



**Maine Turnpike Authority**

# **Traffic Control Plans**

January 2018



*Maine Turnpike Authority*

2360 Congress Street  
Portland, Maine 04102

**MEMO:**

**Date:** January 24, 2018

**Re:** Maine Turnpike Authority Traffic Control Plans – December 2017

This set of MTA Traffic Control plans has been updated to include all changes and revisions made in 2017 and supersedes all previous versions. The notes below should be incorporated as part of the HNTB Plans dated December 22, 2017.

1. When personnel or equipment within a stationary work area will be actively working within four feet of a travel lane an adjacent lane closure, a lane shift or physical barrier will be used if practicable. Trucks/trailers shall be parked at least six feet from an open travel lane when being loaded or unloaded. When these activities are not practicable then they should be elevated to a supervisor and additional traffic control measure devices should be considered which may include truck mounted attenuators (TMAs) or state police. For all operations including setting up or taking down traffic control devices, TMAs should be used between workers and traffic.
2. Work Zones should not be set up in inclement weather unless there is a minimum of ½ mile visibility with improving conditions.
3. Work Zone Speed Limits
  - Work Zone Speed Limits shall be implemented whenever there is a lane closure with the exception of mobile lane closures.
  - Work Zone Speed Limits shall be 10 MPH lower than the normal posted speed limit. Reduced Speed Ahead signs are not required with a 10 MPH reduction. If a lower Work Zone Speed Limit is desired it should be requested through the MTA Engineering Department. If a lower Work Zone Speed Limit is approved, then it will require the use of bracketed Reduced Speed Ahead Signs. Note: For Details TP56A, TP57A and Appendix A the work zone speed shall be as shown in the details.
  - Work Zone Speed Limit Signs do not need to be bracketed. They may be placed directly after the taper within the lane closure, directly after the taper in the median mounted on a post, or prior to the taper depending upon the work activity. Work Zone Speed Limit Signs need to be mounted 5 feet above the pavement. If a work zone speed limit is used, a “End Work Zone Speed” sign is required
  - Any permanent speed limit signs that are within the lane closure setup (located within the Road Work ½ Mile sign) will need to be covered when the Work Zone Speed Limit is implemented.
4. Mobile Operations are defined as not occupying the same location for more than 15 minutes.



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5. Variable Message Signs (VMS) may be utilized to provide advance warning to motorists of Highway Maintenance work if the VMS is within three miles of the work area. The use of the VMS will be secondary to the use of the VMS for higher priority messages. Highway Maintenance should call the Communications Center and inform of project and they will put up standard messages that are appropriate. In addition, Highway Maintenance may put out portable VMS that are at Highway Maintenance yards. VMS should be used as a supplement to and not as a substitute for conventional signs and pavement markings.
6. Tapers should be installed with adequate visibility for approaching vehicles. Spacing between signs can typically be increased one delineator or further if necessary to provide adequate visibility.
7. Work zone signs setup by MTA Highway Maintenance in advance of the work area that are installed less than 5 feet above the pavement shall be installed with dual flags.
8. In closed lanes three cones or barrels should be placed across the lane every 5 delineator posts (1/4 mile) in areas where there is no construction activity occurring.
9. All Stop, Yield, Speed Limit and green Exit signs shown in traffic control plans shall be 5' off the ground.
10. Whenever flaggers are required, use of illuminated flagger paddles are required.
11. When planned operations or shoulder grading leave a continuous 3 inches or less exposed vertical face at edge of new traveled way, channelization devices should be placed 2 feet outside the edge of pavement at intervals not exceeding 600 feet and, depending on type and location of the exposed vertical face, W8-9 Low Shoulder sign shall be placed at a maximum spacing of 1/2 mile. If exposed vertical face is greater than 3", shoulder material needs to be placed at a slope not exceeding 3 horizontals to 1 vertical in advance of putting traffic adjacent to the condition.
12. If Speed Limit Feedback Signs are used, they shall be placed within 1000' of the regulatory reduced speed sign. If using a feedback sign with a regulatory sign included, the sign/device can go where speed limit sign is shown on plans.
13. Use of State Police – State Police Troop G are an invaluable but not unlimited resource to the Authority thus their use for traffic control in work zones must be weighed against their other duties. Maintenance Foreman can check with MTA Communications Center for status of on-duty troopers at any time and use those troopers to assist with set up or take down of traffic control devices on the highway if they are available. In some cases, after consulting with MTA Director of Public Safety, Maintenance may request trooper details as part of weekly lane closure report each Thursday for a specific date and time. Any planned stoppage of traffic on the Turnpike requires the use of State Police. For setting up and taking down traffic control devices in New Hampshire, the use of New Hampshire State Police requires 2 days' notice and can be made through MTA Director of Public Safety.



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14. Temporary Portable Rumble Strips may be utilized with lane closures. Use of temporary portable rumble strips may not be practicable in areas where the roadway has more than two travel lanes, where volume windows do not allow for breaks in traffic to set up and monitor and adjust, or during night time lane closures.
15. Lane closures shall be scheduled in accordance with the lane closure timetables maintained/issued by the Engineering Department. Planned lane closures need to be submitted to the approved e-mail distribution list by noon Thursday the week prior to the closure. Changes to planned lane closures and Emergency lane closure notification shall also be made to the approved e-mail distribution list.

Signed:  Date: 1.24.18

Peter Mills  
Executive Director

Signed:  Date: 01/24/18


Peter Merfeld, P.E.  
Chief Operations Officer

Signed:  Date: 1/24/18

John Cannell, P.E.  
Director of Highway Maintenance

Signed:  Date: 1/24/18

Steve Tartre, P.E.  
Director of Engineering & Building Maintenance

Signed:  Date: 1-24-18

Ralph Norwood IV, P.E., PTOE  
Project Manager



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MEMO:

**Date:** December 10, 2018

**Re:** Maine Turnpike Authority Traffic Control Plans – January 2018

The following revisions to the Traffic Control Plans dated January 2018 shall be implemented when setting up work zone traffic control zones:

**Placement of Arrow Board:**

At locations where the shoulders widths do not allow the placement of the arrow board at the location shown the MOT plans so that the arrow board is completely within the closed area of the roadway then the location of the arrow board shall be adjusted within the taper area as needed keeping it as close as practicable to the beginning of the taper while keeping the arrow board completely located on the back side of cones/barrels within the closed area of the roadway.

**Placement of TMIA:**

For a Stationary work zone operation the shadow vehicle, with or without an impact attenuator, shall be 200 feet (5 skip lines) from the work area to allow for movement if the truck is struck. The shadow vehicle shall have parking brake set; the transmission placed in gear and have front wheel turned away from the work area. No other vehicles or equipment shall park in front of the shadow vehicle or within the buffer space behind the shadow vehicle.

For a mobile work zone operation, shadow vehicles, with or without an impact attenuator, shall be no closer than 250 feet (6 skip lines) from the work vehicle or other shadow vehicles. Maximum distance for each shadow vehicle used shall be determined by the supervisor depending on the operation and in accordance with the notes shown on DETAIL 35R and 35L and 51R and 51L.

**MOT Detail 4B(Page 7):**

During litter picking activities when a “leap frogging” operation is being utilized a W21-5 sign shall be easel mounted in advance of the work area in addition to being mounted on the truck.

Signed: Steve R. Tartre Date: 12/13/18

Steve Tartre, P.E.

Director of Engineering & Building Maintenance



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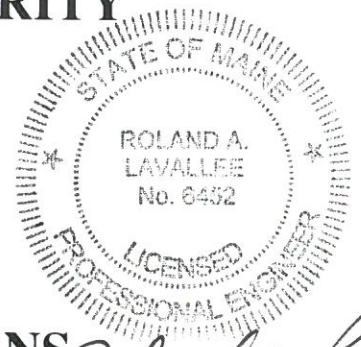
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# MAINE TURNPIKE AUTHORITY



## TRAFFIC CONTROL PLANS

December 22, 2017

*Roland A. Lavallee*

### General Notes:

1. Plans are site specific and are for a particular application. Notes and details from one application shall not be used for another application.
2. "Optional" equipment should be used as detailed unless the optional equipment is not readily available.
3. Drums should be used on all non-emergency lane closures intended to remain in place, more than twelve hours.
4. All drums, cones, and channelizing devices used in non-daylight hours shall meet MUTCD Section 6F requirements for night time use.
5. a. All flagger stations in non-emergency lane closures set up, or intended to remain in place, during non-daylight hours shall be illuminated with at least 10 foot-candles of light. For reference 10 foot candles of illumination is approximately that of a very dark day. See note below.
5. b. All non-emergency work areas, with workers present, shall be properly illuminated during non-daylight hours. The minimum illumination shall be at least 5 foot-candles of light. Additional illumination (foot-candles) may be required, depending on the work activity, to complete the work. See note below.  
Note: Where practical the lighting will be cut off and arranged on stanchions at a height that will provide perimeter lighting for each piece of equipment and will not interfere with traffic, including commercial vehicles, approaching the work site from all directions.
6. When installing a lane closure, a Traffic Control Truck with an internal worker station designed for setting out drums or cones or a truck mounted cone basket shall be used unless one is not readily available. Both types of vehicles should be equipped with a truck mounted attenuator (TMA).



7. Plan sheets are not to scale.
8. All speed limit signs in construction / work zones shall be installed at five feet from bottom of speed plaque to edge of roadway elevation.
9. When a reduced speed zone sign package is used, the “End Work Zone Speed Limit” sign is mandatory and the “End Road Work” sign is optional.
10. All vehicles used to set up, maintain, work within or dismantle work zones and applicable sign packages shall display high – intensity rotating, flashing, oscillating, or strobe lights.
11. MTA engineering shall be consulted prior to traffic control setup on local roads where the setup may create unsafe backups due to high traffic volumes.
12. Layout distances reported in feet, given within the details, are the minimum for that particular segment of the layout. The number of delineator post spaces reported for that same segment is an approximation of segment distance and may be longer than the minimum distance reported. The number of delineator post spaces is provided for ease in field layout.
13. Whenever light towers, man lifts, boom trucks, or similar extendable height equipment is used, days or nights, the operator shall inspect the setup location to make sure the equipment does not come in contact with overhead wires, bridge overpasses, overhead sign structures, toll canopies, overhanging trees, etc. during the extendable height equipment set up, relocation or transport.
14. All construction signs on the Maine Turnpike and Ramps shall meet the size criteria of Freeway in MUTCD, resulting in all diamond shaped advanced warning construction signs being 48” x 48”.

#### Placement of TMIA:

For a Stationary work zone operation the shadow vehicle, with or without an impact attenuator, shall be 80 feet (2 skip lines) from the work area to allow for movement if the truck is struck. The shadow vehicle shall have parking brake set; the transmission placed in gear and have front wheel turned away from the work area. No other vehicles or equipment shall park in front of the shadow vehicle or within the buffer space behind the shadow vehicle.

For a mobile work zone operation, shadow vehicles, with or without an impact attenuator, shall be no closer than 120 feet (3 skip lines) from the work vehicle or other shadow vehicles. Maximum distance for each shadow vehicle used shall be determined by the supervisor depending on the operation and in accordance with the notes shown on DETAIL 35R and 35L and 51R and 51L.

## Flagger Tips

A "STOP/Slow paddle shall be the primary signaling device. It shall have an octagonal shape on a rigid handle. Flag use shall be limited to emergency situations



### Properly Trained Flaggers

- Give clear messages to drivers
- Allow distance for drivers to react
- Coordinate with other flaggers
- Use standard signaling methods

### Properly Equipped Flaggers

- Use approved stop/slow paddles
- Use approved safety apparel
- Use retroreflective equipment
- Use handheld radios, as needed
- Use ANSI Type III outer garment/s

### Proper Flagging Stations

- Good approach site distance
- Highly visible to traffic
- Stand alone away from machinery and other people
- Stand on right edge of pavement or shoulder, proceed to centerline only when first vehicle has stopped.
- At night, flagger station will be illuminated
- Have a good escape route



### Proper Advanced Warning Signs

- Always use warning signs
- Allow reaction distance from signs
- Remove signs if no longer necessary or not flagging
- Use free hand in up-and-down motion to help slow traffic.



## **Flagger Tips continued**

### **Escape Route**

A flagger must always be aware of their surroundings and have a good escape route. A flagger shall never be positioned directly beside or against construction equipment. When a flagger is required to direct traffic in an area where the escape route is partially blocked by a traversable obstruction such as a guardrail, the flagger must be physically capable of getting over the obstruction. Prior to commencing a project, the foreman / supervisor in charge must review the project, including guardrail areas, for safe flagging stations.

### **Pedestrian and Bicyclist Considerations**

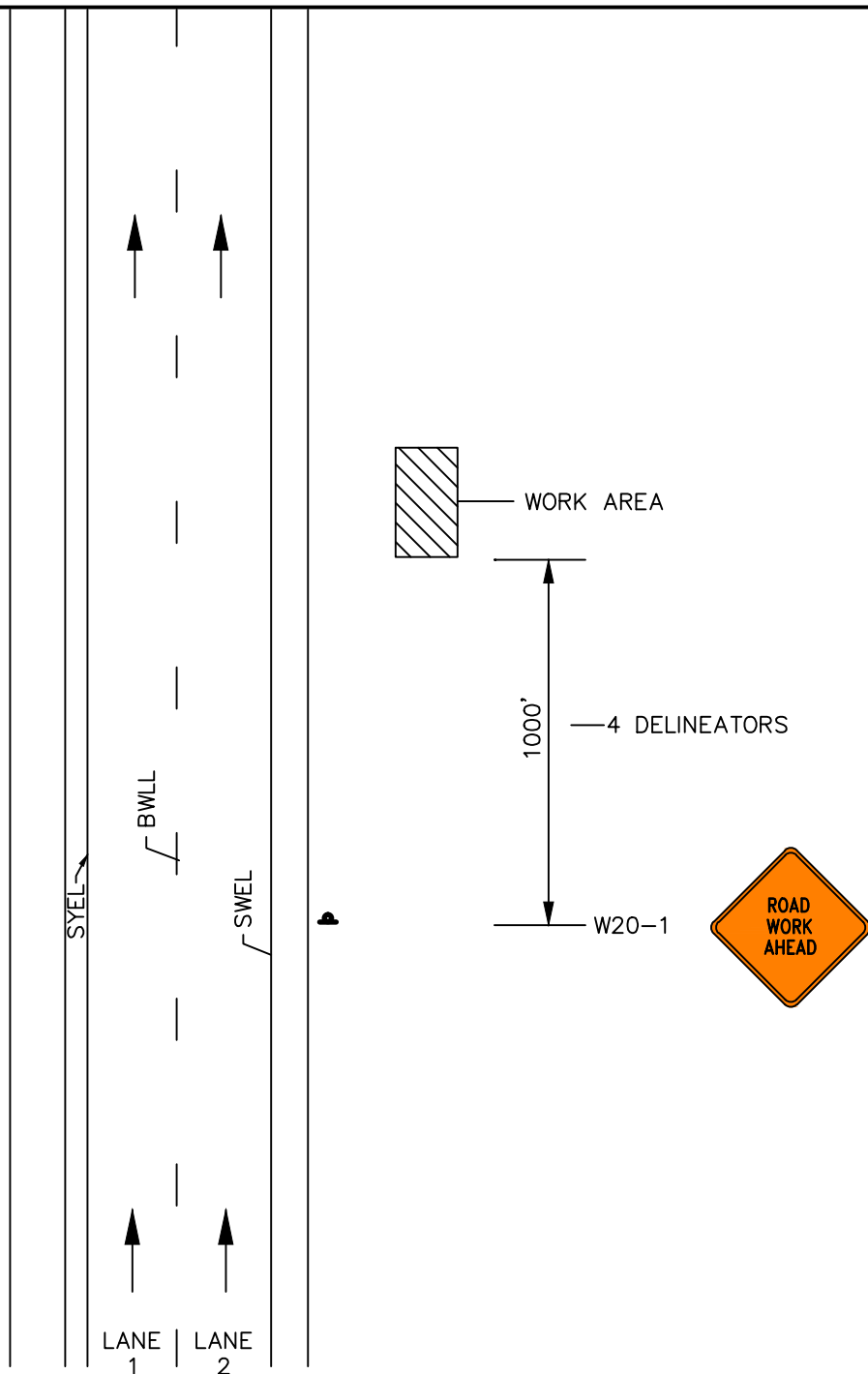
There are three threshold considerations in planning for pedestrian and bicyclist safety in temporary traffic control zones on highways and streets.

Pedestrians and bicyclists shall not be led into direct conflicts with work site vehicles, equipment or operations.

Pedestrians and bicyclists shall not be led into direct conflicts with mainline traffic moving through or around the work site.

Pedestrians and bicyclists shall be provided with a safe, convenient travel path that replicates as nearly as possible the most desirable characteristics of sidewalks or footpaths.

In accommodating the needs of pedestrians at work sites, it should be remembered that the range of pedestrians that can be expected is very wide, including the blind, the hearing impaired, and those with walking disabilities. All pedestrians and bicyclists need protection from potential injury and a smooth, clearly delineated travel path. Every effort shall be made to separate pedestrian and bicyclist movement from both the work site activity and the adjacent traffic.



**NOTES:**

1. THE ROAD WORK AHEAD SIGN MAY BE OMITTED WHERE THE WORK SPACE IS BEHIND BARRIER OR 15 FEET OR MORE FROM THE EDGE OF ROADWAY.
2. FOR SHORT DURATION (<1 HOUR) ACTIVITIES, MOBILE OPERATIONS, OR OPERATIONS SUCH AS MOWING, THE ROAD WORK AHEAD SIGN MAY BE ELIMINATED IF THE WORK VEHICLE ACTIVATES THE HIGH INTENSITY ROTATING, FLASHING, OSCILLATING OR STROBE LIGHTS.
3. IF WORK VEHICLES ARE PARKED IN THE SHOULDER, THEN THE SHORT DURATION OR MOBILE OPERATION ON SHOULDER DETAIL (DETAIL 4B) SHALL BE FOLLOWED.

**DETAIL 1**

NOT TO SCALE

**HNTB**

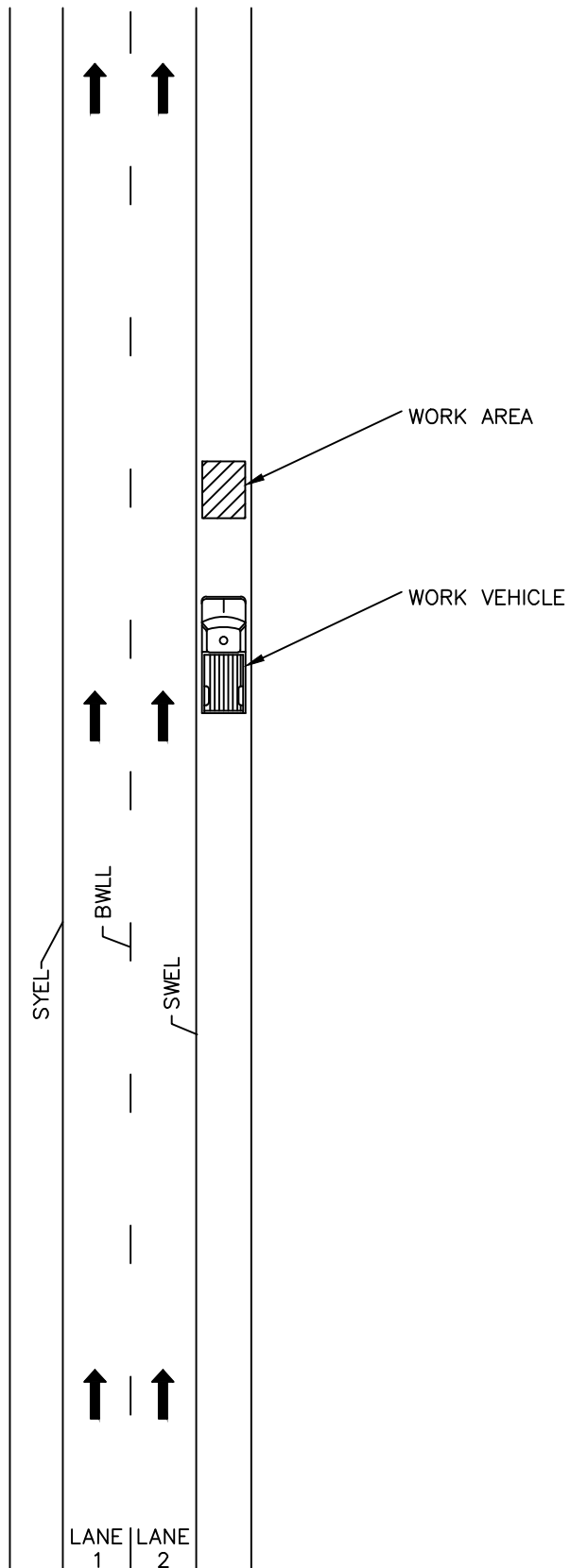
DATE: 12-22-2017



**MAINE TURNPIKE AUTHORITY  
TRAFFIC CONTROL DETAIL**

**WORK BEYOND THE SHOULDER  
SHORT TERM (UP TO 12 DAYLIGHT HOURS)**





DETAIL 4A NOT TO SCALE

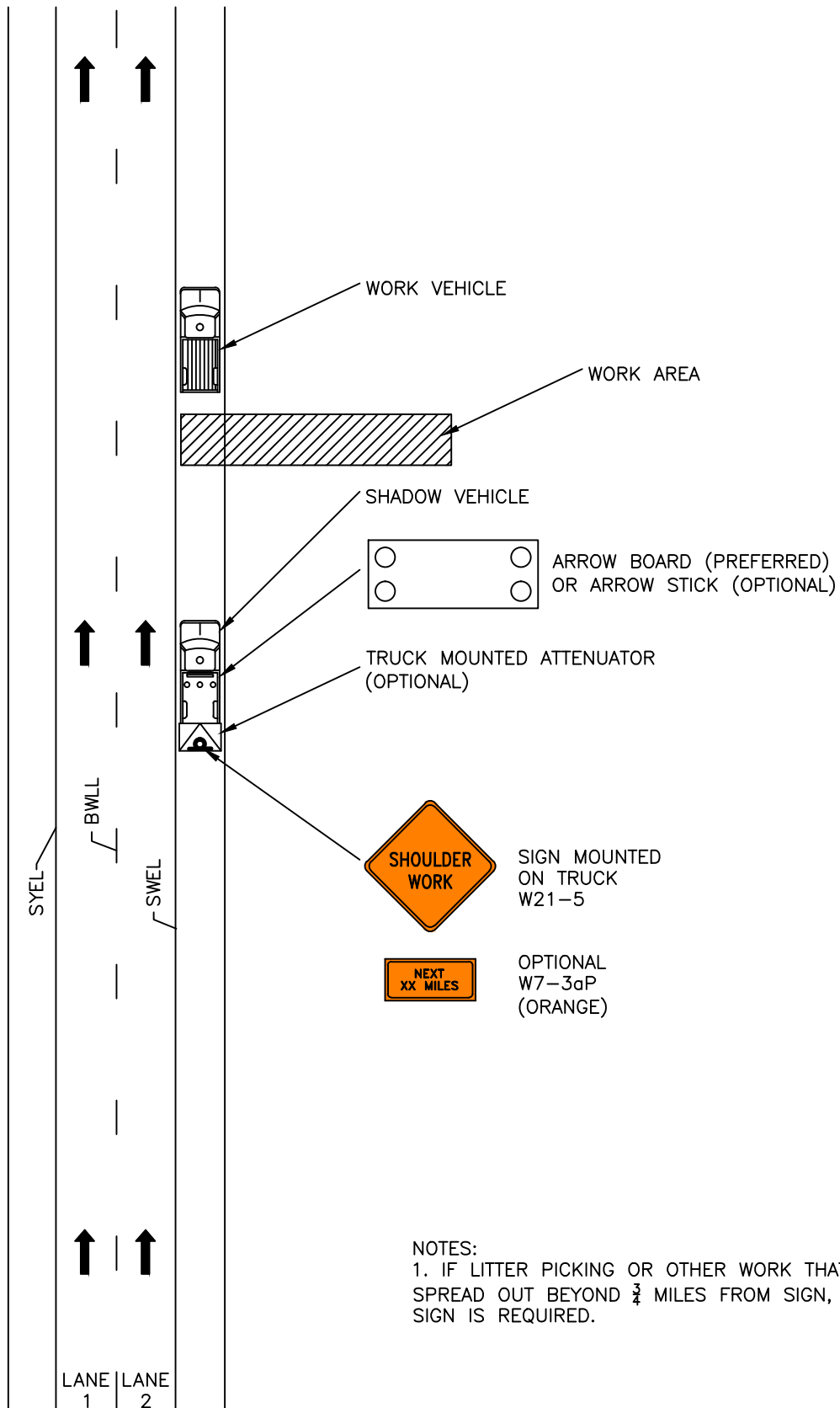
**HNTB**

DATE: 12-22-2017



**MAINE TURNPIKE AUTHORITY**  
**TRAFFIC CONTROL DETAIL**

**SINGLE VEHICLE ON SHOULDER**  
**SHORT DURATION (<1 HOUR)**



NOTES:  
1. IF LITTER PICKING OR OTHER WORK THAT IS  
SPREAD OUT BEYOND  $\frac{3}{4}$  MILES FROM SIGN, W7-3aP  
SIGN IS REQUIRED.

DETAIL 4B NOT TO SCALE

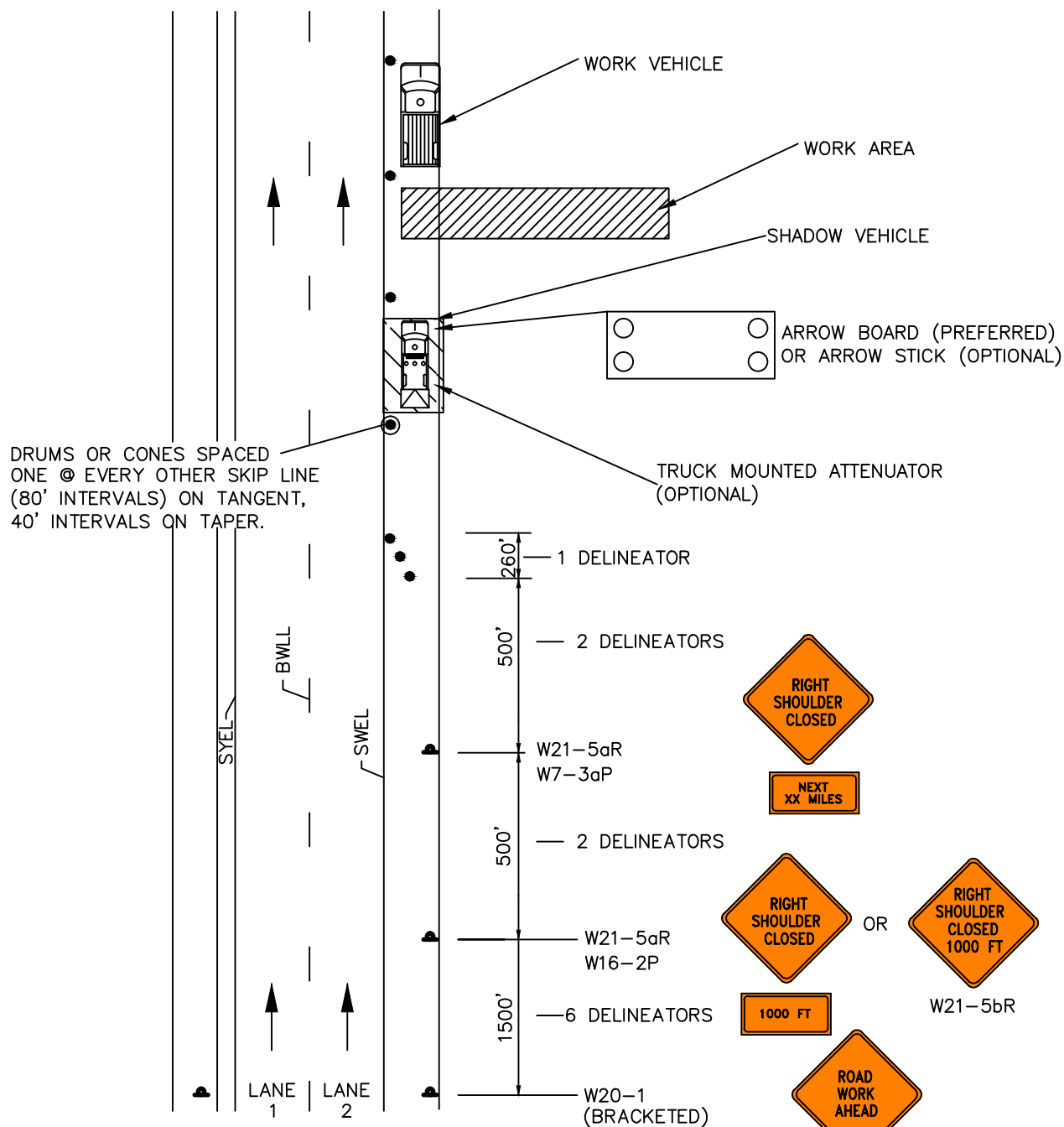
**HNTB**

DATE: 12-22-2017



**MAINE TURNPIKE AUTHORITY**  
**TRAFFIC CONTROL DETAIL**

**SHORT DURATION (< 1 HOUR) OR  
MOBILE OPERATION ON SHOULDER**



NOTES:

1. THE SUPPLEMENTAL PLATE NOTING THE LENGTH OF SHOULDER CLOSURE MAY BE ELIMINATED IF DRIVERS CAN SEE A PULL OFF AREA BEYOND THE CLOSED SHOULDER.
2. DRUMS SHALL BE USED FOR SHOULDER CLOSURES LEFT IN PLACE OVERNIGHT AND WHEN THE CLOSURE IS NOT ATTENDED.

DETAIL 5A NOT TO SCALE

**HNTB**

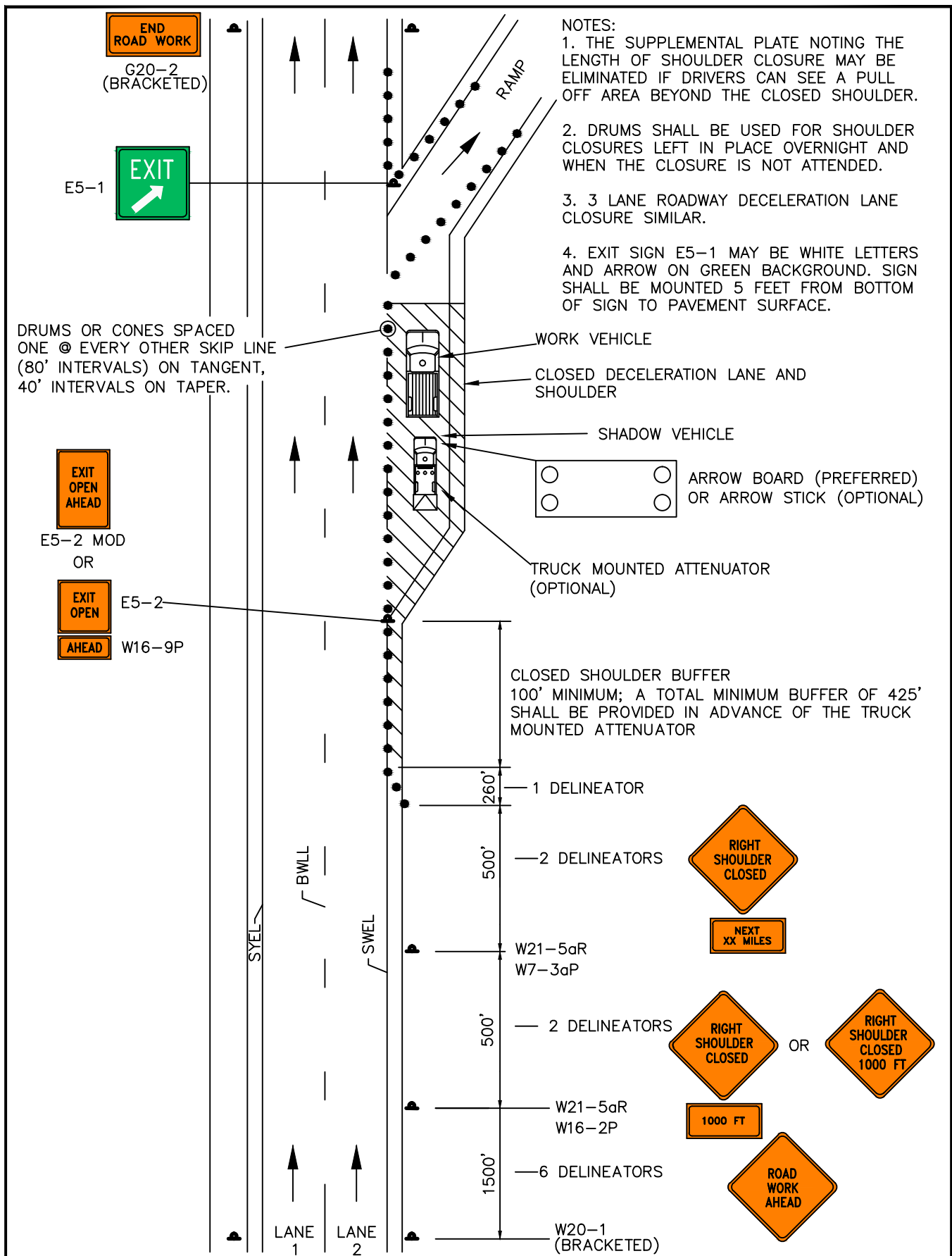
DATE: 12-22-2017



**MAINE TURNPIKE AUTHORITY**  
**TRAFFIC CONTROL DETAIL**

**SHOULDER CLOSURE**  
**FOR WORK ACTIVITY IN THE SHOULDER**





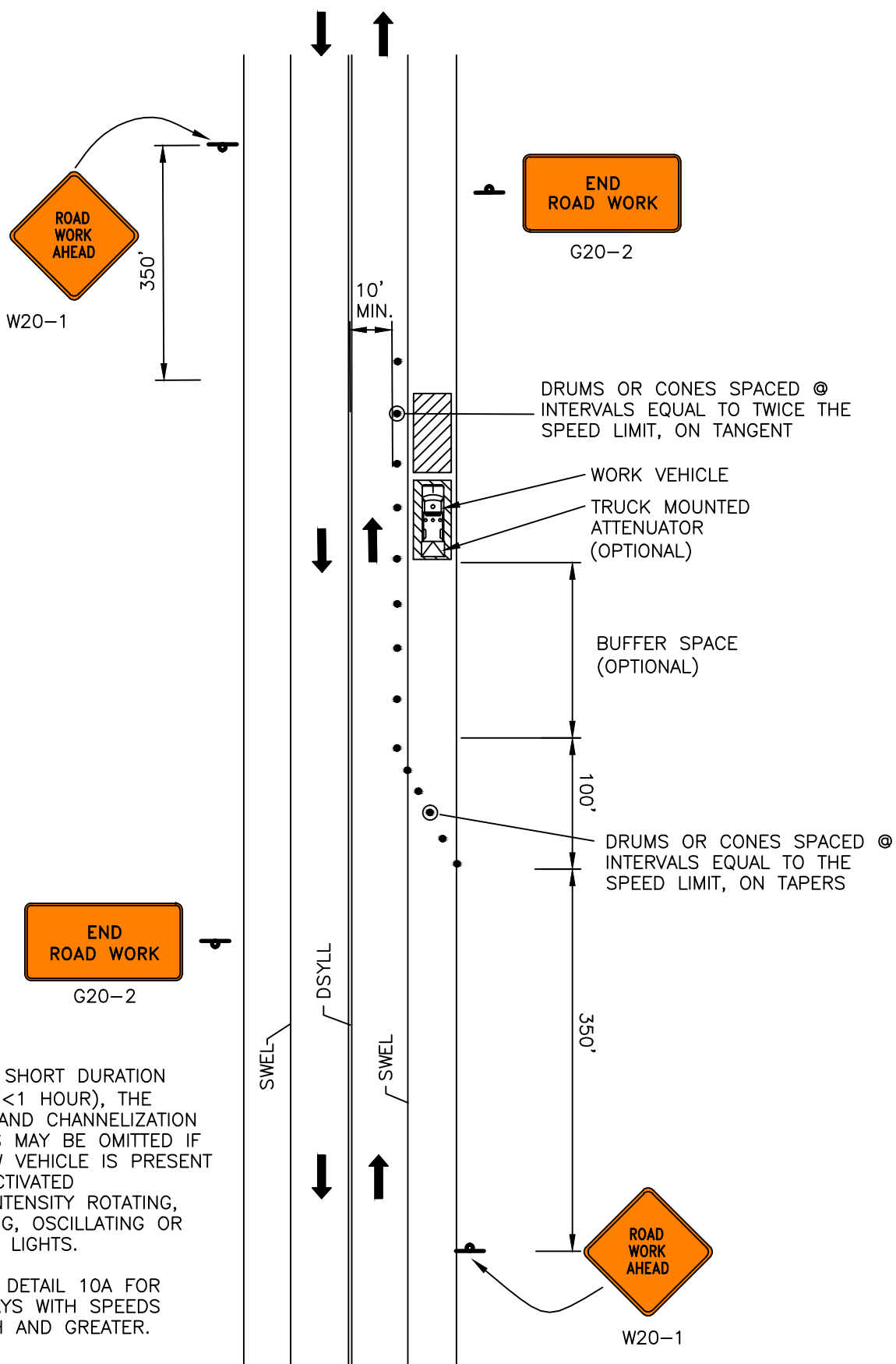
DETAIL 5B NOT TO SCALE

**HNTB**

DATE: 12-22-2017



**MAINE TURNPIKE AUTHORITY**  
**TRAFFIC CONTROL DETAIL**  
**DECELERATION LANE CLOSURE**  
**FOR WORK ACTIVITY IN THE**  
**DECELERATION LANE**



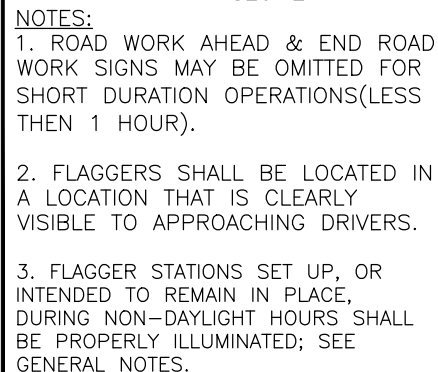
DETAIL 6 NOT TO SCALE

**HNTB**

DATE: 12-22-2017



**MAINE TURNPIKE AUTHORITY**  
**TRAFFIC CONTROL DETAIL**  
 SHOULDER WORK WITH MINOR ENCROACHMENT  
 ON A MINOR ROAD WITH SPEEDS 40 MPH OR LESS  
 SHORT TERM (UP TO 12 DAYLIGHT HOURS)

