



Maine Turnpike Authority

Traffic Control Plans

May 2020 Amendment



MEMO:

Date: May 7, 2020

Re: Amendment to the Maine Turnpike Authority Traffic Control Plans – January 2018 and December 2018 memo

The following revisions to the Traffic Control Plans dated January 2018 shall be implemented when setting up work zone traffic control zones:

Placement of Arrow Board:

At locations where the shoulders widths do not allow the placement of the arrow board at the location shown the MOT plans so that the arrow board is completely within the closed area of the roadway then the location of the arrow board shall be adjusted within the taper area as needed keeping it as close as practicable to the beginning of the taper while keeping the arrow board completely located on the back side of cones/barrels within the closed area of the roadway.

Placement of TMIA:

For a Stationary work zone operation, the shadow vehicle, with or without an impact attenuator, shall be 200 feet (5 skip lines) from the work area to allow for movement if the truck is struck. The shadow vehicle shall have its front wheels turned away from the work area and from traffic, have parking brake set, and be put in park if an automatic transmission; or if a manual transmission it shall have its front wheels turned away from the work area and from traffic, have parking brake set and should be placed in gear and shut off if possible while still maintaining warning lights. If length of time or weather are a concern for the battery since the warning lights must be maintained the engine should be started and run periodically for battery recharging. No other vehicles or equipment shall park in front of the shadow vehicle or within the buffer space behind the shadow vehicle.

For a mobile work zone operation, shadow vehicles, with or without an impact attenuator, shall be no closer than 250 feet (6 skip lines) from the work vehicle or other shadow vehicles. Maximum distance for each shadow vehicle used shall be determined by the supervisor depending on the operation and in accordance with the notes shown on DETAIL 35R and 35L and 51R and 51L.

At ramp intersections with local roads or in cash toll plaza areas where the speed limit is 40 MPH or less it may be necessary to shorten up the buffer space or reduce the distance from TMA to the work area to a minimum of 75 feet so that a TMA can be utilized. There could be limited situations where it may not be practicable to utilize a TMA due to space constraints.

Detail 49 *Snow Removal in Median Opening* a TMA is not required.



MOT Details

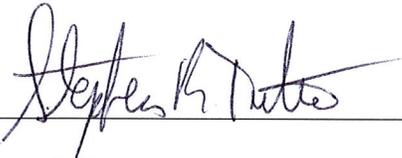
The attached details have been revised/added which include Details: 1, 4A, 42A, 42B, 48, 49, 50, TP-50A, TP-50B, TP-50C, TP-52, Appendix B Page 84.

Detail 4B(Page 7): During litter picking activities when a “leap frogging” operation is being utilized a W21-5 sign shall be easel mounted in advance of the work area in addition to being mounted on the truck.

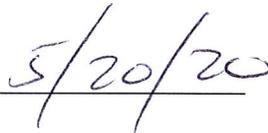
Details 4B, 5A, 5B and 49 where the details call for an *Arrow Board(Preferred) or Arrow Stick (Optional)* both items can be omitted if the truck has a High Intensity LED Light Bar in use.

Local Road Details Numbers 6 to 17: If there is an intersection within the maintenance of traffic sign array then the sign spacing should be adjusted if feasible or a sign array should be added to the approaches of the intersecting street(s).

Signed: _____



Date: _____



Steve Tartre, P.E.

Director of Engineering & Building Maintenance

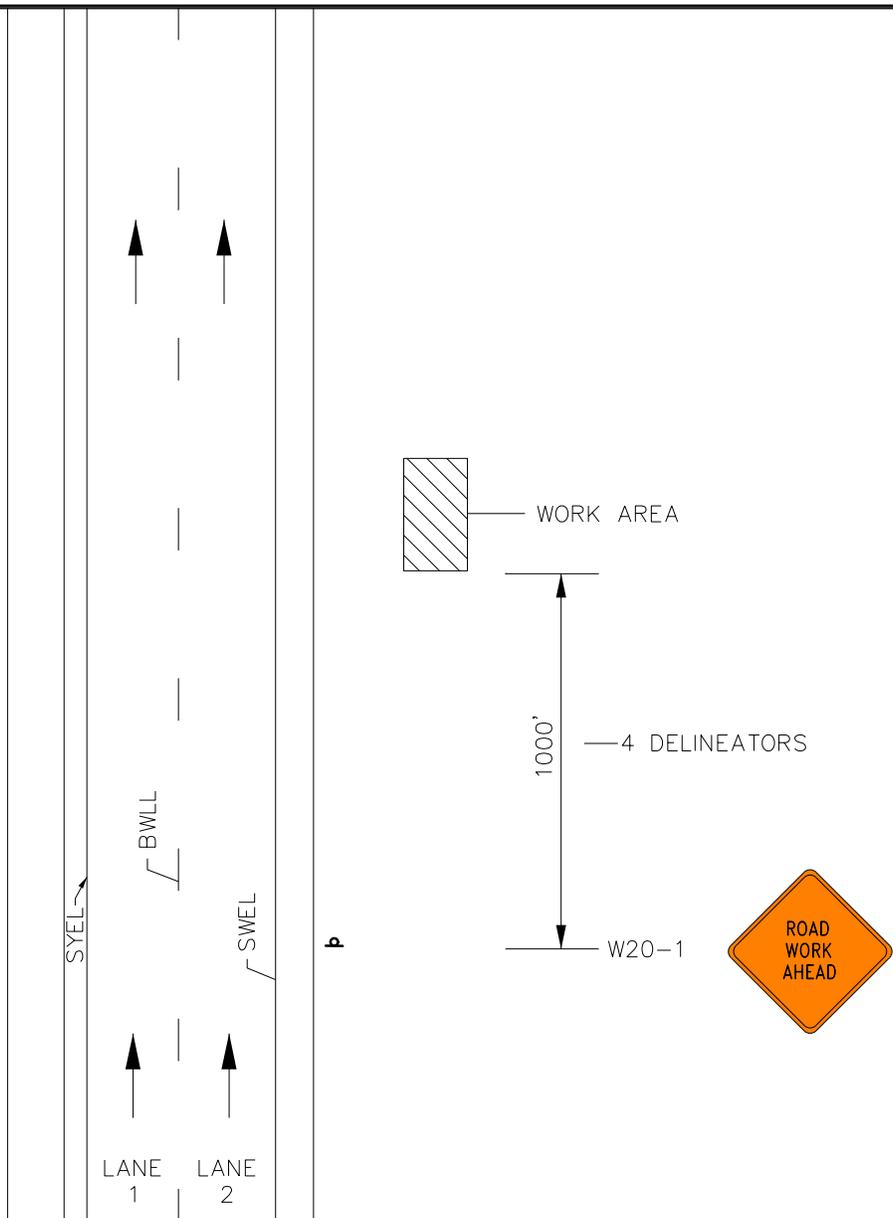


TELEPHONE (207) 871-7771

Turnpike Travel Conditions 1-800-675-7453
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NOTES:

