manager
Maine Turnpike Authority

MAINE TURNPIKE AUTHORITY

Pre-Bid Conference

CONTRACT 2025.07

CULVERT REPAIRS

DUNSTAN RIVER CULVERT (MM 40.30)

UNNAMED STREAM CULVERT (MM 72.00)

CULVERT (MM 72.20)

April 1, 2025, 10:00 AM

1) Location:

The general limits of work are as shown in the Contract Plans. The Dunstan River Culvert is located at MM 40.30 of the Maine Turnpike. The Unnamed Stream Culvert is located at MM 72.00 of the Maine Turnpike. The third Culvert is located at MM 72.20 of the Maine Turnpike.

2) General Description:

The work consists of culvert repairs at three locations. At the Dunstan River Culvert at MM 40.30 in the Town of Scarborough, the work generally consists of constructing access ways as needed to complete the work, including restoration, and sliplining the existing 78" reinforced concrete pipe culvert with a 66" liner pipe and constructing new headwalls and wingwalls.

At the Unnamed Stream Culvert at MM 72.00 in the Town of New Gloucester, the work generally consists of constructing access ways as needed to complete the work, including restoration, and sliplining the existing 60" reinforced concrete pipe culvert with a 54" liner pipe and headwall concrete repairs.

At the culvert at MM 72.20, in the Town of New Gloucester, the work generally consists of modifying existing access ways as needed to complete the work, including restoration, and sliplining the existing 30" culvert with a UV-CIPP.

The work also includes maintenance of traffic and all other work incidental thereto in accordance with the Plans and Specifications.

3) Bid:

- a) Bid opening is April 10, 2025 at 10:00 A.M. at MTA headquarters 2360 Congress Street, Portland.
- b) All bid and contractual questions shall be directed to Purchasing Department, Phone No. (207) 482-8115.
- c) All questions on plans and specifications shall be in writing and shall be directed to Nate Carll, Purchasing Manager, at ~~(207) 871-7739 (fax)~~ or email ncarll@maineturnpike.com.
- d) All questions must be submitted by 5:00pm on Monday April 7, 2025 to be considered.

4) Notification:

- a) Contractor shall notify and obtain approval from the Authority prior to visiting the Project site for field inspection. The contact person is Mr. Steve Tartre at (207) 482-8144 or startre@maineturnpike.com.

5) Construction Schedule/Prosecution of Work:

- a) MTA Board is scheduled to consider the Contract Award on April 24, 2025.
- b) At all locations:
 - i) All work shall be substantially complete by October 31, 2025.
 - ii) All work shall be completed on or before November 30, 2025.
- c) Substantially complete shall be defined as:
 - i) All lining at all culvert locations, as required by the specifications.
 - ii) All access ways and work pads have been restored as directed in the Contract Plans.
 - iii) Supplemental Liquidated damages on a calendar day basis in accordance with Subsection 107.8 shall be assessed for each calendar day that substantial completion is not achieved.
- d) All work completed shall include all permanent erosion control in place and grass growth as approved by the Resident.

6) Maine Department of Labor – Fair Hourly Wages (Special Provision 104.3.8)

- a) 2025 Highway & Earth wages for Cumberland County are included in the Contract Book.

7) Utility Coordination (Special Provision 104.4.6)

- a) Existing utilities were not located for this project.
- b) The Contractor shall contact Dig Safe and any non-member utility operators through OK-TO-DIG prior to any work.
- c) Utility adjustments are not anticipated as part of this project. If any unexpected utility relocations become necessary, they will be scheduled in compliance with Section 104 of the Standard Specifications and will be done by the utilities in conjunction with the work by the Contractor.

8) Cooperation With Other Contractors (Special Provision 104.4.7)

- a) Adjacent contracts currently scheduled for the 2025 construction season include:
 - i) MTA Contract 2022.07– Interchange Improvements Saco (Exit 35 & 36) MM 34.7 – MM 36.6
 - ii) MTA Contract 2025.04 – Bridge Repair (MM 4.80, MM 34.40, MM FS 3.70)
 - iii) MTA Contract 2025.09 – 2025 Roadside Clearing

9) Permit Requirements (Special Provisions 105.8.2)

- a) All work shall be done in accordance with the project's environmental permits. All in-stream work at the Dunstan River Culvert (MM 40.30) and the Unnamed Stream Culvert (MM 72.00) shall be completed during the June 1 to September 30 in-water work window. At the Culvert at MM 72.20 there are no in-stream work window restrictions.
- b) The Contractor shall comply with the conditions outlined in the contract book for the U.S. Army Corps of Engineers General Permits for the State of Maine, the NRPA Permit by Rule Section 11 standards (Dunstan River), the anticipated NRPA Tier 3 Permit (Unnamed Stream), The Stormwater Memorandum of Agreement for Stormwater Management between the Maine Department of

Transportation, Maine Turnpike Authority, and Maine Department of Environmental Protection, the Maine Erosion and Sedimentation Control Law, and, as applicable to the proposed scope of work, the Maine Pollutant Discharge Elimination System General Permit for stormwater discharge associated with construction activity.