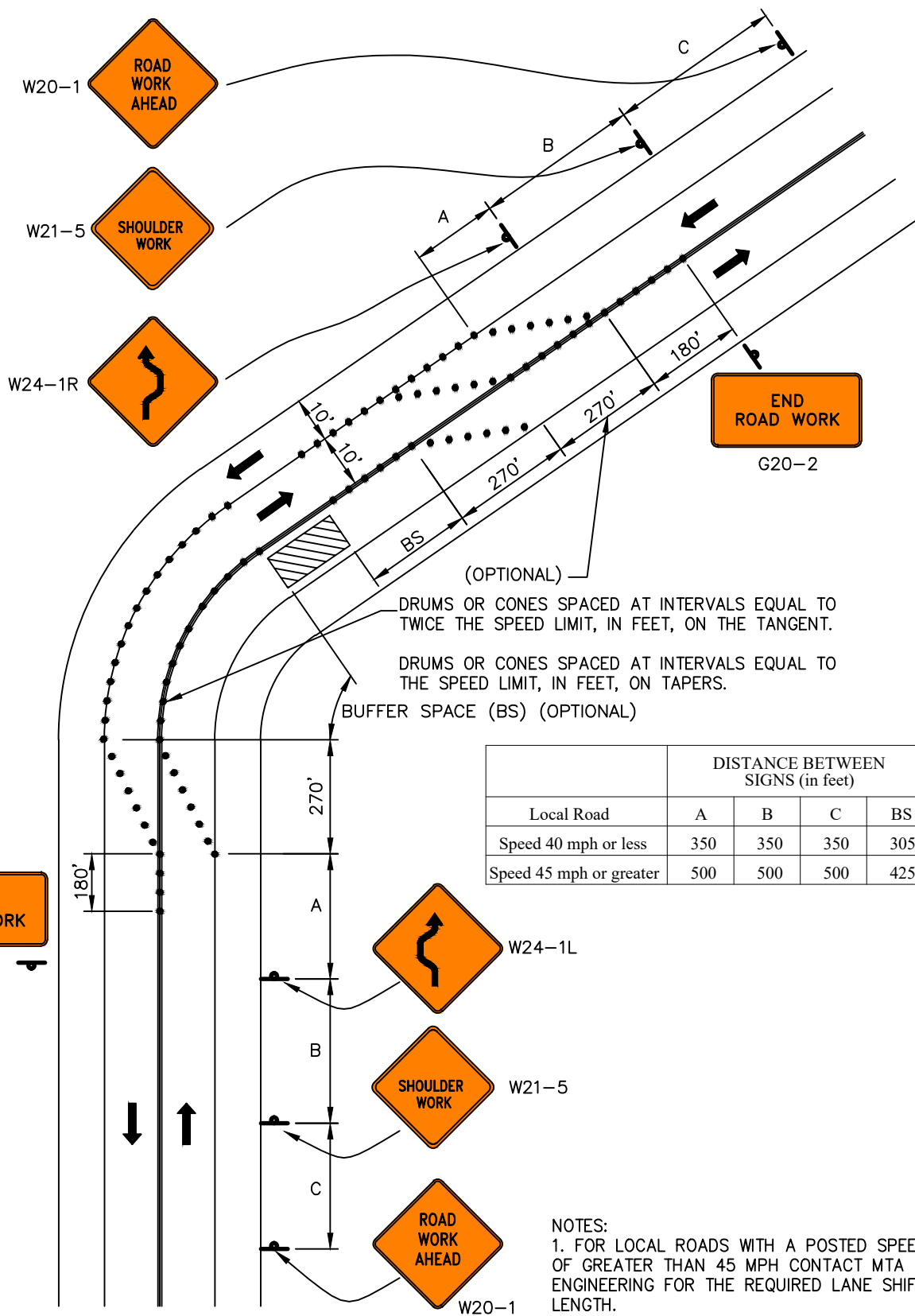


# HNTB



11





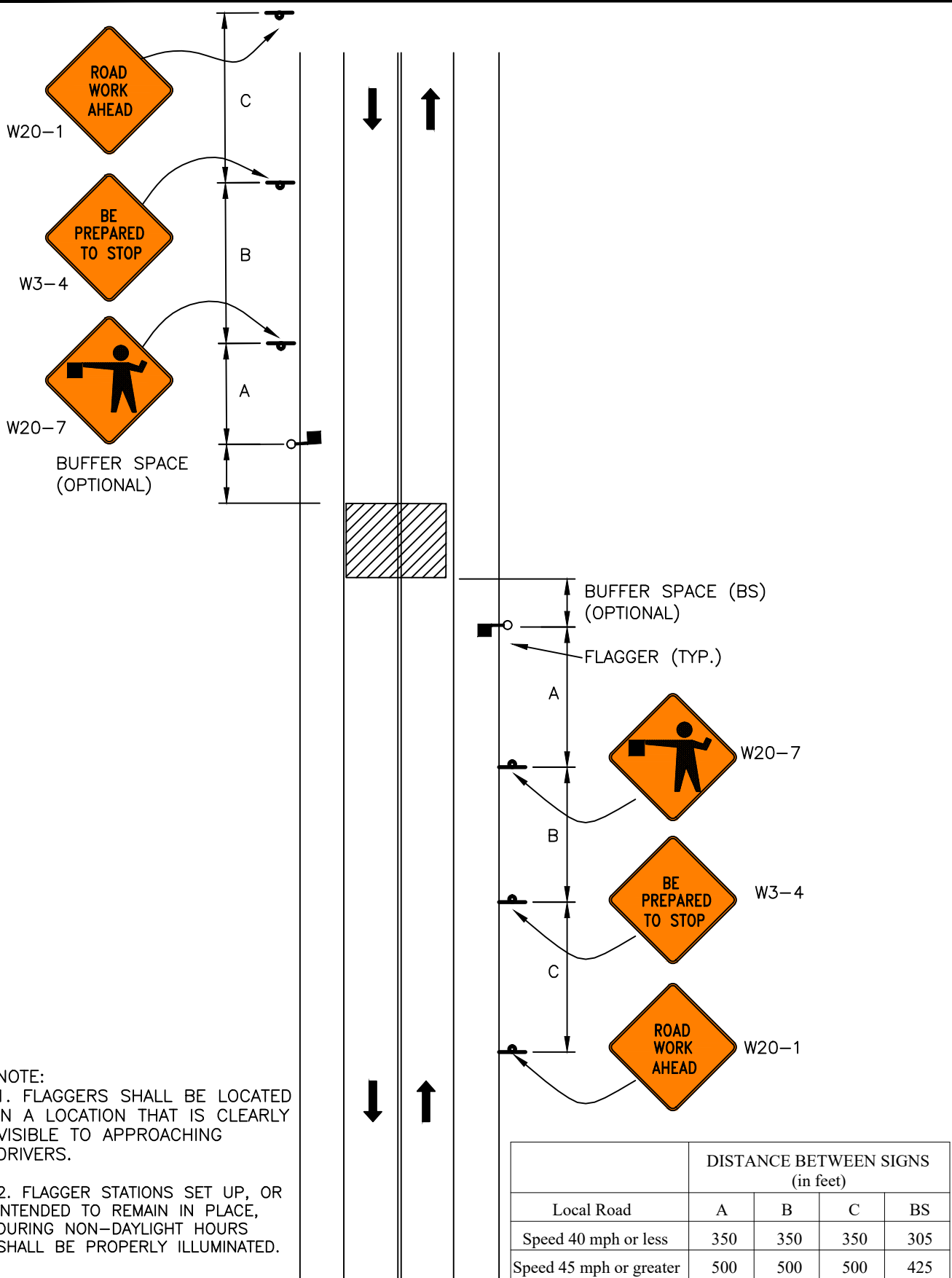
DETAIL 10B NOT TO SCALE

**HNTB**

DATE: 12-22-2017



**MAINE TURNPIKE AUTHORITY**  
**TRAFFIC CONTROL DETAIL**  
 DOUBLE LANE SHIFT ON A  
 TWO-LANE ROAD



DETAIL 13 NOT TO SCALE

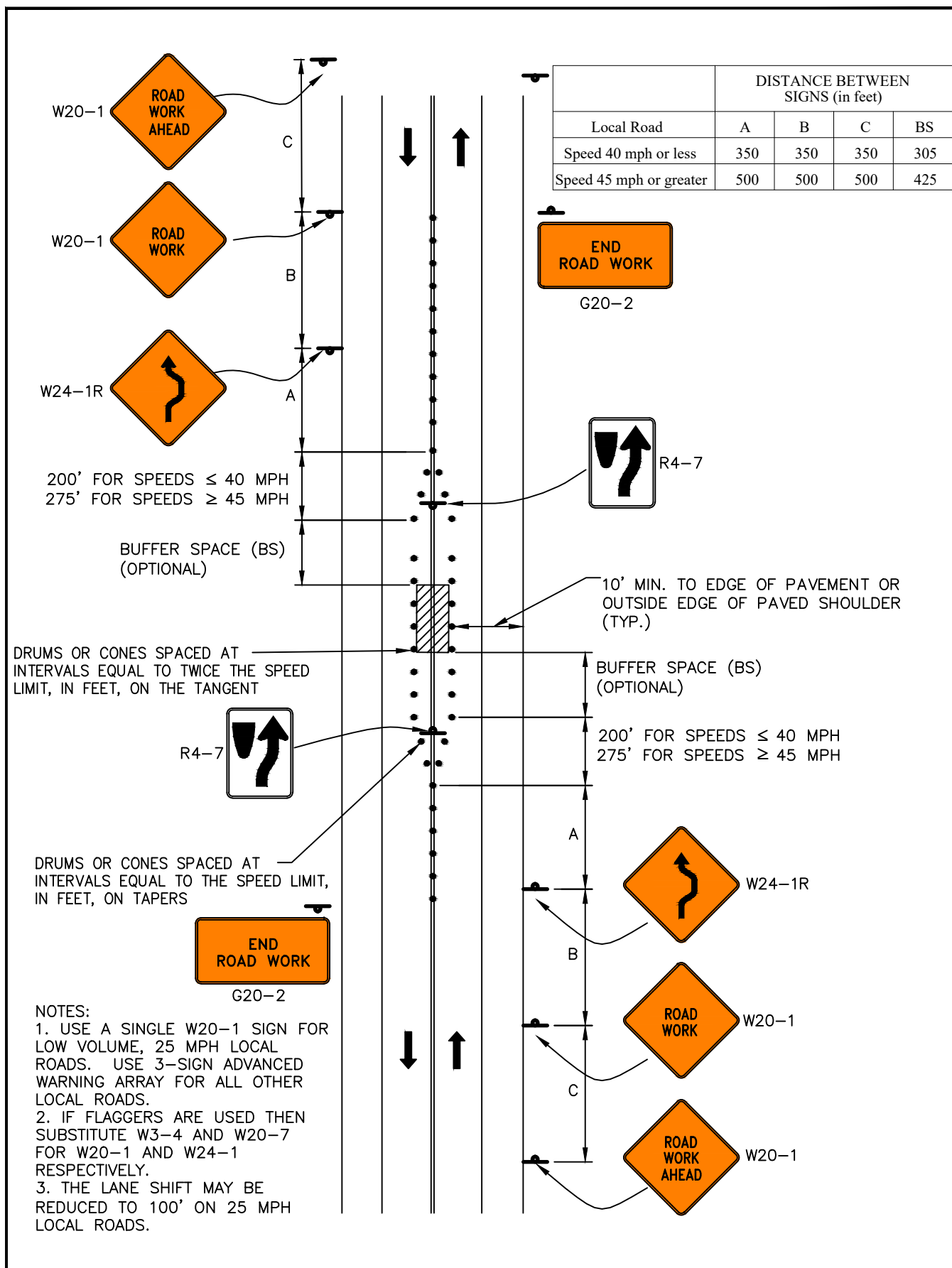
**HNTB**

DATE: 12-22-2017



**MAINE TURNPIKE AUTHORITY  
TRAFFIC CONTROL DETAIL**

**TEMPORARY ROAD CLOSURE  
FOR 20 MINUTES OR LESS**



DETAIL 15 NOT TO SCALE

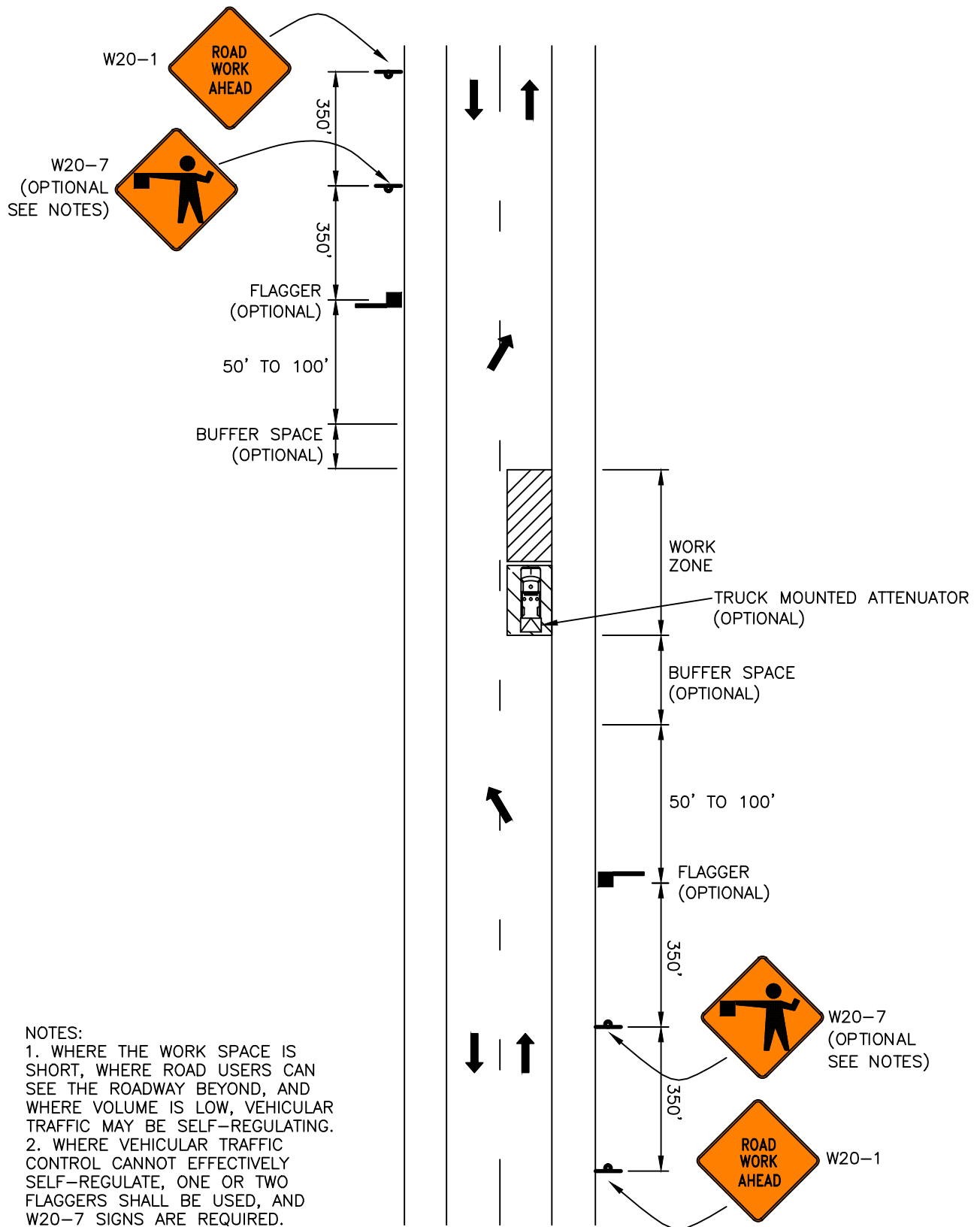
**HNTB**

DATE: 12-22-2017



**MAINE TURNPIKE AUTHORITY**  
**TRAFFIC CONTROL DETAIL**  
**WORK IN THE CENTER OF A ROAD**  
**WITH LOW TRAFFIC VOLUMES**





DETAIL 18 NOT TO SCALE

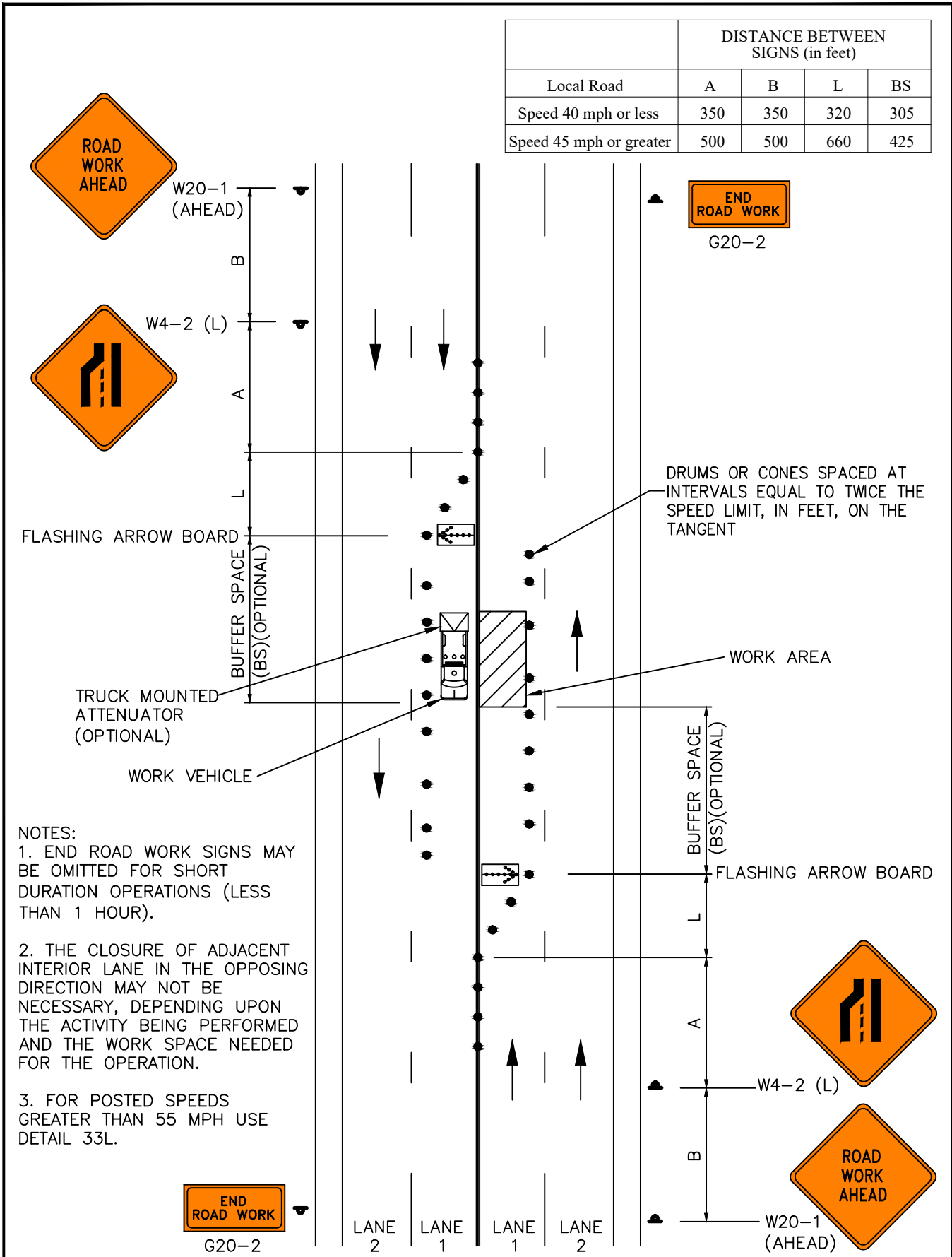
**HNTB**

DATE: 12-22-2017



**MAINE TURNPIKE AUTHORITY**  
**TRAFFIC CONTROL DETAIL**  
**TRAVEL LANE CLOSURE ON**  
**LOW VOLUME LOCAL ROAD**  
**SHORT TERM/MOBILE (< 30 MINUTES)**

	DISTANCE BETWEEN SIGNS (in feet)			
Local Road	A	B	L	BS
Speed 40 mph or less	350	350	320	305
Speed 45 mph or greater	500	500	660	425



- NOTES:
1. END ROAD WORK SIGNS MAY BE OMITTED FOR SHORT DURATION OPERATIONS (LESS THAN 1 HOUR).
  2. THE CLOSURE OF ADJACENT INTERIOR LANE IN THE OPPOSING DIRECTION MAY NOT BE NECESSARY, DEPENDING UPON THE ACTIVITY BEING PERFORMED AND THE WORK SPACE NEEDED FOR THE OPERATION.
  3. FOR POSTED SPEEDS GREATER THAN 55 MPH USE DETAIL 33L.

DETAIL 30A NOT TO SCALE

**HNTB**

DATE: 12-22-2017

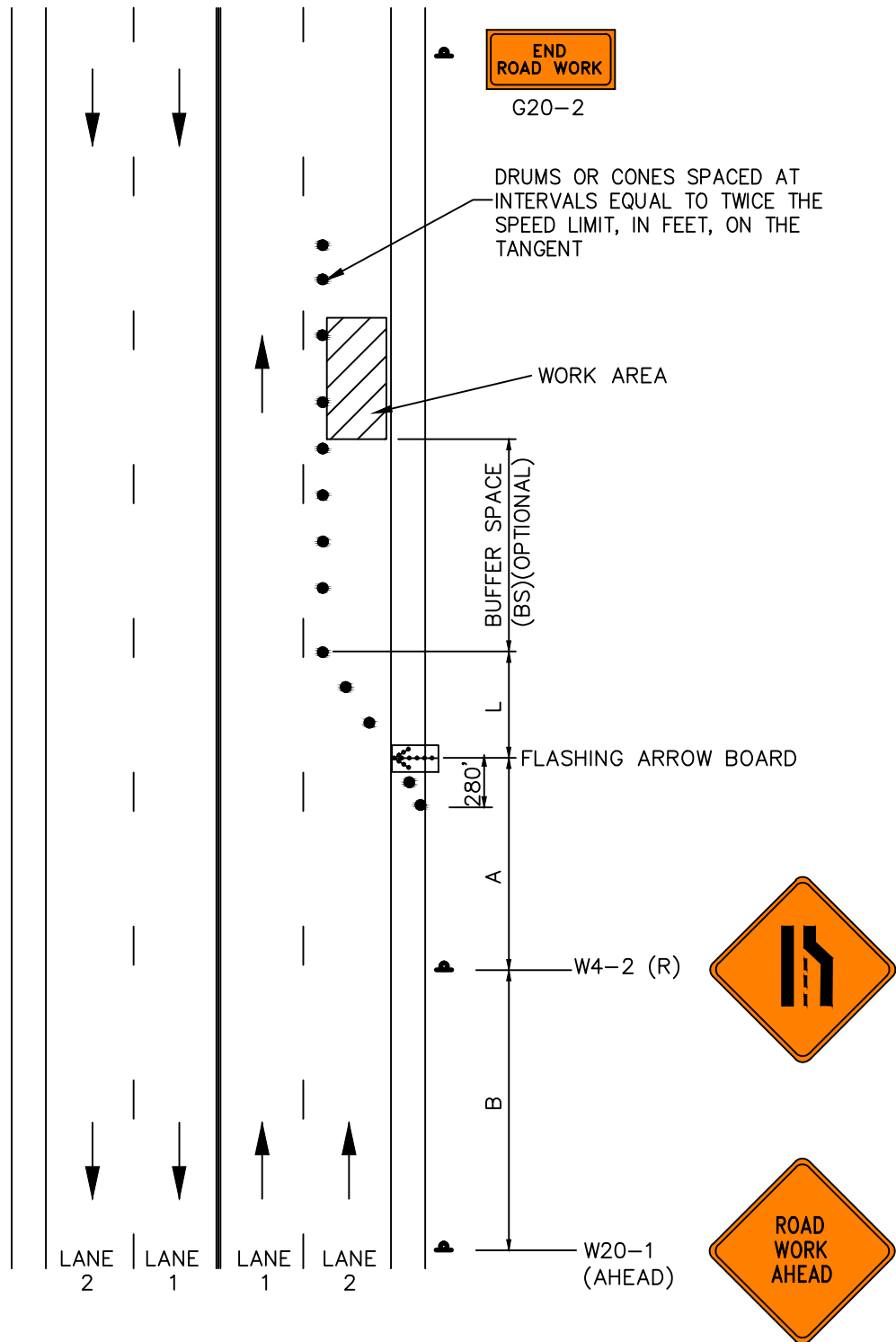


**MAINE TURNPIKE AUTHORITY**  
**TRAFFIC CONTROL DETAIL**  
 LEFT LANE CLOSURE ON A  
 MULTI-LANE LOCAL ROAD

NOTES:

1. THIS DETAIL MAY ALSO BE UTILIZED FOR LANE CLOSURES ON MULTILANE, LOW SPEED RAMPS.
2. END ROAD WORK SIGNS MAY BE OMITTED FOR SHORT DURATION OPERATIONS (LESS THAN 1 HOUR).

	DISTANCE BETWEEN SIGNS (in feet)			
	A	B	L	BS
Local Road	A	B	L	BS
Speed 40 mph or less	350	350	320	305
Speed 45 mph or greater	500	500	660	425



DETAIL 30B NOT TO SCALE

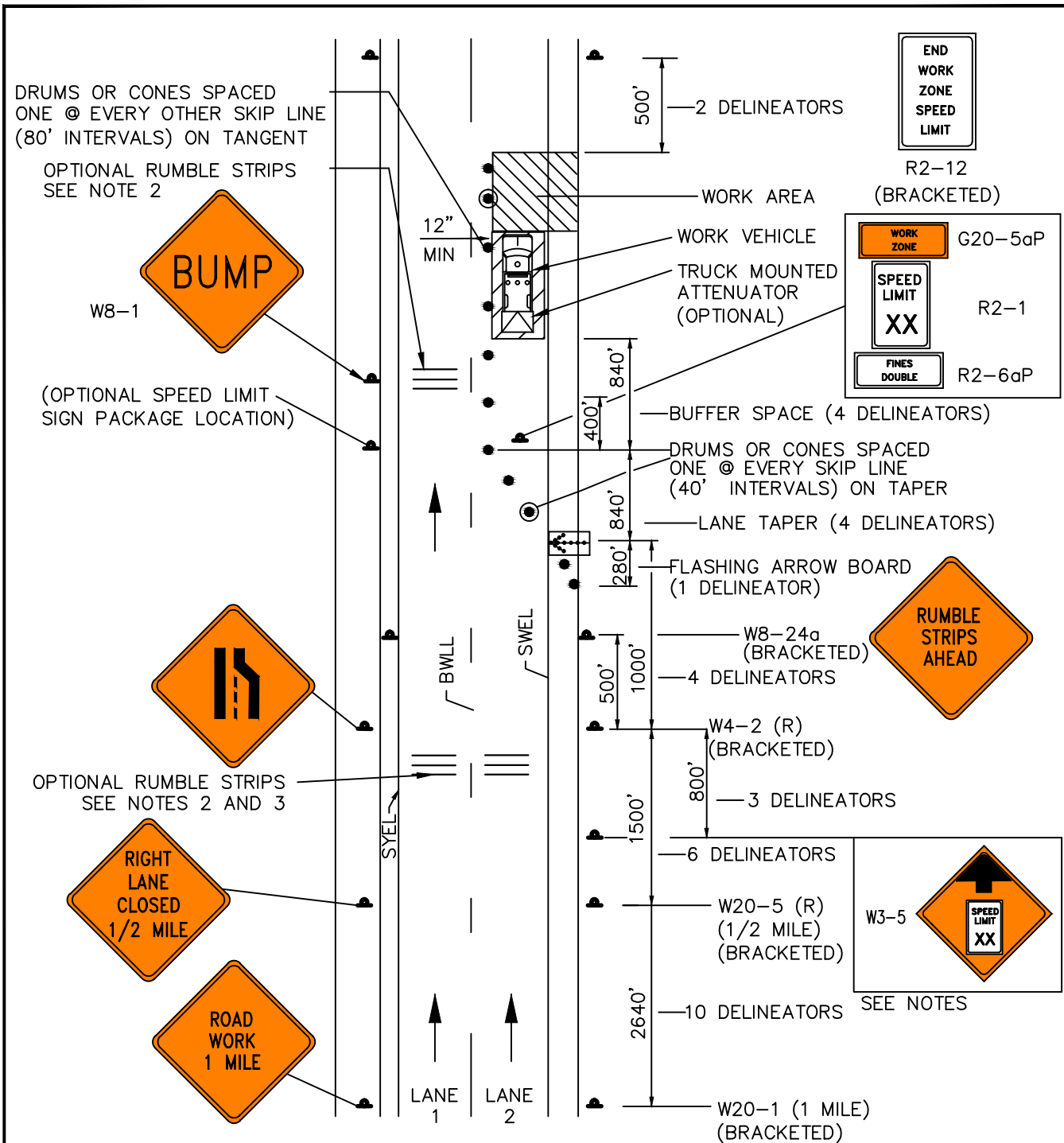
**HNTB**

DATE: 12-22-2017



**MAINE TURNPIKE AUTHORITY**  
**TRAFFIC CONTROL DETAIL**

**RIGHT LANE CLOSURE ON A  
MULTI-LANE LOCAL ROAD**



NOTES:

- FOR LANE CLOSURE AND REDUCED SPEED ZONE NOTES SEE DETAILS 33R AND 33L.
- IF RUMBLE STRIPS ARE USED THEY SHALL BE PLACED IN ONE OF THE FOLLOWING CONFIGURATIONS:
  - \* ADJACENT TO THE WORK ZONE (1 UNIT)
  - \* UPSTREAM FROM THE TAPER FOR THE WORKZONE (2 UNITS)
  - \* BOTH ADJACENT TO THE WORK ZONE AND PRIOR TO THE TAPER (3 UNITS)

W8-1 SIGNS SHALL BE PLACED ADJACENT TO THE FIRST RUMBLE STRIP AT ANY LOCATION. ONLY ONE SET OF W8-24a SIGNS ARE REQUIRED FOR ANY OF THE ABOVE CONFIGURATIONS.
- RUMBLE STRIPS MAY BE PLACED UPSTREAM OF THE TAPER BETWEEN THE W3-5 SIGNS AND THE W4-2 SIGNS. IF RUMBLE STRIPS ARE INSTALLED PRIOR TO TAPER, W8-1 SIGNS SHALL BE PLACED ADJACENT TO THE FIRST STRIP AND THE W8-24a SIGNS SHALL BE MOVED TO 400' AFTER THE W20-5 SIGNS.

DETAIL 33 NOT TO SCALE

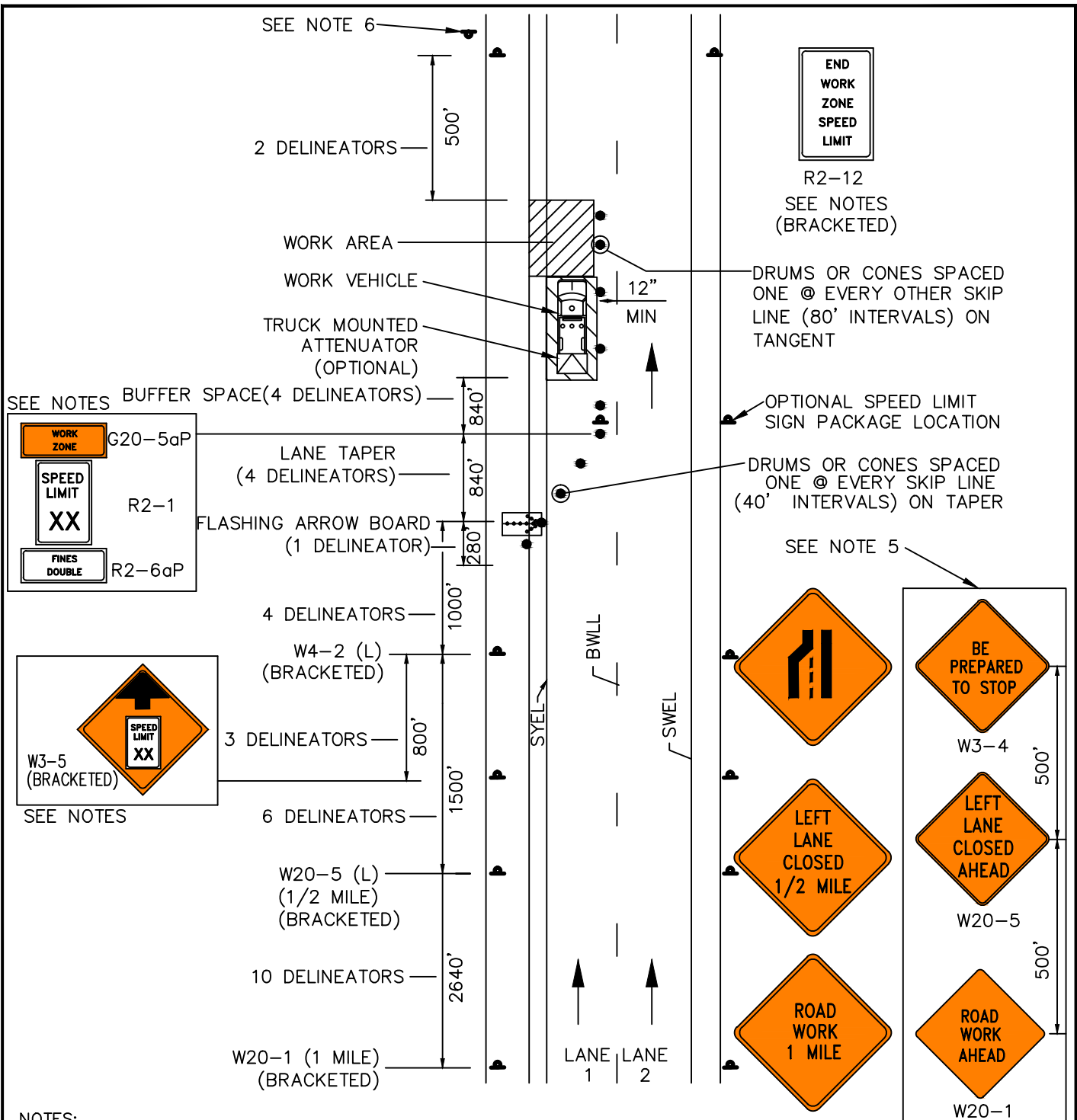
**HNTB**

DATE: 12-22-2017



**MAINE TURNPIKE AUTHORITY**  
**TRAFFIC CONTROL DETAIL**

**LANE CLOSURE WITH**  
**TEMPORARY RUMBLE STRIPS**



#### NOTES:

1. USE OF REGULATORY REDUCED SPEEDS SHALL BE USED WHEN WORKERS ARE PRESENT OR SITE CONDITIONS WARRANT. 10 MPH SPEED REDUCTION MANDATORY; IF REDUCTION GREATER THEN 10 MPH IS WARRANTED THEN REQUEST THROUGH MTA ENGINEERING. SPEED LIMIT SIGN IS ONLY NEEDED ON ONE SIDE OR THE OTHER. SPEED LIMIT SIGNS SHALL BE 5' OFF GROUND. SIGN W3-5 NOT NEEDED FOR 10 MPH REDUCTION.
2. OPTIONAL - SPEED LIMIT SIGN PACKAGE MAY BE POST MOUNTED ON THE RIGHT SHOULDER. POST MOUNTED SIGNS SHALL BE COVERED WHEN NOT IN USE.
3. OPTIONAL - THE WORK ZONE AND FINES DOUBLE SIGN MAY BE MOUNTED ON A SEPARATE EASEL OR POST.
4. OPTIONAL - THE FINES DOUBLE SIGN MAY BE OMITTED.
5. WHEN ON-RAMP EXISTS WITHIN MAINLINE SIGN ARRAY, THIS 3-SIGN ARRAY SHALL BE SET UP ON THE RIGHT SHOULDER OF THE RAMP. THE LAST SIGN SHALL BE AT THE RAMP GORE.
6. USE "WORKERS IN MEDIAN" SIGN FOR THE OPPOSITE DIRECTION OF TRAVEL IF WORKERS WILL BE IN THE MEDIAN.

DETAIL 33L NOT TO SCALE

**HNTB**

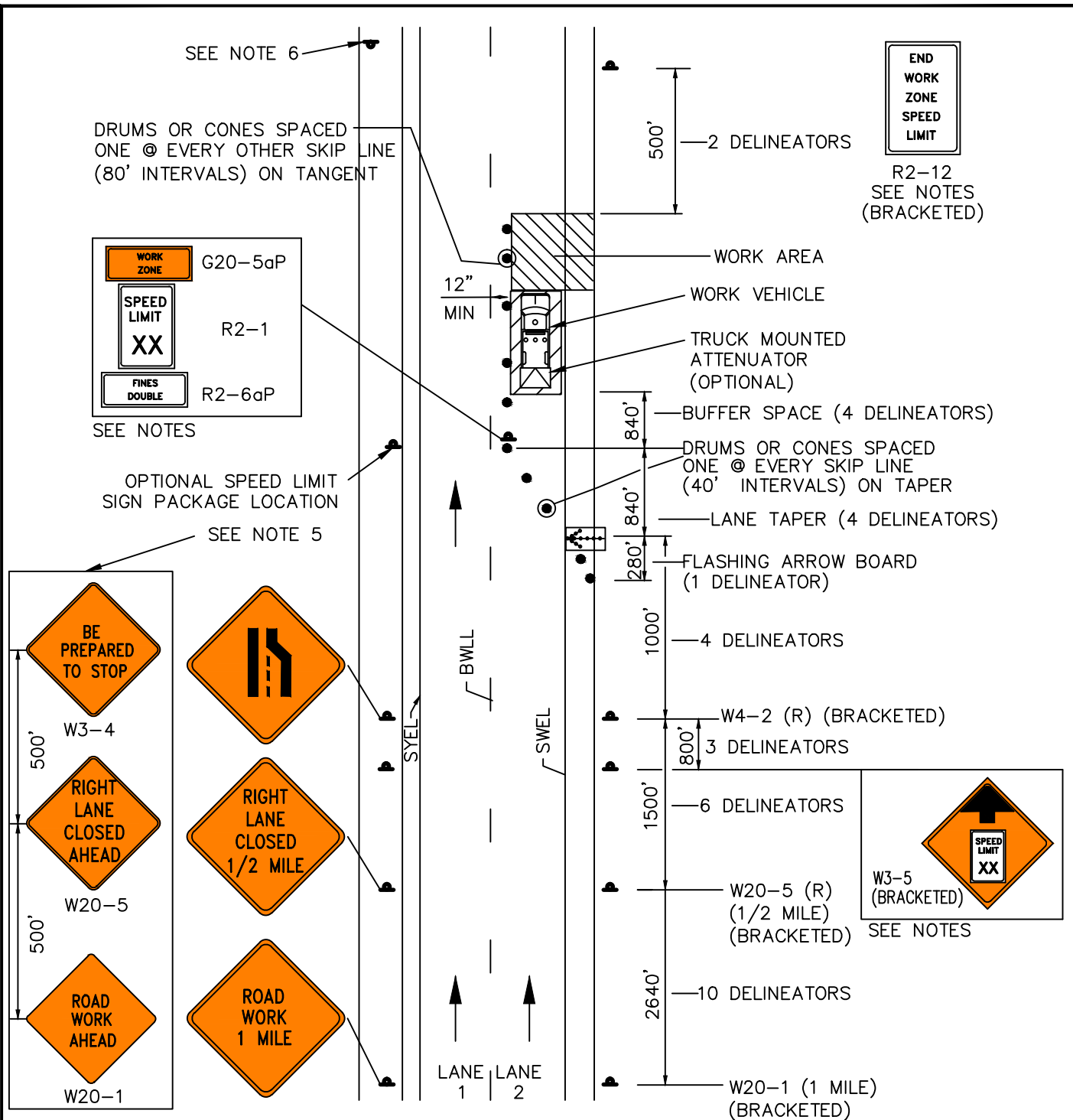
DATE: 12-22-2017



**MAINE TURNPIKE AUTHORITY**  
**TRAFFIC CONTROL DETAIL**

**SINGLE MAINLINE LANE CLOSURE - LEFT**





**NOTES:**

1. USE OF REGULATORY REDUCED SPEEDS SHALL BE USED WHEN WORKERS ARE PRESENT OR SITE CONDITIONS WARRANT. 10 MPH SPEED REDUCTION MANDATORY; IF REDUCTION GREATER THEN 10 MPH IS WARRANTED THEN REQUEST THROUGH MTA ENGINEERING. SPEED LIMIT SIGN IS ONLY NEEDED ON ONE SIDE OR THE OTHER. SPEED LIMIT SIGNS SHALL BE 5' OFF GROUND. SIGN W3-5 NOT NEEDED FOR 10 MPH REDUCTION.
2. OPTIONAL — THE SPEED LIMIT SIGN PACKAGE MAY BE POST MOUNTED ON THE LEFT SHOULDER. POST MOUNTED SIGNS SHALL BE COVERED WHEN NOT IN USE.
3. OPTIONAL — THE WORK ZONE AND FINES DOUBLE SIGN MAY BE MOUNTED ON A SEPARATE EASEL OR POST.
4. OPTIONAL — THE FINES DOUBLE SIGN MAY BE OMITTED.
5. WHEN ON-RAMP EXISTS WITHIN MAINLINE SIGN ARRAY, THIS 3-SIGN ARRAY SHALL BE SET UP ON THE LEFT SHOULDER OF THE RAMP. THE LAST SIGN SHALL BE AT THE RAMP GORE.
6. USE "WORKERS IN MEDIAN" SIGN FOR THE OPPOSITE DIRECTION OF TRAVEL IF WORKERS WILL BE IN THE MEDIAN.