1. THE ROAD WORK AHEAD SIGN MAY BE OMITTED WHERE THE WORK SPACE IS BEHIND BARRIER OR 15 FEET OR MORE FROM THE EDGE OF ROADWAY.
2. FOR SHORT DURATION (<1 HOUR) ACTIVITIES, MOBILE OPERATIONS, OR OPERATIONS SUCH AS MOWING, THE ROAD WORK AHEAD SIGN MAY BE ELIMINATED IF THE WORK VEHICLE ACTIVATES THE HIGH INTENSITY ROTATING, FLASHING, OSCILLATING OR STROBE LIGHTS.
3. IF WORK VEHICLES ARE PARKED IN THE SHOULDER, THEN THE SHORT DURATION OR MOBILE OPERATION ON SHOULDER DETAIL (DETAIL 4B) SHALL BE FOLLOWED.
4. FOR MOWING OR SIMILAR ACTIVITIES, IF VEHICLES ARE PARKED MORE THAN 15' OFF THE EDGE OF PAVEMENT OR BEHIND BARRIER (GUARDRAIL), THE ROAD WORK AHEAD SIGN MAY BE ELIMINATED AND PARKED VEHICLE LIGHTS DO NOT NEED TO BE ON.

DETAIL 1

NOT TO SCALE

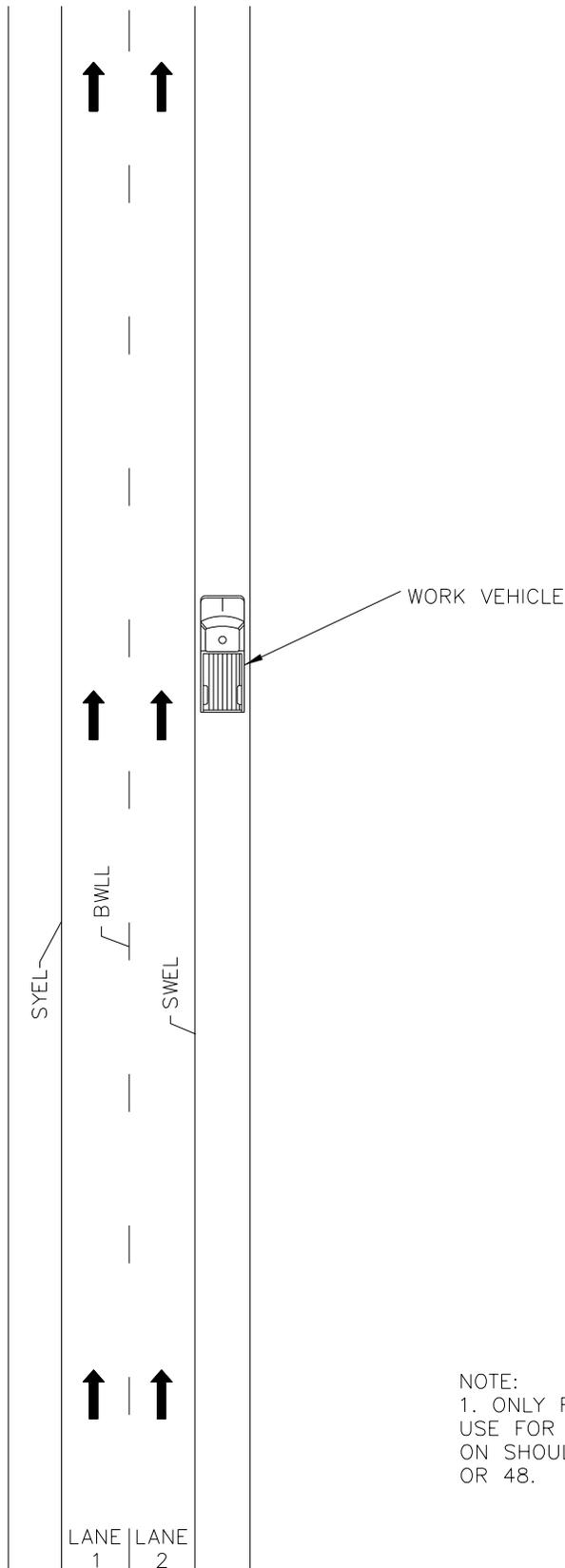
HNTB

DATE: 03-31-2020



**MAINE TURNPIKE AUTHORITY
TRAFFIC CONTROL DETAIL**

**WORK BEYOND THE SHOULDER
SHORT TERM (UP TO 12 DAYLIGHT HOURS)**



DETAIL 4A NOT TO SCALE

HNTB

DATE: 03-31-2020



MAINE TURNPIKE AUTHORITY
TRAFFIC CONTROL DETAIL

SINGLE VEHICLE ON SHOULDER
SHORT DURATION (< 1 HOUR)

2 DELINEATORS — 500'

TRUCK MOUNTED ATTENUATOR (OPTIONAL)

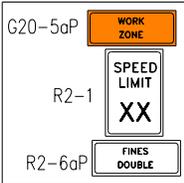
EXIT OPEN AHEAD
E5-2 MOD
OR
EXIT OPEN AHEAD
E5-2



SEE NOTE 5

W16-9P

SEE NOTES

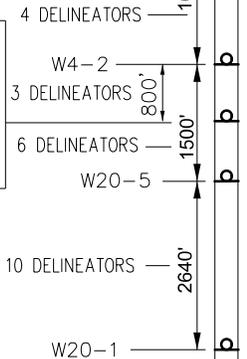


OPTIONAL SPEED LIMIT SIGN PACKAGE LOCATION

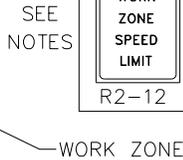
DRUMS OR CONES SPACED ONE @ EVERY SKIP LINE (40' INTERVALS) ON TAPERS



SEE NOTES



LANE 1 | LANE 2



SEE NOTES

NOTES:

1. USE OF REGULATORY REDUCED SPEEDS SHALL BE USED WHEN WORKERS ARE PRESENT OR SITE CONDITIONS WARRANT. 10MPH SPEED REDUCTION MANDATORY; IF REDUCTION GREATER THEN 10MPH IS WARRANTED THEN REQUEST THROUGH MTA ENGINEERING. SPEED LIMIT SIGN IS ONLY NEEDED ON ONE SIDE OR THE OTHER. SPEED LIMIT SIGNS SHALL BE 5' OFF GROUND. SIGN W3-5 NOT NEEDED FOR A 10 MPH REDUCTION.
2. OPTIONAL - THE SPEED LIMIT SIGN PACKAGE MAY BE POST MOUNTED ON THE LEFT SHOULDER. POST MOUNTED SIGNS SHALL BE COVERED WHEN NOT IN USE.
3. OPTIONAL - THE WORK ZONE AND FINES DOUBLE SIGN MAY BE MOUNTED ON A SEPARATE EASEL OR POST.
4. OPTIONAL - THE FINES DOUBLE SIGN MAY BE OMITTED.
5. EXIT SIGN E5-1 MAY BE WHITE LETTERS AND ARROW ON GREEN BACKGROUND OR BLACK LETTERS AND ARROW ON ORANGE BACKGROUND. SIGN SHALL BE MOUNTED 5 FEET FROM BOTTOM OF SIGN TO PAVEMENT SURFACE.



DRUMS OR CONES SPACED @ 35' INTERVALS ON TAPERS

3 DELINEATORS
800'

DRUMS OR CONES SPACED ONE @ EVERY OTHER SKIP LINE (80' INTERVALS) ON TANGENTS (TYP.)

840'
BUFFER SPACE
4 DELIN.

840'
TAPER
4 DELIN.

FLASHING ARROW BOARD ON SHOULDER

1 DELIN. 280'



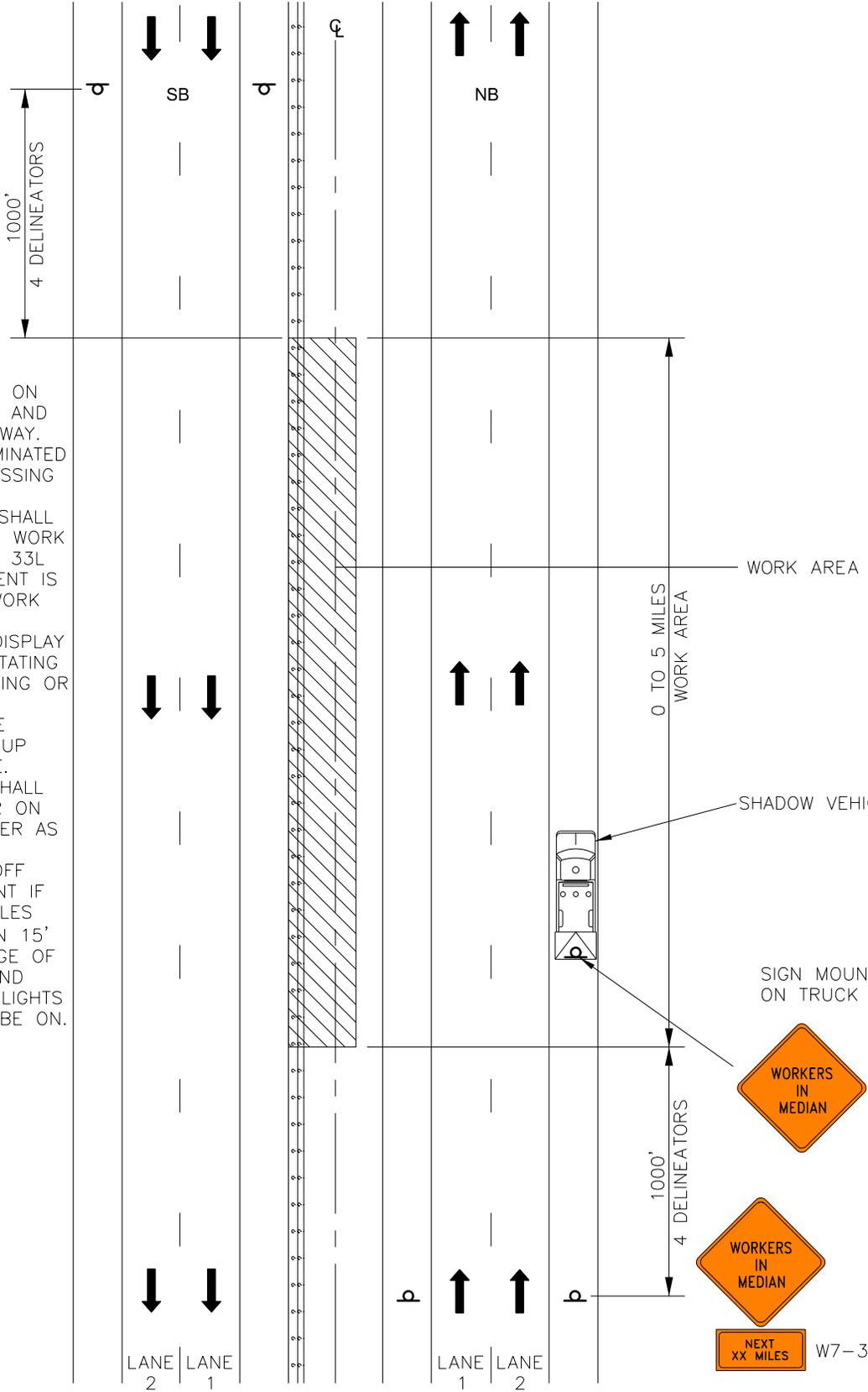
DETAIL 42B NOT TO SCALE



DATE: 03-31-2020



MAINE TURNPIKE AUTHORITY
TRAFFIC CONTROL DETAIL
TRAVEL LANE CLOSURE AT AN EXIT RAMP
WORK AREA AFTER RAMP



- NOTES:
1. SIGNS REQUIRED ON BOTH NORTHBOUND AND SOUTHBOUND ROADWAY. SIGNS MAY BE ELIMINATED ON ROADWAY IF PASSING LANE IS CLOSED.
 2. NO EQUIPMENT SHALL BE LOCATED WITHIN WORK AREA. USE DETAIL 33L OR 35L IF EQUIPMENT IS REQUIRED WITHIN WORK AREA.
 3. VEHICLE MUST DISPLAY HIGH-INTENSITY ROTATING FLASHING, OSCILLATING OR STROBE LIGHT.
 4. SHADOW VEHICLE SHOULD BE A PICKUP TRUCK IF POSSIBLE. SHADOW VEHICLE SHALL BE PARKED AS FAR ON THE RIGHT SHOULDER AS POSSIBLE
 5. PARK VEHICLE OFF SHOULDER PAVEMENT IF POSSIBLE. IF VEHICLES PARKED MORE THAN 15' OFF THE RIGHT EDGE OF PAVEMENT OR BEHIND BARRIER, WARNING LIGHTS NOT REQUIRED TO BE ON.