10) As-Built Plans (Special Provision 105.11)

- a) As-builts shall be submitted to reflect any deviations from the design plans.

11) General Requirements

- a) U-Turns at toll plazas and median openings not allowed. (Supplemental Specification 105.5.1)
- b) Contractor access to and from the mainline shall not negatively impact mainline traffic flow. The Contractor may be required to establish lane closures to provide for safe access. Refer to Special Provision 652, Specific Project Maintenance of Traffic Requirements, for lane closure requirements and restrictions.
- c) All vehicles used on the Project, including concrete delivery trucks, shall be equipped with amber flashing beacons in accordance with Supplemental Specification 652.3.4.
- d) Class III safety vests must be worn at all times.

12) Traffic Control (Special Provision Section 652):

- a) Special Provision Section 652 replaces the MaineDOT Standard Specification 2014 Edition and MTA 2016 Supplemental Specification Section 652.
- b) Substantive revisions have been bolded in the 652 SP.
- c) Contractor is responsible for supplying all traffic control devices.
- d) Contractor is responsible for placement, relocation, removal and maintenance of traffic control devices. Maintenance of traffic control devices is a 24-hour a day, seven days per week responsibility. Contractor shall inspect devices as required.
- e) Temporary detours, lane closures and construction phasing shall be established and maintained at all times in accordance with the details shown on the Plans and the requirements of the Special Provisions.
 - i) A truck mounted attenuator shall be utilized for all lane closures on the Turnpike mainline, all temporary shoulder closures (i.e. closures that do not include temporary concrete barrier) on the Turnpike mainline, and other construction operations where workers are exposed to traffic and not protected by positive means.
 - ii) A truck mounted attenuator shall be utilized for all work completed behind guardrail if that work is being completed within the deflection zone of the guardrail (within four feet behind the guardrail post).
 - iii) Automated trailer mounted speed limit signs shall only be used when a work zone speed limit is in place and shall be required when the work zone speed limit is active.
 - iv) Truck mounted attenuators and automated trailer mounted speed limit signs are incidental to the related Maintenance of Traffic Control Devices item.
- f) Turnpike closures
 - i) One lane of traffic flowing in each direction at all times. Minimum traveling width of 14' required unless otherwise shown in the Plans.
 - ii) All lane closures require the approval of the Resident. The Resident is required to submit a request to MTA for lane closures by noon on Thursday for lane closures needed for the

following week. The Contractor shall plan the work and requests for lane closures accordingly. Requests are subject to approval by MTA.

- g) All signs, which do not apply to current construction activity, shall be 100% covered or removed in accordance with the plans. This includes any speed limit signs when work zone speed is in operation.
- h) Traffic control devices shall be NCHRP 350 compliant. All traffic control devices shall meet MASH guidelines if the date of manufacture was after December 31, 2019.

13) Specific Contract Items:

- a) The maintenance of traffic control shown on the plans represents one potential configuration. The Contractor may submit alternate maintenance of traffic plans, including temporary lane closures, in accordance with the MTA Standard Maintenance of Traffic Details and Special Provision 652. Any alternate maintenance of traffic plans shall be approved by the Resident.

14) Questions:



**Maine
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MTA Contract 2025.07



Culvert Repairs Dunstan River Culvert Unnamed Stream Culvert Culvert at MM 72.20

Pre-Bid Conference

April 1, 2025

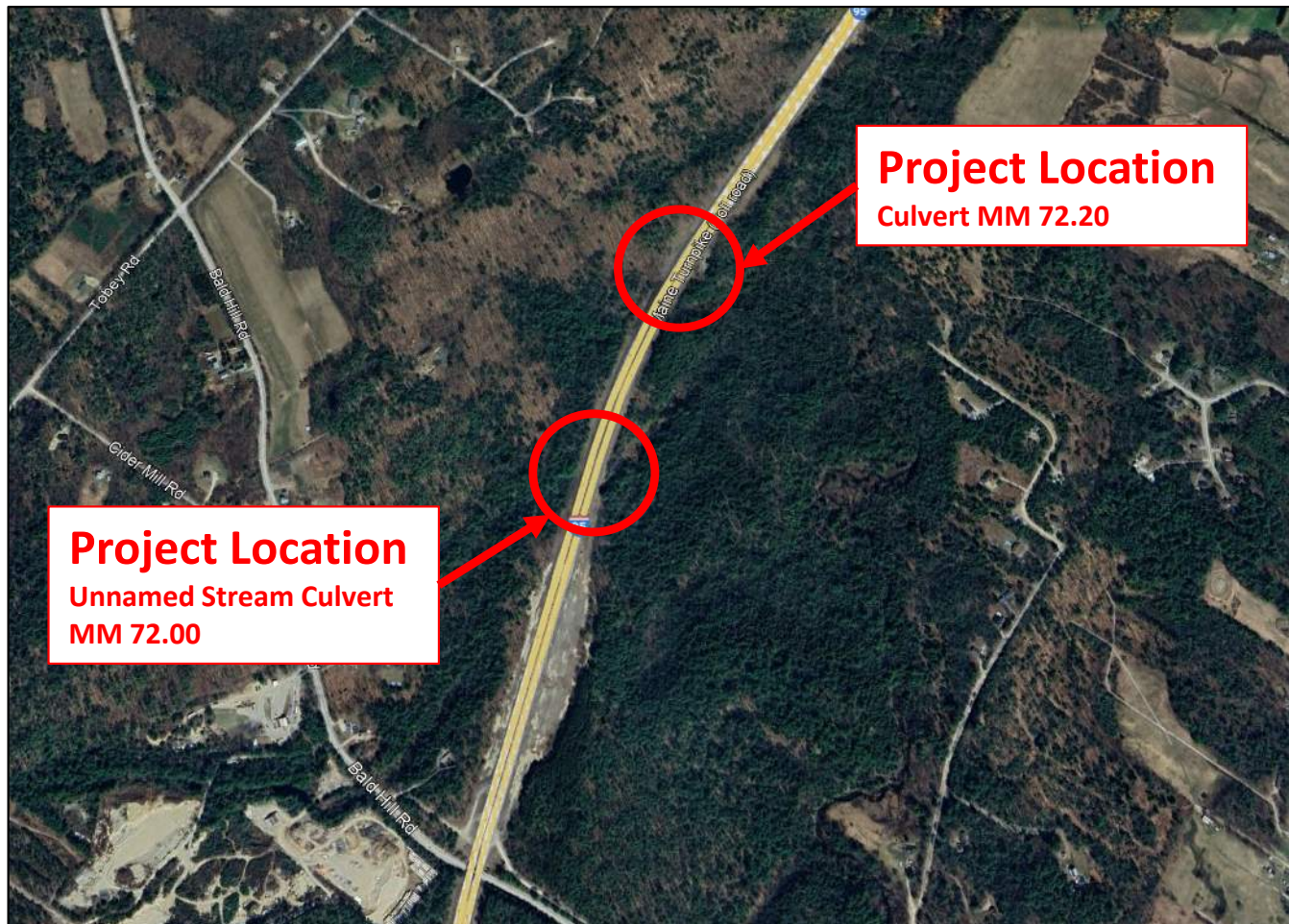


1) Location – Dunstan River Culvert (MM 40.30)





1) Location – Unnamed Stream Culvert (MM 72.00) – Culvert (MM 72.20)





2) General Description

- At the Dunstan River Culvert at MM 40.30, the work includes:
 - Constructing access ways as needed to complete the work, including restoration
 - Sliplining existing 78" RCP culvert with a 66" liner pipe
 - New headwalls and wingwalls
- At the Unnamed Stream Culvert at MM 72.00, the work includes:
 - Constructing access ways as needed to complete the work, including restoration
 - Sliplining existing 60" RCP culvert with a 54" liner pipe
 - Concrete headwall repairs
- At the Culvert at MM 72.20, the work includes:
 - Modifying existing access ways as needed to complete the work, including restoration
 - Sliplining existing 30" RCP culvert with a UV-CIPP



3) Bid

- **Bid opening:** April 10, 2025, at 10:00 A.M.
- All **bid & contractual questions** direct to Purchasing Dept.
 - Phone #: (207) 482-8115
- All **plan & spec questions** submit in writing directed to Nate Carll, Purchasing Manager
 - ~~Fax: (207) 871-7739~~
 - Email: ncarll@maineturnpike.com
- All questions **must be submitted** by 5:00 pm on Monday April 7, 2025