**DETAIL 33R** NOT TO SCALE

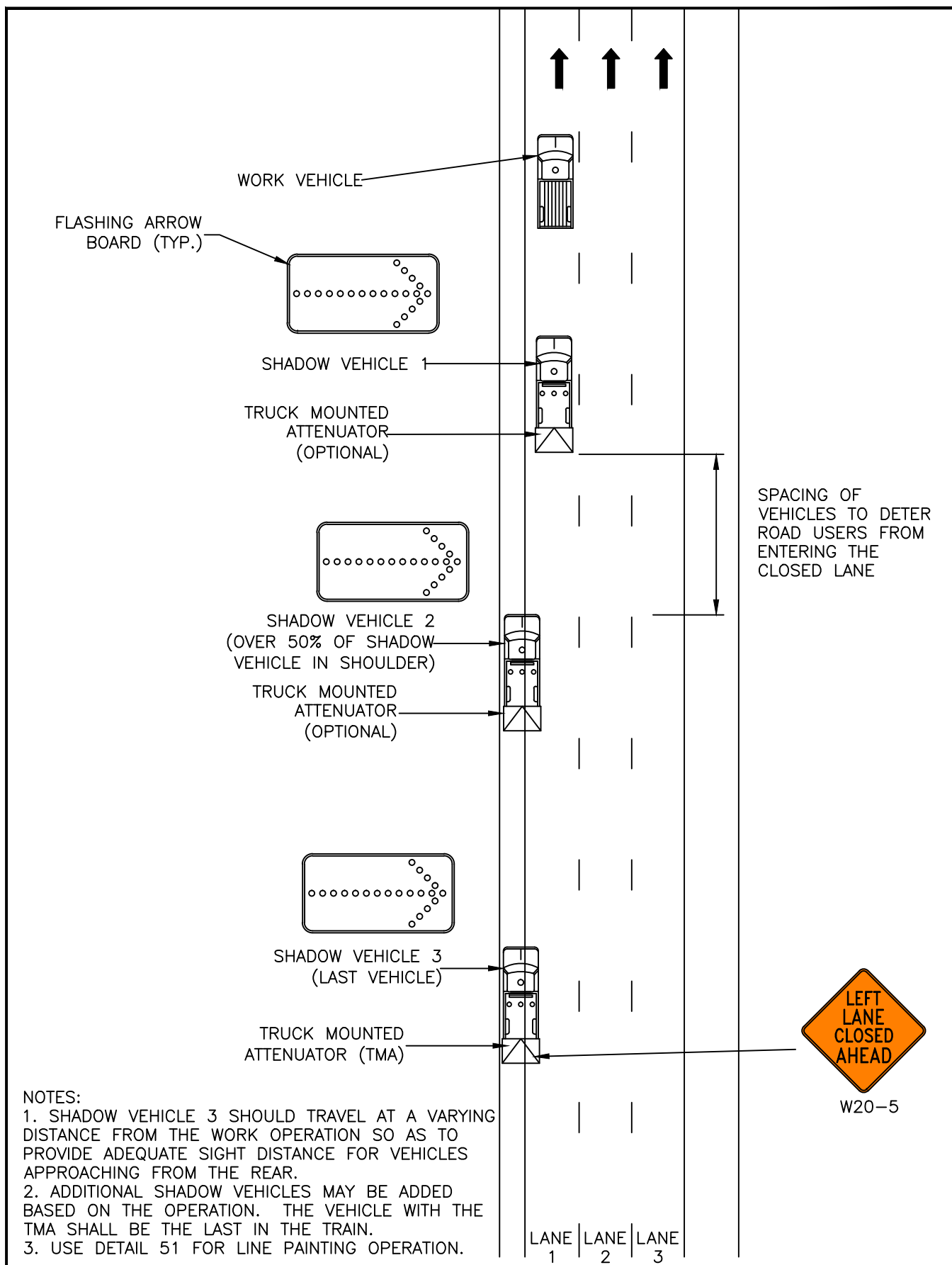
**HNTB**

DATE: 12-22-2017



**MAINE TURNPIKE AUTHORITY**  
**TRAFFIC CONTROL DETAIL**

**SINGLE MAINLINE LANE CLOSURE - RIGHT**



DETAIL 35 L NOT TO SCALE

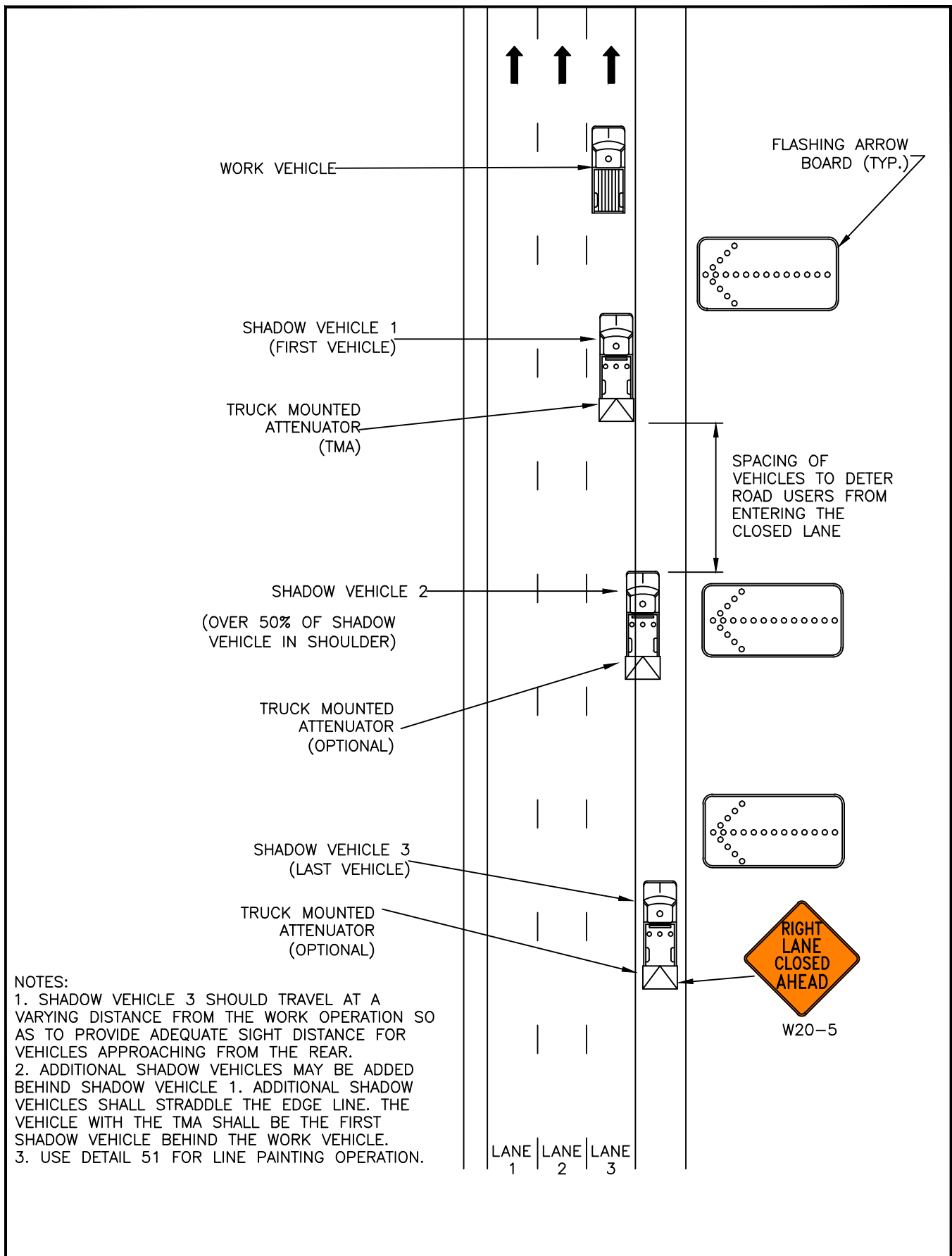
**HNTB**

DATE: 12-22-2017



**MAINE TURNPIKE AUTHORITY**  
**TRAFFIC CONTROL DETAIL**

**MOBILE OPERATION - PASSING LANE CLOSURE**



DETAIL 35 R NOT TO SCALE

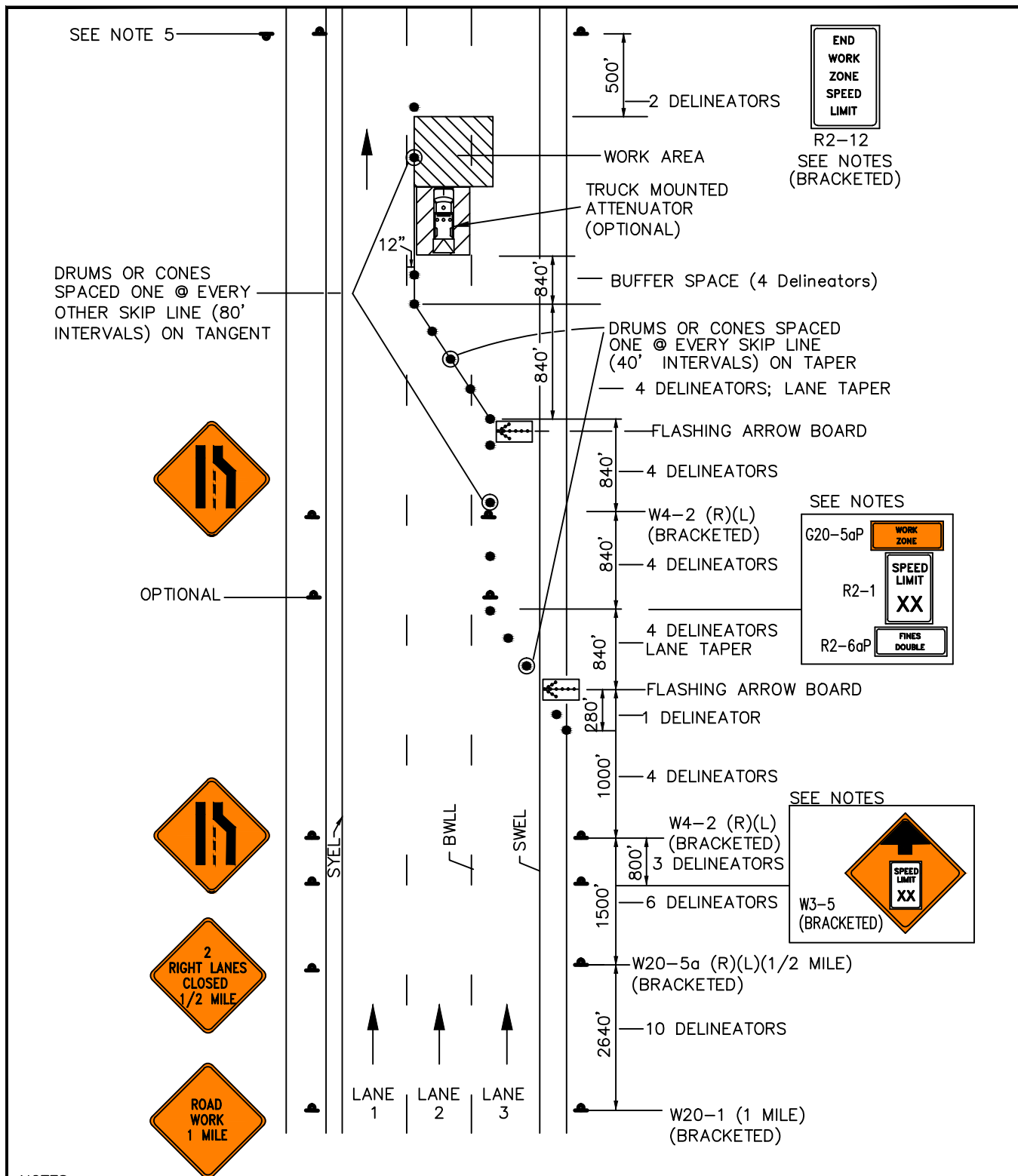
**HNTB**

DATE: 12-22-2017



**MAINE TURNPIKE AUTHORITY**  
**TRAFFIC CONTROL DETAIL**

**MOBILE OPERATION - TRAVEL LANE CLOSURE**



NOTES:

1. USE OF REGULATORY REDUCED SPEEDS SHALL BE USED WHEN WORKERS ARE PRESENT OR SITE CONDITIONS WARRANT. 10 MPH SPEED REDUCTION MANDATORY; IF REDUCTION GREATER THEN 10 MPH IS WARRANTED THEN REQUEST THROUGH MTA ENGINEERING. SPEED LIMIT SIGN IS ONLY NEEDED ON ONE SIDE OR THE OTHER. SPEED LIMIT SIGNS SHALL BE 5' OFF GROUND. SIGN W3-5 NOT NEEDED FOR 10 MPH REDUCTION.
2. OPTIONAL — THE SPEED LIMIT SIGN PACKAGE MAY BE POST MOUNTED ON THE LEFT SHOULDER. POST MOUNTED SIGNS SHALL BE COVERED WHEN NOT IN USE.
3. OPTIONAL — THE WORK ZONE AND FINES DOUBLE SIGNS MAY BE MOUNTED ON A SEPARATE EASEL OR POST.
4. OPTIONAL — THE FINES DOUBLE SIGNS MAY BE OMITTED.
5. USE "WORKERS IN MEDIAN" SIGN FOR THE OPPOSITE DIRECTION OF TRAVEL IF WORKERS WILL BE IN THE MEDIAN.

DETAIL 37 NOT TO SCALE

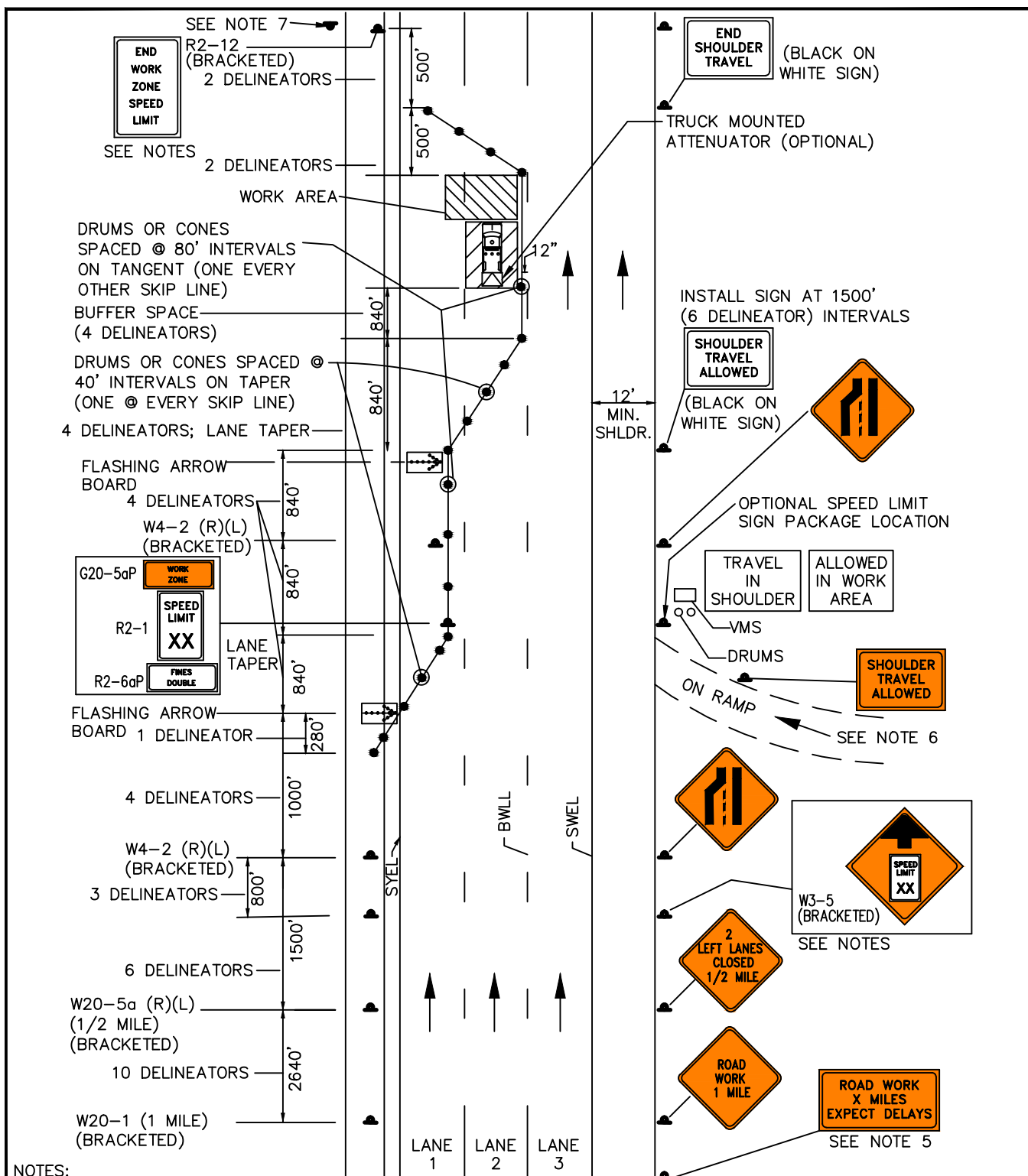
**HNTB**

DATE: 12-22-2017



**MAINE TURNPIKE AUTHORITY**  
**TRAFFIC CONTROL DETAIL**

**DOUBLE MAINLINE LANE CLOSURE**



NOTES:

1. USE OF REGULATORY REDUCED SPEEDS SHALL BE USED WHEN WORKERS ARE PRESENT OR SITE CONDITIONS WARRANT. 10 MPH SPEED REDUCTION MANDATORY; IF REDUCTION GREATER THEN 10 MPH IS WARRANTED THEN REQUEST THROUGH MTA ENGINEERING. SPEED LIMIT SIGN IS ONLY NEEDED ON ONE SIDE OR THE OTHER. SPEED LIMIT SIGNS SHALL BE 5' OFF GROUND. SIGN W3-5 NOT NEEDED FOR 10 MPH REDUCTION.
2. OPTIONAL - THE SPEED LIMIT SIGN PACKAGE MAY BE POST MOUNTED ON THE RIGHT SHOULDER. POST MOUNTED SIGNS SHALL BE COVERED WHEN NOT IN USE.
3. OPTIONAL - THE WORK ZONE AND FINES DOUBLE SIGNS MAY BE MOUNTED ON A SEPARATE EASEL OR POST.
4. OPTIONAL - THE FINES DOUBLE SIGNS MAY BE OMITTED.
5. INSTALL ROAD WORK - X MILES (3 TO 5 MILES) PRIOR TO WORK AREA AND BEFORE NEAREST INTERCHANGE.
6. SEE DETAIL 60A AND 60B FOR GUIDANCE WHEN ENTERING RAMP IS WITHIN CLOSURE.
7. USE "WORKERS IN MEDIAN" SIGN FOR THE OPPOSITE DIRECTION OF TRAVEL IF WORKERS WILL BE IN THE MEDIAN.

DETAIL 37A NOT TO SCALE

**HNTB**

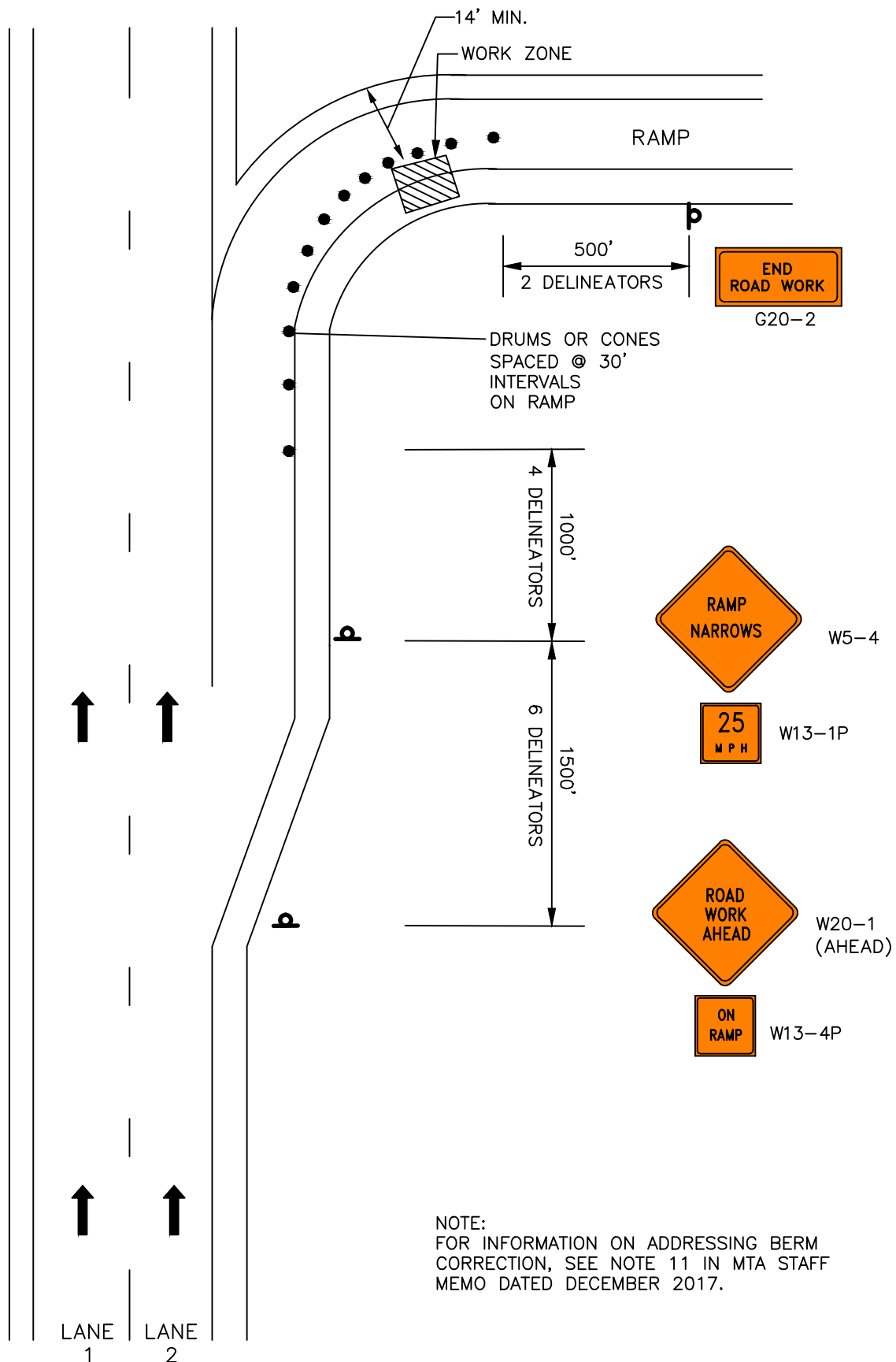
DATE: 12-22-2017



**MAINE TURNPIKE AUTHORITY**  
TRAFFIC CONTROL DETAIL  
DOUBLE MAINLINE LANE CLOSURE  
WITH TRAFFIC IN OUTSIDE 12' SHOULDER







DETAIL 43R NOT TO SCALE

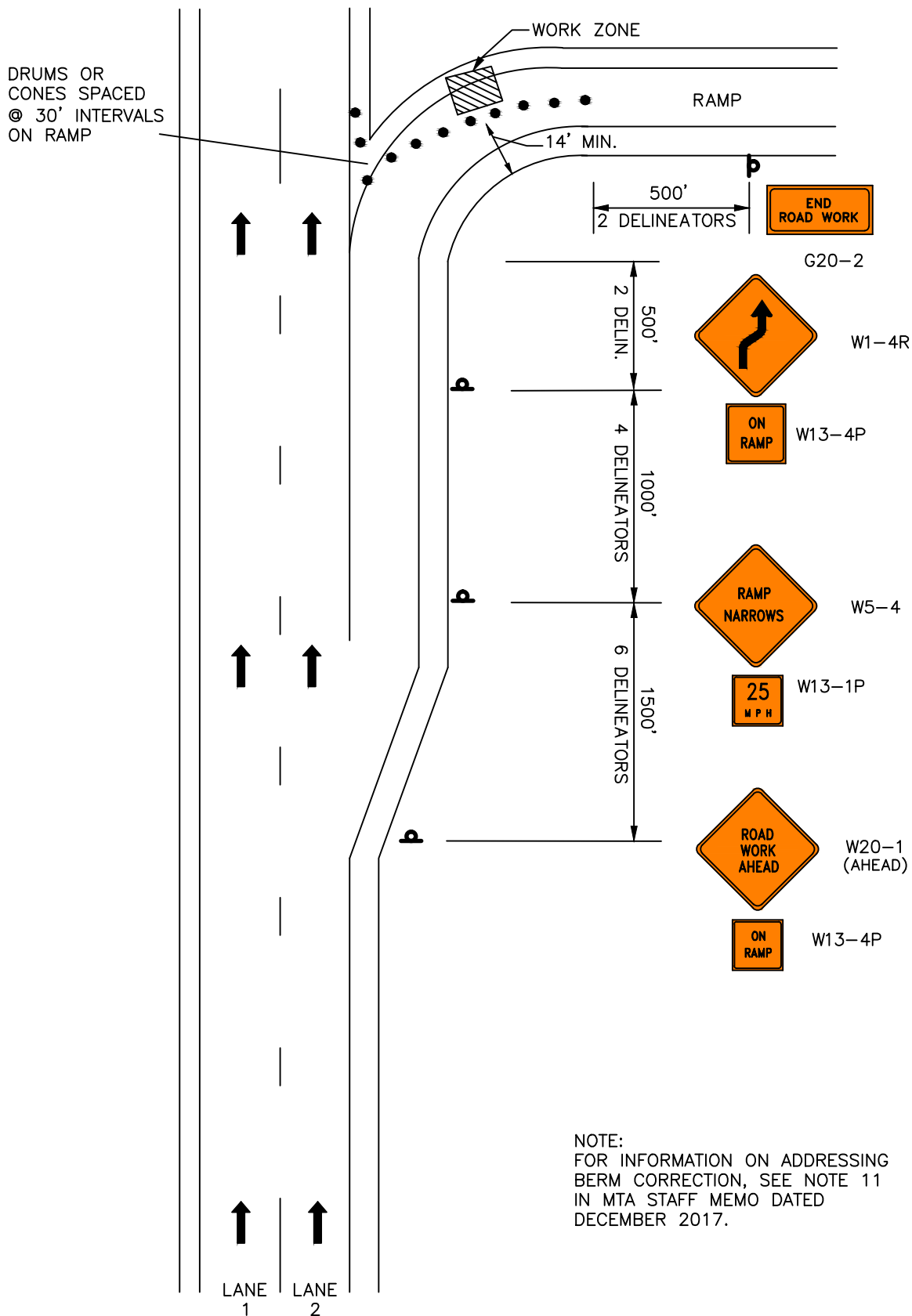
**HNTB**

DATE: 12-22-2017



**MAINE TURNPIKE AUTHORITY  
TRAFFIC CONTROL DETAIL**

**PARTIAL EXIT RAMP CLOSURE**



DETAIL 43L NOT TO SCALE

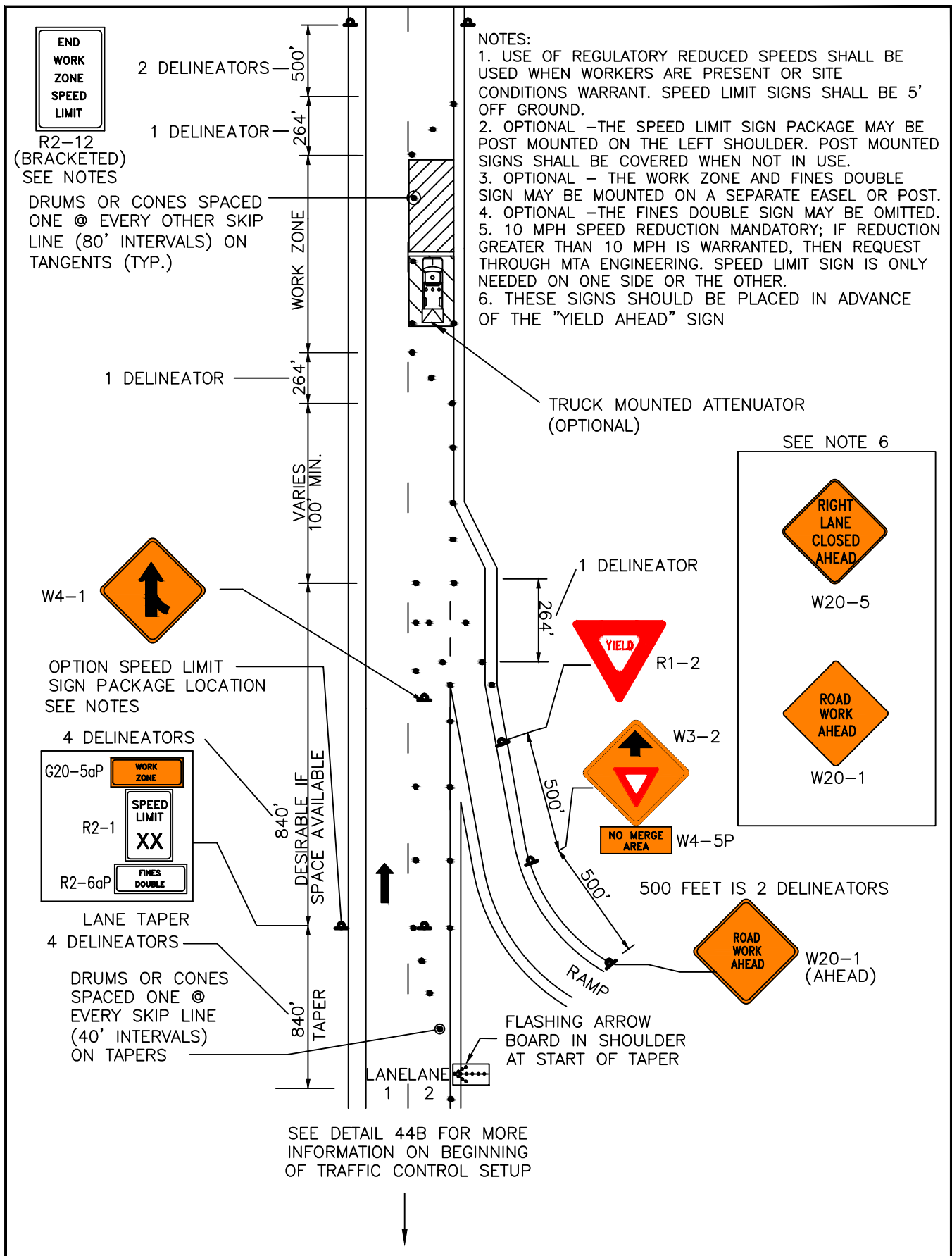
**HNTB**

DATE: 12-22-2017



**MAINE TURNPIKE AUTHORITY**  
**TRAFFIC CONTROL DETAIL**

**PARTIAL EXIT RAMP CLOSURE**



DETAIL 44A NOT TO SCALE

**HNTB**

DATE: 12-22-2017



**MAINE TURNPIKE AUTHORITY**  
**TRAFFIC CONTROL DETAIL**

**TRAVEL LANE CLOSURE AT AN ENTRANCE RAMP**  
**WITH ACCELERATION LANE**

2 DELINEATORS —

NOTES:

1. STOP CONDITION SHALL BE USED AS DETERMINED BY MTA DIRECTOR OF ENGINEERING.
2. USE OF REGULATORY REDUCED SPEEDS SHALL BE USED WHEN WORKERS ARE PRESENT OR SITE CONDITIONS WARRANT. SPEED LIMIT SIGNS SHALL BE 5' OFF GROUND. SIGN W3-5 NOT NEEDED FOR 10 MPH REDUCTION.

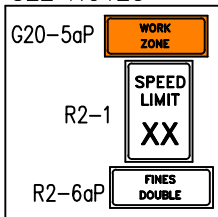


W4-1

1 DELINEATOR —

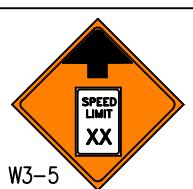
4 DELINEATORS —

OPTION SPEED LIMIT SIGN PACKAGE LOCATION  
SEE NOTES



4 DELINEATORS —  
LANE TAPER

DRUMS OR CONES  
SPACED ONE @  
EVERY SKIP LINE  
(40' INTERVALS)  
ON TAPERS



W3-5  
(BRACKETED)  
SEE NOTES

4 DELINEATORS —

W4-2  
(BRACKETED)

3 DELINEATORS —

6 DELINEATORS —

W20-5R  
(1/2 MILE)

(BRACKETED)

10 DELINEATORS —

W20-1 (1 MILE)  
(BRACKETED)

500'

WORK ZONE

264'

840'

DESIRABLE IF  
SPACE AVAILABLE

840'

TAPER

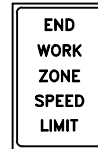
1000'

800'

1500'

2640'

LANE 1  
LANE 2



R2-12  
(BRACKETED)  
SEE NOTES

DRUMS OR CONES SPACED ONE @  
EVERY OTHER SKIP LINE (80'  
INTERVALS) ON TANGENTS (TYP.)

TRUCK MOUNTED  
ATTENUATOR



R1-1



W3-1

SEE NOTE 7



W20-5



W20-1

500'

500'

500'

RAMP

500 FEET IS 2 DELINEATORS

FLASHING  
ARROW BOARD  
IN SHOULDER  
AT START OF  
TAPER

NOTES CONTINUED:

3. OPTIONAL —THE SPEED LIMIT SIGN PACKAGE MAY BE POST MOUNTED ON THE LEFT SHOULDER. POST MOUNTED SIGNS SHALL BE COVERED WHEN NOT IN USE.
4. OPTIONAL — THE WORK ZONE AND FINES DOUBLE SIGN MAY BE MOUNTED ON A SEPARATE EASEL OR POST.
5. OPTIONAL —THE FINES DOUBLE SIGN MAY BE OMITTED.
6. 10 MPH SPEED REDUCTION MANDATORY; IF REDUCTION GREATER THEN 10 MPH IS WARRANTED THEN REQUEST THROUGH MTA ENGINEERING. SPEED LIMIT SIGN IS ONLY NEEDED ON ONE SIDE OR THE OTHER.
7. THESE SIGNS SHOULD BE PLACED IN ADVANCE OF THE "STOP AHEAD" SIGN

DETAIL 44B NOT TO SCALE

**HNTB**

DATE: 12-22-2017

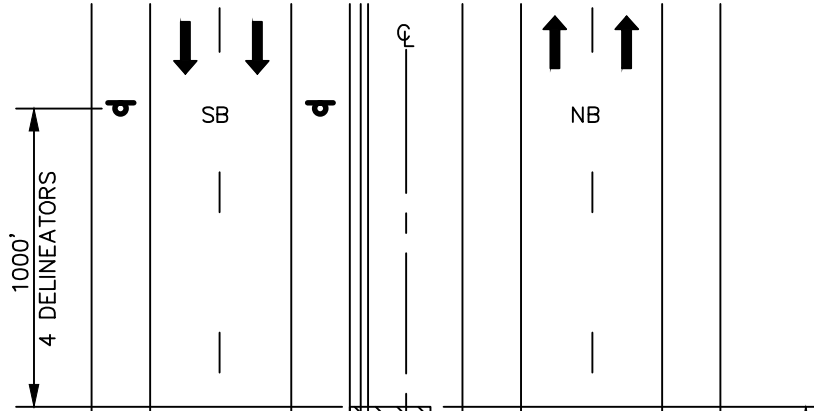


**MAINE TURNPIKE AUTHORITY  
TRAFFIC CONTROL DETAIL**

**TRAVEL LANE CLOSURE AT AN ENTRANCE RAMP  
WITHOUT ACCELERATION LANE**

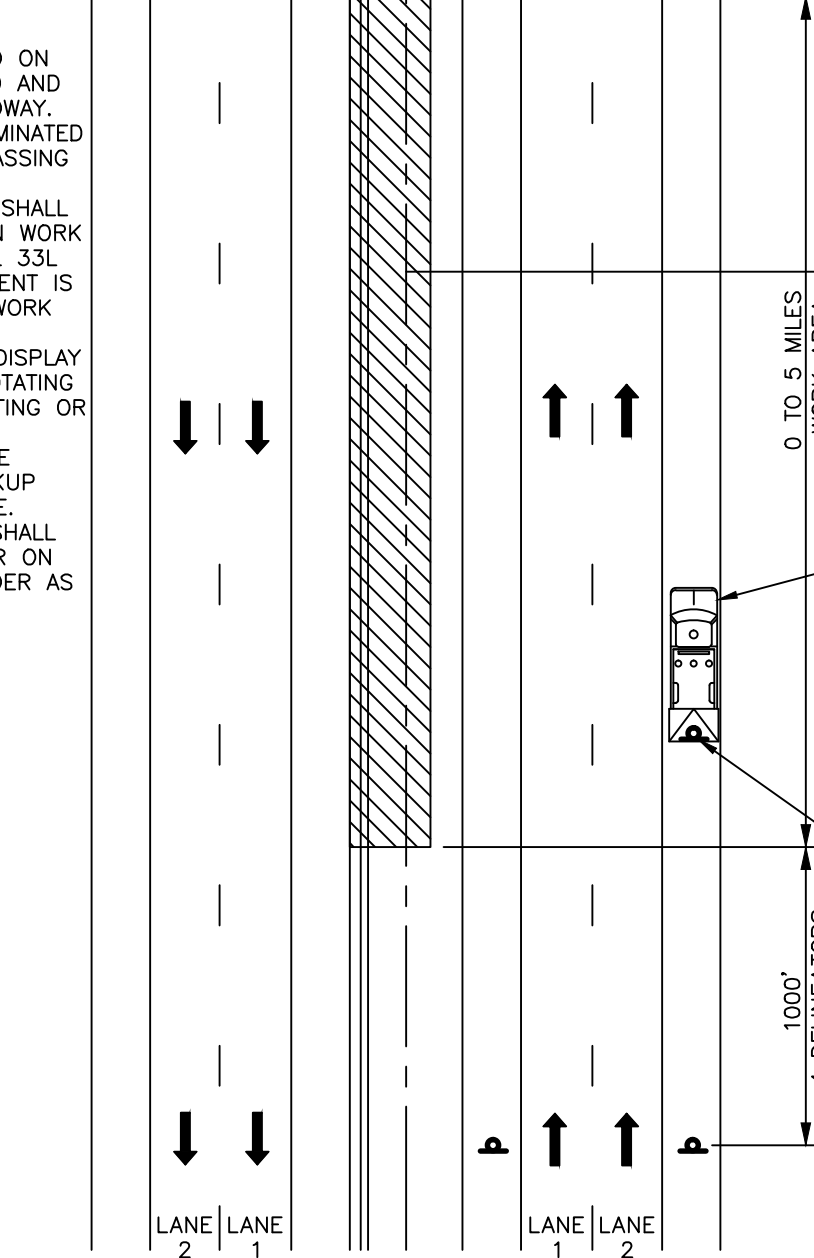


NEXT  
XX MILES  
W7-3aP  
(BRACKETED)



NOTES:

1. SIGNS REQUIRED ON BOTH NORTHBOUND AND SOUTHBOUND ROADWAY. SIGNS MAY BE ELIMINATED ON ROADWAY IF PASSING LANE IS CLOSED.
2. NO EQUIPMENT SHALL BE LOCATED WITHIN WORK AREA. USE DETAIL 33L OR 35L IF EQUIPMENT IS REQUIRED WITHIN WORK AREA.
3. VEHICLE MUST DISPLAY HIGH-INTENSITY ROTATING FLASHING, OSCILLATING OR STROBE LIGHT.
4. SHADOW VEHICLE SHOULD BE A PICKUP TRUCK IF POSSIBLE. SHADOW VEHICLE SHALL BE PARKED AS FAR ON THE RIGHT SHOULDER AS POSSIBLE



WORK AREA

0 TO 5 MILES  
WORK AREA

SHADOW VEHICLE

SIGN MOUNTED  
ON TRUCK



(BRACKETED)

NEXT  
XX MILES

W7-3aP

DETAIL 48 NOT TO SCALE

**HNTB**

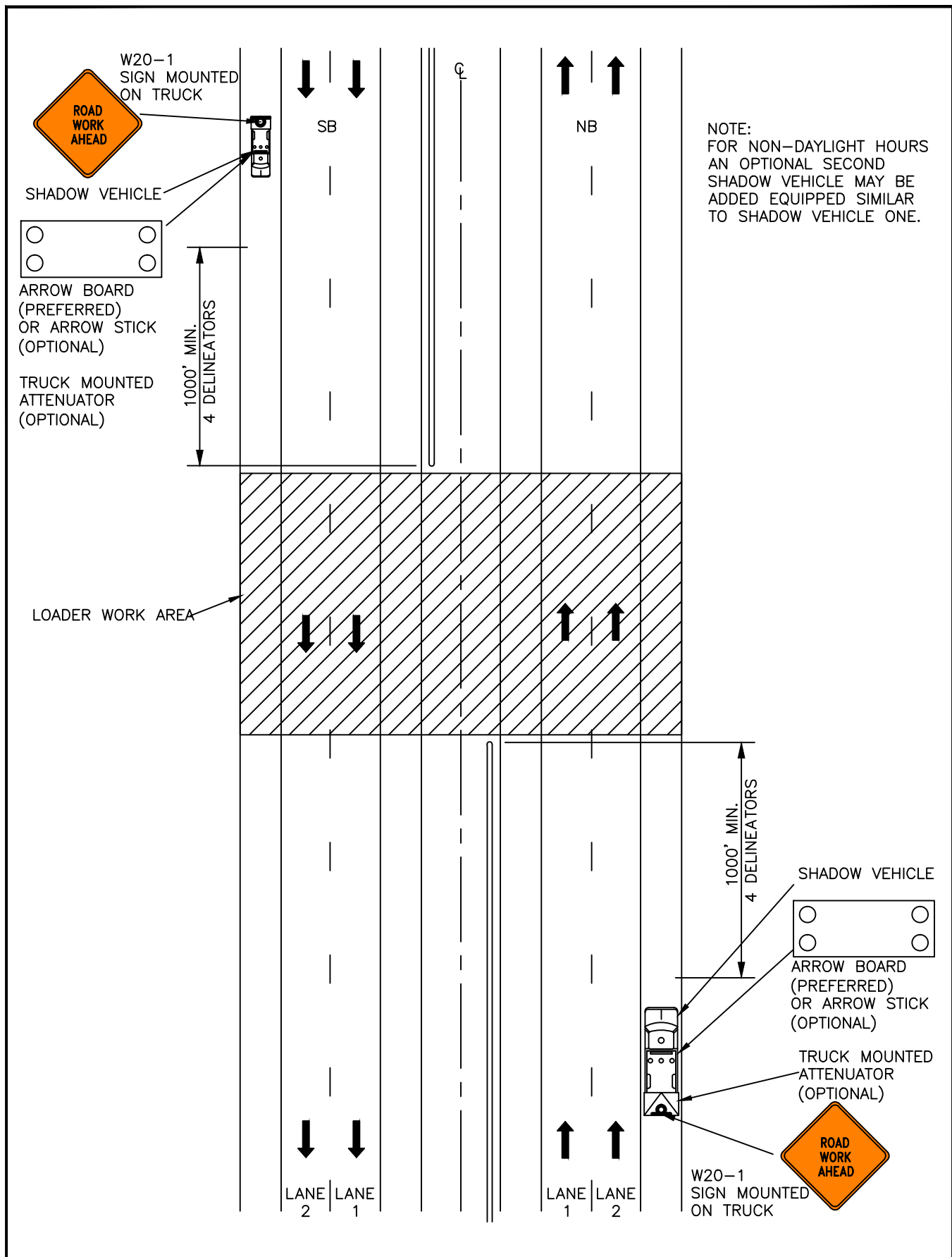
DATE: 12-22-2017



**MAINE TURNPIKE AUTHORITY**  
**TRAFFIC CONTROL DETAIL**

**WORK IN MEDIAN - LITTER PATROL**  
**SHORT TERM (UP TO 12 DAYLIGHT HOURS)**





DETAIL 49 NOT TO SCALE

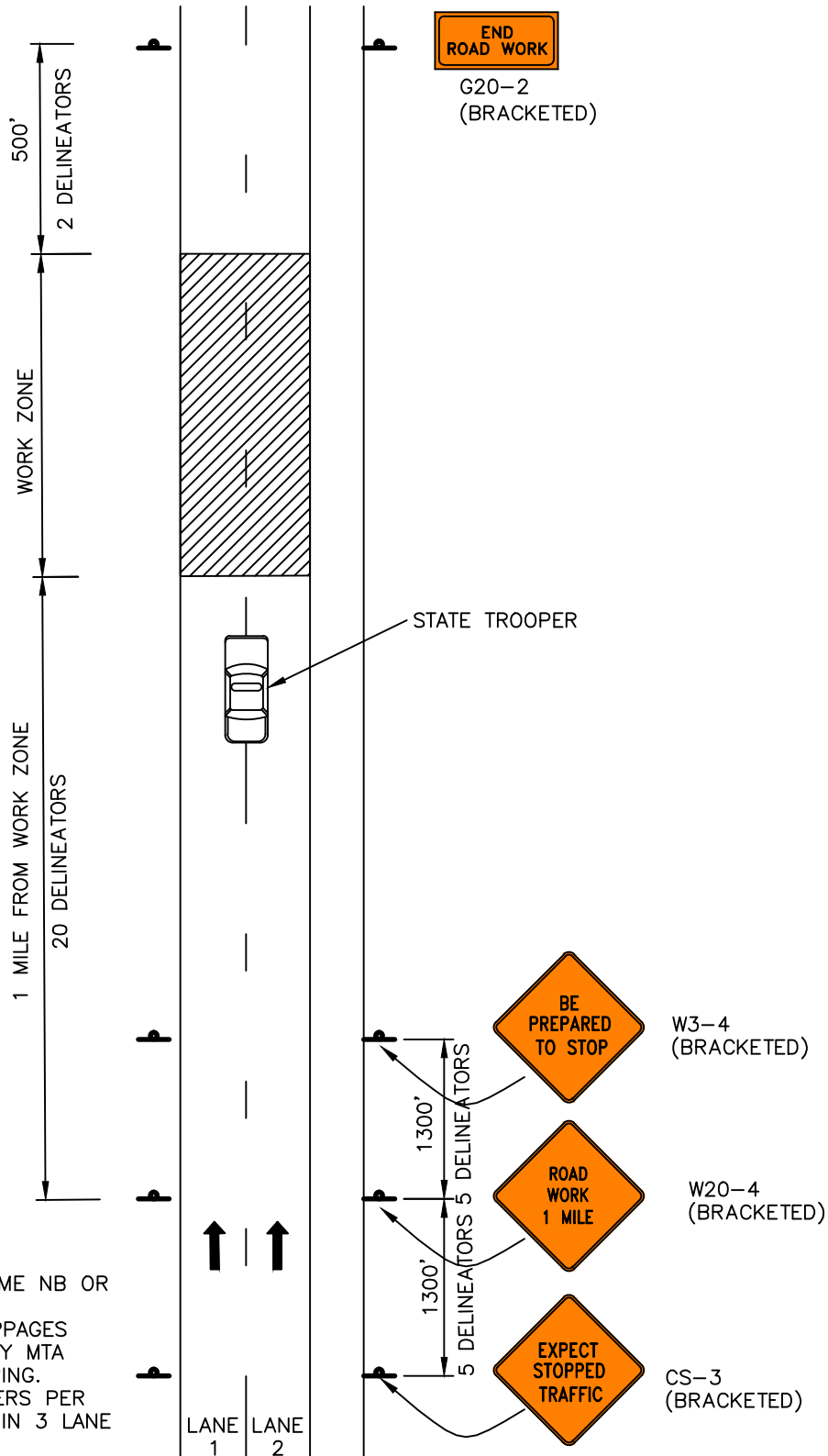
**HNTB**

DATE: 12-22-2017



**MAINE TURNPIKE AUTHORITY**  
**TRAFFIC CONTROL DETAIL**

**SNOW REMOVAL IN MEDIAN OPENING**  
**SHORT TERM (UP TO 1 HOUR)**



NOTES:

1. SET UP IS THE SAME NB OR SB, 2 LANES OR 3.
2. ALL MAINLINE STOPPAGES SHALL BE APPROVED BY MTA DIRECTOR OF ENGINEERING.
3. TWO STATE TROOPERS PER BOUND ARE REQUIRED IN 3 LANE SECTION.