DETAIL 48 NOT TO SCALE

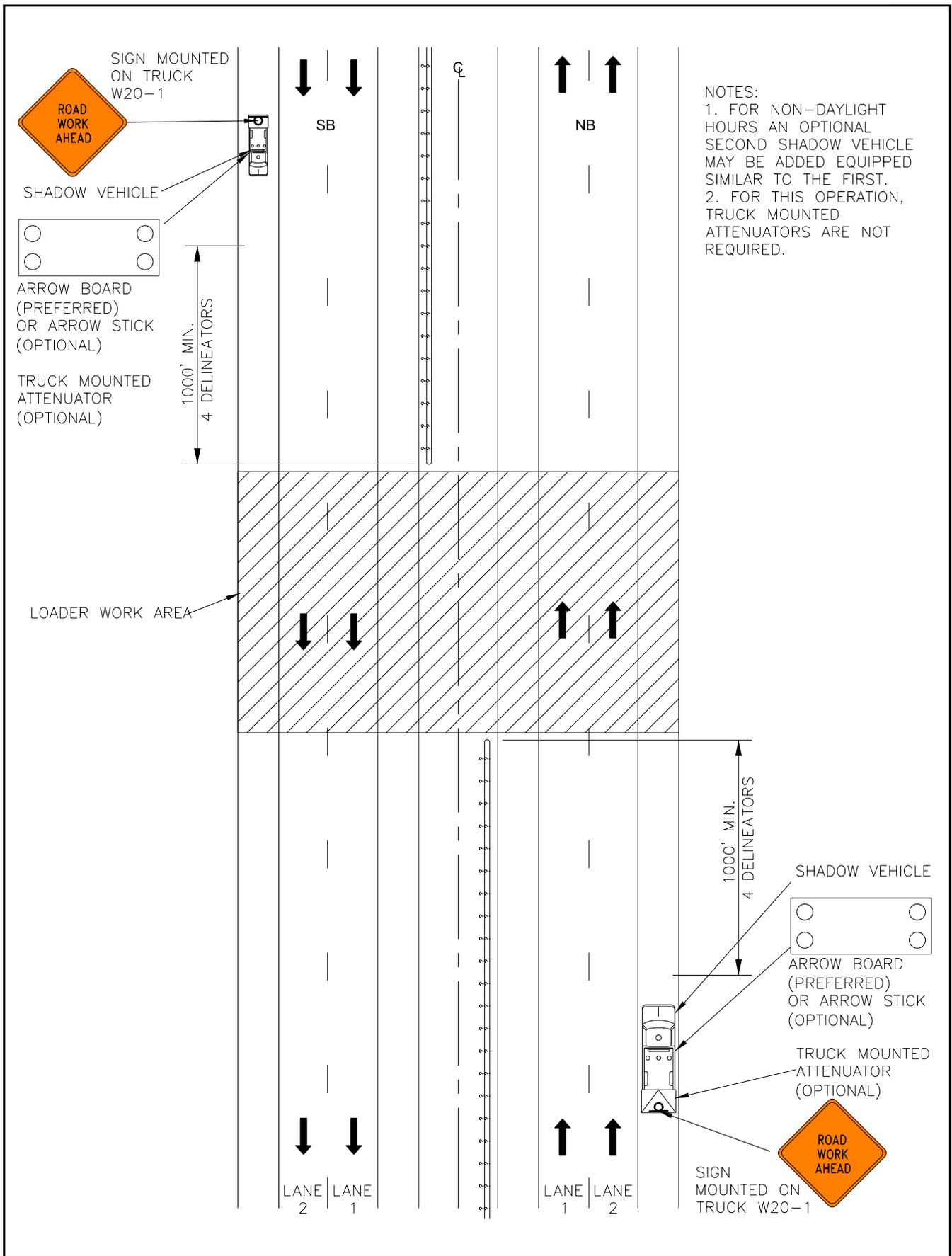


DATE: 03-31-2020



MAINE TURNPIKE AUTHORITY
TRAFFIC CONTROL DETAIL

MOBILE OPERATION - WORK IN MEDIAN
SHORT TERM (UP TO 12 DAYLIGHT HOURS)