4) Notification

- Contractor shall **notify and obtain approval** from the Authority (Steve Tartre) **prior to visiting the Project site** for field inspection.
 - Phone #: (207) 482-8144
 - email: startre@maineturnpike.com



5) Construction Schedule/Prosecution of Work

- MTA Board is scheduled to consider the **Contract Award on April 24, 2025.**
- At all locations:
 - Substantially complete: **October 31, 2025**
 - All work complete: **November 30, 2025**
- Substantially complete shall be defined as:
 - All lining at all culvert locations, as required by the specifications.
 - All access ways and work pads have been restored as directed in the Contract Plans.
- All work completed shall include all permanent erosion control and place and grass growth as approved by the Resident.



6) Maine Department of Labor – Fair Hourly Wages

- 2025 Highway & Earth wages for Cumberland County are included in the contract book.

7) Utility Coordination

- Existing utilities were not located for this project.
- The Contractor shall **contact Dig Safe** and any non-member utility operators through **OK-TO-DIG prior to any work.**
- If any **unexpected utility relocations** become necessary, they will be scheduled **in compliance with Section 104** of the Standard Specifications and will be **done by the utilities** in conjunction with the work by the Contractor.



8) Cooperation With Other Contractors

- Adjacent contracts currently scheduled for the 2025 construction season include:
 - **MTA Contract 2022.07** – Interchange Improvements Saco (Exit 35 & 36)
MM 34.7 – MM 36.6
 - **MTA Contract 2025.04** – Bridge Repair (MM 4.80, MM 34.40, MM FS 3.70)
 - **MTA Contract 2025.09** – 2025 Roadside Clearing



9) Permit Requirements

- All in-stream work at the Dunstan River Culvert (MM 40.30) and the Unnamed Stream Culvert (MM 72.00) shall be completed during the **June 1 to September 30 in-water work window**. At the Culvert at MM 72.20 there are no in-stream work restrictions.
- The Contractor shall comply with the conditions outlined in the contract book for the following permit items:
 - The **U.S. Army Corps of Engineers General Permits** for the State of Maine,
 - The **NRPA Permit by Rule Section 11** standards (Dunstan River),
 - The anticipated **NRPA Tier 3 Permit** (Unnamed Stream),
 - The **Stormwater Memorandum of Agreement for Stormwater Management** between the Maine Department of Transportation, Maine Turnpike Authority, and Maine Department of Environmental Protection.
 - The **Maine Erosion and Sedimentation Control Law**, and,
 - As applicable to the proposed scope of work, the **Maine Pollutant Discharge Elimination System General Permit** for stormwater discharge associated with construction activity.



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MTA Contract 2025.07



10) As-Built Plans

- As-builts shall be submitted to reflect any deviations from the design plans.



11) General Requirements

- **U-Turns** at toll plazas and median openings **not allowed**.
- **Contractor access** to and from the mainline **shall not negatively impact** mainline traffic flow. The Contractor may be required to establish lane closures to provide for safe access. Refer to Special Provision 652.
- **All vehicles** used on the Project, including concrete delivery trucks, shall be **equipped with amber flashing beacons** in accordance with Supplemental Specification 652.3.4.
- **Class III safety vests** must be worn at all times.



12) Traffic Control (Special Provision Section 652)

- SP 652 **replaces** the MaineDOT Standard Specs 2014 Edition and MTA 2016 Supplemental Specs.
- Substantive **revisions have been bolded** in the 652 SP.
- Contractor is responsible for supplying all traffic control devices.
- Contractor is responsible for placement, relocation, removal and maintenance of traffic control devices. Maintenance of traffic control devices is a **24-hour a day, seven days per week** responsibility. Contractor shall inspect devices as required.



12) Traffic Control (Special Provision Section 652)

- Temporary detours, lane closures and construction phasing shall be established and **maintained at all times**.
 - **A truck mounted attenuator** shall be utilized for **all lane closures** on the Turnpike mainline, shall be utilized for **all temporary shoulder closures** (i.e. closures that do not include temporary concrete barrier) on the Turnpike mainline, and other construction operations where workers are exposed to traffic and not protected by positive means.
 - **A truck mounted attenuator** shall be utilized for **all work completed behind guardrail** if that work is being completed **within the deflection zone** of the guardrail (within four feet behind the guardrail post).
 - **Automated trailer mounted speed limit signs** shall only be used when a work zone speed limit is in place and shall be required when the work zone speed limit is active.
 - Truck mounted attenuators and automated trailer mounted speed limit signs are incidental to the related Maintenance of Traffic Control Devices items.