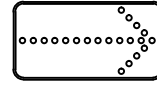
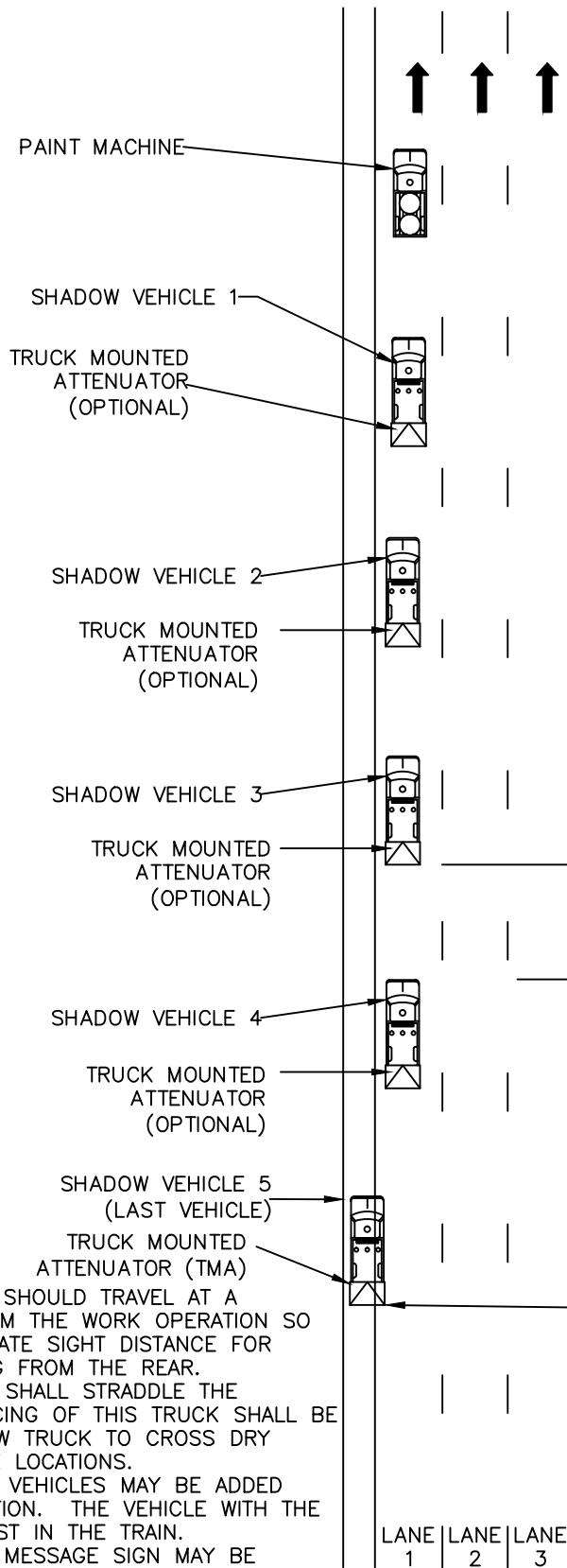
DETAIL 50 NOT TO SCALE

**HNTB**

DATE: 12-22-2017



**MAINE TURNPIKE AUTHORITY**  
**TRAFFIC CONTROL DETAIL**  
**STOPPING MAINLINE TRAFFIC**



NOTES:

1. ALL SHADOW VEHICLES SHALL HAVE AN ARROW BOARD.
2. SHADOW VEHICLES 1 THROUGH 4 MAY HAVE THE FOLLOWING SIGN.



3. SHADOW VEHICLES 1, 2, AND 3 MAY HAVE THE FOLLOWING SIGN.



SPACING OF VEHICLES TO DETER ROAD USERS FROM ENTERING THE CLOSED LANE (TYP)



W20-5

NOTES:

1. SHADOW VEHICLE 5 SHOULD TRAVEL AT A VARYING DISTANCE FROM THE WORK OPERATION SO AS TO PROVIDE ADEQUATE SIGHT DISTANCE FOR VEHICLES APPROACHING FROM THE REAR.
2. SHADOW VEHICLE 5 SHALL STRADDLE THE SHOULDER LINE. SPACING OF THIS TRUCK SHALL BE AS REQUIRED TO ALLOW TRUCK TO CROSS DRY PAINT LINES AT BRIDGE LOCATIONS.
3. ADDITIONAL SHADOW VEHICLES MAY BE ADDED BASED ON THE OPERATION. THE VEHICLE WITH THE TMA SHALL BE THE LAST IN THE TRAIN.
4. A MOBILE VARIABLE MESSAGE SIGN MAY BE UTILIZED IN ADVANCE OF THE WORK AND SHADOW VEHICLE TRAIN TO ALERT APPROACHING VEHICLE OF THE WORK.

DETAIL 51 L NOT TO SCALE

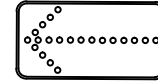
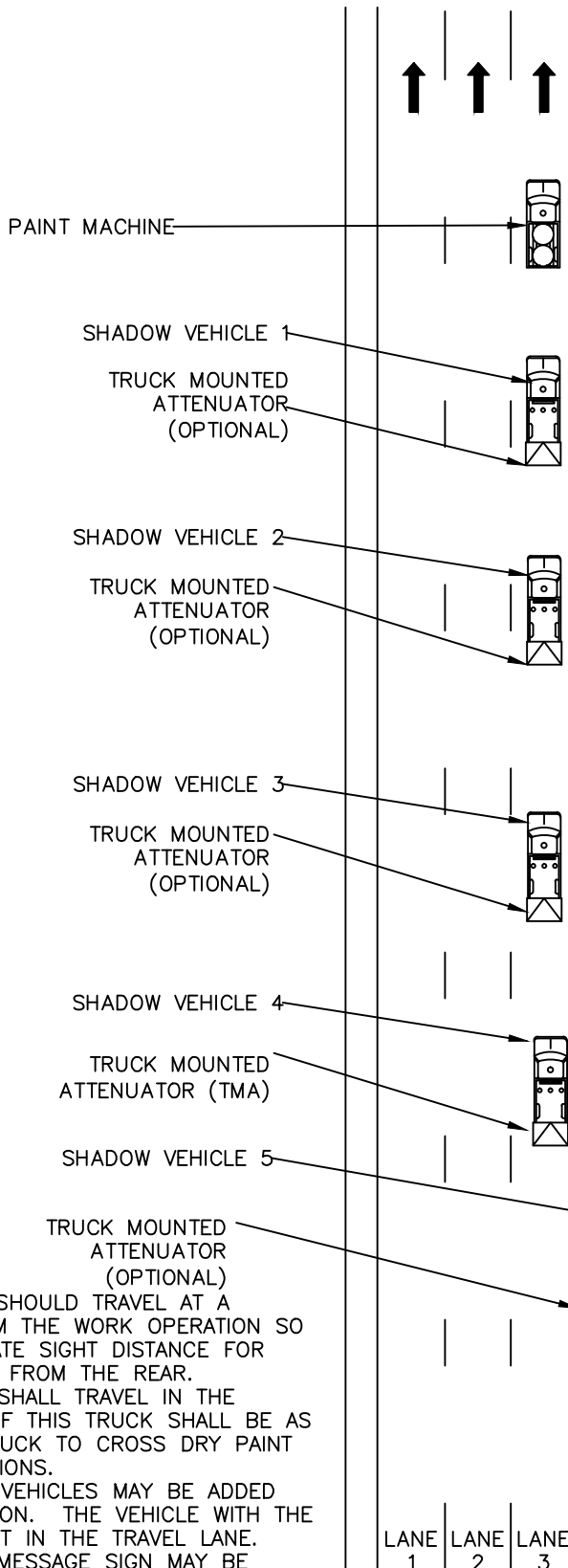
**HNTB**

DATE: 12-22-2017



**MAINE TURNPIKE AUTHORITY**  
**TRAFFIC CONTROL DETAIL**

**MOBILE OPERATION - PAINTING**  
**PASSING LANE CLOSURE**



- NOTES:
1. ALL SHADOW VEHICLES SHALL HAVE AN ARROW BOARD.
  2. SHADOW VEHICLES 1 THROUGH 4 MAY HAVE THE FOLLOWING SIGN.



3. SHADOW VEHICLES 1, 2, AND 3 MAY HAVE THE FOLLOWING SIGN.



SPACING OF VEHICLES TO DETER ROAD USERS FROM ENTERING THE CLOSED LANE (TYP)



W20-5

NOTES:

1. SHADOW VEHICLE 5 SHOULD TRAVEL AT A VARYING DISTANCE FROM THE WORK OPERATION SO AS TO PROVIDE ADEQUATE SIGHT DISTANCE FOR VEHICLES APPROACHING FROM THE REAR.
2. SHADOW VEHICLE 5 SHALL TRAVEL IN THE SHOULDER. SPACING OF THIS TRUCK SHALL BE AS REQUIRED TO ALLOW TRUCK TO CROSS DRY PAINT LINES AT BRIDGE LOCATIONS.
3. ADDITIONAL SHADOW VEHICLES MAY BE ADDED BASED ON THE OPERATION. THE VEHICLE WITH THE TMA SHALL BE THE LAST IN THE TRAVEL LANE.
4. A MOBILE VARIABLE MESSAGE SIGN MAY BE UTILIZED IN ADVANCE OF THE WORK AND SHADOW VEHICLE TRAIN TO ALERT APPROACHING VEHICLE OF THE WORK.

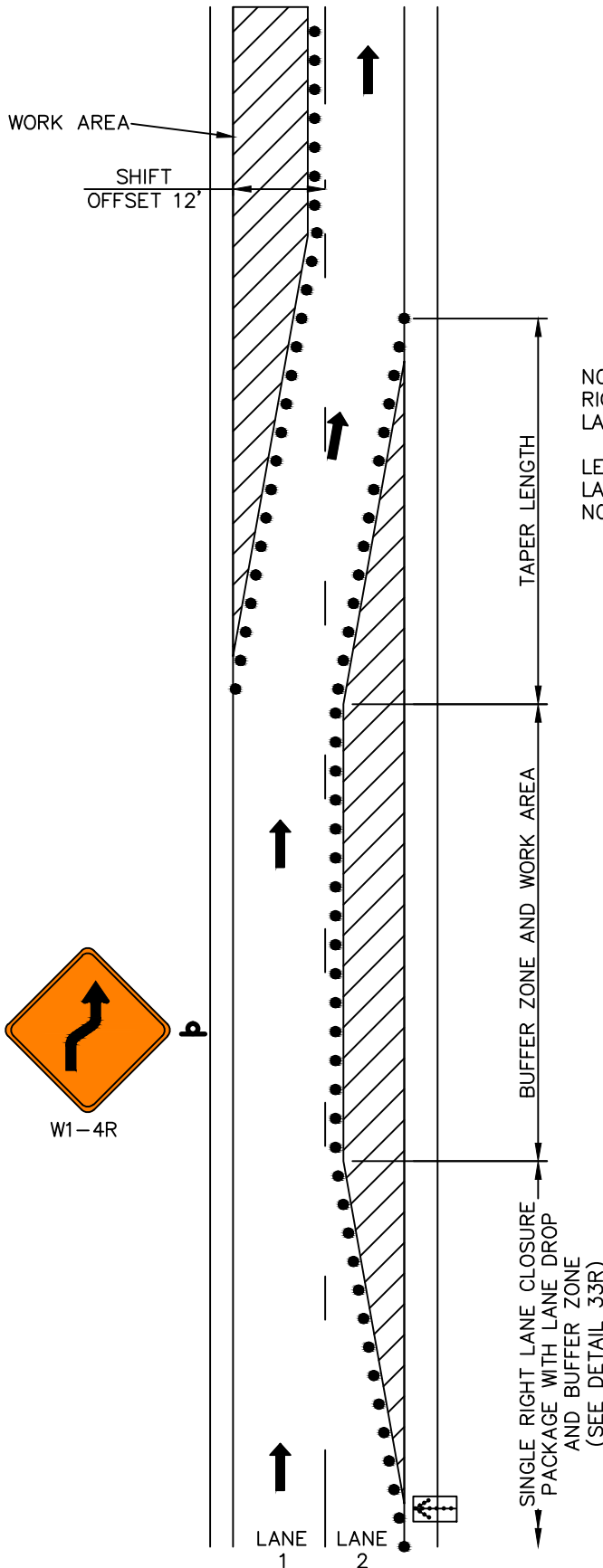
DETAIL 51 R NOT TO SCALE

**HNTB**

DATE: 12-22-2017



**MAINE TURNPIKE AUTHORITY**  
**TRAFFIC CONTROL DETAIL**  
**MOBILE OPERATION - PAINTING**  
**TRAVEL LANE CLOSURE**



SHIFT OFFSET	SPEED	TAPER LENGTH (MIN.)
12'	50	300'
12'	60	360'
12'	70	420'

NOTES:  
RIGHT LANE CLOSED SHIFTING TO LEFT  
LANE CLOSED SHOWN.

LEFT LANE CLOSED SHIFTING TO RIGHT  
LANE CLOSED SIMILAR. USE W1-4L SIGN,  
NOT W1-4R SIGN.



DETAIL 58A NOT TO SCALE

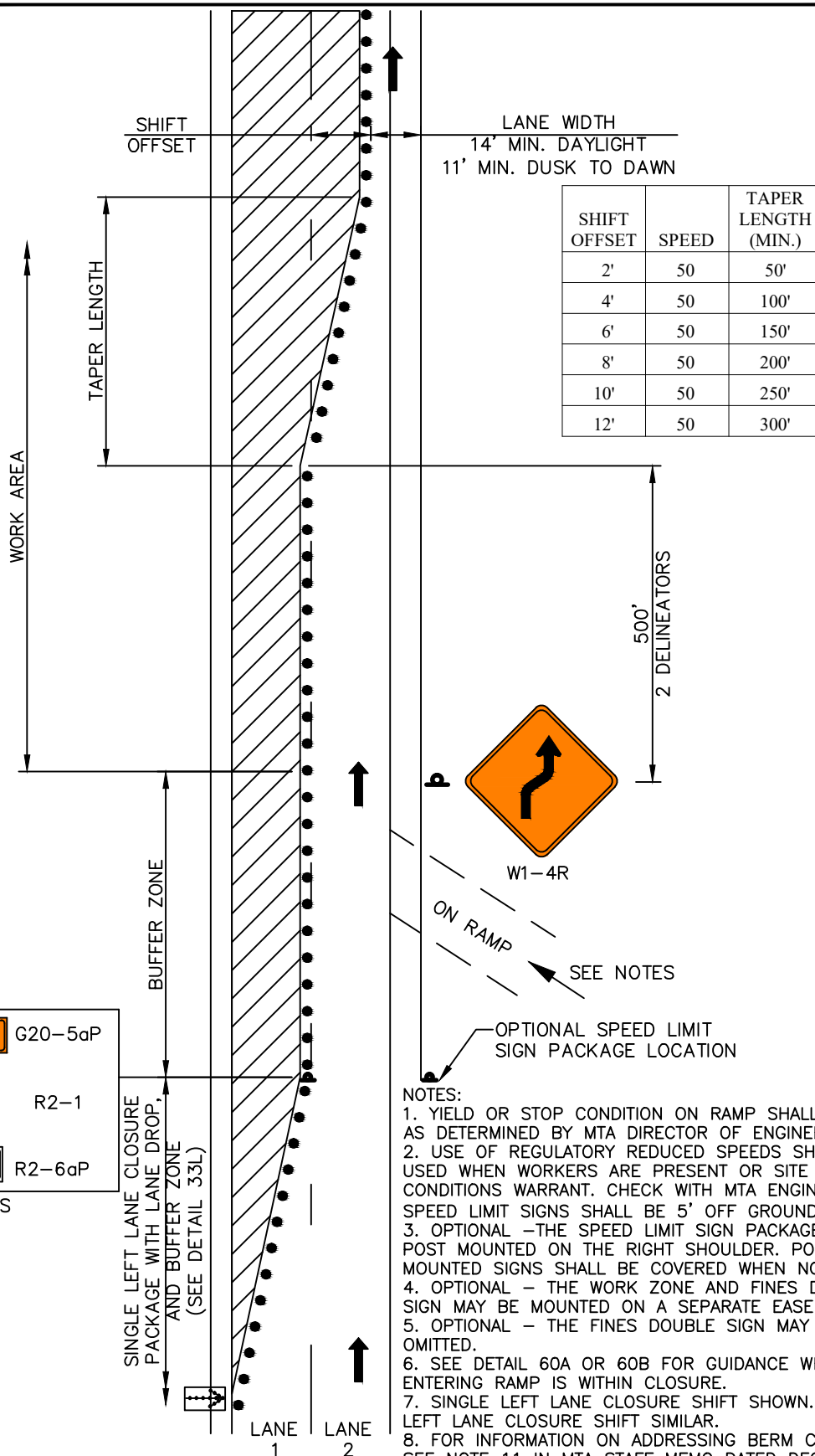
**HNTB**

DATE: 12-22-2017



**MAINE TURNPIKE AUTHORITY**  
**TRAFFIC CONTROL DETAIL**

**SINGLE LANE CLOSURE**  
**LANE SHIFT TO ADJACENT LANE**



- NOTES:
1. YIELD OR STOP CONDITION ON RAMP SHALL BE USED AS DETERMINED BY MTA DIRECTOR OF ENGINEERING.
  2. USE OF REGULATORY REDUCED SPEEDS SHALL BE USED WHEN WORKERS ARE PRESENT OR SITE CONDITIONS WARRANT. CHECK WITH MTA ENGINEERING.
  3. SPEED LIMIT SIGNS SHALL BE 5' OFF GROUND.
  4. OPTIONAL - THE SPEED LIMIT SIGN PACKAGE MAY BE POST MOUNTED ON THE RIGHT SHOULDER. POST MOUNTED SIGNS SHALL BE COVERED WHEN NOT IN USE.
  5. OPTIONAL - THE WORK ZONE AND FINES DOUBLE SIGN MAY BE MOUNTED ON A SEPARATE EASEL OR POST.
  6. OPTIONAL - THE FINES DOUBLE SIGN MAY BE OMITTED.
  7. SEE DETAIL 60A OR 60B FOR GUIDANCE WHEN ENTERING RAMP IS WITHIN CLOSURE.
  8. SINGLE LEFT LANE CLOSURE SHIFT SHOWN. DOUBLE LEFT LANE CLOSURE SHIFT SIMILAR.
  9. FOR INFORMATION ON ADDRESSING BERM CORRECTION, SEE NOTE 11 IN MTA STAFF MEMO DATED DECEMBER 2017.

DETAIL 58B NOT TO SCALE

**HNTB**

DATE: 12-22-2017



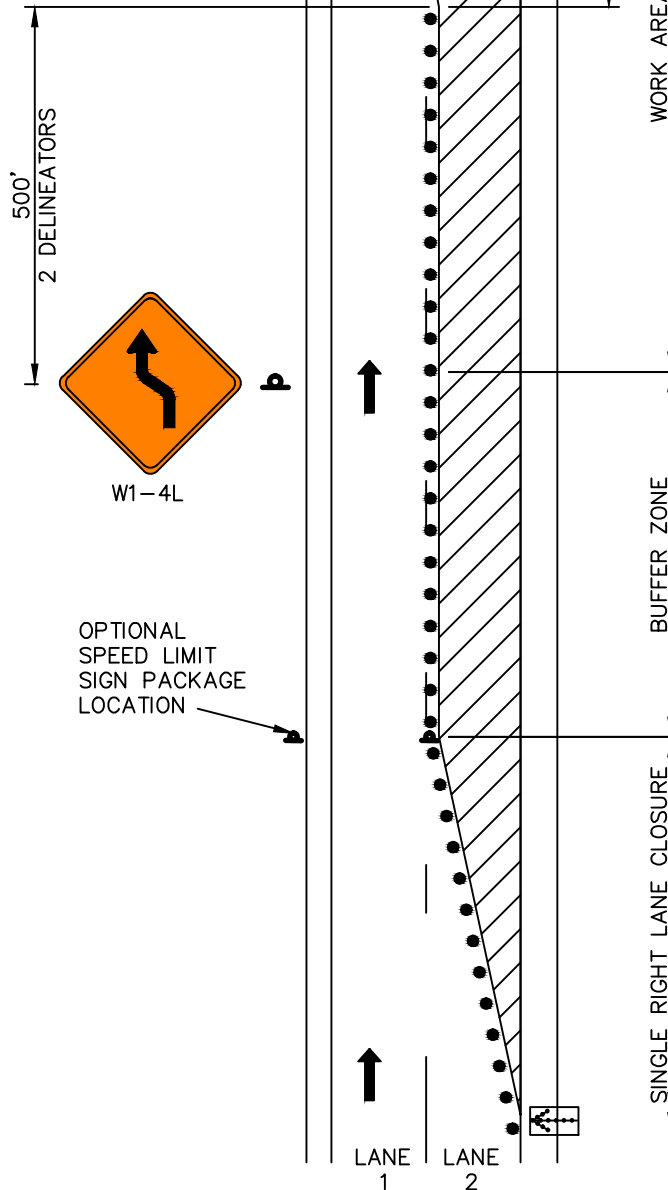
**MAINE TURNPIKE AUTHORITY**  
**TRAFFIC CONTROL DETAIL**  
**SINGLE OR DOUBLE LEFT LANE CLOSURE WITH**  
**RIGHT LANE SHIFT ONTO SHOULDER**



LANE WIDTH  
14' MIN. DAYLIGHT  
11' MIN. DUSK TO DAWN

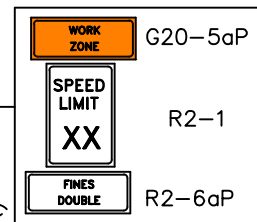
SHIFT  
OFFSET

SHIFT OFFSET	SPEED	TAPER LENGTH (MIN.)
2'	50	50'
4'	50	100'
5'	50	125'



#### NOTES:

1. USE OF REGULATORY REDUCED SPEEDS SHALL BE USED WHEN WORKERS ARE PRESENT OR SITE CONDITIONS WARRANT. CHECK WITH MTA ENGINEERING. SPEED LIMIT SIGNS SHALL BE 5' OFF GROUND.
2. OPTIONAL -THE SPEED LIMIT SIGN PACKAGE MAY BE POST MOUNTED ON THE LEFT SHOULDER. POST MOUNTED SIGNS SHALL BE COVERED WHEN NOT IN USE.
3. OPTIONAL - THE WORK ZONE AND FINES DOUBLE SIGN MAY BE MOUNTED ON A SEPARATE EASEL OR POST.
4. OPTIONAL -THE FINES DOUBLE SIGN MAY BE OMITTED.
5. SINGLE RIGHT LANE CLOSURE SHIFT SHOWN. DOUBLE RIGHT LANE CLOSURE SHIFT SIMILAR.
6. FOR INFORMATION ON ADDRESSING BERM CORRECTION, SEE NOTE 11 IN MTA STAFF MEMO DATED DECEMBER 2017.



DETAIL 58C NOT TO SCALE

**HNTB**

DATE: 12-22-2017



**MAINE TURNPIKE AUTHORITY**  
**TRAFFIC CONTROL DETAIL**  
**SINGLE OR DOUBLE RIGHT LANE CLOSURE WITH**  
**LEFT LANE SHIFT ONTO SHOULDER**

SEE NOTE 4



W20-5



W20-1

NOTES:

1. CHECK WITH MTA ENGINEERING FOR WORK PERIODS THIS LANE CLOSURE SETUP CAN BE INSTALLED.
2. SHIFT ENTERING RAMP MERGE TO THE RIGHT THE SAME DISTANCE WHEN LANE 2 IS SHIFTED TO THE RIGHT.
3. IF SKIP LINE DELINEATION CROWDS LANE 2 INTO RAMP, DELINEATION OF GORE MAY BE EXTENDED OR SHIFTED Laterally INTO RAMP ACCELERATION LANE TO PROVIDE SMOOTHER MERGE AREA.
4. THESE SIGNS SHOULD BE PLACED IN ADVANCE OF THE "YIELD AHEAD" SIGN ON RAMP

DRUMS OR CONES SPACED @ 80' INTERVALS ON TANGENT, 40' INTERVALS ON TAPER

DELINEATE RIGHT SHOULDER WITH DRUMS. SEE NOTES.

730'  
3 DELINEATORS

500'  
2 DELINEATORS



W4-1

SINGLE LEFT LANE CLOSURE PACKAGE WITH LANE DROP AND BUFFER ZONE (SEE DETAIL 33L)

LANE 1

LANE 2

RAMP

TAPERED RAMP



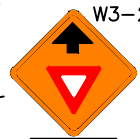
R1-2

500'



W4-1

500'



W3-2

NO MERGE AREA

W4-5P

DETAIL 60A

NOT TO SCALE

**HNTB**

DATE: 12-22-2017




**MAINE TURNPIKE AUTHORITY**  
**TRAFFIC CONTROL DETAIL**

LANE 1 CLOSED  
MAINLINE TRAFFIC IN LANE 2  
TAPER ENTERING RAMP OPEN


- NOTES:
1. CHECK WITH MTA ENGINEERING FOR WORK PERIODS THIS LANE CLOSURE SETUP CAN BE INSTALLED.
  2. SHIFT ENTERING RAMP MERGE TO THE RIGHT THE SAME DISTANCE WHEN LANE 2 IS SHIFTED TO THE RIGHT.
  3. THESE SIGNS SHOULD BE PLACED IN ADVANCE OF THE "YIELD AHEAD" SIGN ON RAMP

DRUMS OR CONES SPACED @  
80' INTERVALS ON TANGENT,  
40' INTERVALS ON TAPER

SEE NOTE 3



W20-5



W20-1

DELINEATE RIGHT SHOULDER WITH DRUMS. SEE NOTES.

PAINTED GORE

PARALLEL RAMP  
ACCELERATION LANE, LENGTH VARIES

SINGLE LEFT LANE CLOSURE PACKAGE  
WITH LANE DROP AND BUFFER ZONE  
(SEE DETAIL 33L)



W4-1

LANE 1

LANE 2

RAMP



R1-2



W4-1



W3-2

DETAIL 60B

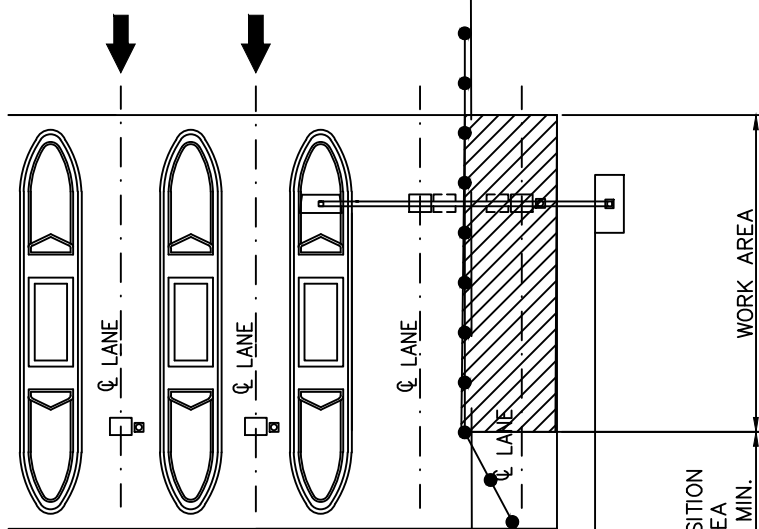
NOT TO SCALE

**HNTB**

DATE: 12-22-2017



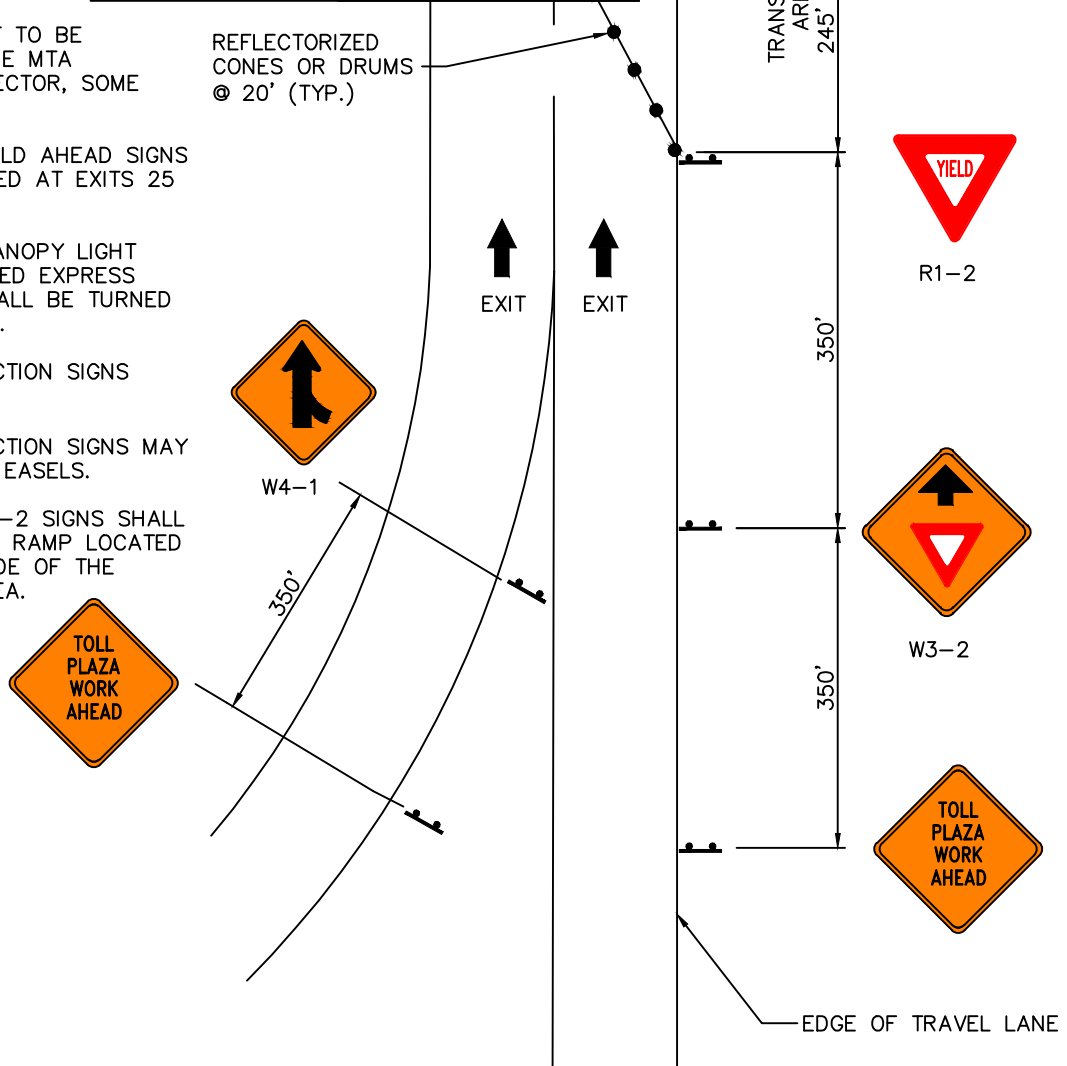
**MAINE TURNPIKE AUTHORITY**  
**TRAFFIC CONTROL DETAIL**  
**LANE 1 CLOSED**  
**MAINLINE TRAFFIC IN LANE 2**  
**PARALLEL ENTERING RAMP OPEN**



NOTES:

1. EXACT LAYOUT TO BE APPROVED BY THE MTA ENGINEERING DIRECTOR, SOME SITES MAY VARY.
2. YIELD AND YIELD AHEAD SIGNS ARE NOT REQUIRED AT EXITS 25 AND 46.
3. THE GREEN CANOPY LIGHT ABOVE THE CLOSED EXPRESS EXITING LANE SHALL BE TURNED OFF OR COVERED.
4. ALL CONSTRUCTION SIGNS SHALL BE 4'X4'.
5. ALL CONSTRUCTION SIGNS MAY BE MOUNTED ON EASELS.
6. R1-2 AND W3-2 SIGNS SHALL BE USED ON THE RAMP LOCATED ON THE SAME SIDE OF THE PLAZA WORK AREA.

REFLECTORIZED CONES OR DRUMS @ 20' (TYP.)



EXPRESS EXITING LANES – LANE CLOSURE  
EXITS 19, 25, 32, 36, 42, 45, 46, 47, 48, 53

N.T.S.

DETAIL TP-50 NOT TO SCALE

**HNTB**

DATE: 12-22-2017



**MAINE TURNPIKE AUTHORITY  
TOLL PLAZA TRAFFIC CONTROL**

**LANE CLOSURE - EXPRESS EXITING LANES**



W20-5

EXISTING  
CONCRETE PAD



W20-5

SIGN AT THIS LOCATION  
ONLY REQUIRED FOR  
MAINLINE BARRIER PLAZAS

1000' OR  
25' FROM INTERSECTION

ENTER

ENTER

ENTER

ENTER

REFLECTORIZED  
CONES OR DRUMS  
@ 10' (TYP.)

10'  
(TYP.)

5'-0" (TYP.)

LANE

LANE

LANE

LANE

LANE

EXIT

THIS SIGN AND DRUMS ARE  
REQUIRED ON REVERSIBLE  
LANES WHEN THE ADJACENT  
LANE CARRIES TRAFFIC IN THE  
OPPOSITE DIRECTION TO THE  
CLOSED LANE.

#### NOTES:

1. A SINGLE LANE CLOSURE IS REQUIRED FOR CONSTRUCTION AND MAINTENANCE WORK IN THE LANE OR ON AN ISLAND.
2. CANOPY LIGHT ABOVE CLOSED LANE SHALL BE RED.

3. ALL CONSTRUCTION SIGNS SHALL BE 4'X4'.
4. ALL CONSTRUCTION SIGNS MAY BE MOUNTED ON EASELS.

DETAIL TP-51 NOT TO SCALE

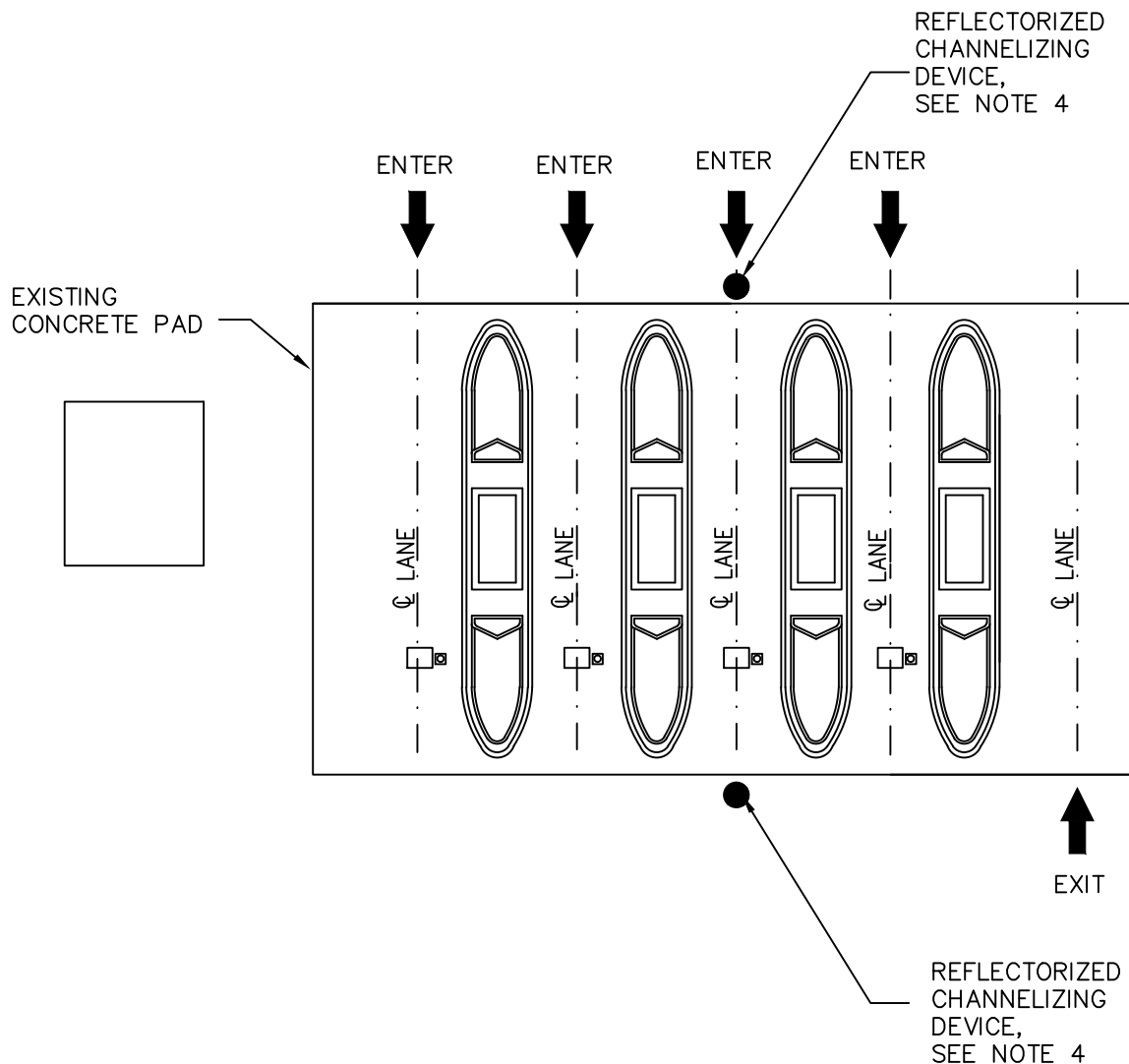
**HNTB**

DATE: 12-22-2017



**MAINE TURNPIKE AUTHORITY**  
**TOLL PLAZA TRAFFIC CONTROL**  
**STATIONARY LANE CLOSURE**  
**BARRIER AND SIDE PLAZAS**  
**LONG TERM (MORE THAN 3 DAYS)**





NOTES:

1. A SINGLE LANE CLOSURE IS REQUIRED FOR CONSTRUCTION AND MAINTENANCE WORK IN THE LANE OR ON AN ISLAND.
2. CANOPY LIGHT ABOVE CLOSED LANE SHALL BE RED.
3. FOR OVERHEAD WORK, USE TP-51 OR TP-52.
4. REFLECTORIZED CHANNELIZING DEVICE SHALL BE CONES OR TUBULAR MARKER MEETING MUTCD SECTION 6F.64 OR 6F.65 FOR NIGHT TIME USE. ALL DEVICES SHALL BE IN GOOD CONDITION WITH NO DAMAGE TO ANY REFLECTORIZED SURFACE.

DETAIL TP-53 NOT TO SCALE

MAINE TURNPIKE AUTHORITY  
TOLL PLAZA TRAFFIC CONTROL

**HNTB**



DATE: 12-22-2017

CLOSURE BARRIER AND SIDE PLAZAS  
SHORT DURATION (<1 HOUR) STATIONARY LANE

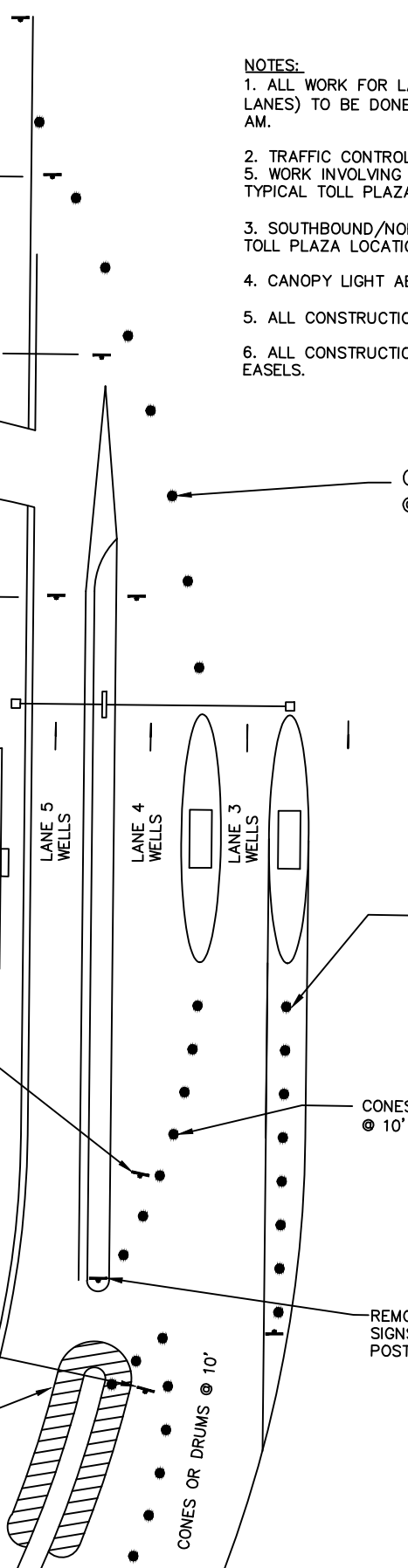




W20-5



6' APRON OF ASPHALT GRINDINGS AROUND RAISED ISLAND (OPTIONAL)



CONES OR DRUMS @ 20'

EXISTING DELINEATOR (REMAIN IN PLACE)

CONES OR DRUMS @ 10' (TYP.)

REMOVE & RESET SIGNS & 4x4 POSTS

CONES OR DRUMS @ 10'

#### NOTES:

1. ALL WORK FOR LANES 4 AND 5 (ALL WIDE LOAD LANES) TO BE DONE BETWEEN HOURS OF 8 PM AND 6 AM.
2. TRAFFIC CONTROL SHOWN ABOVE IS FOR WORK IN LANE 5. WORK INVOLVING LANE 4 CAN BE DONE FOLLOWING TYPICAL TOLL PLAZA LANE CLOSURE DETAILS.
3. SOUTHBOUND/NORTHBOUND ONLY SIGNS DEPENDENT OF TOLL PLAZA LOCATION.
4. CANOPY LIGHT ABOVE CLOSED LANE SHALL BE OFF.
5. ALL CONSTRUCTION SIGNS SHALL BE 4'X4'.
6. ALL CONSTRUCTION SIGNS MAY BE MOUNTED ON EASELS.

DETAIL TP-54 NOT TO SCALE

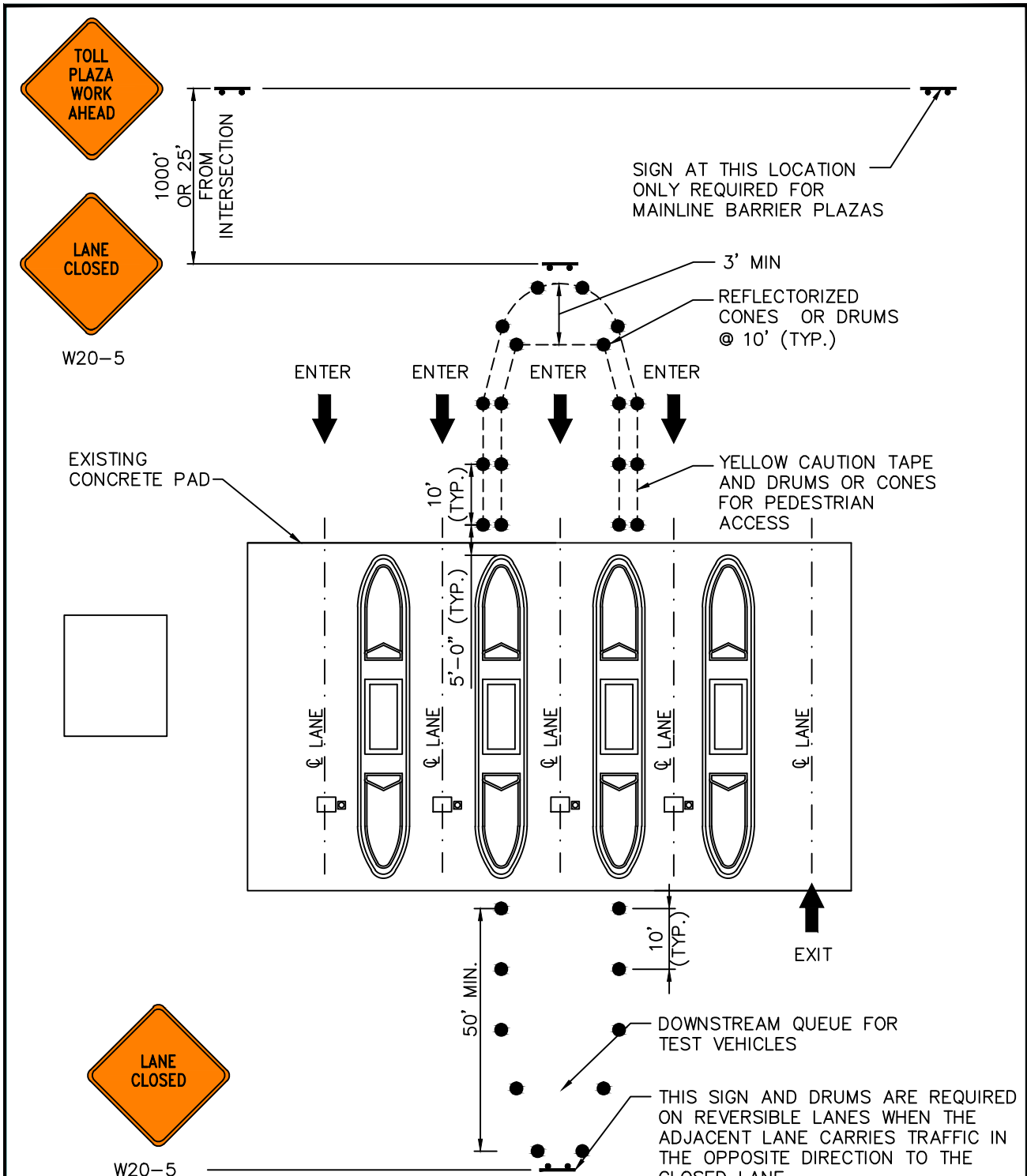
## MAINE TURNPIKE AUTHORITY TOLL PLAZA TRAFFIC CONTROL

STATIONARY LANE CLOSURE  
OF EXPRESS ENTRY LANE  
AT WELLS

# HNTB

DATE: 12-22-2017





**NOTES:**

1. CANOPY LIGHT WILL REMAIN RED THROUGH COMMISSIONING TESTING.
2. WHERE TUNNEL ACCESS IS NOT AVAILABLE, A CHANNELIZED PEDESTRIAN ACCESS WILL BE REQUIRED UPSTREAM OF THE TOLL PLAZA WHERE SITE CONDITIONS ALLOW. CONTACT MTA ENGINEERING FOR LANE SPECIFIC PEDESTRIAN ACCESS PLANS, IF NEEDED.
3. IF REQUIRED, A STANDARD LANE CLOSURE WILL BE INSTALLED FOR WORK IN EXITING EXPRESS LANE TYPES AS SHOWN IN DETAIL TP-50. FOR ENTERING EXPRESS LANES AT WELLS, TRAFFIC CONTROL(IF REQUIRED) IS SHOWN IN DETAIL TP-54.
4. ALL CONSTRUCTION SIGNS SHALL BE 4'X4' AND MAY BE MOUNTED ON EASELS.