DETAIL 49 NOT TO SCALE

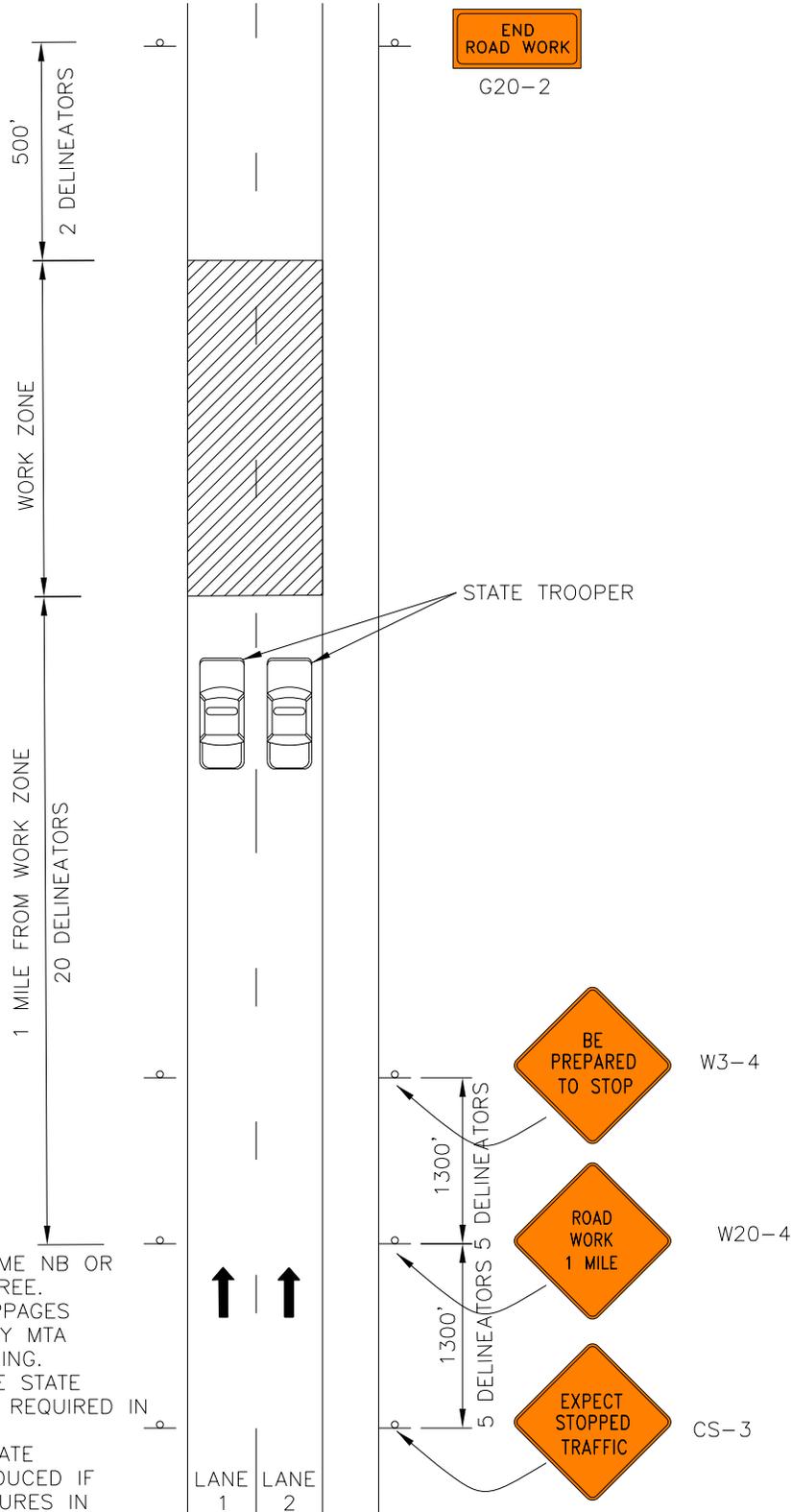


DATE: 03-31-2020



MAINE TURNPIKE AUTHORITY
TRAFFIC CONTROL DETAIL

SNOW REMOVAL IN MEDIAN OPENING
SHORT TERM (UP TO 1 HOUR)



NOTES:

1. SET UP IS THE SAME NB OR SB, TWO LANES OR THREE.
2. ALL MAINLINE STOPPAGES SHALL BE APPROVED BY MTA DIRECTOR OF ENGINEERING.
3. A MINIMUM OF ONE STATE TROOPER PER LANE IS REQUIRED IN THREE LANE SECTION.
4. THE NUMBER OF STATE TROOPERS MAY BE REDUCED IF THERE ARE LANE CLOSURES IN PLACE.

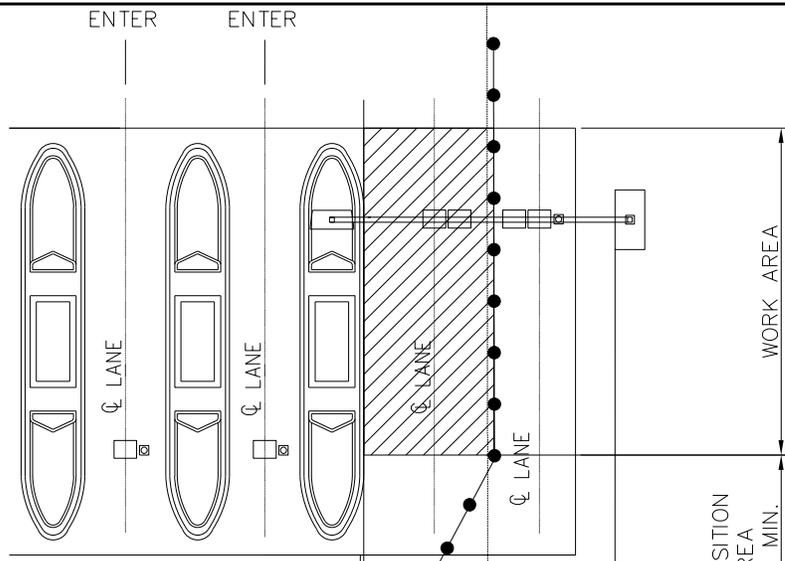
DETAIL 50 NOT TO SCALE



DATE: 03-31-2020



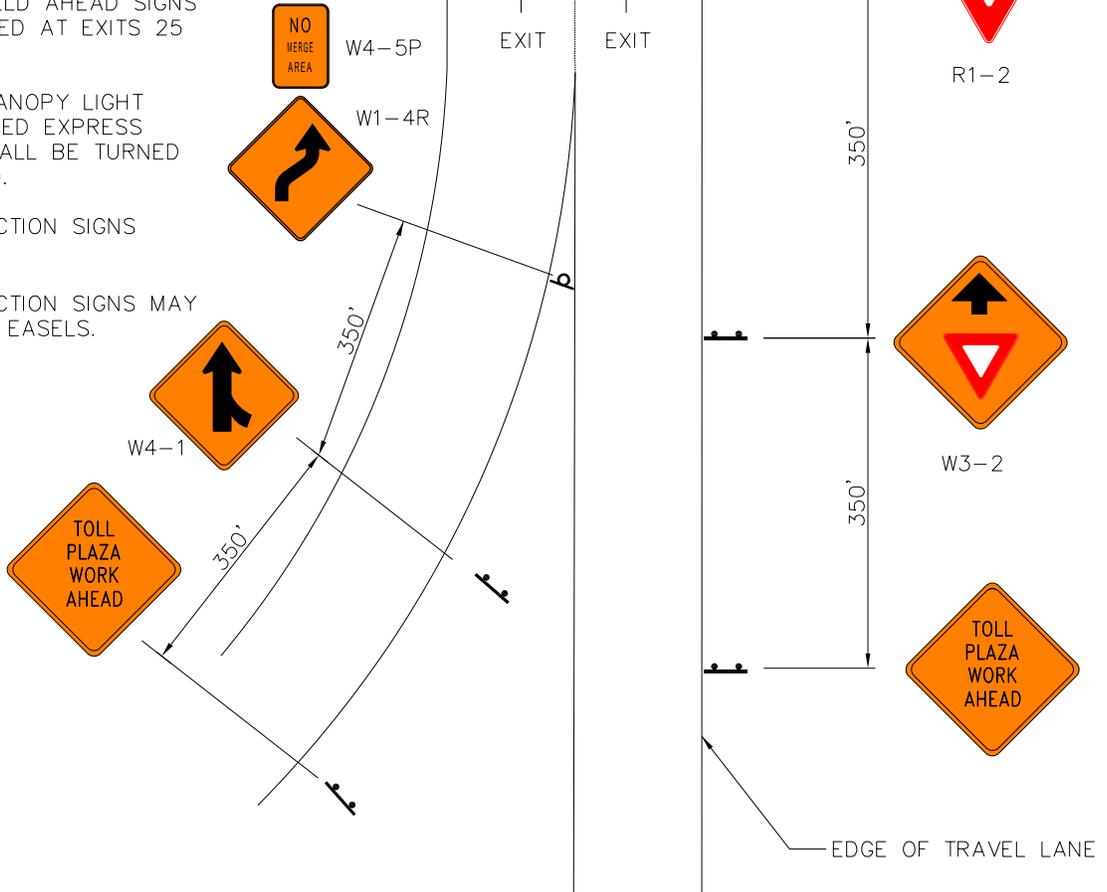
**MAINE TURNPIKE AUTHORITY
TRAFFIC CONTROL DETAIL
STOPPING MAINLINE TRAFFIC**



NOTES:

1. EXACT LAYOUT TO BE APPROVED BY THE MTA ENGINEERING DIRECTOR, SOME SITES MAY VARY.
2. YIELD AND YIELD AHEAD SIGNS ARE NOT REQUIRED AT EXITS 25 AND 46.
3. THE GREEN CANOPY LIGHT ABOVE THE CLOSED EXPRESS EXITING LANE SHALL BE TURNED OFF OR COVERED.
4. ALL CONSTRUCTION SIGNS SHALL BE 4'X4'.
5. ALL CONSTRUCTION SIGNS MAY BE MOUNTED ON EASELS.

REFLECTORIZED CONES OR DRUMS @ 20' (TYP.)



EXPRESS EXITING LANES – LANE CLOSURE
 EXITS 19, 25, 32, 36, 42, 45, 46, 47, 48, 53

DETAIL TP-50A NOT TO SCALE



DATE: 03-31-2020

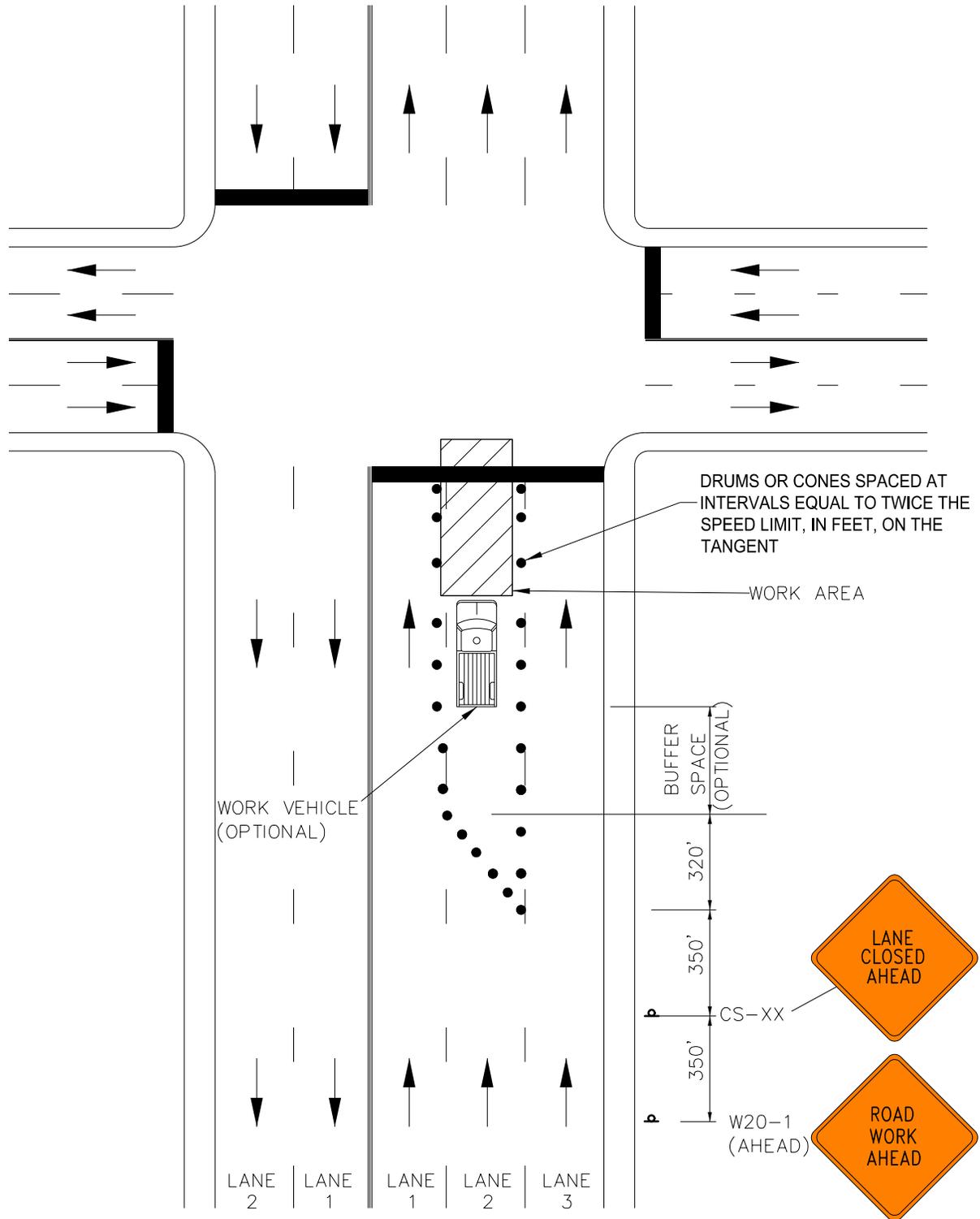


MAINE TURNPIKE AUTHORITY
 TOLL PLAZA TRAFFIC CONTROL

LANE CLOSURE - EXPRESS EXITING LANES

NOTES:

1. TO BE USED FOR PAVEMENT MARKING INSTALLATION, ONLY DURING NON-PEAK HOURS.
2. ALL CONSTRUCTION SIGNS SHALL BE 4'X4'.
3. ALL CONSTRUCTION SIGNS MAY BE MOUNTED ON EASELS.