12) Traffic Control (Special Provision Section 652)

- Turnpike Lane closures
 - **One lane of traffic flowing** in each direction at all times. **Minimum traveling width of 14'** required unless otherwise shown in the Plans.
 - All lane **closures require the approval of the Resident**. The Resident is required to submit a request to MTA for lane closures by **noon on Thursday** for lane closures needed for the following week. The Contractor shall plan the work and requests for lane closures accordingly. Requests are subject to approval by MTA.



12) Traffic Control (Special Provision Section 652)

- All signs, which do not apply to current construction activity, **shall be 100% covered or removed** in accordance with the plans. This includes any speed limit signs when work zone speed is in operation.
- Traffic control devices **shall be NCHRP 350 compliant**. All traffic control devices **shall meet MASH guidelines** if the date of manufacture was **after December 31, 2019**.



12) Specific Contract Items

- The maintenance of traffic control shown on the plans represents one potential configuration. The Contractor may submit alternate maintenance of traffic plans, including temporary lane closures, in accordance with the MTA Standard Maintenance of Traffic Details and Special Provision 652. Any alternate maintenance of traffic plans shall be approved by the Resident.



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MTA Contract 2025.07



Questions?

Dunstan River Culvert MM 40.30 Photographs

Dunstan River Culvert Rehabilitation
Scarborough, Maine



Photo #1: West Elevation of Culvert Inlet.



Photo #2: East Elevation of Culvert Outlet.

Dunstan River Culvert Rehabilitation
Scarborough, Maine



Photo #3: Interior of the Culvert Looking Upstream from the Outlet.



Photo #4: Interior of the Culvert Looking Downstream from the Inlet.

Dunstan River Culvert Rehabilitation
Scarborough, Maine



Photo #5: South Embankment of West Side of the Culvert.



Photo #6: Exposed Section of Culvert at the Outlet.

Dunstan River Culvert Rehabilitation
Scarborough, Maine



Photo #7: Dunstan River Downstream of Culvert



Photo #8: Dunstan River Downstream of Culvert.

Unnamed Stream Culvert MM 72.00

Photographs

Unnamed Stream Culvert Rehabilitation at MM 72.00
New Gloucester, Maine



Photo #1: West Elevation of Culvert Inlet.



Photo #2: Culvert Inlet.

Unnamed Stream Culvert Rehabilitation at MM 72.00
New Gloucester, Maine



Photo #3: East Elevation of Culvert Outlet.



Photo #4: Unnamed Stream Upstream of Culvert

Unnamed Stream Culvert Rehabilitation at MM 72.00
New Gloucester, Maine



Photo #5: Unnamed Stream Downstream of Culvert.



Photo #6: Headwall Scour at Culvert Outlet.

(Photo from MTA 2022 Annual Inspection Report)

Unnamed Stream Culvert Rehabilitation at MM 72.00
New Gloucester, Maine



Photo #7: Culvert Joint Deterioration Under Northbound Barrel.
(Photo from MTA 2022 Annual Inspection Report)



Photo #8: Heavy Spalling with Exposed Rebar Under Southbound Barrel.
(Photo from MTA 2022 Annual Inspection Report)

Culvert MM 72.20 Photographs

Culvert Rehabilitation at MM 72.20
New Gloucester, Maine



Photo #1: Culvert Inlet, Facing West.
(Photo from MTA 2024 Slope Culvert Repair Report)



Photo #2: Culvert Inlet.
(Photo from MTA 2024 Slope Culvert Repair Report)

Culvert Rehabilitation at MM 72.20
New Gloucester, Maine



Photo #3: General View of Culvert Inlet, Facing West.
(Photo from MTA 2024 Slope Culvert Repair Report)



Photo #4: Culvert Outlet, Facing North.
(Photo from MTA 2024 Slope Culvert Repair Report)

Culvert Rehabilitation at MM 72.20
New Gloucester, Maine



Photo #5: Culvert Outlet.
(Photo from MTA 2024 Slope Culvert Repair Report)



Photo #6: General View of Culvert Outlet, Facing East.
(Photo from MTA 2024 Slope Culvert Repair Report)

Culvert Rehabilitation at MM 72.20
New Gloucester, Maine



Photo #7: Unnamed Stream Downstream of Culvert Outlet.
(Photo from MTA 2024 Slope Culvert Repair Report)



Photo #8: Joint Separation in Culvert Pipe Before Repair.
(Photo from HNTB 2024 Damage Assessment and Recommendation Report)