**DETAIL TP-55** NOT TO SCALE

**MAINE TURNPIKE AUTHORITY  
TOLL PLAZA TRAFFIC CONTROL**

**LANE COMMISSIONING TESTING**

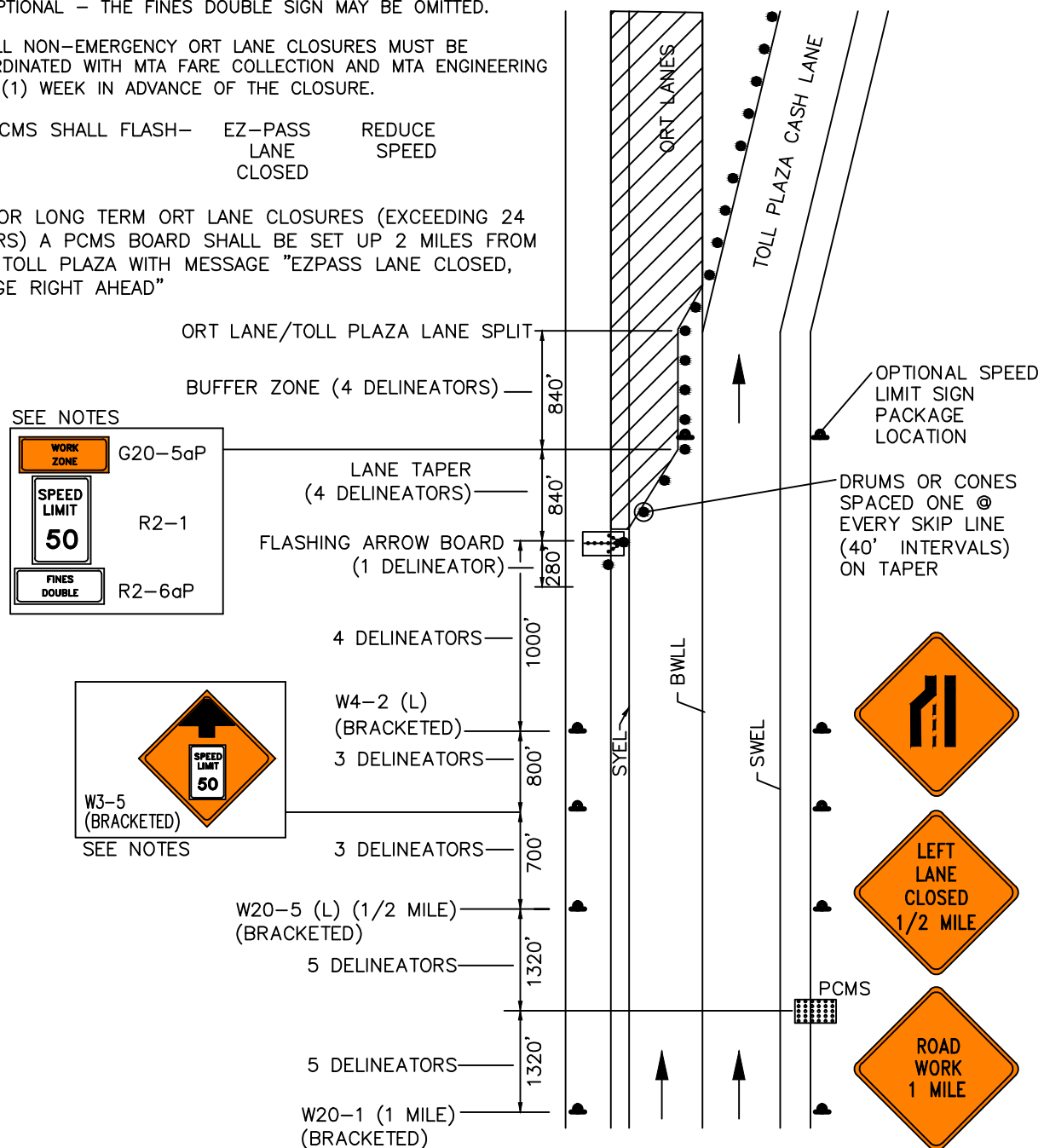
**HNTB**



DATE: 12-22-2017

NOTES:

1. A SINGLE LANE CLOSURE IS REQUIRED FOR ALL CONSTRUCTION OR MAINTENANCE WORK IN THE ORT LANE.
2. USE OF REGULATORY REDUCED SPEEDS SHALL BE USED FOR THE ORT LANE CLOSURE. SPEED LIMIT SIGNS SHALL BE 5' OFF GROUND.
3. ALL CONSTRUCTION SIGNS SHALL BE 4X4 AND MAY BE EASEL OR POST MOUNTED.
4. OPTIONAL - THE SPEED LIMIT SIGN PACKAGE MAY BE POST MOUNTED ON THE RIGHT SHOULDER. POST MOUNTED SIGNS SHALL BE COVERED WHEN NOT IN USE.
5. OPTIONAL - THE WORK ZONE AND FINES DOUBLE SIGN MAY BE MOUNTED ON A SEPARATE EASEL OR POST.
6. OPTIONAL - THE FINES DOUBLE SIGN MAY BE OMITTED.
7. ALL NON-EMERGENCY ORT LANE CLOSURES MUST BE COORDINATED WITH MTA FARE COLLECTION AND MTA ENGINEERING ONE (1) WEEK IN ADVANCE OF THE CLOSURE.
8. PCMS SHALL FLASH- EZ-PASS LANE CLOSED REDUCE SPEED
9. FOR LONG TERM ORT LANE CLOSURES (EXCEEDING 24 HOURS) A PCMS BOARD SHALL BE SET UP 2 MILES FROM THE TOLL PLAZA WITH MESSAGE "EZPASS LANE CLOSED, MERGE RIGHT AHEAD"



DETAIL TP-56A NOT TO SCALE

MAINE TURNPIKE AUTHORITY  
TOLL PLAZA TRAFFIC CONTROL

**HNTB**

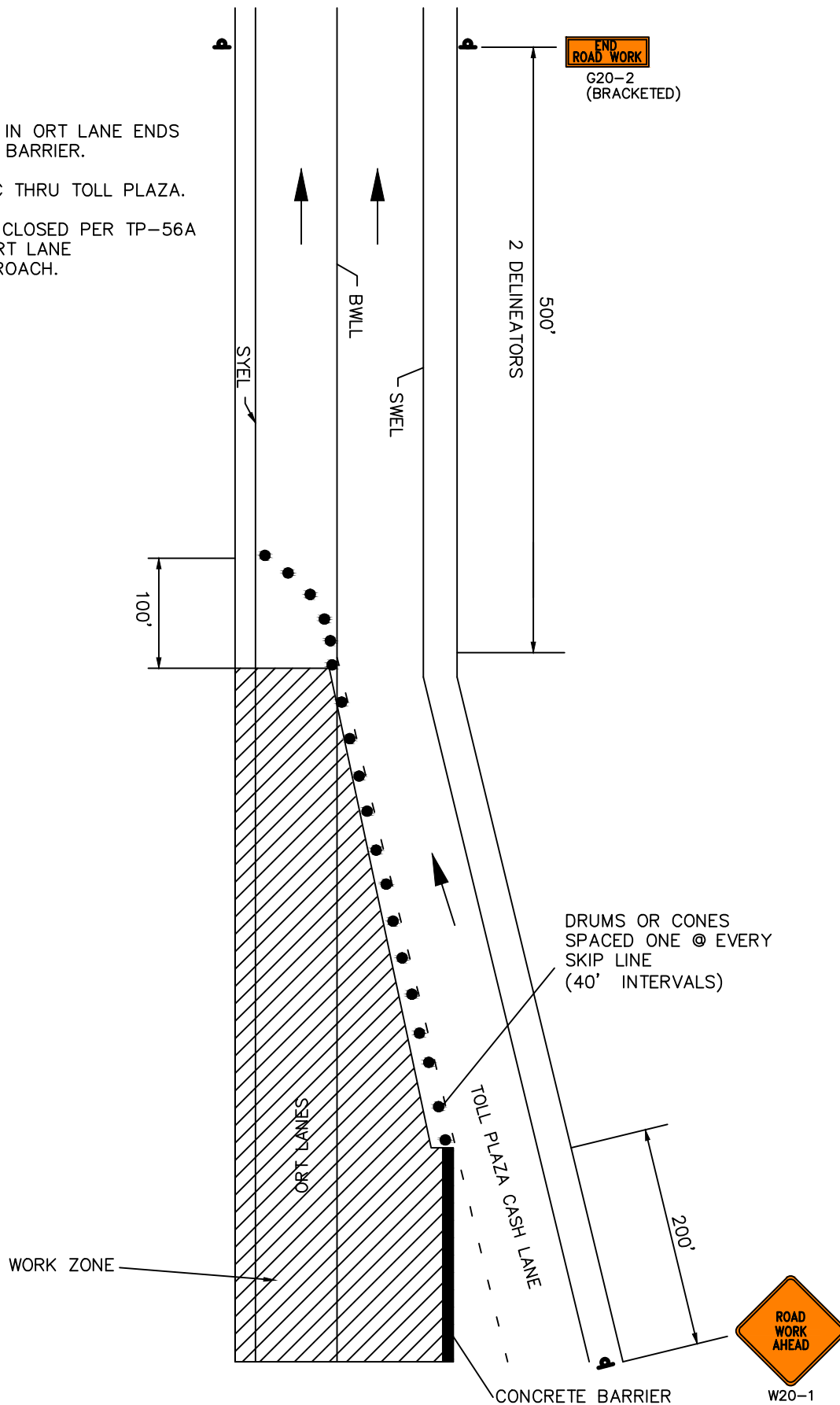


ORT BARRIER  
STATIONARY ORT LANE CLOSURE - APPROACH

DATE: 12-22-2017

NOTES:

1. WORK ZONE IN ORT LANE ENDS PRIOR TO EXIT BARRIER.
2. ALL TRAFFIC THRU TOLL PLAZA.
3. ORT LANES CLOSED PER TP-56A STATIONARY ORT LANE CLOSURE-APPROACH.



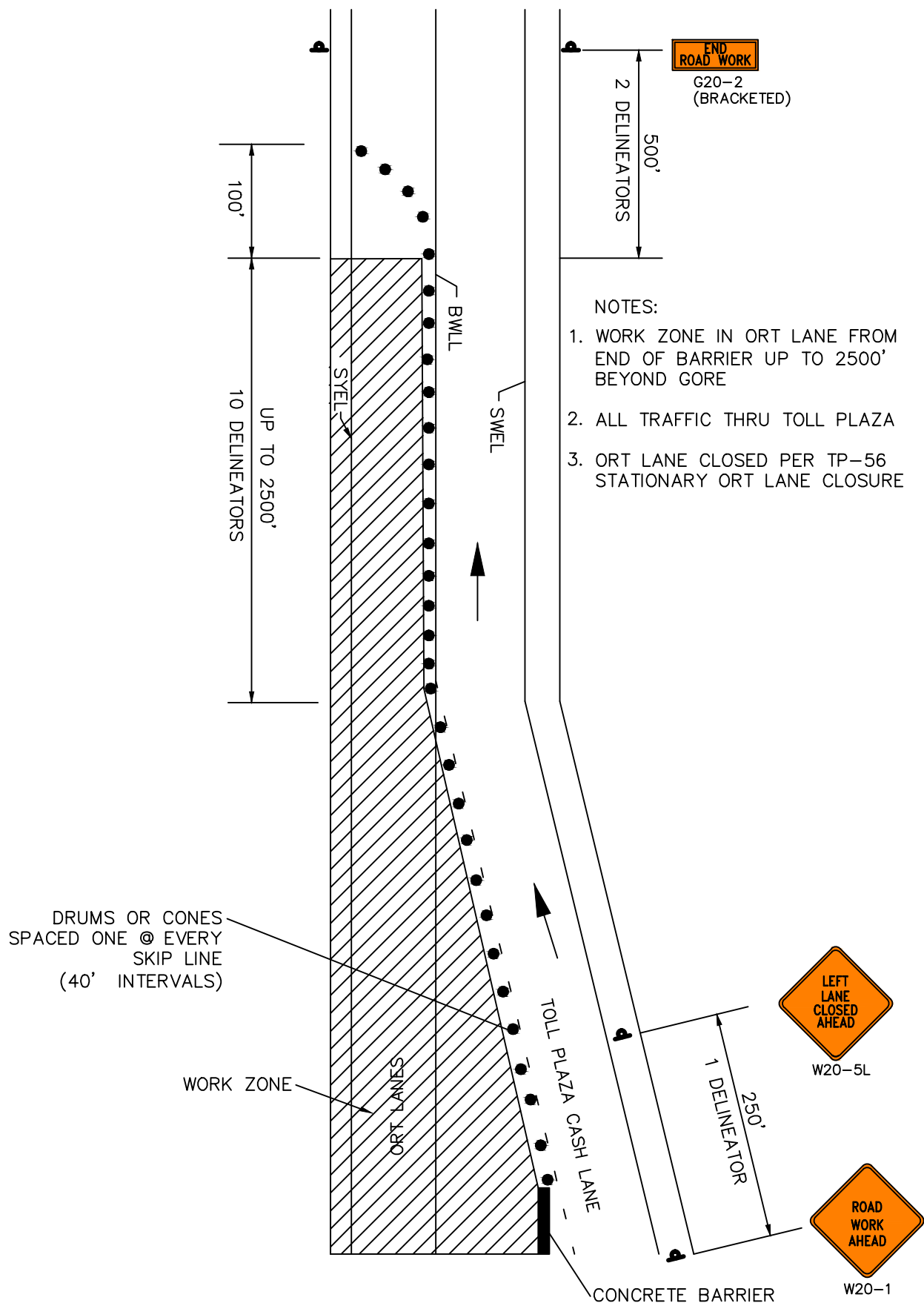
DETAIL TP-56B NOT TO SCALE

**HNTB**

DATE: 12-22-2017



**MAINE TURNPIKE AUTHORITY**  
**TOLL PLAZA TRAFFIC CONTROL**  
 ORT BARRIER  
 STATIONARY ORT LANE CLOSURE - DEPARTURE 1



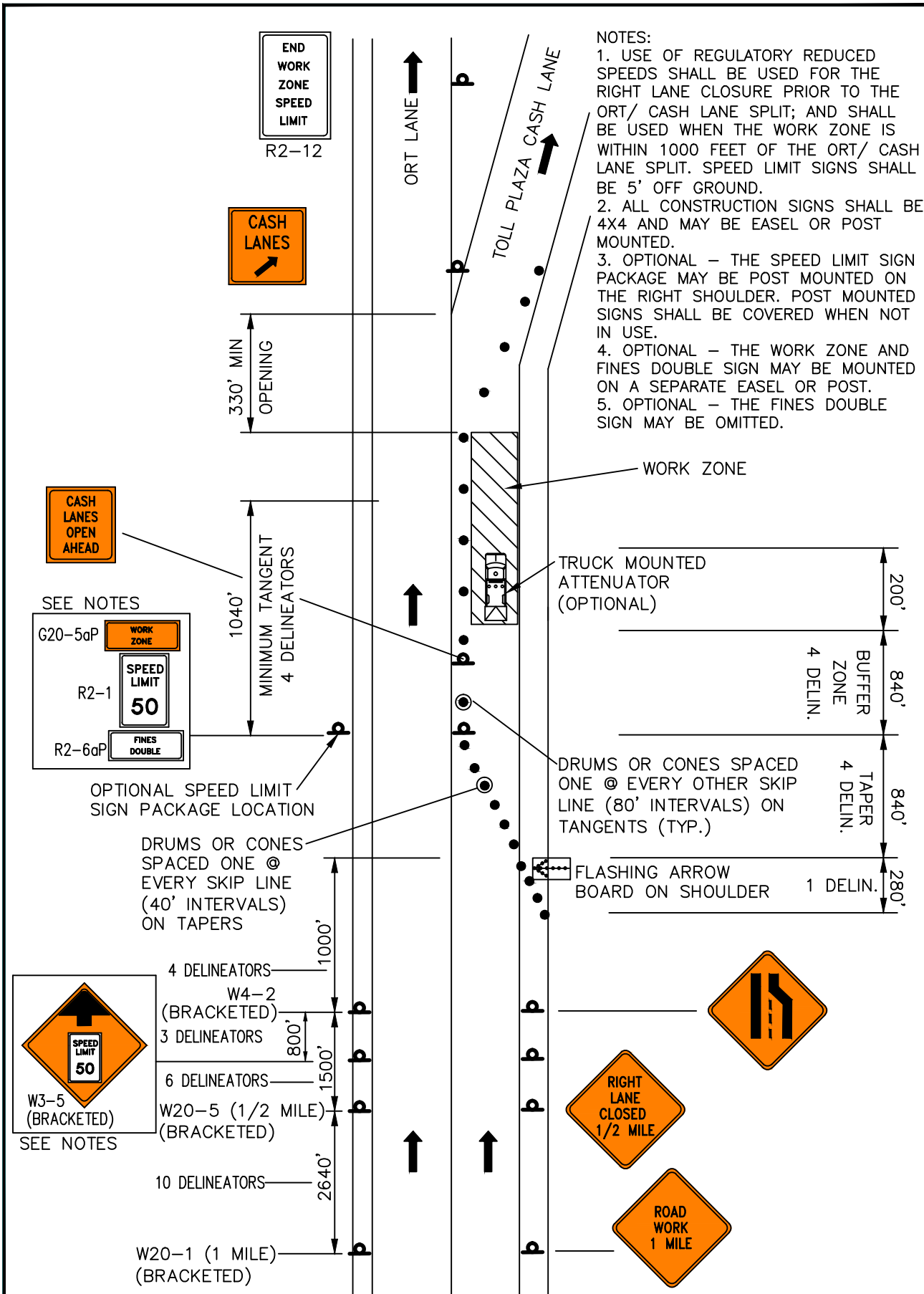
DETAIL TP-56C NOT TO SCALE

**HNTB**

DATE: 12-22-2017



**MAINE TURNPIKE AUTHORITY**  
**TOLL PLAZA TRAFFIC CONTROL**  
 ORT BARRIER  
 STATIONARY ORT LANE CLOSURE - DEPARTURE 2



DETAIL TP-57A NOT TO SCALE

**HNTB**

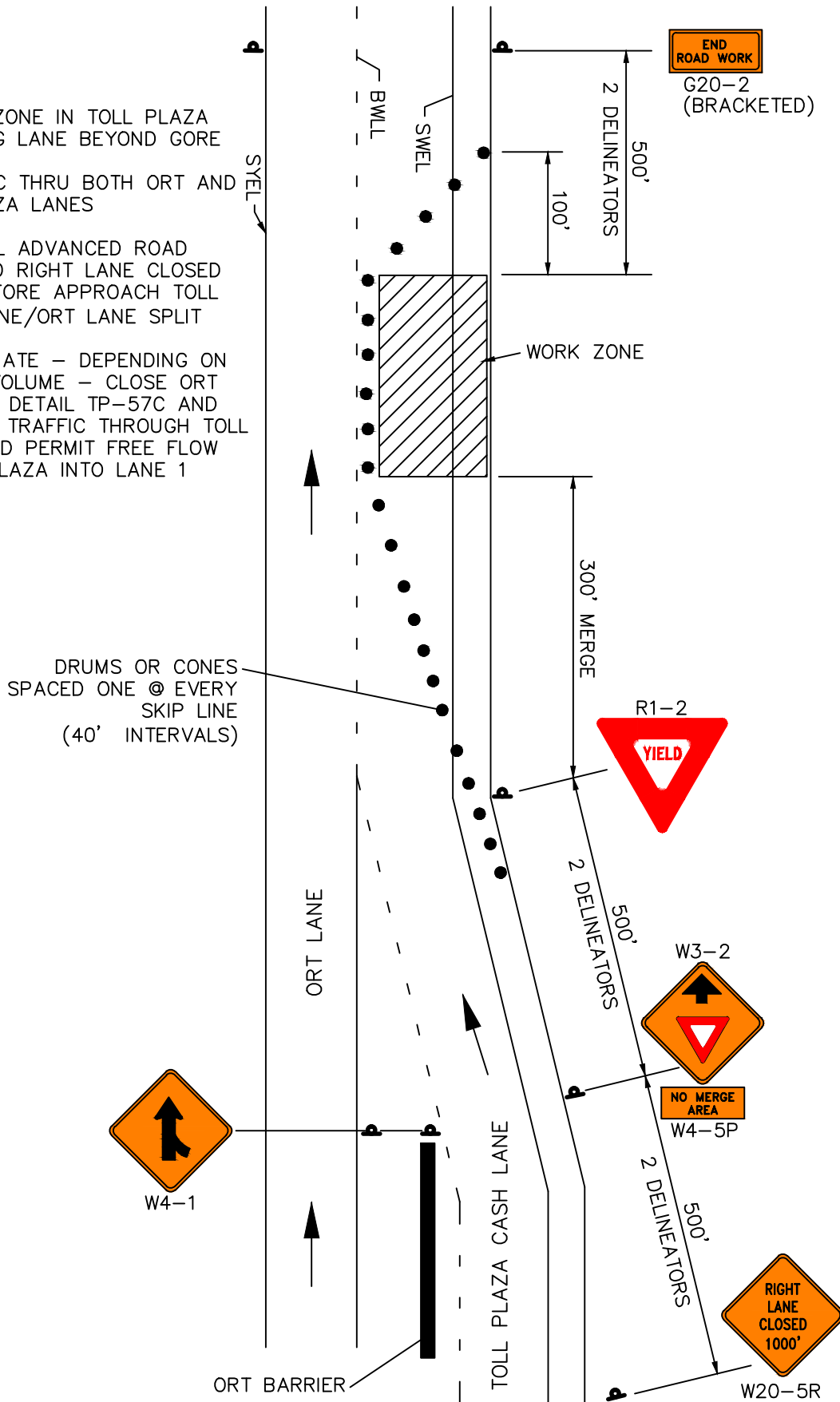
DATE: 12-22-2017



**MAINE TURNPIKE AUTHORITY**  
**TOLL PLAZA TRAFFIC CONTROL**  
 ORT BARRIER  
 STATIONARY RIGHT LANE CLOSURE - APPROACH  
 CASH LANES OPEN

NOTES:

1. WORK ZONE IN TOLL PLAZA DEPARTING LANE BEYOND GORE
2. TRAFFIC THRU BOTH ORT AND TOLL PLAZA LANES
3. INSTALL ADVANCED ROAD WORK AND RIGHT LANE CLOSED SIGNS BEFORE APPROACH TOLL PLAZA LANE/ORT LANE SPLIT
4. ALTERNATE — DEPENDING ON TRAFFIC VOLUME — CLOSE ORT LANE PER DETAIL TP-57C AND SEND ALL TRAFFIC THROUGH TOLL PLAZA AND PERMIT FREE FLOW OUT OF PLAZA INTO LANE 1



DETAIL TP-57B NOT TO SCALE

**HNTB**

DATE: 12-22-2017



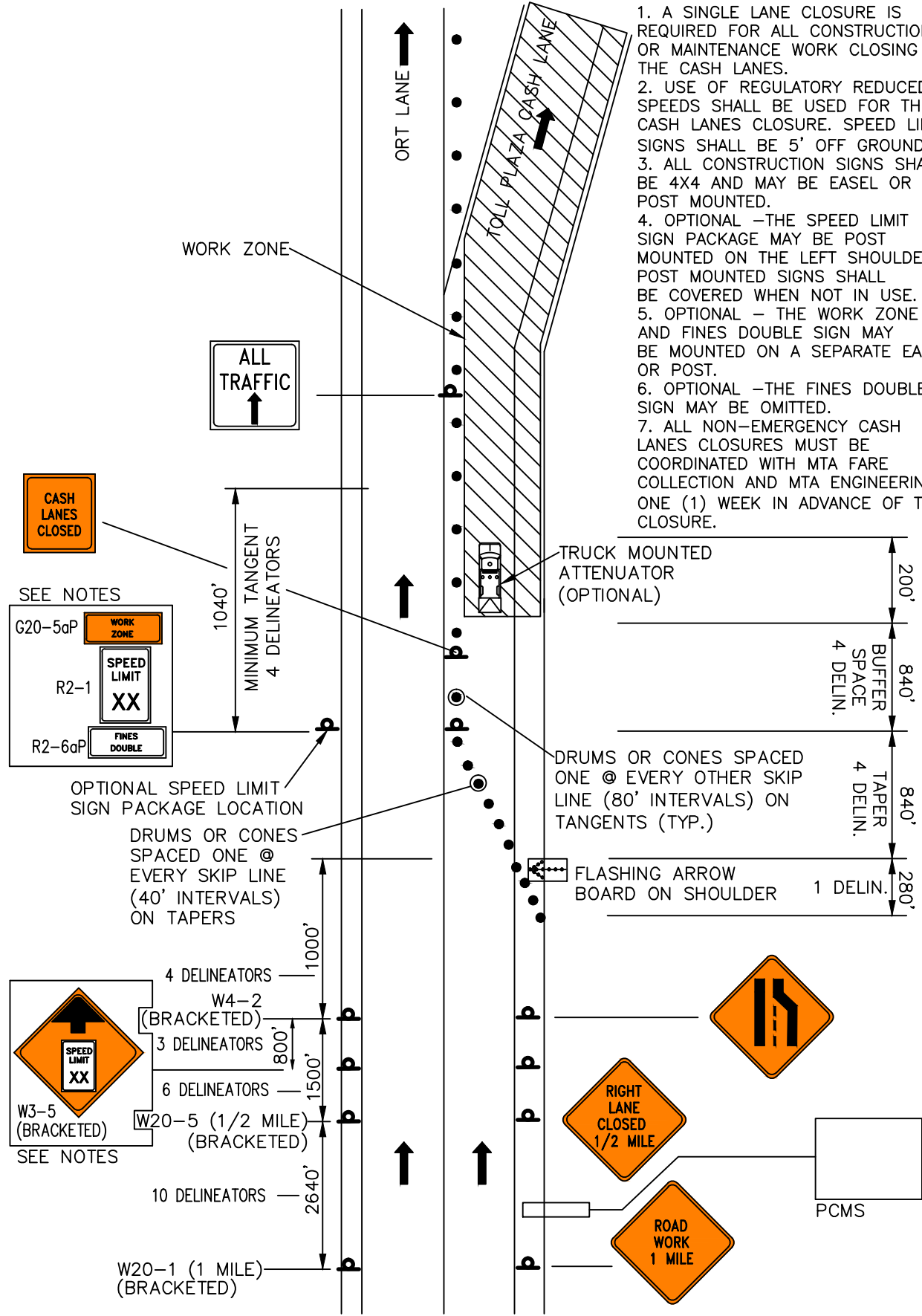
**MAINE TURNPIKE AUTHORITY  
TOLL PLAZA TRAFFIC CONTROL**

**ORT BARRIER  
STATIONARY RIGHT LANE CLOSURE - DEPARTURE  
CASH LANES OPEN**



NOTES:

1. A SINGLE LANE CLOSURE IS REQUIRED FOR ALL CONSTRUCTION OR MAINTENANCE WORK CLOSING THE CASH LANES.
2. USE OF REGULATORY REDUCED SPEEDS SHALL BE USED FOR THE CASH LANES CLOSURE. SPEED LIMIT SIGNS SHALL BE 5' OFF GROUND.
3. ALL CONSTRUCTION SIGNS SHALL BE 4X4 AND MAY BE EASEL OR POST MOUNTED.
4. OPTIONAL -THE SPEED LIMIT SIGN PACKAGE MAY BE POST MOUNTED ON THE LEFT SHOULDER. POST MOUNTED SIGNS SHALL BE COVERED WHEN NOT IN USE.
5. OPTIONAL - THE WORK ZONE AND FINES DOUBLE SIGN MAY BE MOUNTED ON A SEPARATE EASEL OR POST.
6. OPTIONAL -THE FINES DOUBLE SIGN MAY BE OMITTED.
7. ALL NON-EMERGENCY CASH LANES CLOSURES MUST BE COORDINATED WITH MTA FARE COLLECTION AND MTA ENGINEERING ONE (1) WEEK IN ADVANCE OF THE CLOSURE.



DETAIL TP-57C NOT TO SCALE

**HNTB**

DATE: 12-22-2017



**MAINE TURNPIKE AUTHORITY**  
**TOLL PLAZA TRAFFIC CONTROL**  
**ORT BARRIER**  
**STATIONARY RIGHT LANE CLOSURE - APPROACH**  
**CASH LANES CLOSED**

NOTES:

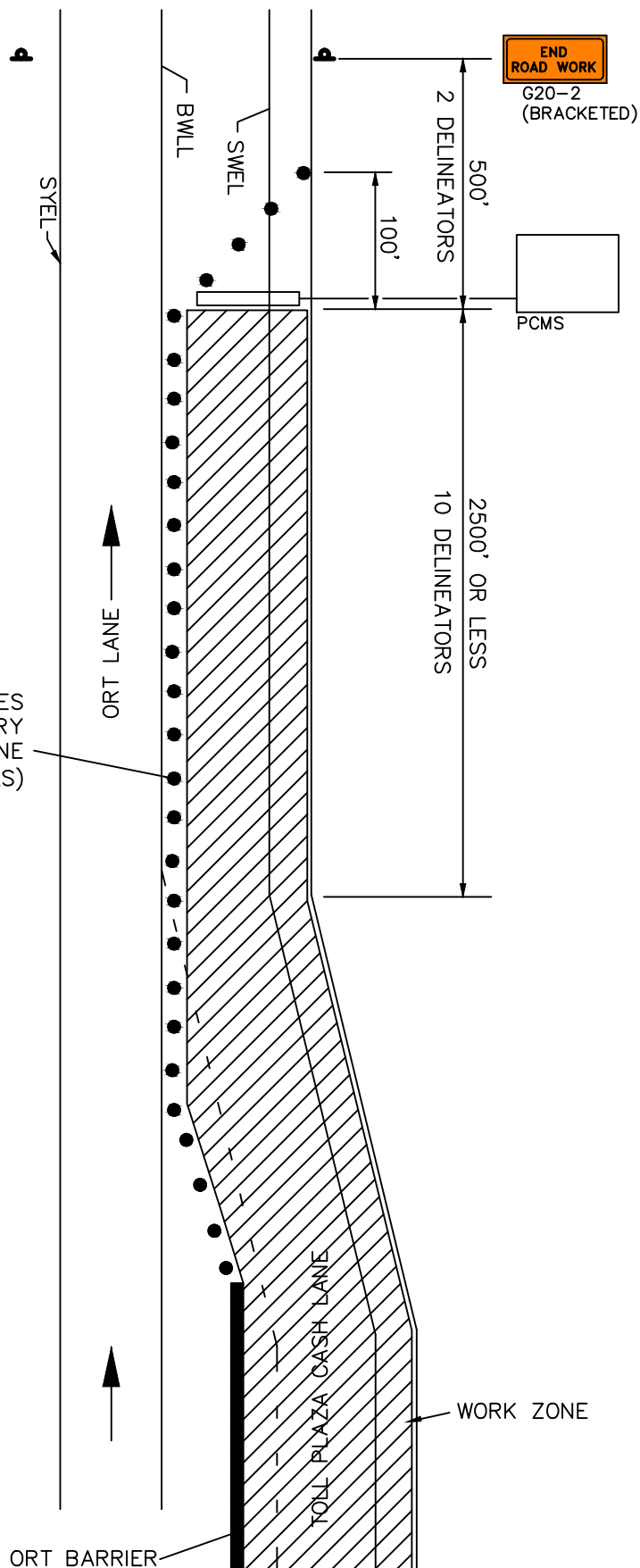
1. WORK ZONE IN TOLL PLAZA LANE  
END OF BARRIER UP TO 2500'  
BEYOND GORE.

2. ALL TRAFFIC THRU ORT LANE.

3. TOLL PLAZA LANES CLOSED PER  
TP-57C STATIONARY RIGHT LANE  
CLOSURE-APPROACH CASH LANES  
CLOSED.

4. ALL NON-EMERGENCY CASH LANES  
CLOSURES MUST BE COORDINATED  
WITH MTA FARE COLLECTION AND MTA  
ENGINEERING ONE (1) WEEK IN  
ADVANCE OF THE CLOSURE.

DRUMS OR CONES  
SPACED ONE @ EVERY  
SKIP LINE  
(40' INTERVALS)



DETAIL TP-57D NOT TO SCALE

**HNTB**

DATE: 12-22-2017



**MAINE TURNPIKE AUTHORITY**  
**TOLL PLAZA TRAFFIC CONTROL**  
**ORT BARRIER**  
**STATIONARY RIGHT LANE CLOSURE - DEPARTURE**  
**CASH LANES CLOSED**

NOTES:

1. ALL CONSTRUCTION SIGNS SHALL BE 4X4 AND MAY BE EASEL OR POST MOUNTED.

EXISTING CONCRETE BARRIER

ORT LANE

TOLL PLAZA CASH LANE

END  
WORK  
ZONE  
SPEED  
LIMIT

R2-12

WORK ZONE

DRUMS OR CONES  
SPACED AT 30'

TRUCK MOUNTED  
ATTENUATOR  
(OPTIONAL)

W21-5a  
2 DELINEATORS  
500'  
W21-5b (1000 FT)  
6 DELINEATORS  
1500'  
W20-1 (AHEAD)  
(BRACKETED)



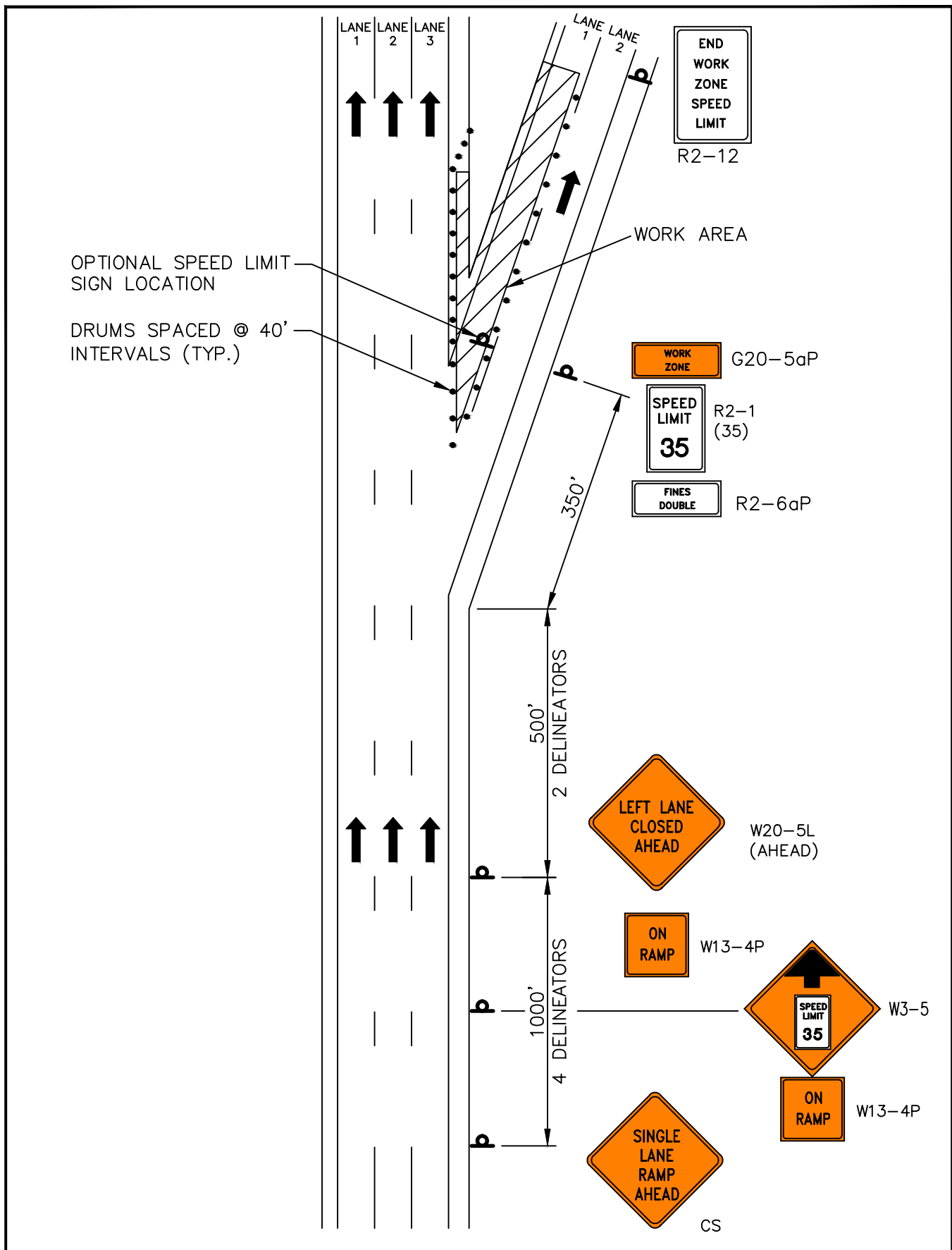
DETAIL TP-58 NOT TO SCALE

**HNTB**

DATE:12-22-2017



**MAINE TURNPIKE AUTHORITY**  
**TOLL PLAZA TRAFFIC CONTROL**  
**ORT BARRIER**  
**CASH LANES SHOULDER CLOSURE**



APPENDIX A NOT TO SCALE  
EXIT 2 SB OFF  
DETAIL 1

**HNTB**

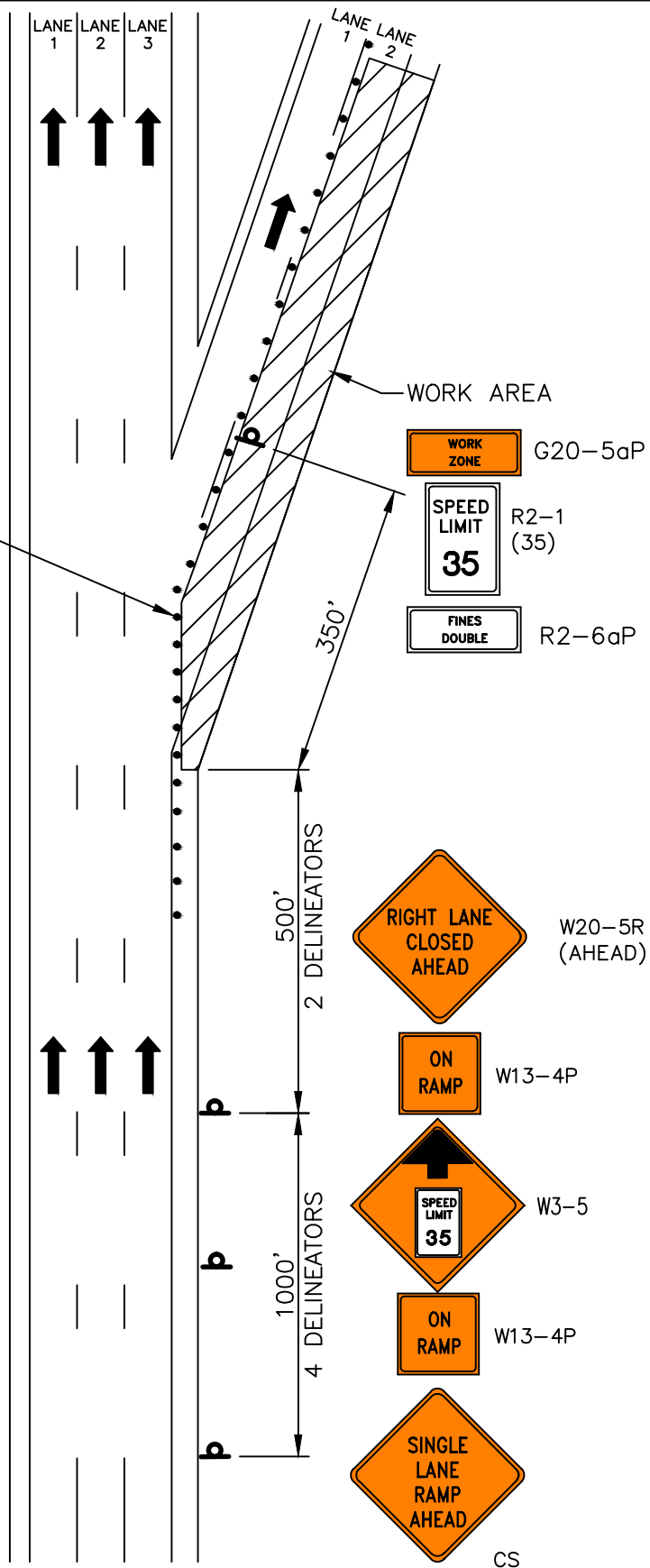


DATE: 12-22-2017

## MAINE TURNPIKE AUTHORITY TRAFFIC CONTROL DETAIL

LEFT LANE CLOSURE AT AN EXIT RAMP

DRUMS SPACED @ 40'  
INTERVALS (TYP.)