DETAIL TP-50B NOT TO SCALE

HNTB

DATE: 03-31-2020

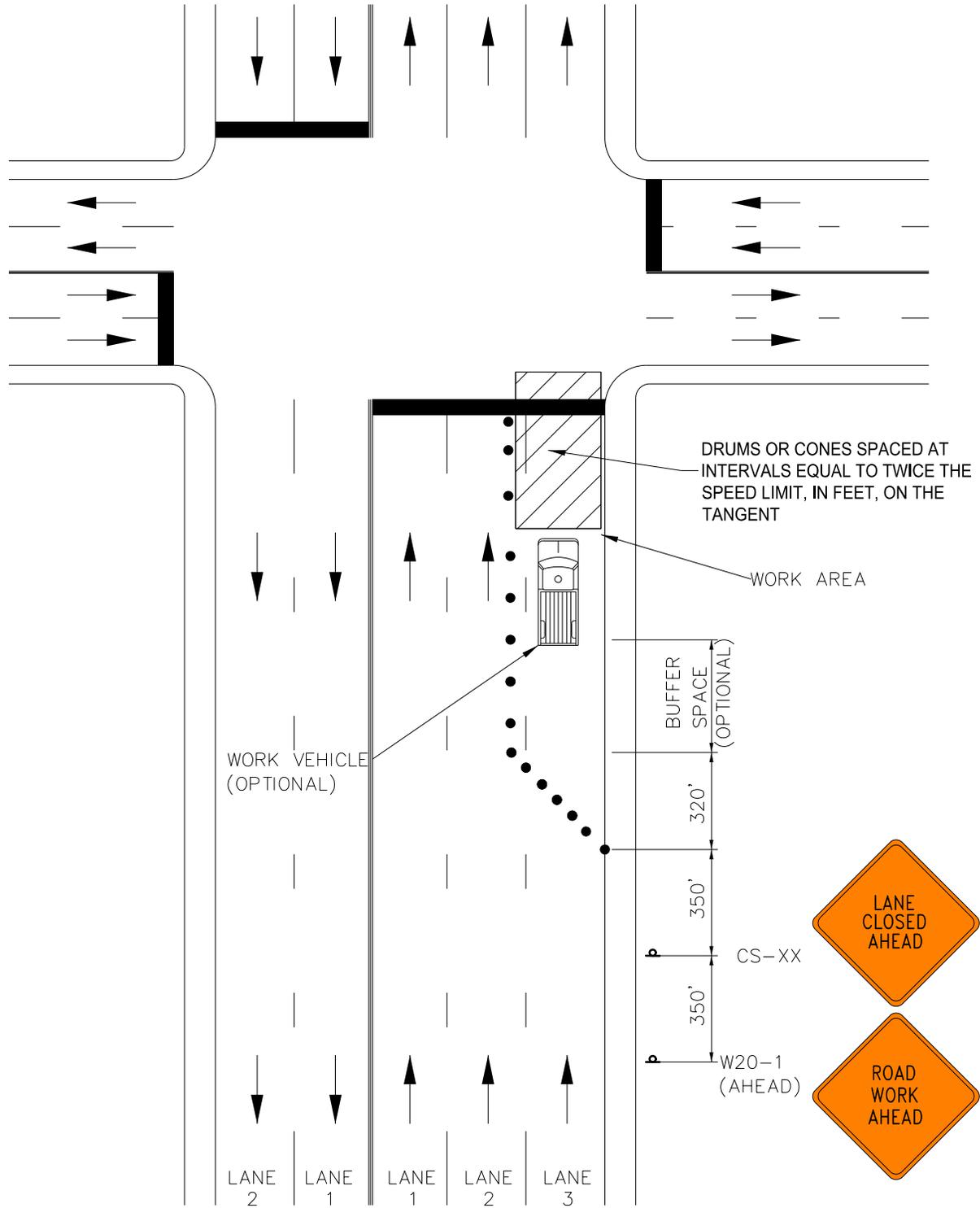


MAINE TURNPIKE AUTHORITY
TOLL PLAZA TRAFFIC CONTROL

MIDDLE LANE CLOSURE AT AN
 INTERSECTION WITH A LOCAL ROAD

NOTES:

1. TO BE USED FOR PAVEMENT MARKING INSTALLATION, ONLY DURING NON-PEAK HOURS.
2. ALL CONSTRUCTION SIGNS SHALL BE 4'X4'.
3. ALL CONSTRUCTION SIGNS MAY BE MOUNTED ON EASELS.