APPENDIX A NOT TO SCALE  
EXIT 2 SB OFF  
DETAIL 2

**HNTB**

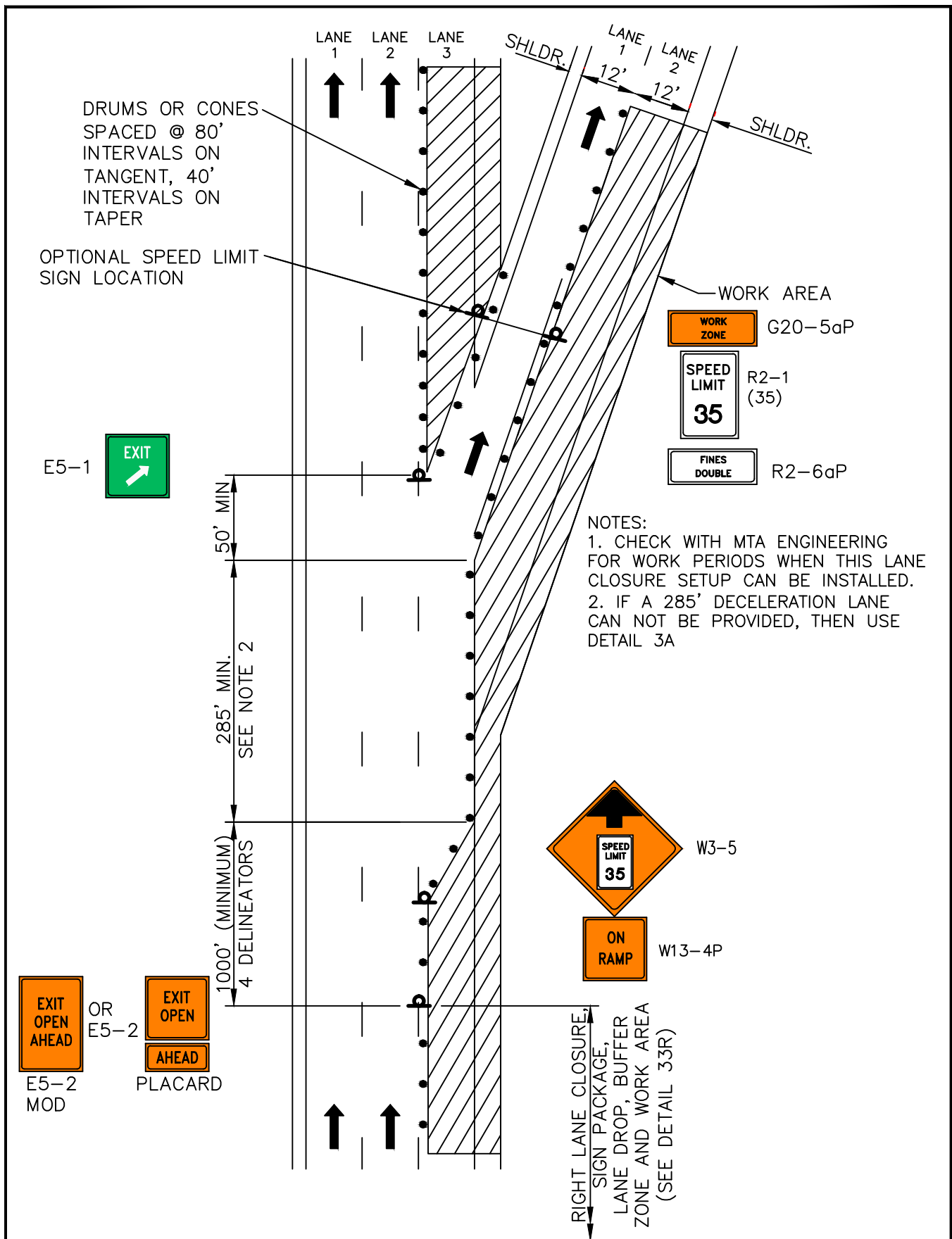
DATE: 12-22-2017



**MAINE TURNPIKE AUTHORITY  
TRAFFIC CONTROL DETAIL**

**RIGHT LANE CLOSURE AT AN EXIT RAMP**





APPENDIX A NOT TO SCALE  
EXIT 2 SB OFF  
DETAIL 3B

**HNTB**

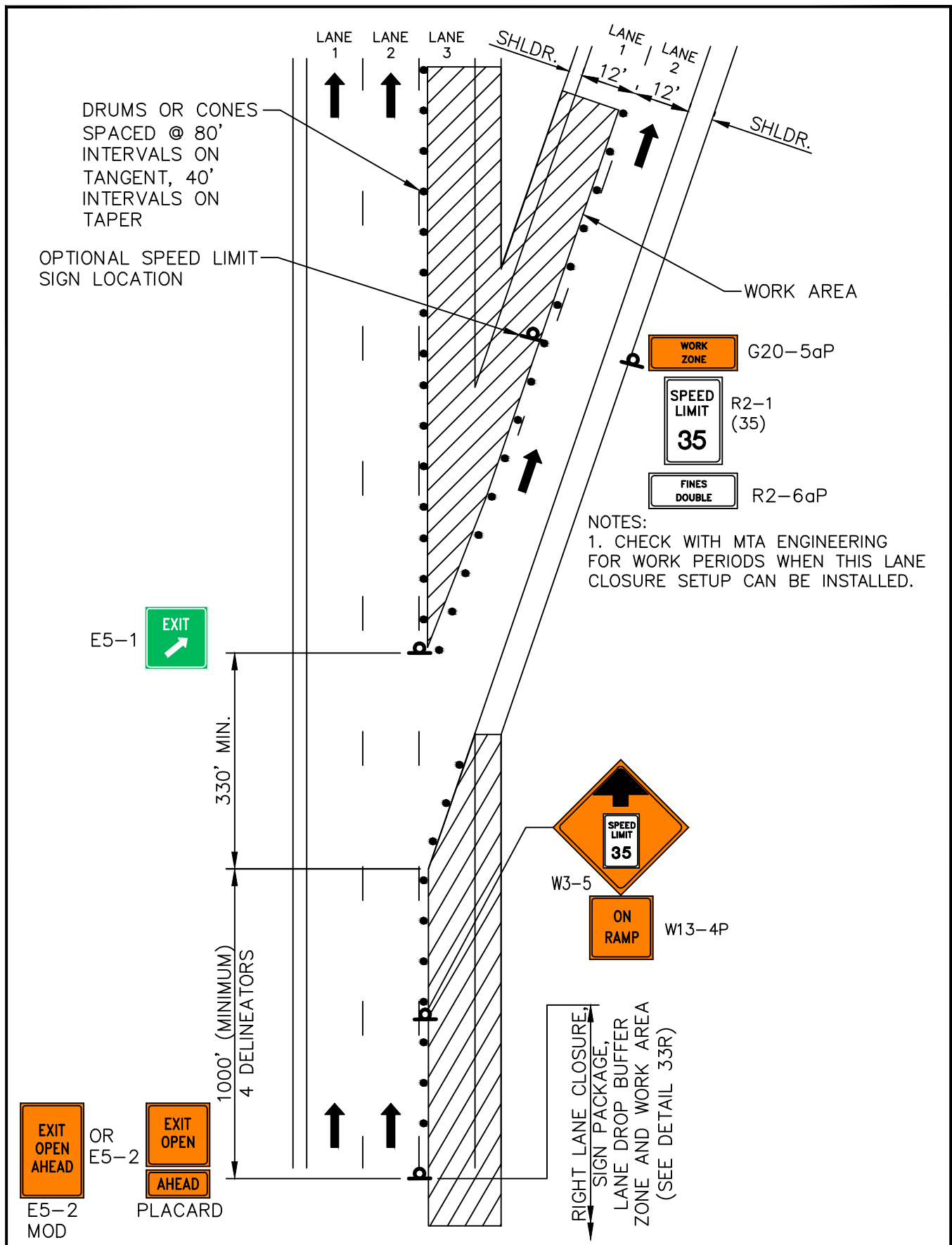
DATE: 12-22-2017



# MAINE TURNPIKE AUTHORITY TRAFFIC CONTROL DETAIL

EXIT 2 SB LANE 3 CLOSED  
TRAFFIC IN LANES 1 AND 2. SB EXITING RAMP OPEN -  
LANE 1 WITH DECELERATION LANE





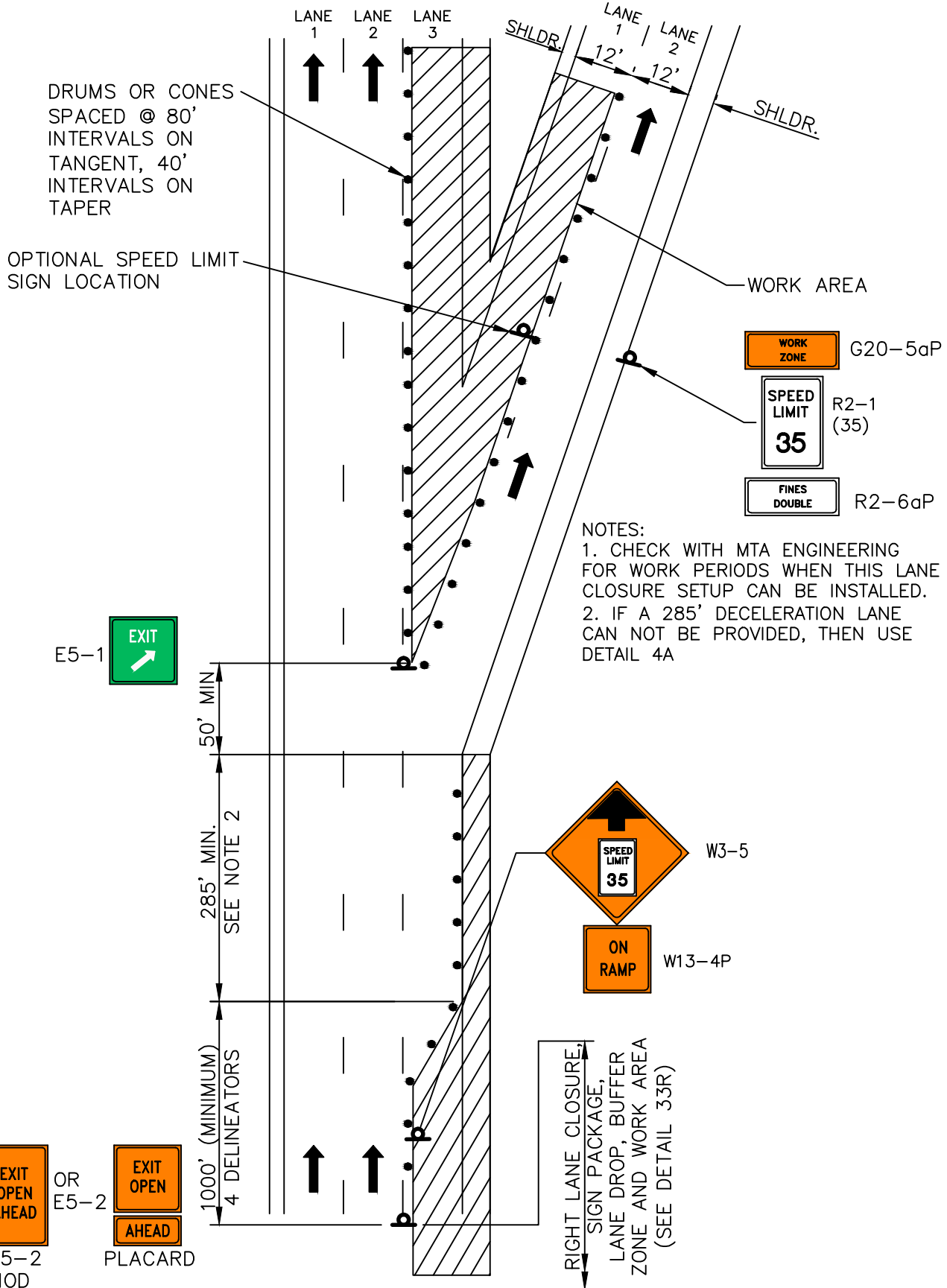
APPENDIX A NOT TO SCALE  
EXIT 2 SB OFF  
DETAIL 4A

**HNTB**

DATE: 12-22-2017



**MAINE TURNPIKE AUTHORITY**  
**TRAFFIC CONTROL DETAIL**  
EXIT 2 SB LANE 3 CLOSED  
TRAFFIC IN LANES 1 AND 2  
SOUTHBOUND EXITING RAMP OPEN - LANE 2



APPENDIX A NOT TO SCALE  
EXIT 2 SB OFF  
DETAIL 4B

**HNTB**

DATE: 12-22-2017



**MAINE TURNPIKE AUTHORITY  
TRAFFIC CONTROL DETAIL**

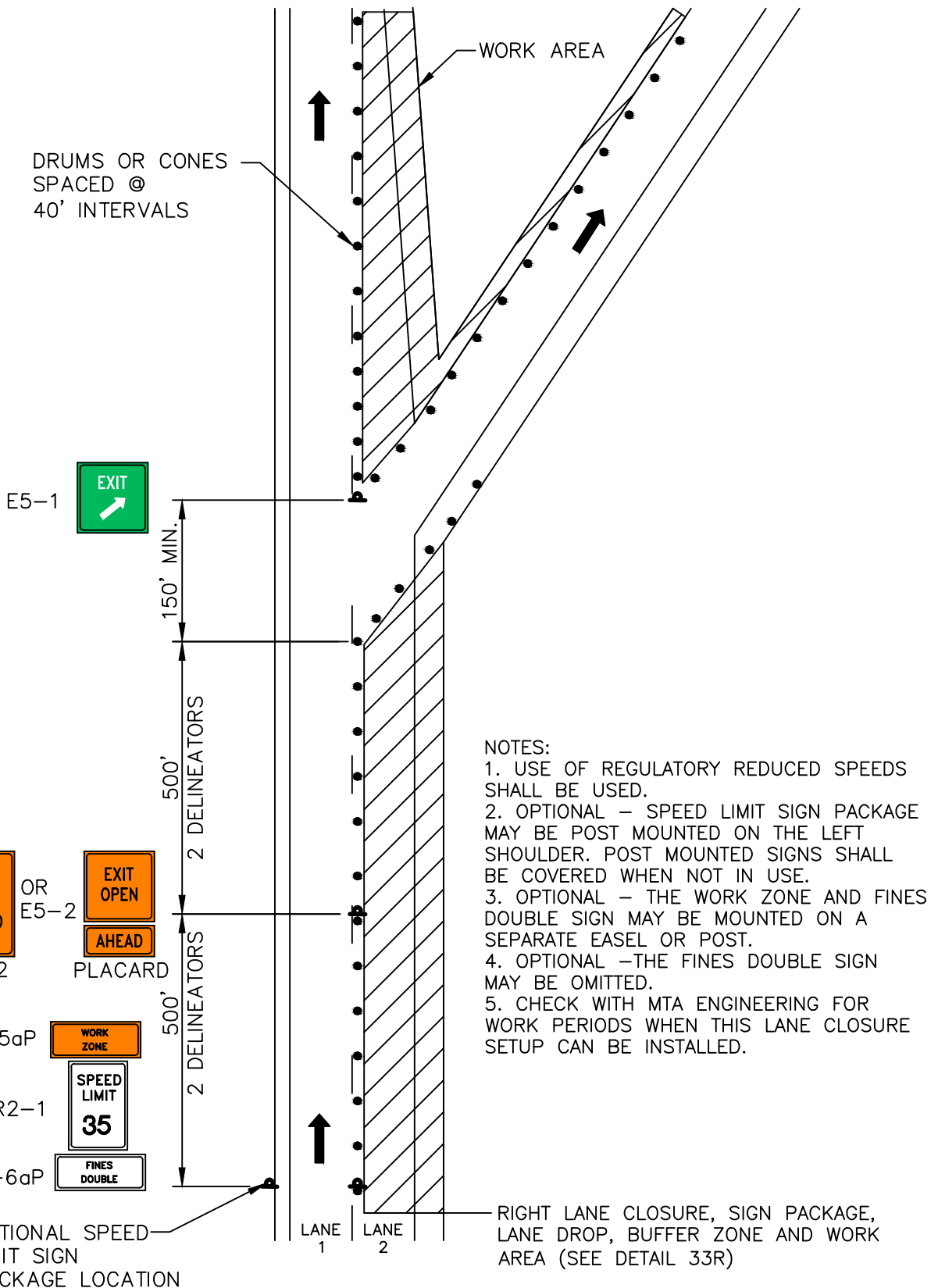
EXIT 2 SB LANE 3 CLOSED  
TRAFFIC IN LANES 1 AND 2. SB EXITING RAMP OPEN -  
LANE 2 WITH DECELERATION LANE



60







APPENDIX A NOT TO SCALE

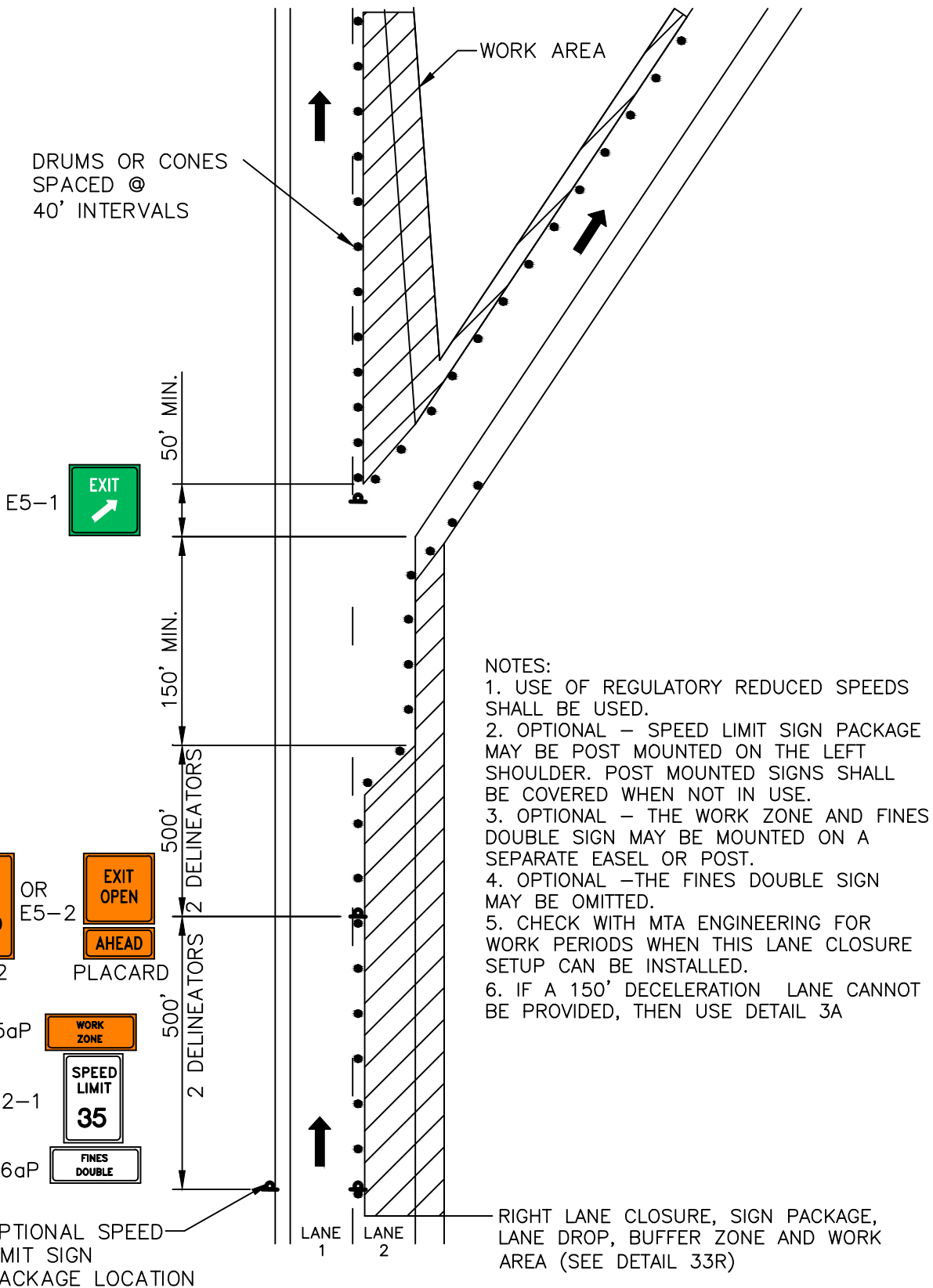
EXIT 2 SB  
COLLECTOR/  
DISTRIBUTOR  
DETAIL 3A

**HNTB**

DATE: 12-22-2017



**MAINE TURNPIKE AUTHORITY**  
**TRAFFIC CONTROL DETAIL**  
WORK ZONE IN LANE 2 OF  
COLLECTOR/DISTRIBUTOR RAMP -  
EXIT RAMP TO ROUTE 236 OPEN



APPENDIX A NOT TO SCALE

EXIT 2 SB  
COLLECTOR/  
DISTRIBUTOR  
DETAIL 3B

**HNTB**

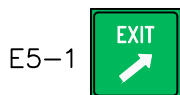
DATE: 12-22-2017



**MAINE TURNPIKE AUTHORITY**  
**TRAFFIC CONTROL DETAIL**  
WORK ZONE IN LANE 2 OF  
COLLECTOR/DISTRIBUTOR RAMP - EXIT RAMP  
TO ROUTE 236 OPEN WITH DECELERATION LANE



DRUMS OR CONES  
SPACED @  
40' INTERVALS



E5-1

WORK AREA

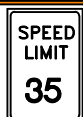
NOTES:

1. USE OF REGULATORY REDUCED SPEEDS SHALL BE USED.
2. OPTIONAL - SPEED LIMIT SIGN PACKAGE MAY BE POST MOUNTED ON THE RIGHT SHOULDER. POST MOUNTED SIGNS SHALL BE COVERED WHEN NOT IN USE.
3. OPTIONAL - THE WORK ZONE AND FINES DOUBLE SIGN MAY BE MOUNTED ON A SEPARATE EASEL OR POST.
4. OPTIONAL - THE FINES DOUBLE SIGN MAY BE OMITTED.
5. CHECK WITH MTA ENGINEERING FOR PERIODS WHEN THIS LANE CLOSURE SETUP CAN BE INSTALLED.

G20-5aP



R2-1



R2-6aP



LANE 1

LANE 2

OPTIONAL SPEED  
LIMIT SIGN  
PACKAGE LOCATION

APPENDIX A NOT TO SCALE

EXIT 2 SB

COLLECTOR/  
DISTRIBUTOR  
DETAIL 4

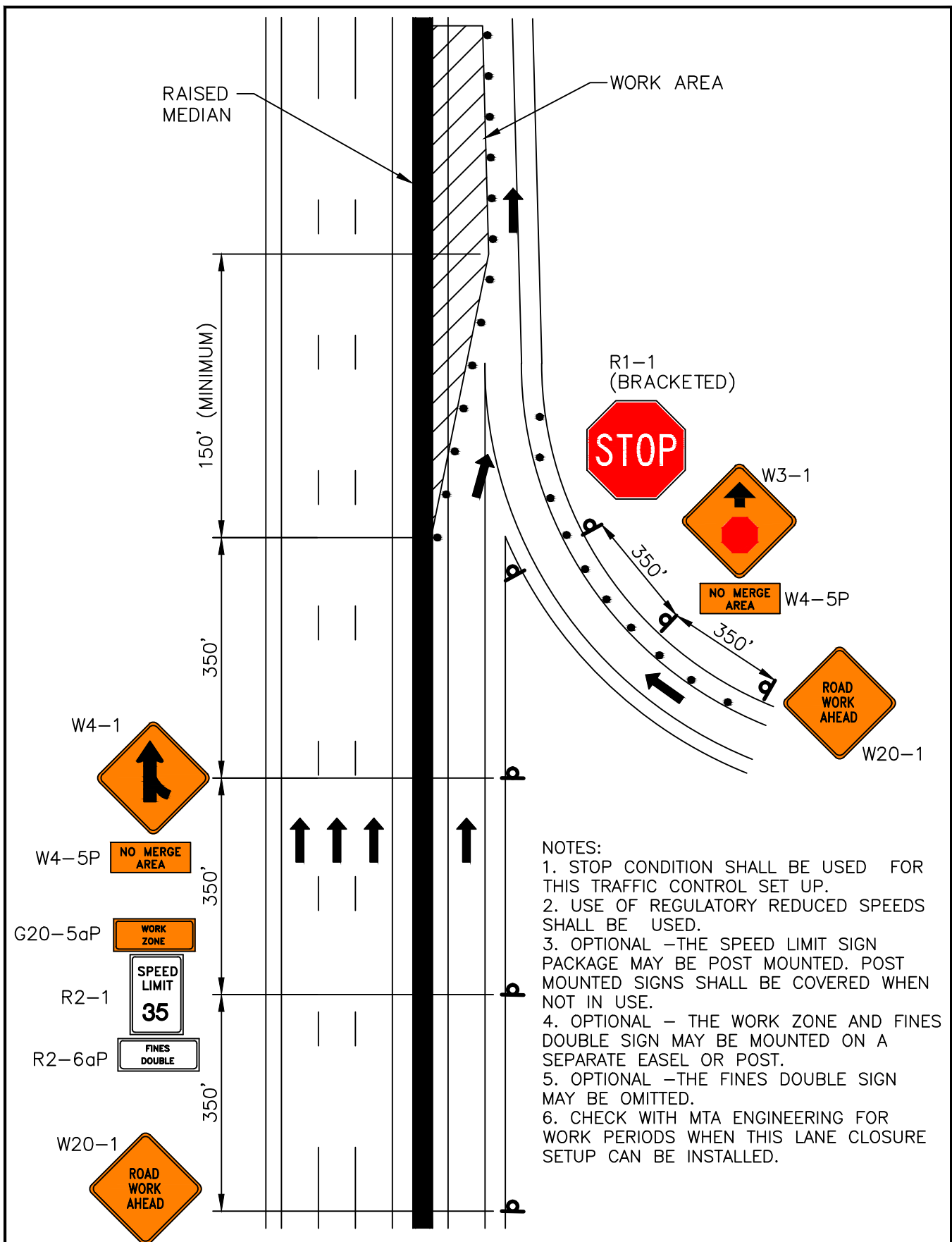
**HNTB**

DATE: 12-22-2017



**MAINE TURNPIKE AUTHORITY  
TRAFFIC CONTROL DETAIL**

**WORK ZONE IN LANE 1 OF  
COLLECTOR/DISTRIBUTOR RAMP -  
EXIT RAMP TO ROUTE 236 OPEN**



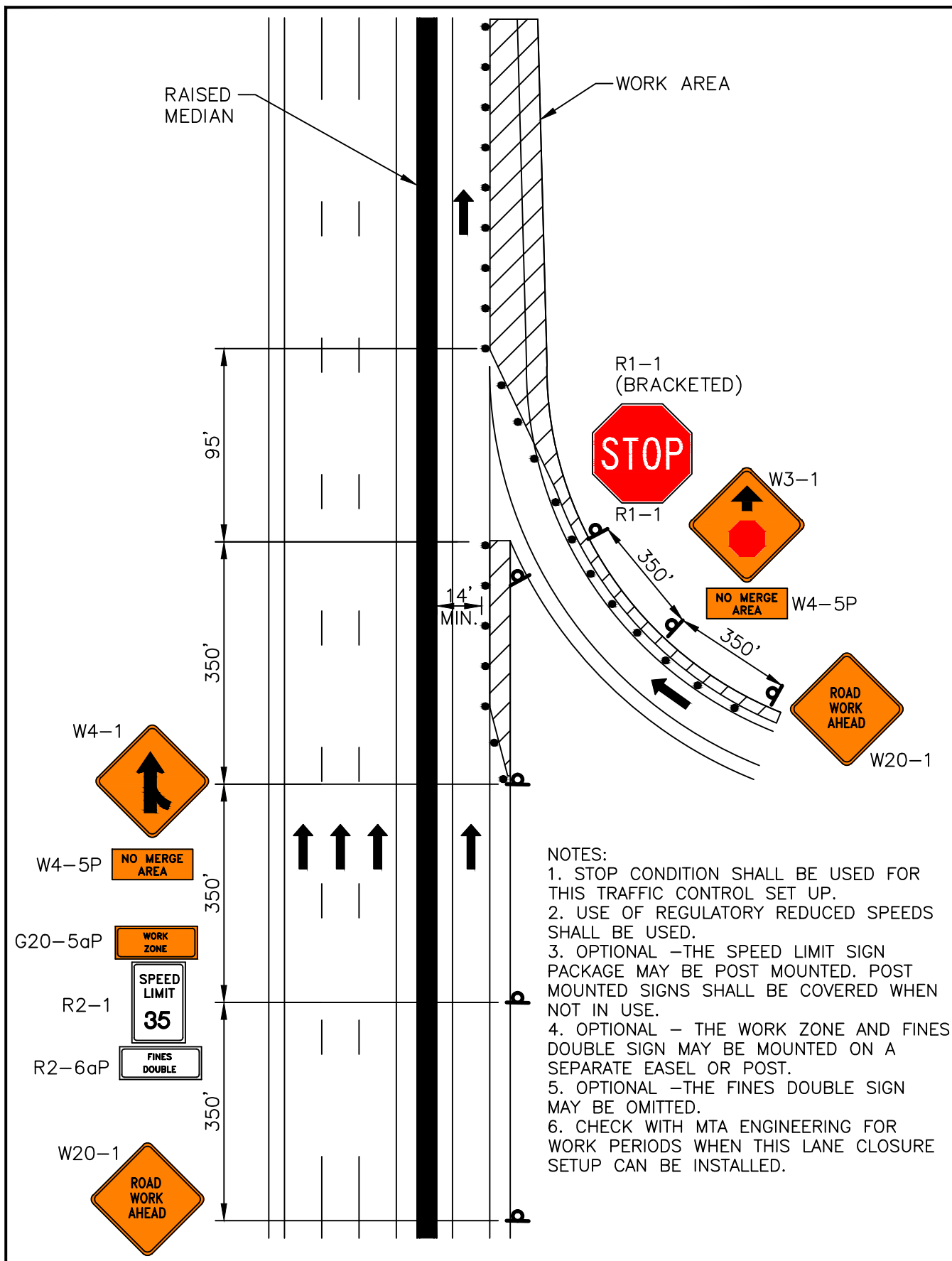
APPENDIX A NOT TO SCALE  
 EXIT 2 SB  
 COLLECTOR/  
 DISTRIBUTOR  
 DETAIL 5

**HNTB**

DATE: 12-22-2017



**MAINE TURNPIKE AUTHORITY**  
**TRAFFIC CONTROL DETAIL**  
 WORK ZONE ON LEFT  
 SHOULDER - RAMP ON FROM  
 ROUTE 236 E



APPENDIX A NOT TO SCALE

EXIT 2 SB  
COLLECTOR/  
DISTRIBUTOR  
DETAIL 6

**HNTB**

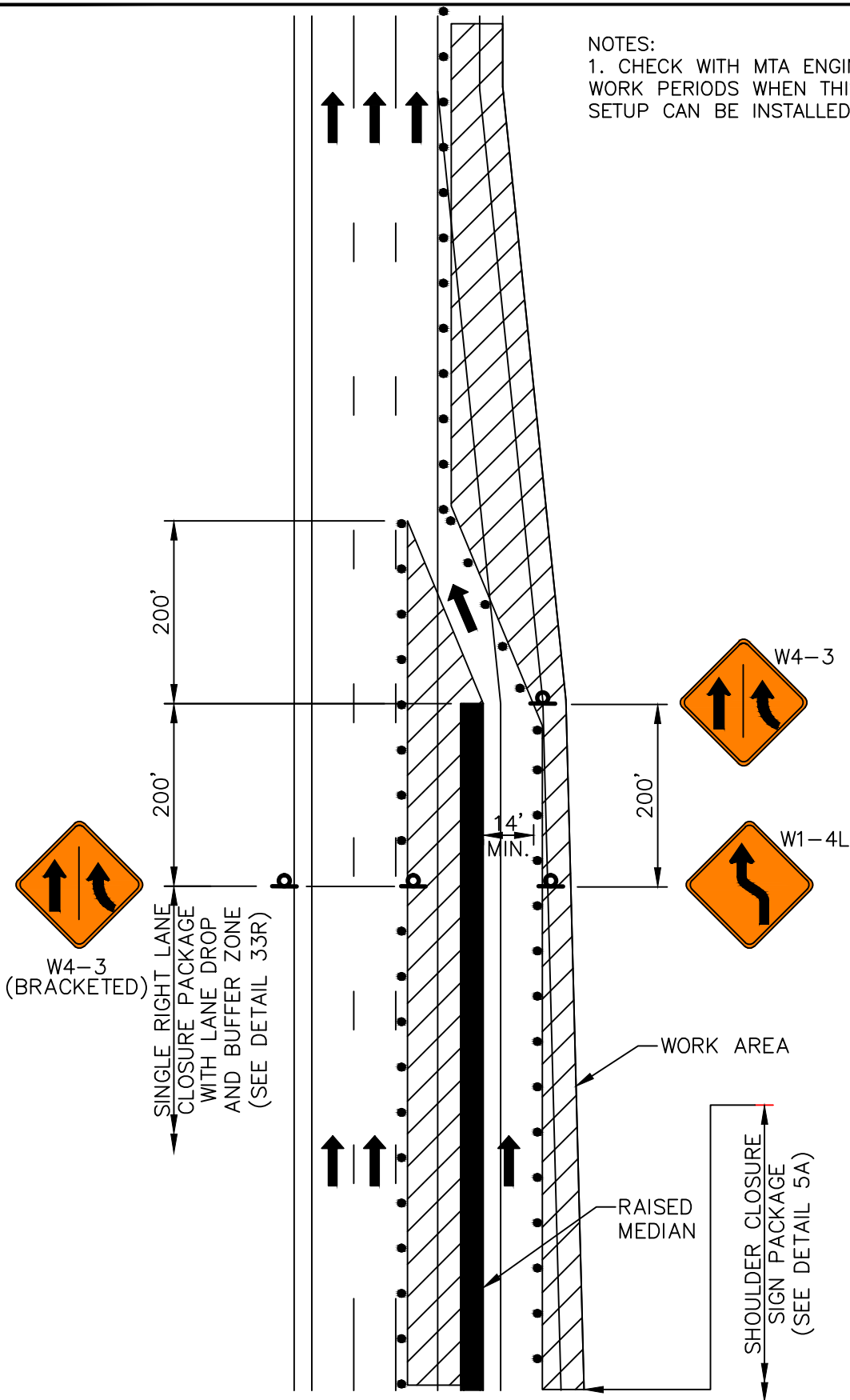
DATE: 12-22-2017



**MAINE TURNPIKE AUTHORITY  
TRAFFIC CONTROL DETAIL**

**WORK ZONE ON RIGHT  
SHOULDER - RAMP ON FROM  
ROUTE 236 E**

NOTES:  
1. CHECK WITH MTA ENGINEERING FOR  
WORK PERIODS WHEN THIS LANE CLOSURE  
SETUP CAN BE INSTALLED.



APPENDIX A NOT TO SCALE

EXIT 2 SB  
COLLECTOR/  
DISTRIBUTOR  
DETAIL 7

**HNTB**

DATE: 12-22-2017

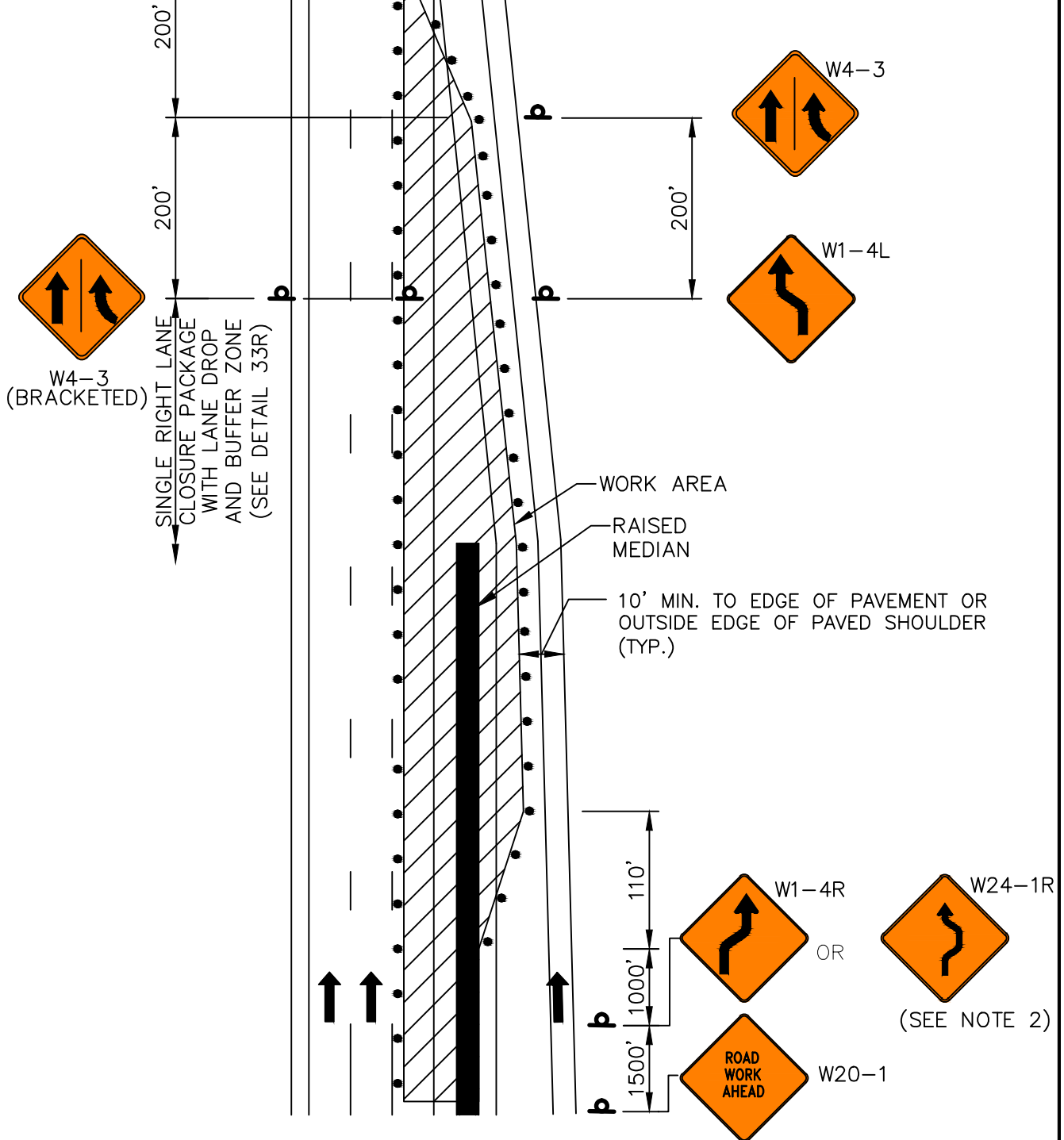


**MAINE TURNPIKE AUTHORITY  
TRAFFIC CONTROL DETAIL**

**WORK ZONE ON RIGHT SHOULDER  
EXIT 2 SB ON RAMP**

NOTES:

1. CHECK WITH MTA ENGINEERING FOR WORK PERIODS WHEN THIS LANE CLOSURE SETUP CAN BE INSTALLED.
2. IF THE TANGENT DISTANCE ALONG THE SHIFTED ROADWAY IS LESS THAN 600 FEET, A DOUBLE REVERSE CURVE SIGN (W24-1) SHOULD BE INSTALLED INSTEAD OF THE W1-4R AND THE W1-4L SHOULD BE OMITTED.



APPENDIX A NOT TO SCALE

EXIT 2 SB  
COLLECTOR/  
DISTRIBUTOR  
DETAIL 8

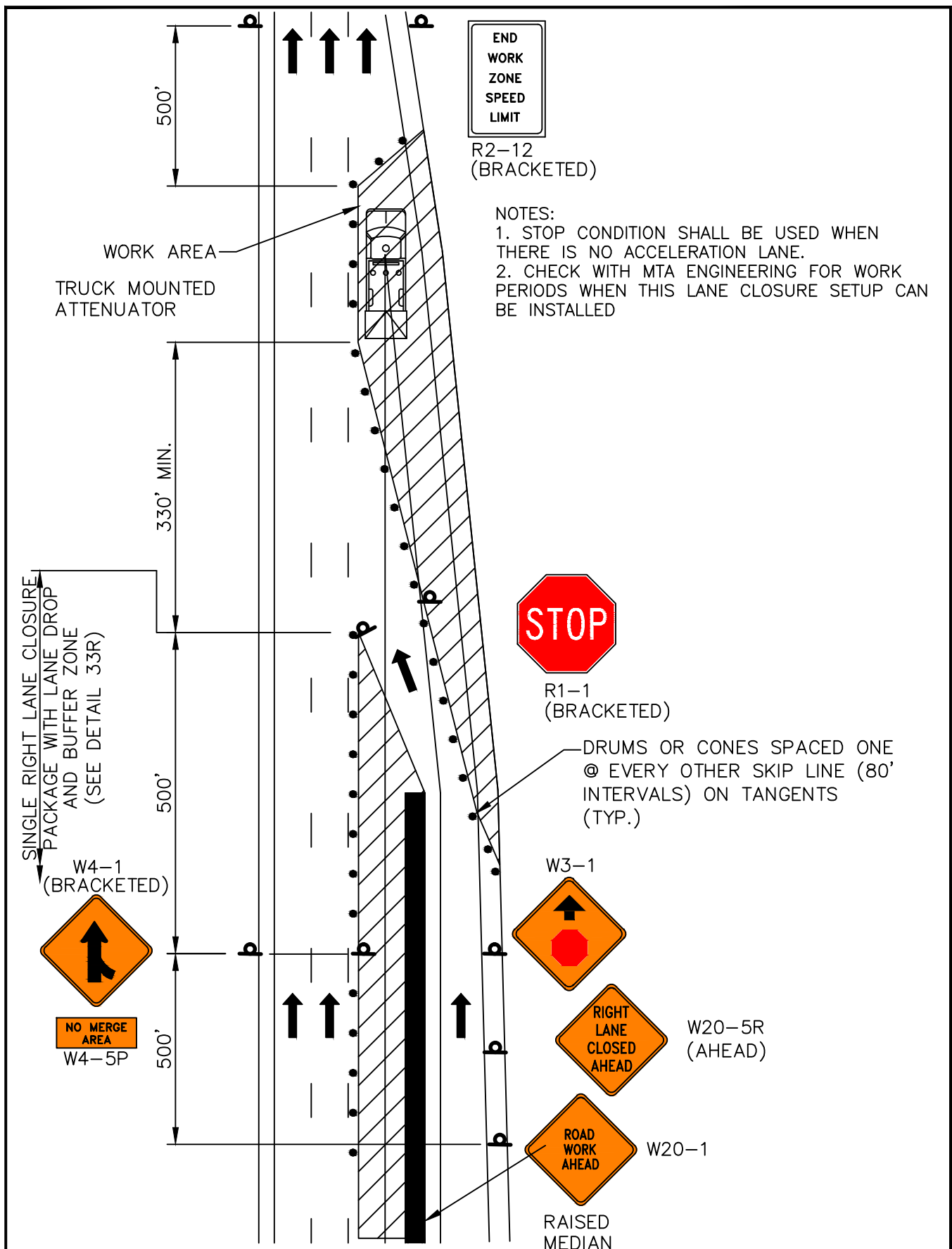
**HNTB**

DATE: 12-22-2017



**MAINE TURNPIKE AUTHORITY  
TRAFFIC CONTROL DETAIL**

**WORK ZONE ON LEFT SHOULDER  
EXIT 2 SB ON RAMP**



APPENDIX A NOT TO SCALE

EXIT 2 NB ON  
EXIT 2 SB ON  
EXIT 1 SB ON  
DETAIL 1A

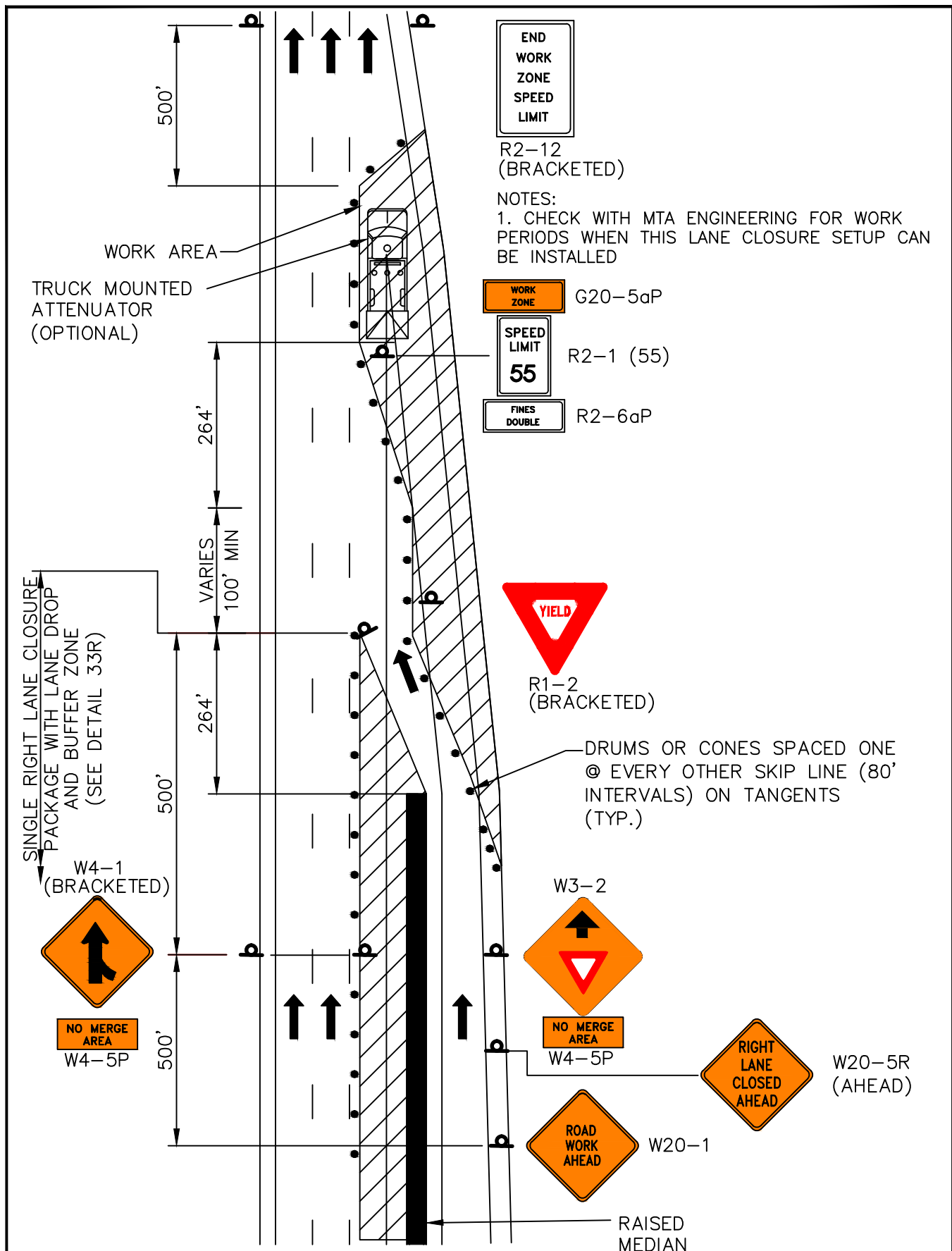
**HNTB**

DATE: 12-22-2017



**MAINE TURNPIKE AUTHORITY  
TRAFFIC CONTROL DETAIL**

TRAVEL LANE CLOSURE AT AN ENTRANCE RAMP



APPENDIX A NOT TO SCALE

EXIT 2 NB ON  
EXIT 2 SB ON  
EXIT 1 SB ON  
DETAIL 1B

**HNTB**

DATE: 12-22-2017

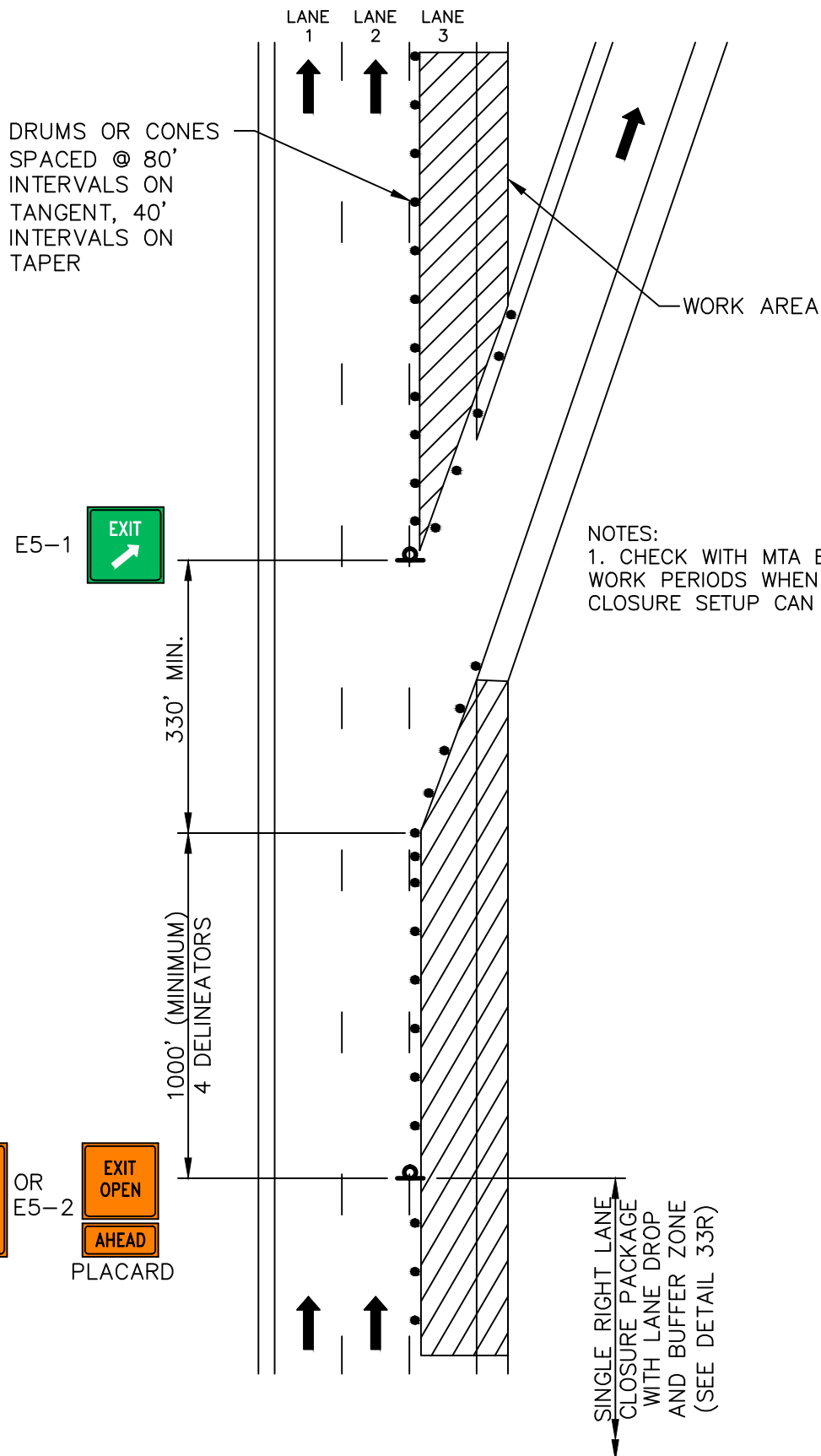


**MAINE TURNPIKE AUTHORITY  
TRAFFIC CONTROL DETAIL**

**TRAVEL LANE CLOSURE AT AN ENTRANCE RAMP  
WITH ACCELERATION LANE**







APPENDIX A NOT TO SCALE

EXIT 1 NB OFF  
EXIT 2 NB OFF  
DETAIL 1

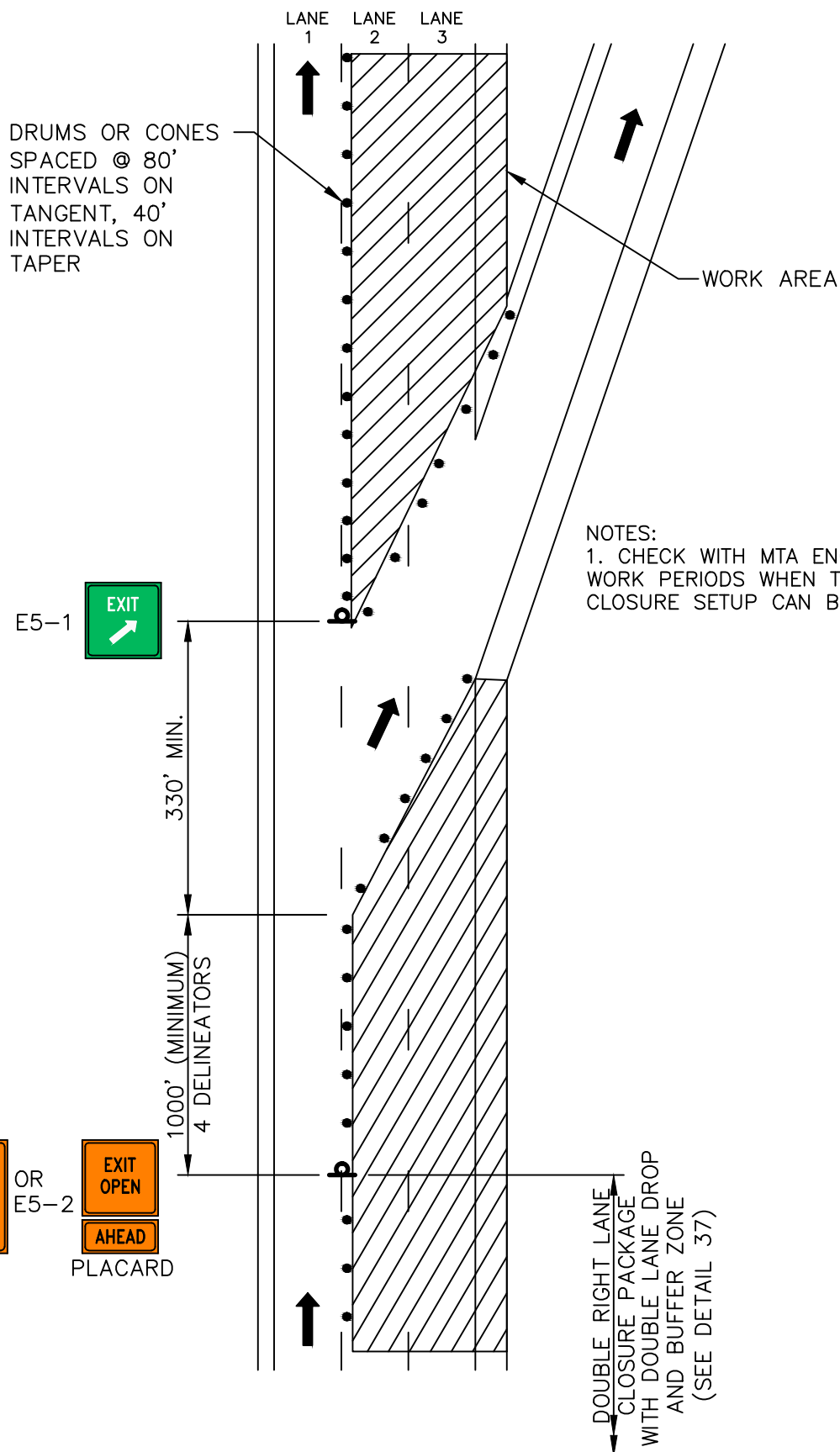
**HNTB**

DATE: 12-22-2017



**MAINE TURNPIKE AUTHORITY  
TRAFFIC CONTROL DETAIL**

LANE 3 CLOSED  
TRAFFIC IN LANES 1 AND 2  
NORTHBOUND EXIT RAMP OPEN



APPENDIX A NOT TO SCALE

EXIT 1 NB OFF  
EXIT 2 NB OFF  
DETAIL 2

**HNTB**

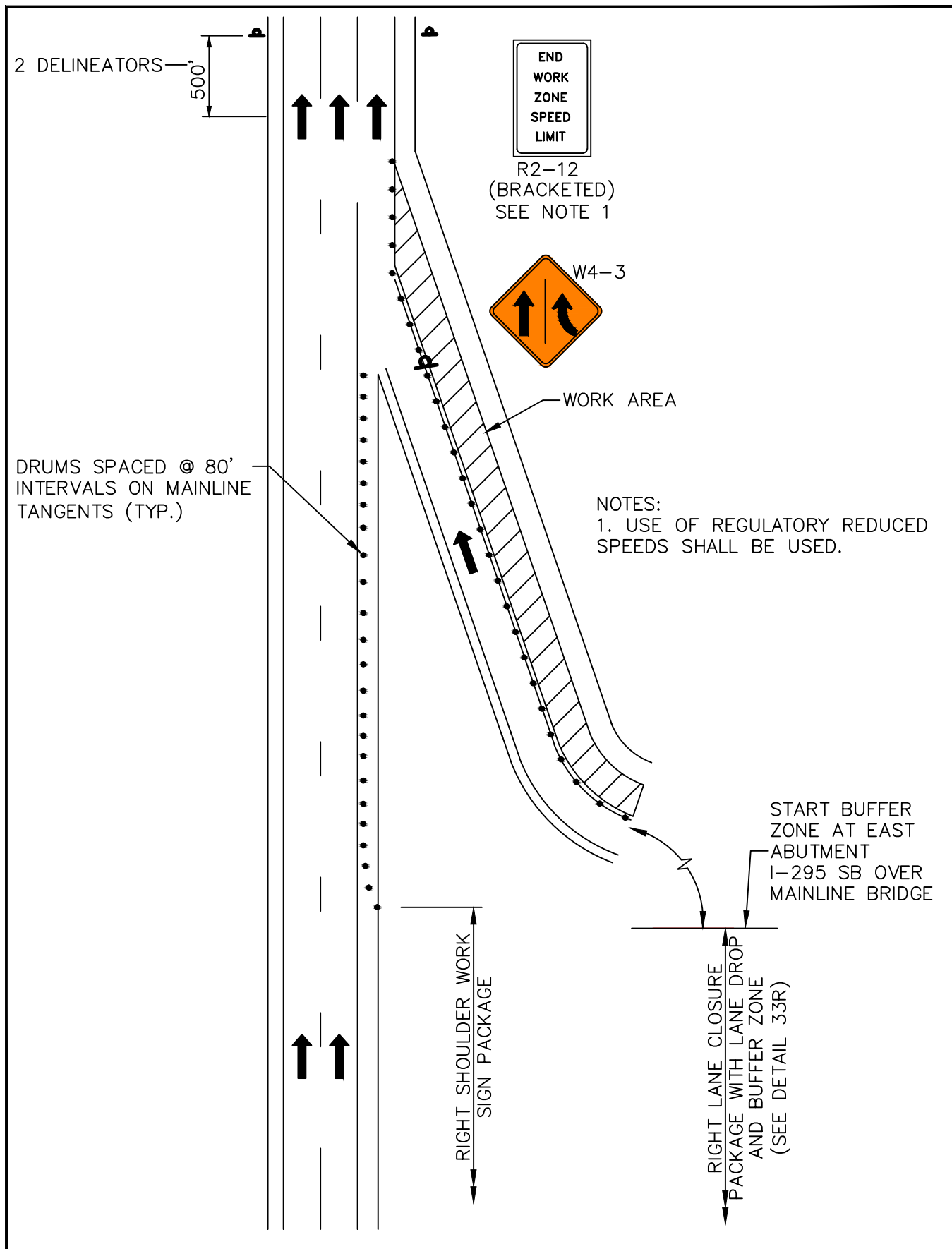
DATE: 12-22-2017



**MAINE TURNPIKE AUTHORITY  
TRAFFIC CONTROL DETAIL**

LANES 2 AND 3 CLOSED  
TRAFFIC IN LANE 1

NORTHBOUND EXIT RAMP OPEN



APPENDIX B NOT TO SCALE  
EXIT 44 SB ON  
DETAIL 1

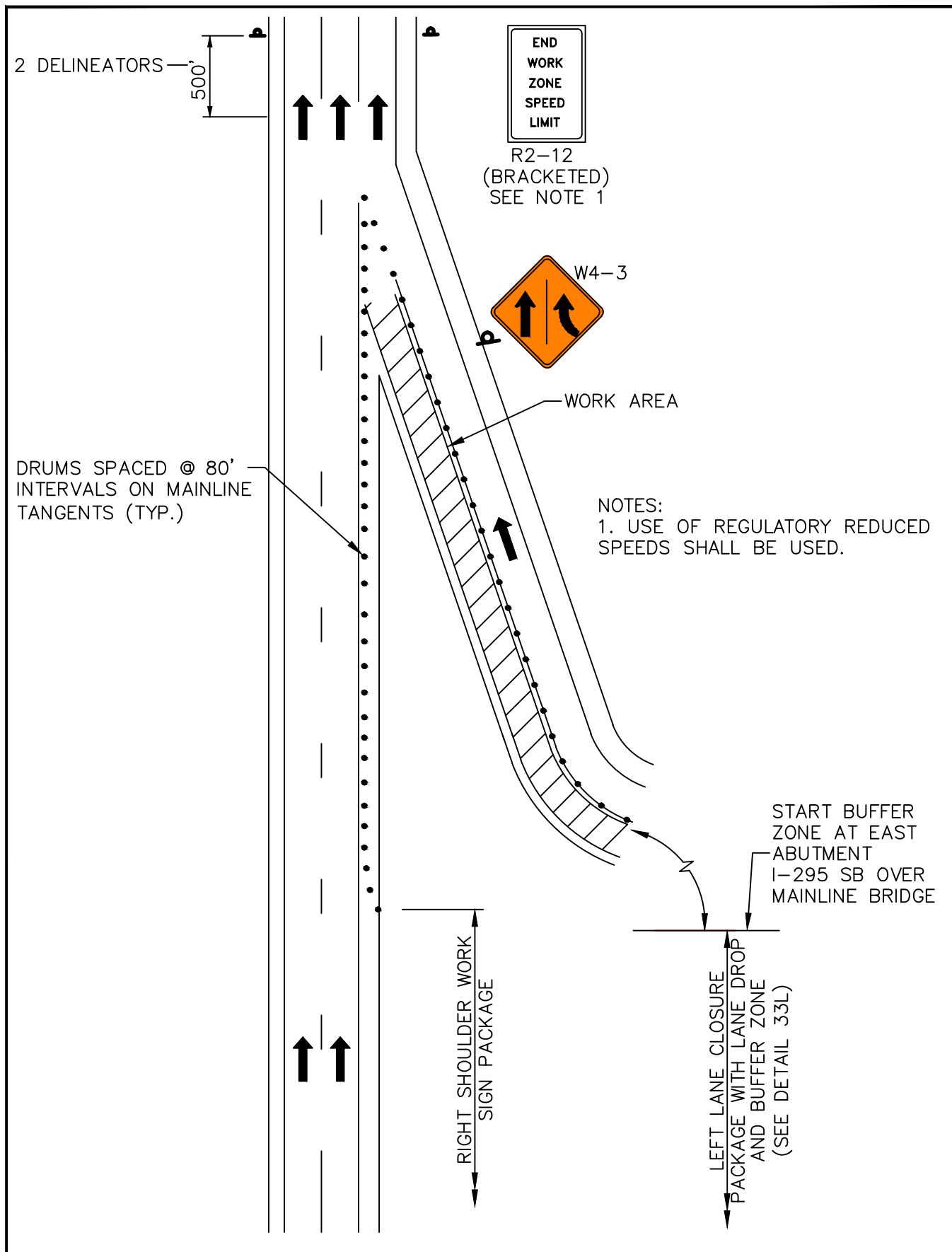
**HNTB**

DATE: 12-22-2017



**MAINE TURNPIKE AUTHORITY  
TRAFFIC CONTROL DETAIL**

**RIGHT LANE CLOSURE ON  
EXIT 44 SOUTHBOUND ON RAMP**



APPENDIX B NOT TO SCALE  
EXIT 44 SB ON  
DETAIL 2

**HNTB**

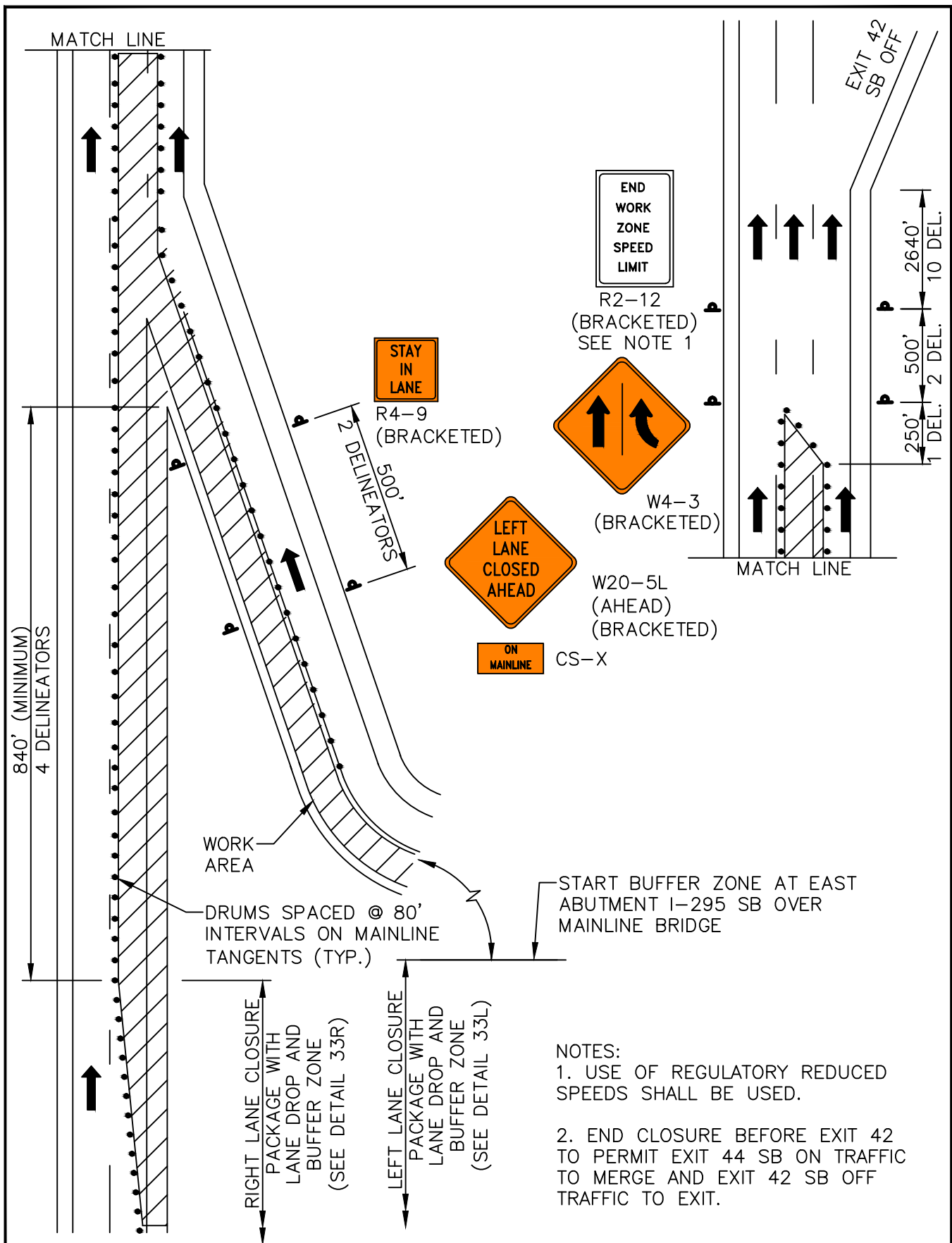
DATE: 12-22-2017



**MAINE TURNPIKE AUTHORITY  
TRAFFIC CONTROL DETAIL**

**LEFT LANE CLOSURE ON  
EXIT 44 SOUTHBOUND ON RAMP**





APPENDIX B NOT TO SCALE  
EXIT 44 SB ON  
DETAIL 4

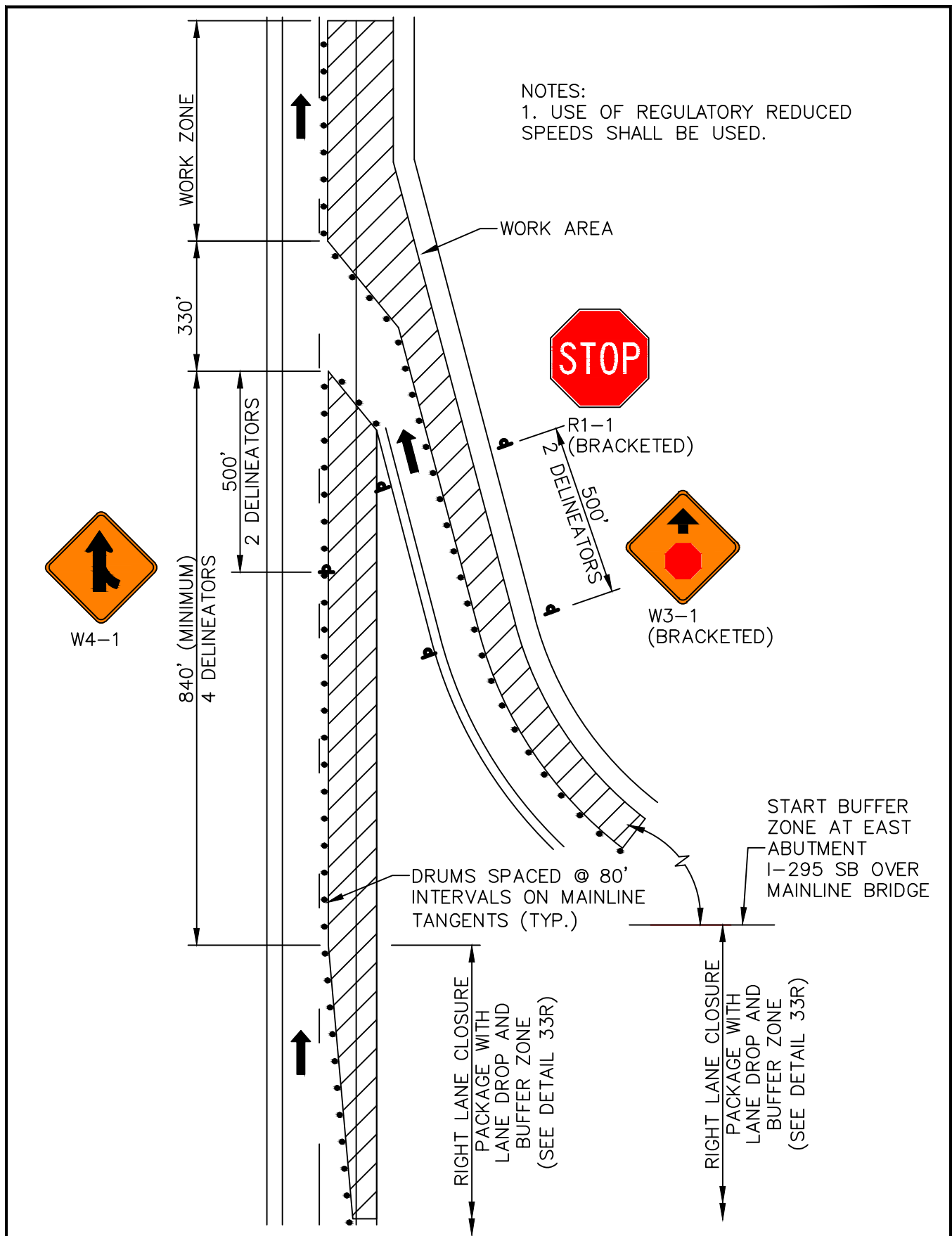
**HNTB**

DATE: 12-22-2017



**MAINE TURNPIKE AUTHORITY  
TRAFFIC CONTROL DETAIL**

**MAINLINE RIGHT LANE CLOSURE AT  
EXIT 44 SOUTHBOUND ON RAMP**



APPENDIX B NOT TO SCALE  
EXIT 44 SB ON  
DETAIL 5A

**HNTB**

DATE: 12-22-2017

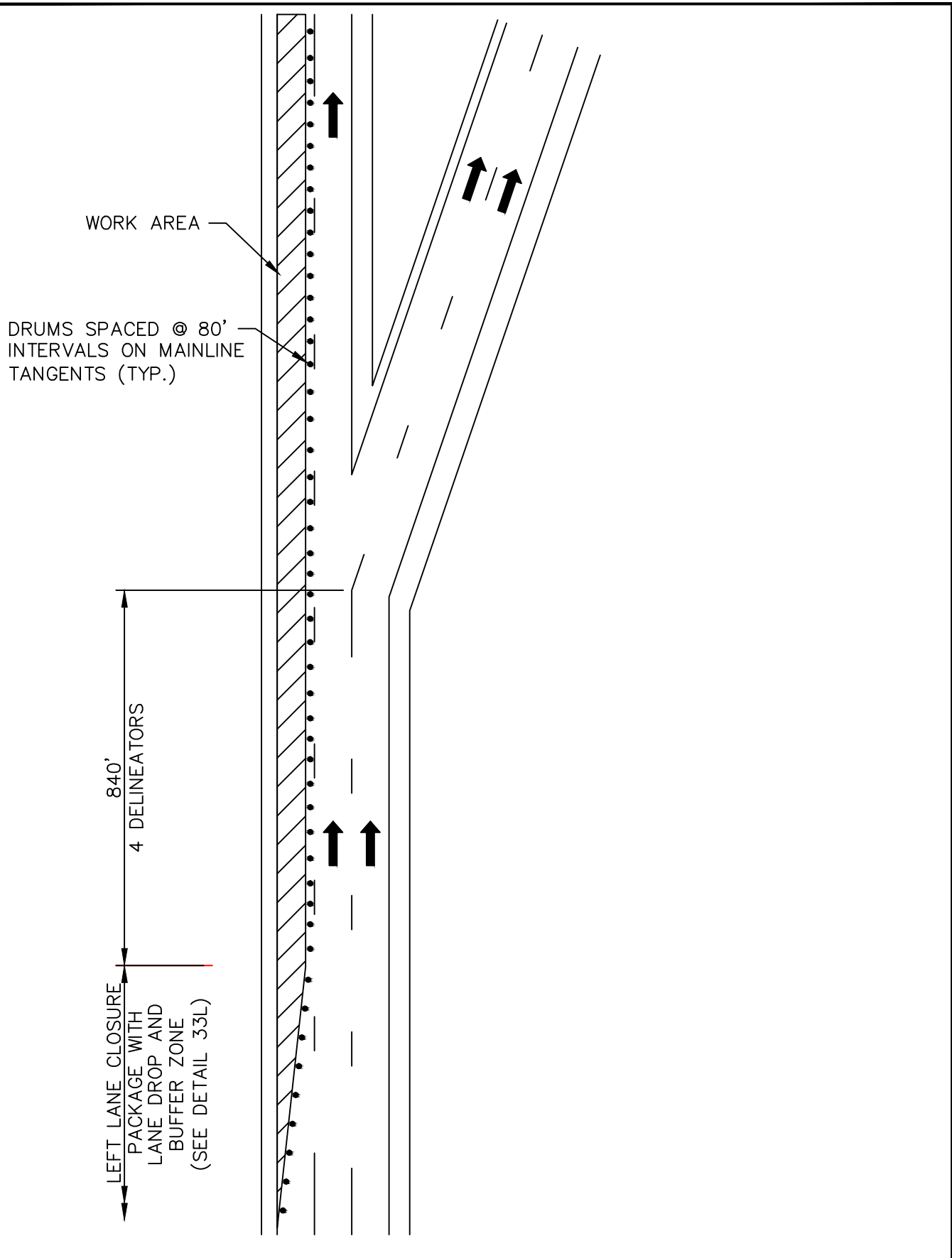


**MAINE TURNPIKE AUTHORITY  
TRAFFIC CONTROL DETAIL**

**MAINLINE RIGHT LANE CLOSURE AT  
EXIT 44 SOUTHBOUND ON RAMP**







APPENDIX B NOT TO SCALE  
EXIT 44 NB OFF  
DETAIL 1

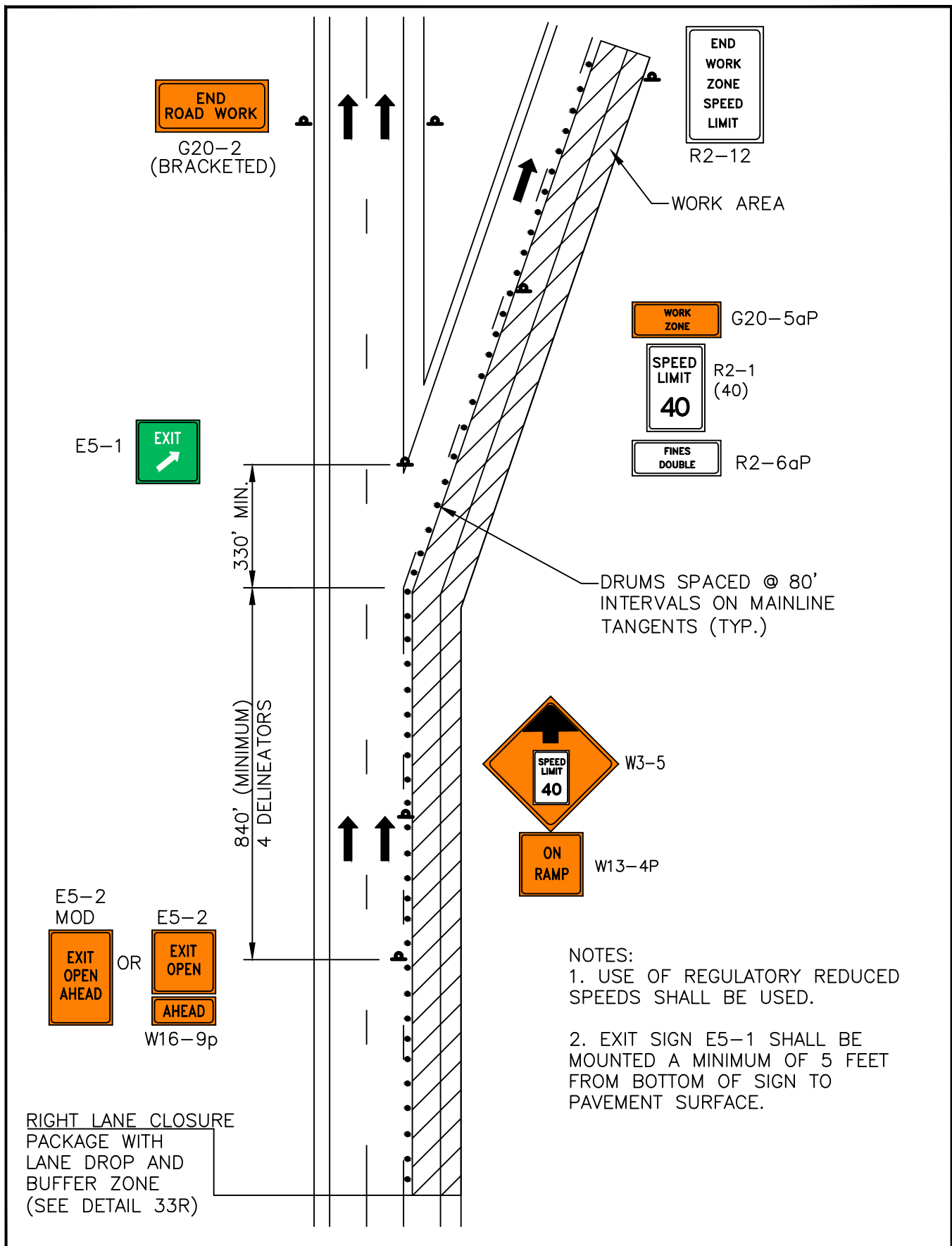
**HNTB**

DATE: 12-22-2017



**MAINE TURNPIKE AUTHORITY**  
**TRAFFIC CONTROL DETAIL**

**PASSING LANE CLOSURE AT AN EXIT RAMP**



APPENDIX B NOT TO SCALE  
EXIT 44 NB OFF  
DETAIL 2

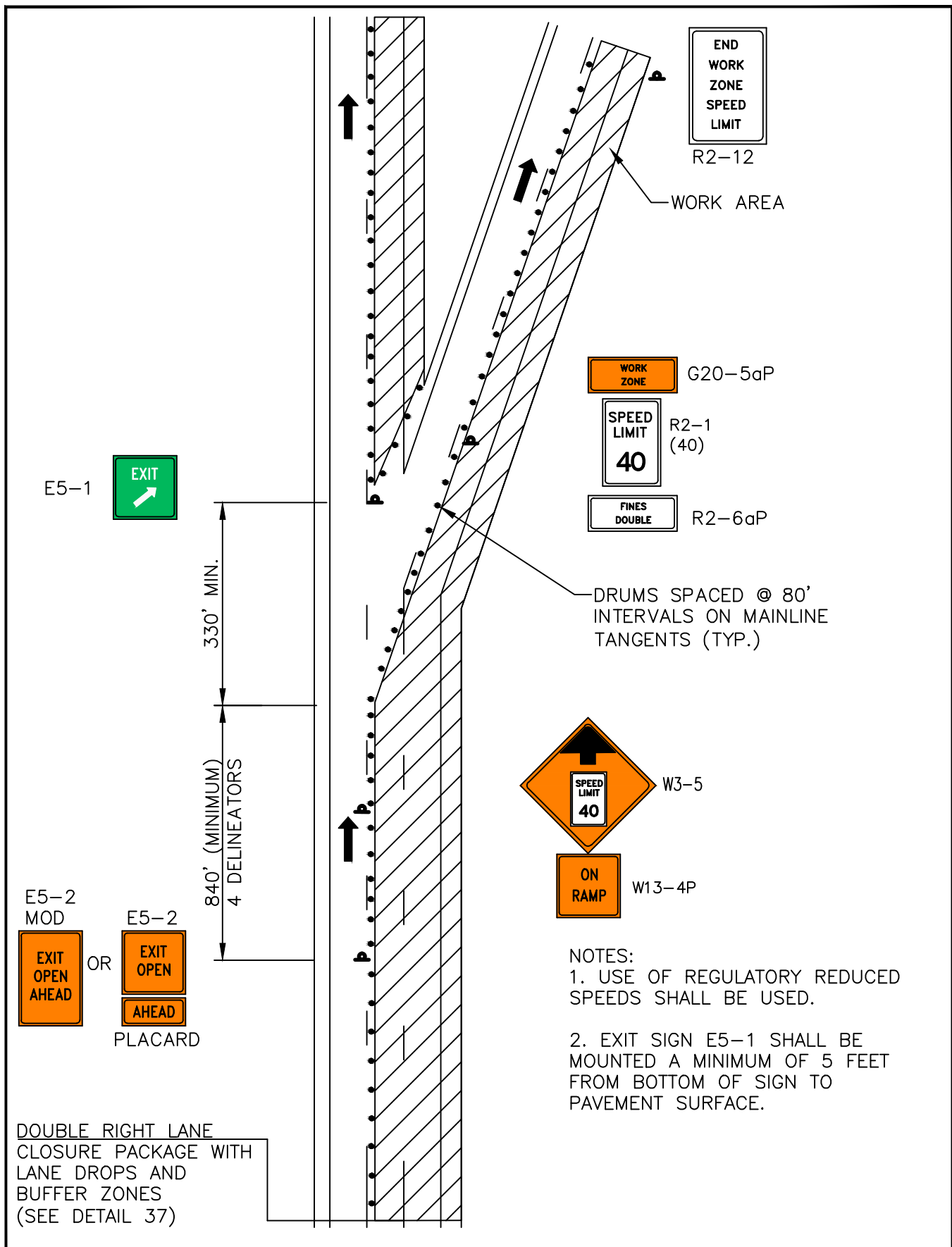
**HNTB**



DATE: 12-22-2017

**MAINE TURNPIKE AUTHORITY  
TRAFFIC CONTROL DETAIL**

**TRAVEL LANE CLOSURE AT AN EXIT RAMP**



APPENDIX B NOT TO SCALE  
EXIT 44 NB OFF  
DETAIL 3

**HNTB**

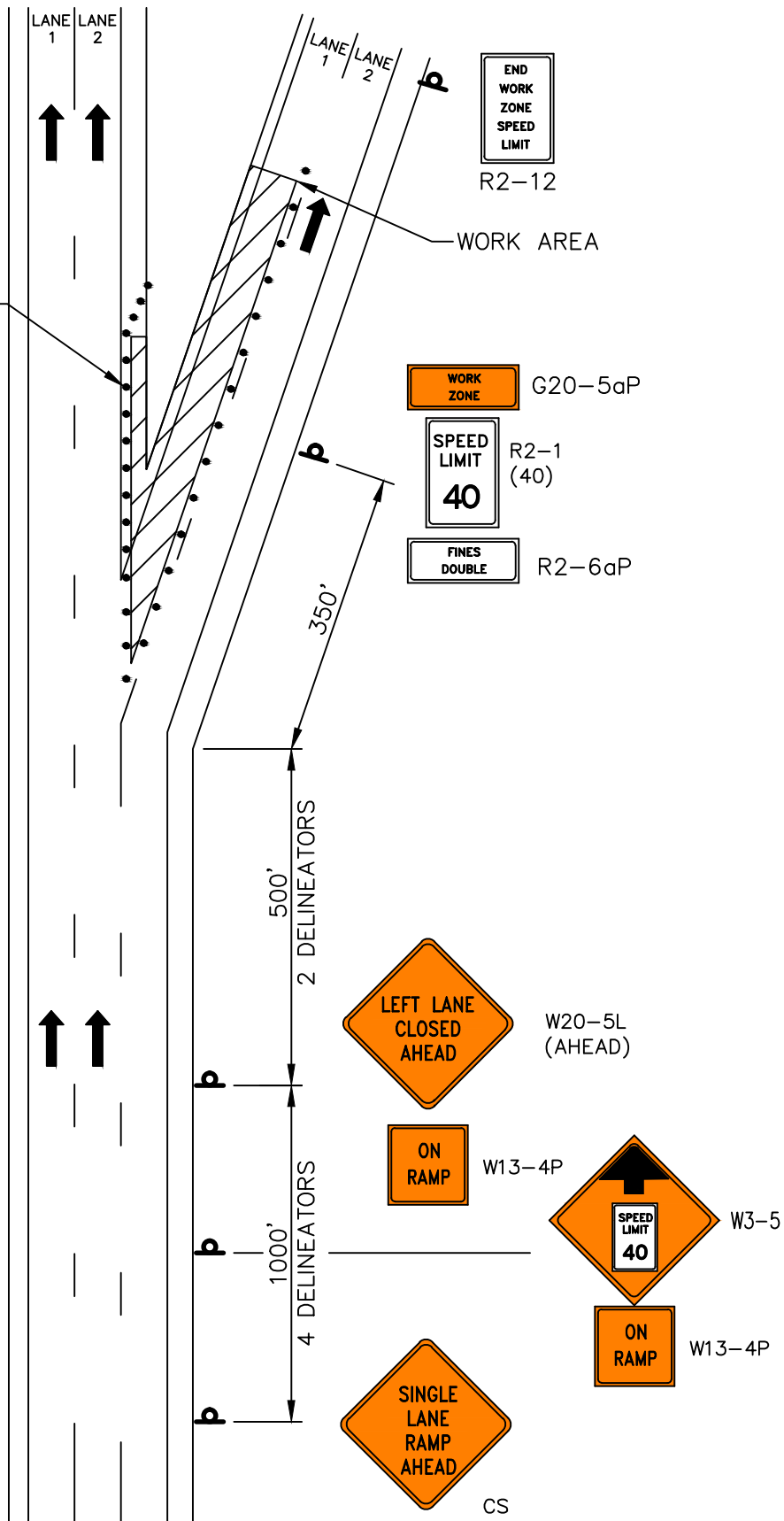
DATE: 12-22-2017



**MAINE TURNPIKE AUTHORITY  
TRAFFIC CONTROL DETAIL**

**DOUBLE LANE CLOSURE AT AN EXIT RAMP**

DRUMS SPACED @ 40' INTERVALS



APPENDIX B NOT TO SCALE  
EXIT 44 NB OFF  
DETAIL 3

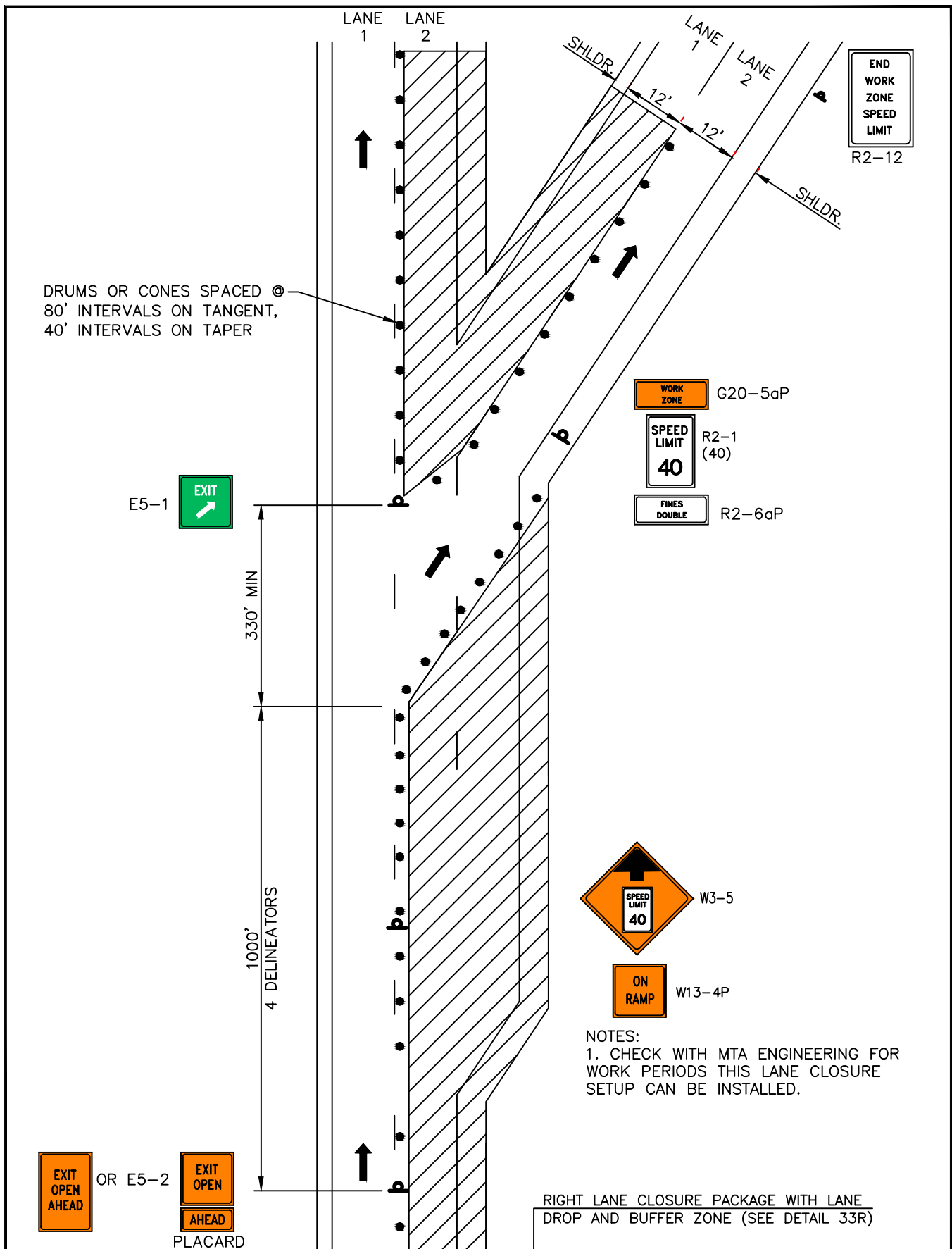
**HNTB**

DATE: 12-22-2017



**MAINE TURNPIKE AUTHORITY  
TRAFFIC CONTROL DETAIL**

**LEFT LANE CLOSURE AT AN EXIT RAMP**



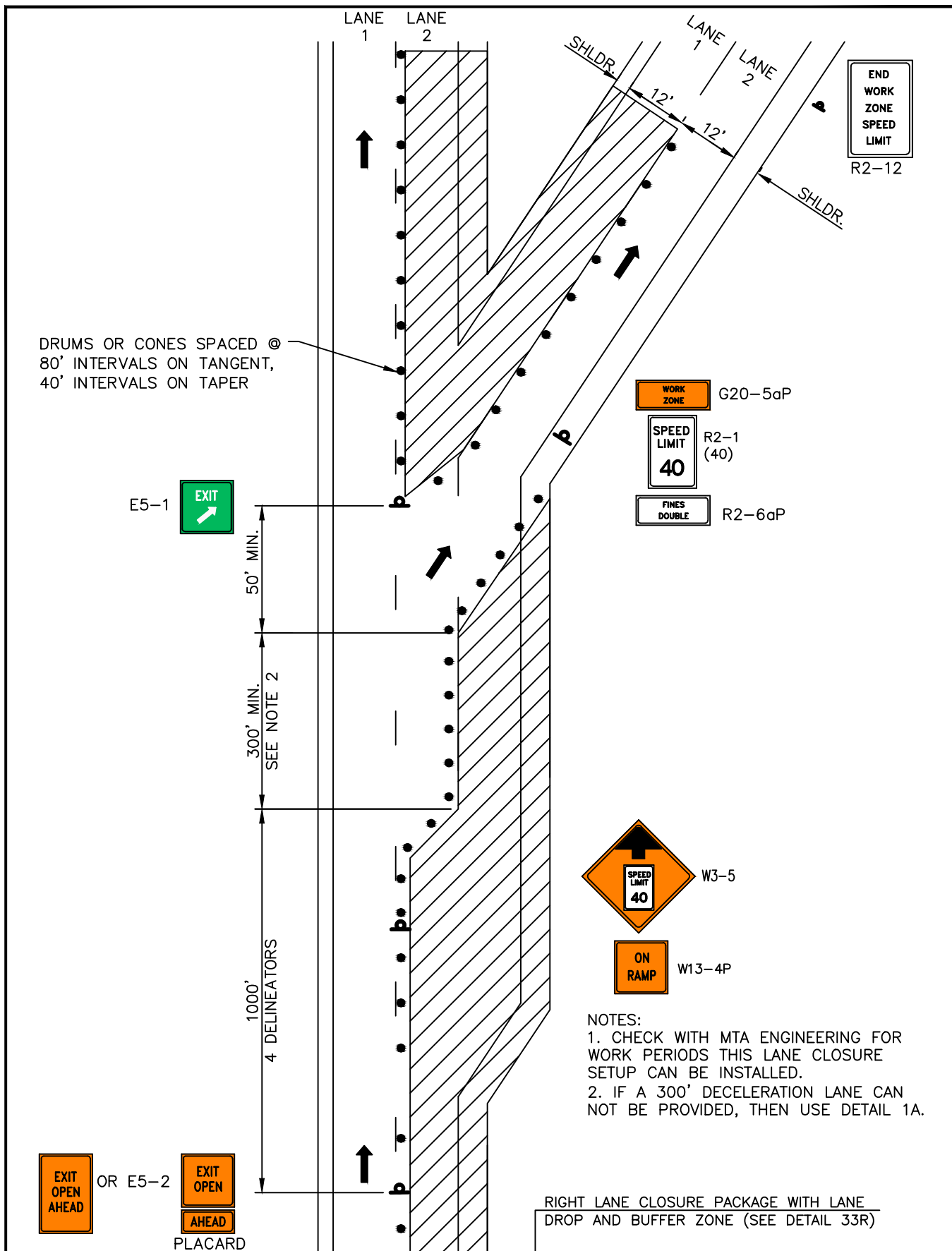
APPENDIX C NOT TO SCALE  
EXIT 103 SB OFF  
DETAIL 1A

**HNTB**

DATE: 12-22-2017



**MAINE TURNPIKE AUTHORITY**  
**TRAFFIC CONTROL DETAIL**  
EXIT 103 SB LANE 2 CLOSED  
TRAFFIC IN LANE 1  
SOUTHBOUND EXITING RAMP OPEN - LANE 2



APPENDIX C NOT TO SCALE  
EXIT 103 SB OFF  
DETAIL 1B

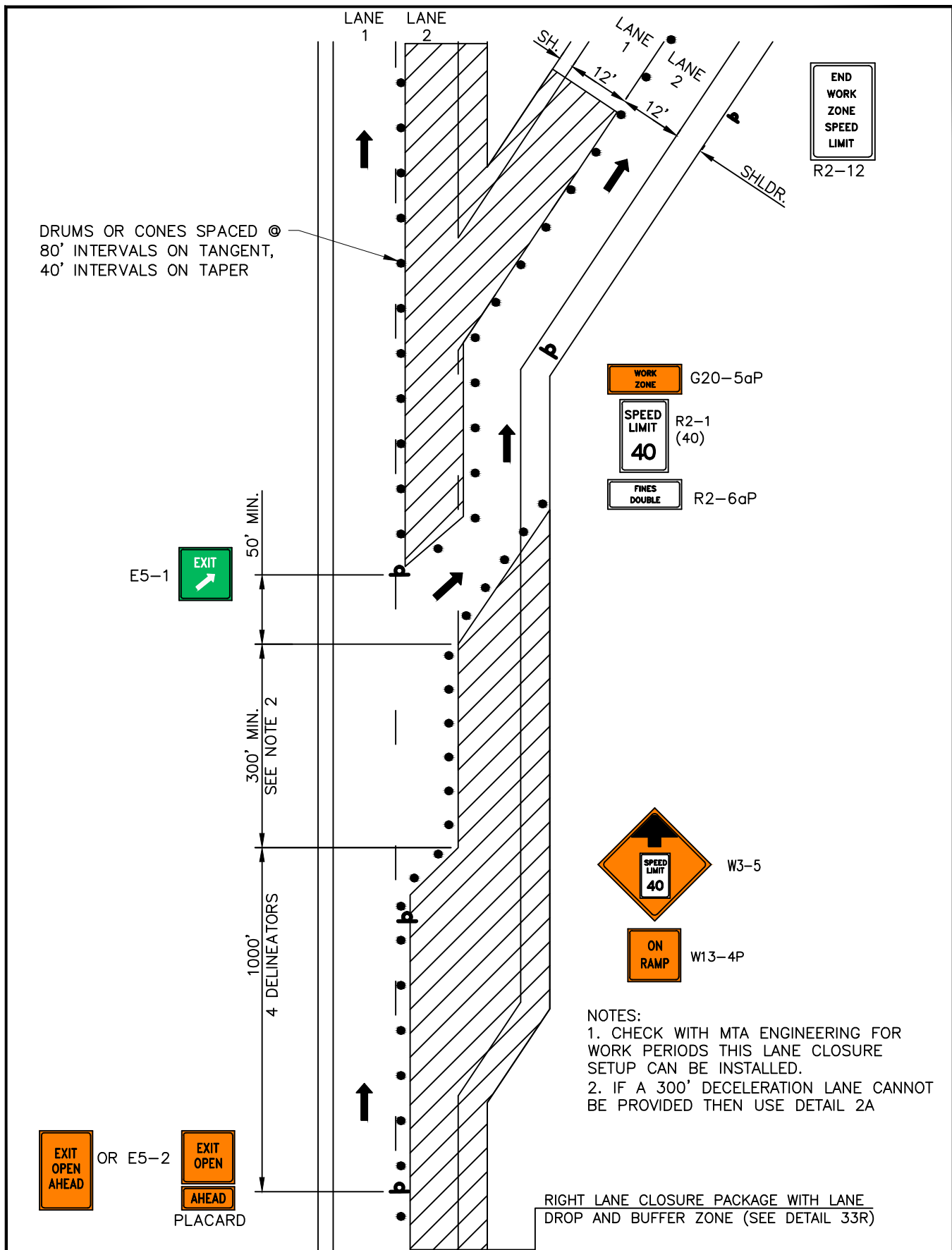
**HNTB**

DATE: 12-22-2017



**MAINE TURNPIKE AUTHORITY**  
**TRAFFIC CONTROL DETAIL**  
EXIT 103 SB LANE 2 CLOSED  
TRAFFIC IN LANE 1. SB EXITING RAMP OPEN -  
LANE 2 WITH DECELERATION LANE





APPENDIX C NOT TO SCALE  
EXIT 103 SB OFF  
DETAIL 2B

**HNTB**

DATE: 12-22-2017



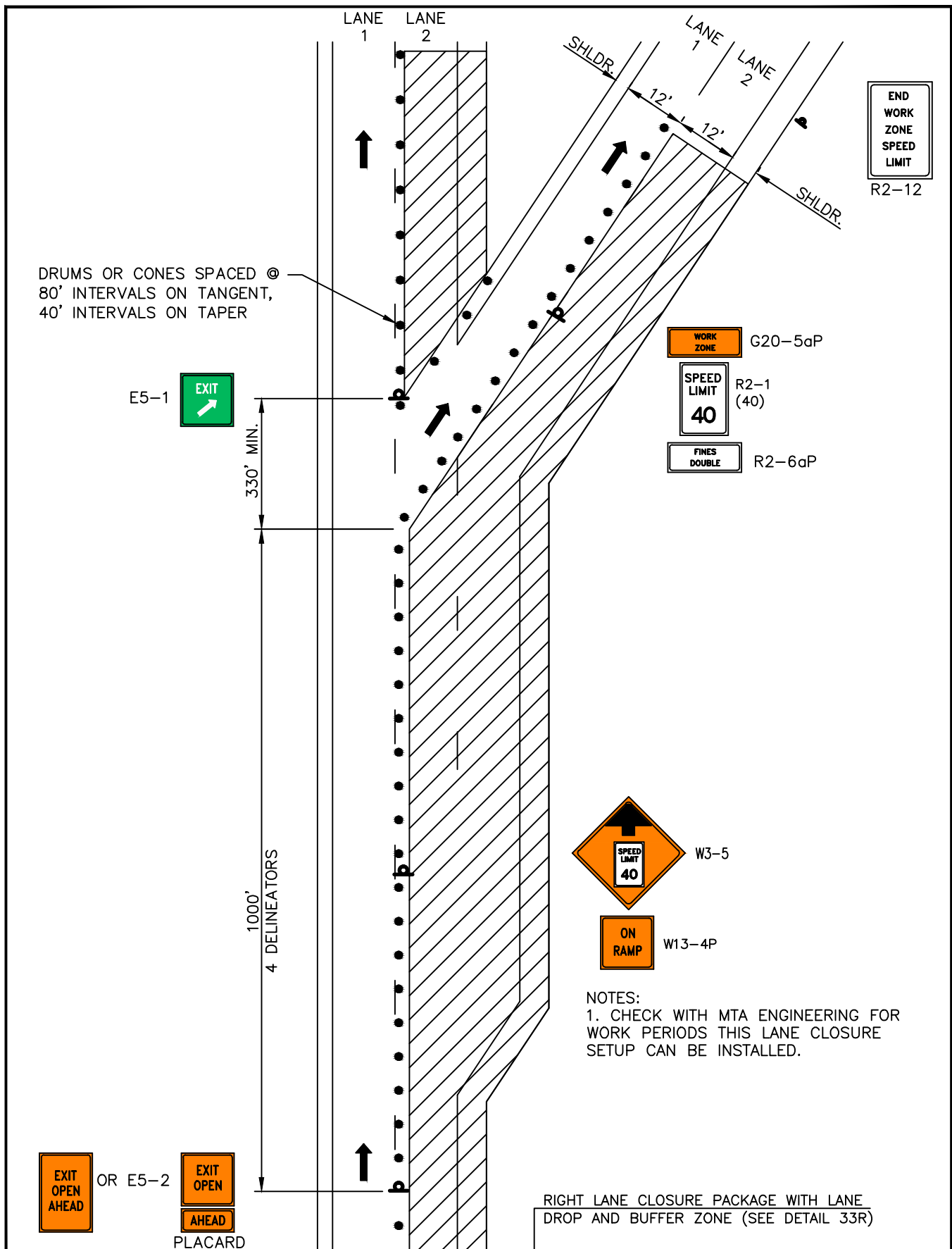
**MAINE TURNPIKE AUTHORITY**  
**TRAFFIC CONTROL DETAIL**

EXIT 103 SB LANE 2 CLOSED

TRAFFIC IN LANE 1

SB EXITING RAMP OPEN - LANE 2 WITH DECELERATION LANE  
WORK REQUIRED IN LANE 2 GORE





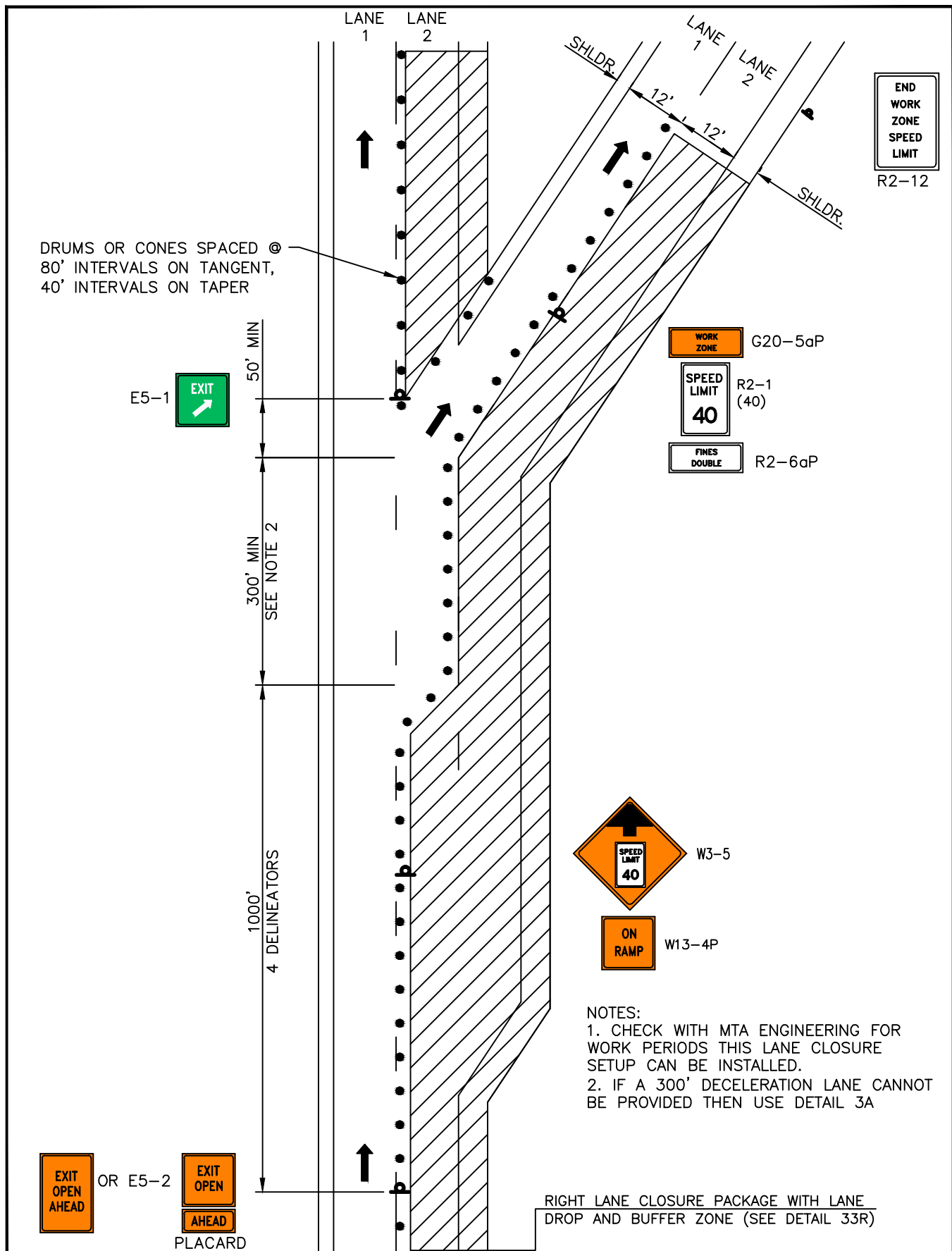
APPENDIX C NOT TO SCALE  
EXIT 103 SB OFF  
DETAIL 3A

**HNTB**

DATE: 12-22-2017



**MAINE TURNPIKE AUTHORITY**  
**TRAFFIC CONTROL DETAIL**  
EXIT 103 SB LANE 2 CLOSED  
TRAFFIC IN LANE 1  
SOUTHBOUND EXITING RAMP OPEN - LANE 1



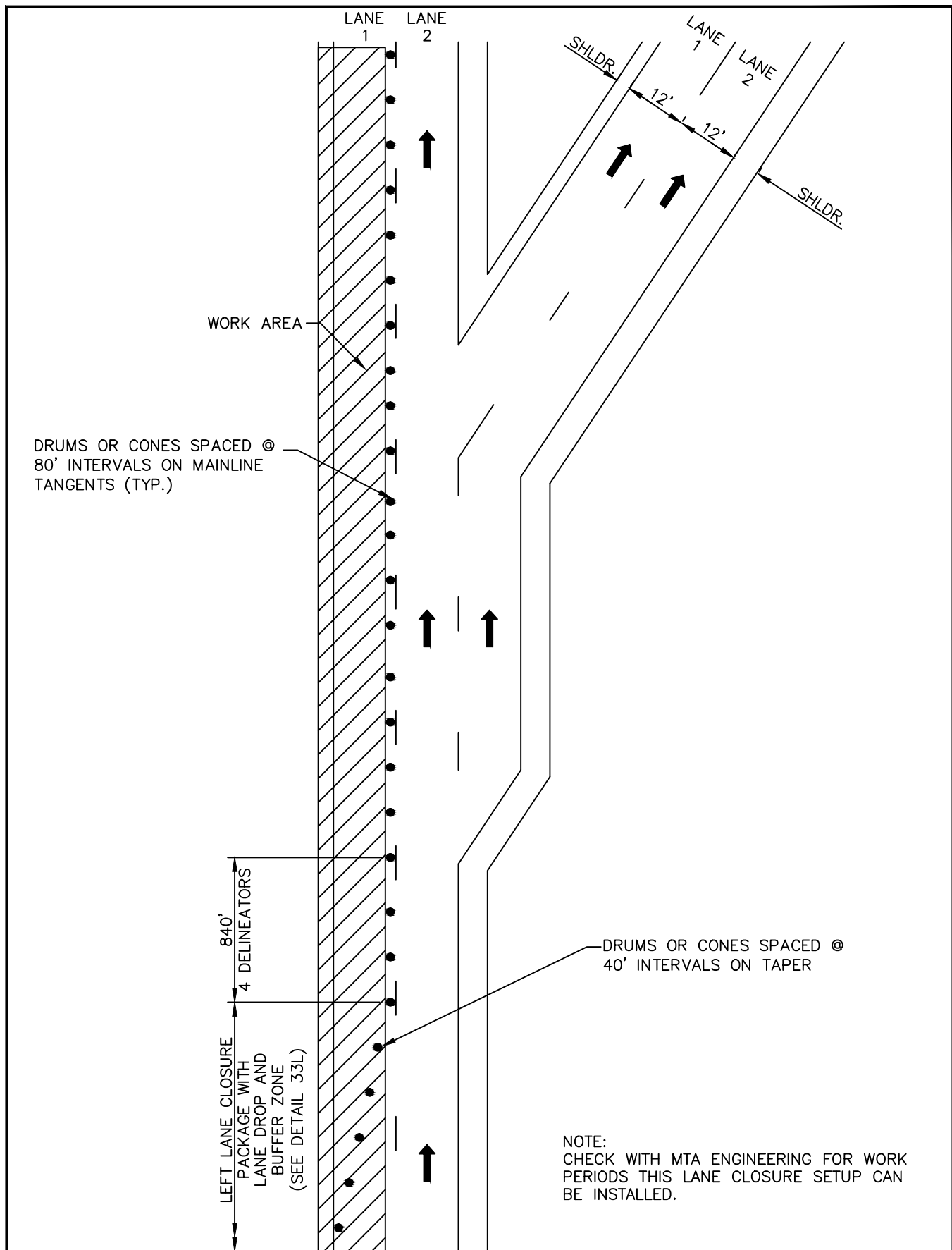
APPENDIX C NOT TO SCALE  
EXIT 103 SB OFF  
DETAIL 3B

**HNTB**

DATE: 12-22-2017



**MAINE TURNPIKE AUTHORITY**  
**TRAFFIC CONTROL DETAIL**  
EXIT 103 SB LANE 2 CLOSED  
TRAFFIC IN LANE 1. SB EXITING RAMP OPEN -  
LANE 1 WITH DECELERATION LANE



APPENDIX C NOT TO SCALE  
EXIT 103 SB OFF  
DETAIL 4

**HNTB**

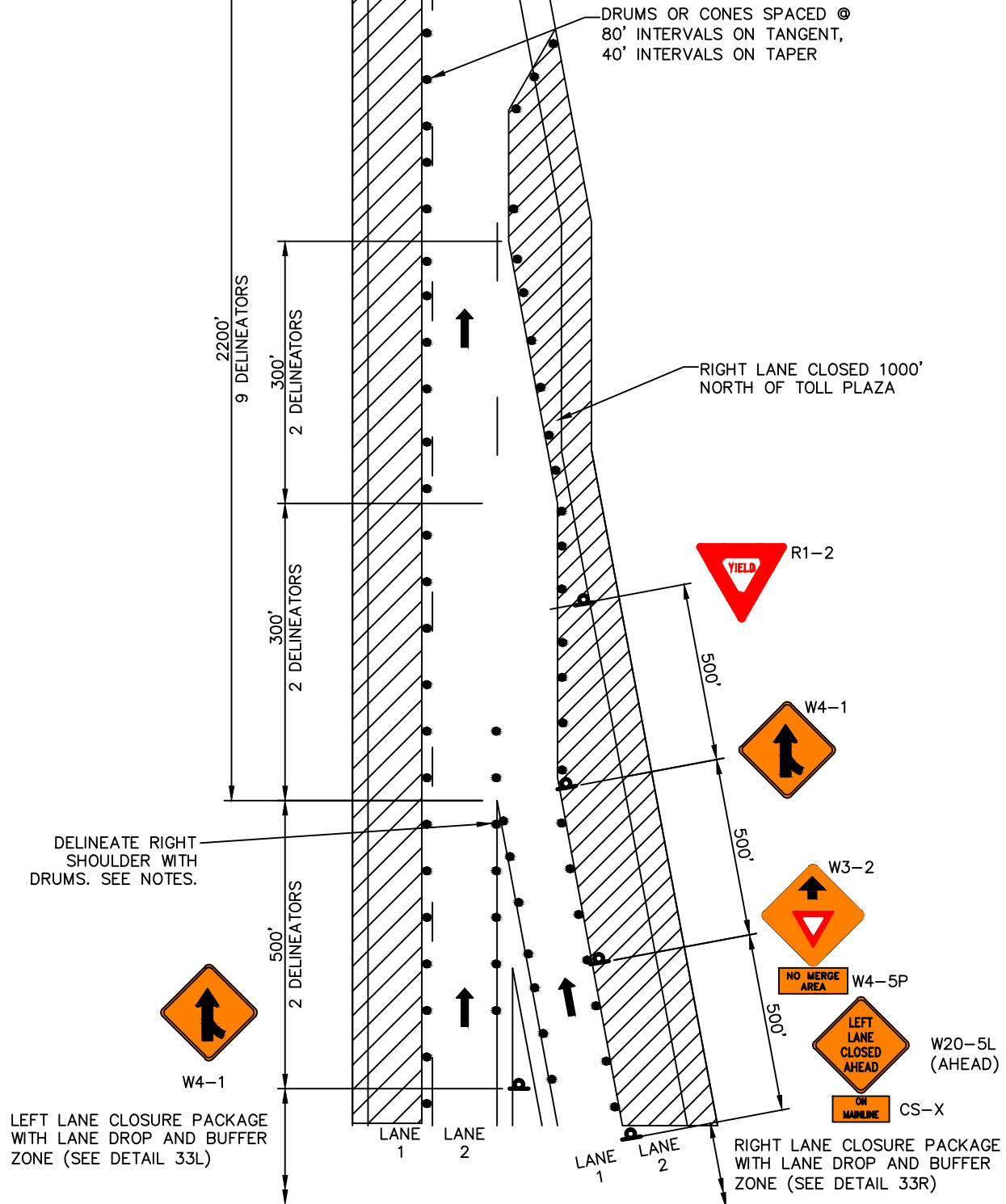
DATE: 12-22-2017



**MAINE TURNPIKE AUTHORITY**  
**TRAFFIC CONTROL DETAIL**  
EXIT 103 SB LANE 1 CLOSED  
TRAFFIC IN LANE 2  
SOUTHBOUND EXITING RAMP OPEN



- NOTES:
1. CHECK WITH MTA ENGINEERING FOR WORK PERIODS THIS LANE CLOSURE SETUP CAN BE INSTALLED.
  2. SHIFT ENTERING RAMP MERGE TO THE RIGHT THE SAME DISTANCE WHEN LANE 2 IS SHIFTED TO THE RIGHT.



APPENDIX C  
EXIT 103 NB ON  
DETAIL 1

NOT TO SCALE

**HNTB**

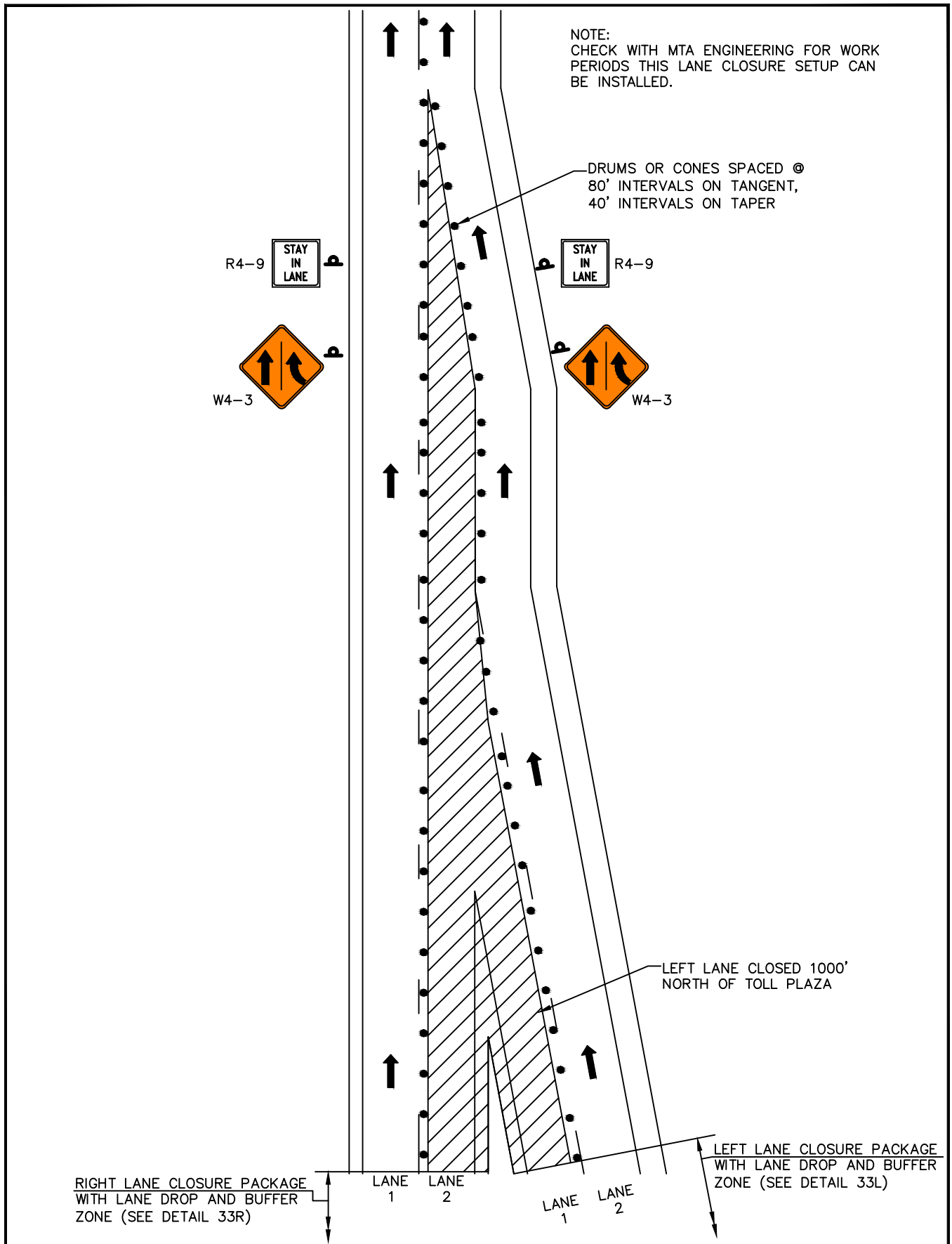
DATE: 12-22-2017



# MAINE TURNPIKE AUTHORITY TRAFFIC CONTROL DETAIL

EXIT 103 NB LANE 1 CLOSED  
MAINLINE TRAFFIC IN LANE 2

NORTHBOUND ENTERING RAMP OPEN - LANE 1



APPENDIX C  
EXIT 103 NB ON  
DETAIL 2

NOT TO SCALE

**HNTB**

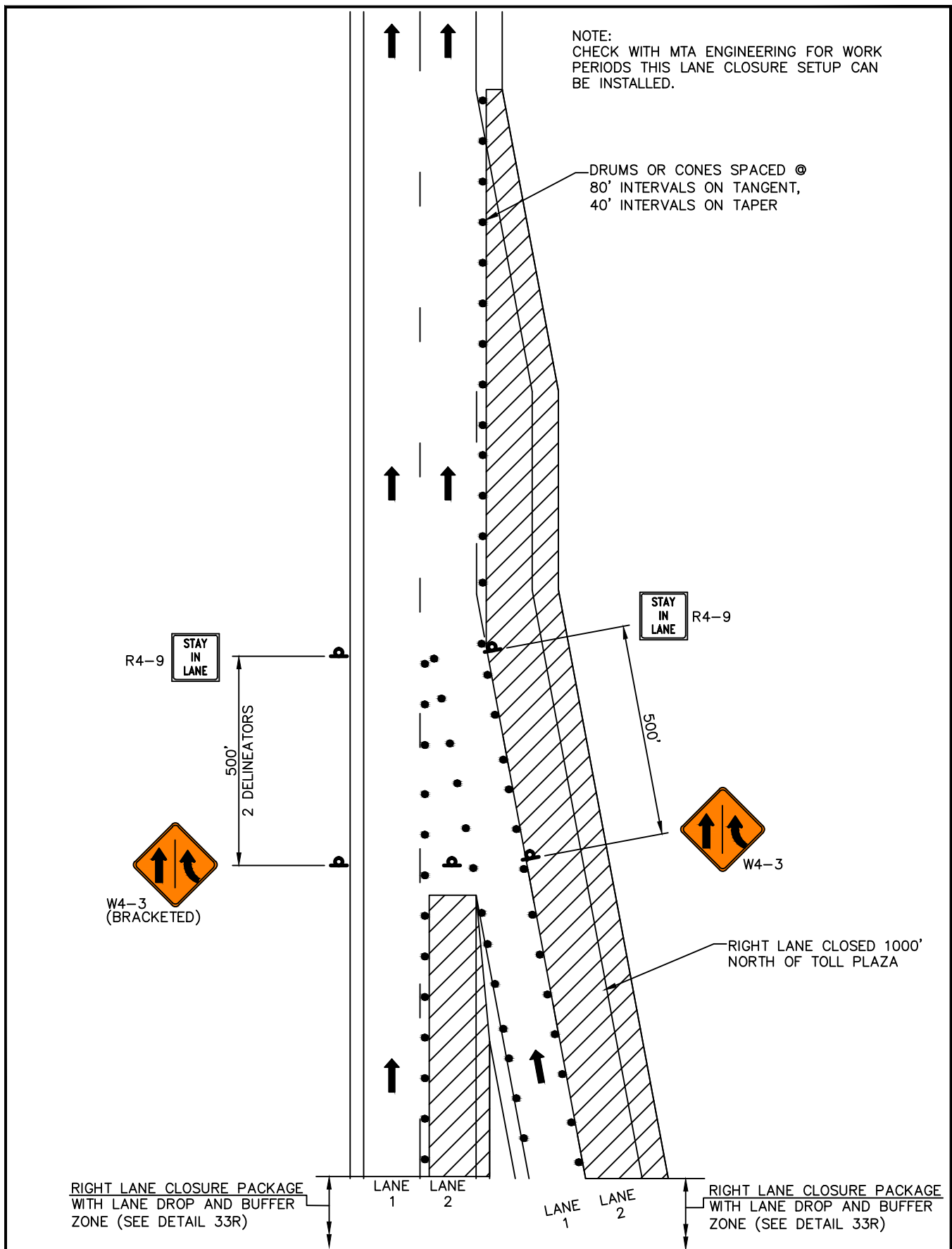
DATE: 12-22-2017



**MAINE TURNPIKE AUTHORITY  
TRAFFIC CONTROL DETAIL**

**EXIT 103 NB LANE 2 CLOSED AT RAMP  
MAINLINE TRAFFIC IN LANE 1**

**NORTHBOUND ENTERING RAMP OPEN - LANE 2**



APPENDIX C  
EXIT 103 NB ON  
DETAIL 3

NOT TO SCALE

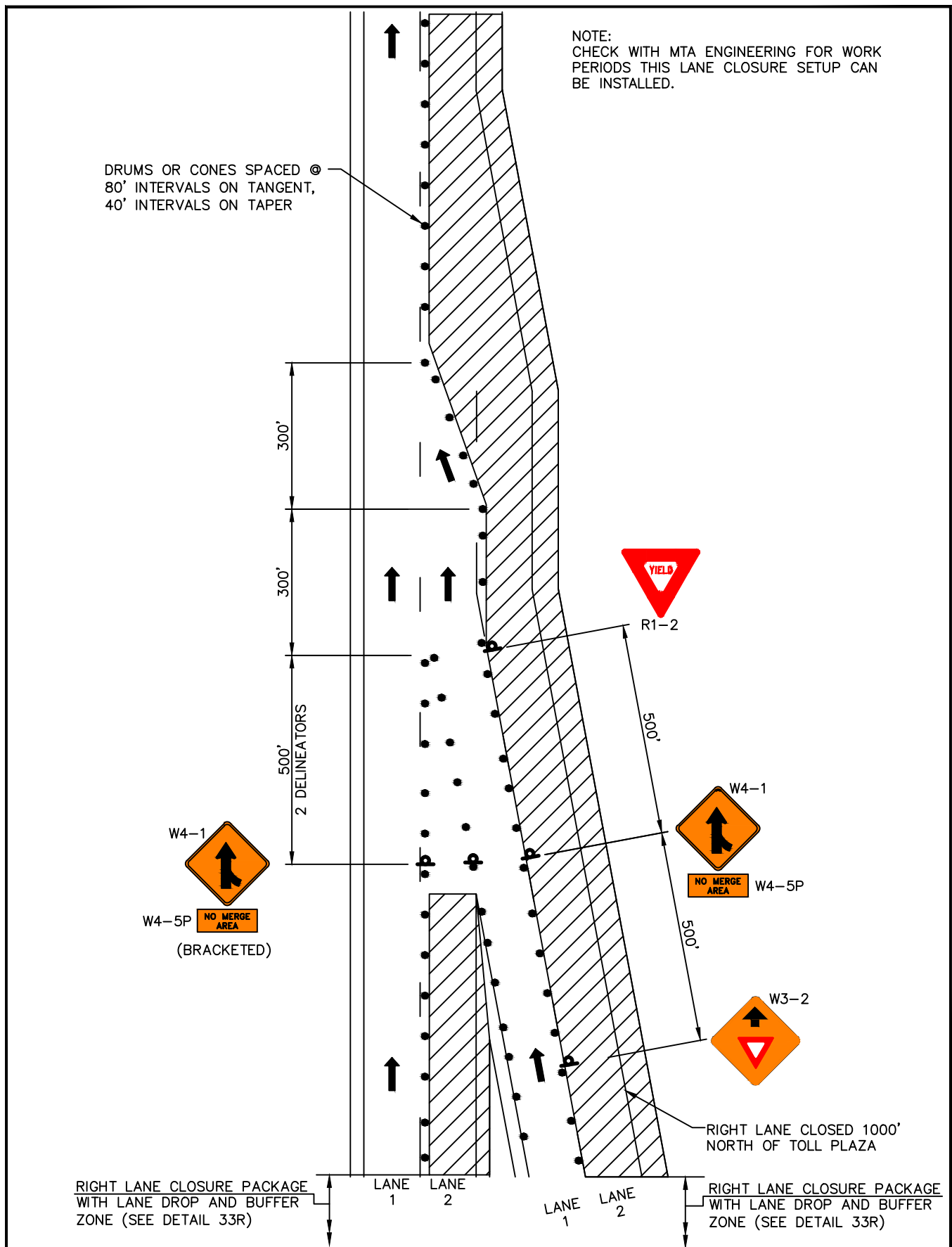
**HNTB**

DATE: 12-22-2017



**MAINE TURNPIKE AUTHORITY  
TRAFFIC CONTROL DETAIL**

EXIT 103 NB LANE 2 CLOSED BEFORE ON RAMP  
MAINLINE TRAFFIC IN LANE 1  
NORTHBOUND ENTERING RAMP OPEN - LANE 1



APPENDIX C NOT TO SCALE  
EXIT 103 NB ON  
DETAIL 4

**HNTB**

DATE: 12-22-2017



**MAINE TURNPIKE AUTHORITY**  
**TRAFFIC CONTROL DETAIL**  
EXIT 103 NB - LANE 2 CLOSED BEFORE AND  
AFTER ON RAMP. EXIT 103 NB ON RAMP - LANE 2  
CLOSED LIMITED MERGE AREA