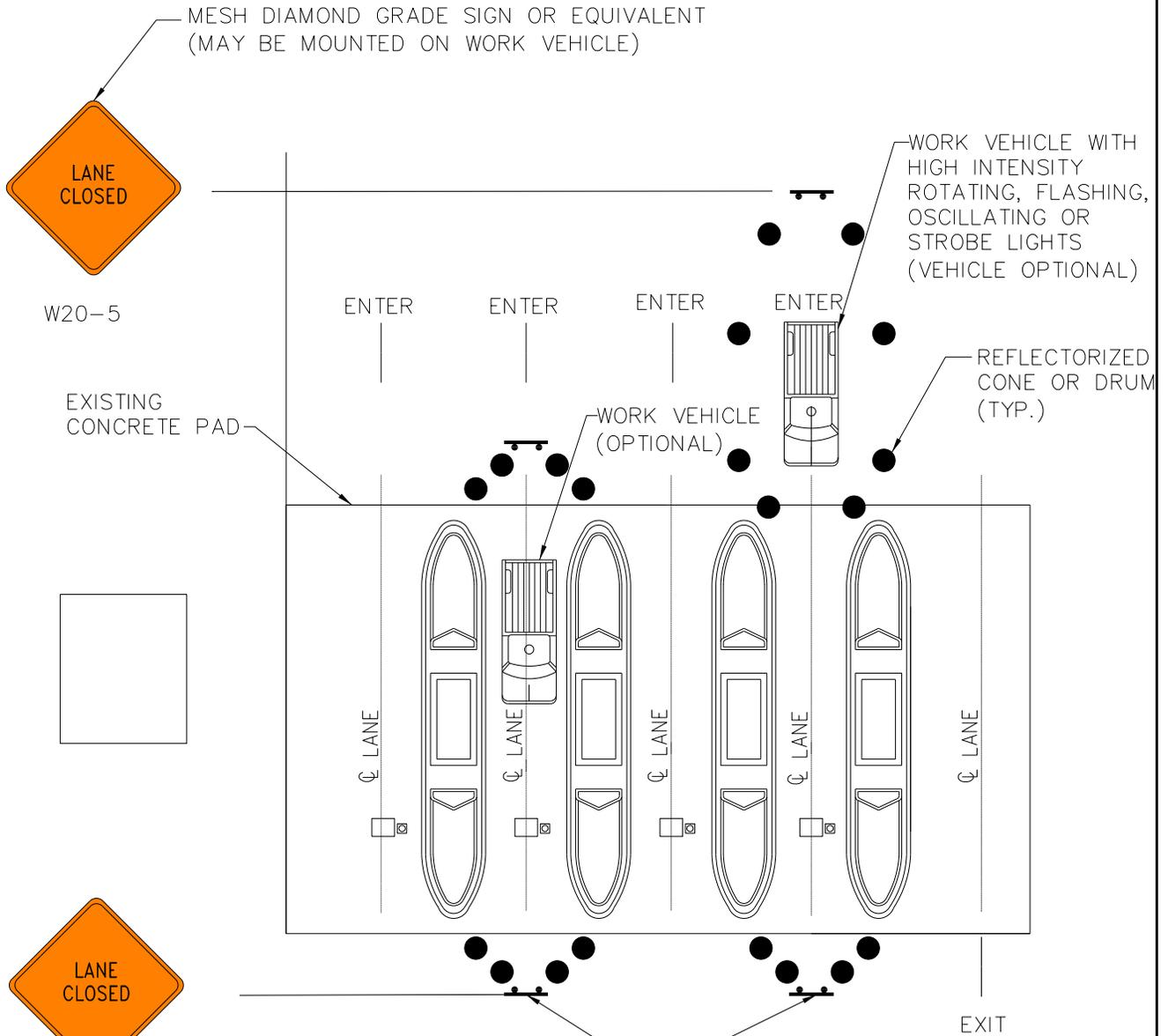
DETAIL TP-50C NOT TO SCALE



DATE: 03-31-2020



MAINE TURNPIKE AUTHORITY
TOLL PLAZA TRAFFIC CONTROL
OUTSIDE LANE CLOSURE AT AN
INTERSECTION WITH A LOCAL ROAD



NOTES:

1. A SINGLE LANE CLOSURE IS REQUIRED FOR CONSTRUCTION AND MAINTENANCE WORK IN THE LANE OR ON AN ISLAND.
2. CANOPY LIGHT ABOVE CLOSED LANE SHALL BE RED.
3. CONES SHALL BE PLACED SO THAT VEHICLES IN ADJACENT LANES CANNOT ENTER THE CLOSED LANE.

THIS SIGN AND DRUMS ARE REQUIRED ON REVERSIBLE LANES WHEN THE ADJACENT LANE CARRIES TRAFFIC IN THE OPPOSITE DIRECTION TO THE CLOSED LANE.

4. ALL CONSTRUCTION SIGNS SHALL BE 4'X4' WITH THE EXCEPTION OF THE MESH FABRIC SIGNS WHICH MAY BE 3'X3'.
5. ALL CONSTRUCTION SIGNS MAY BE MOUNTED ON EASELS.

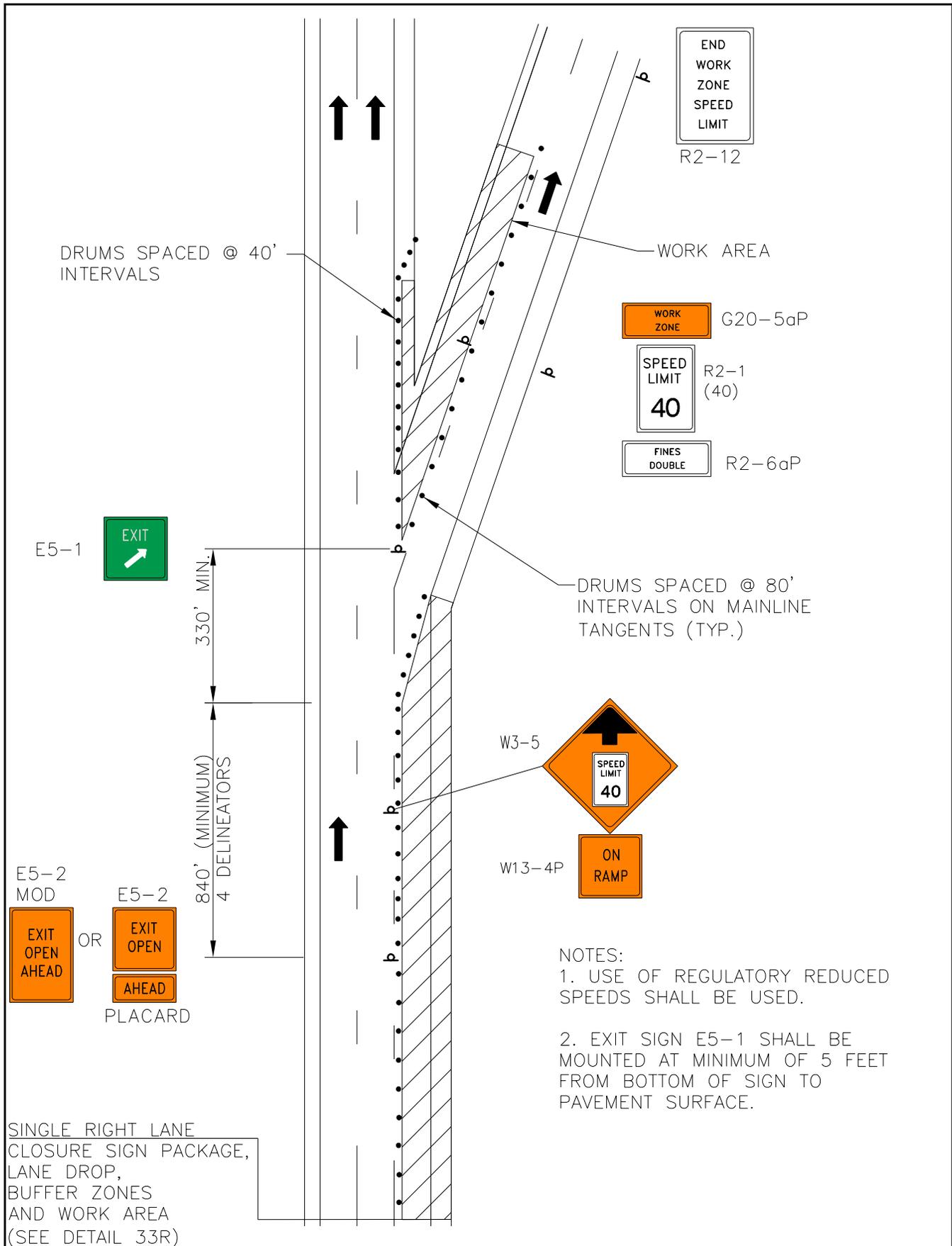
DETAIL TP-52 NOT TO SCALE



DATE: 03-31-2020



MAINE TURNPIKE AUTHORITY
TOLL PLAZA TRAFFIC CONTROL
 STATIONARY LANE CLOSURE
 BARRIER AND SIDE PLAZAS
 INTERMEDIATE TERM (> 1 HOUR TO 3 DAYS)



APPENDIX B NOT TO SCALE
 EXIT 44 NB OFF
 DETAIL 3A



MAINE TURNPIKE AUTHORITY
 TRAFFIC CONTROL DETAIL

EXIT RAMP LEFT LANE CLOSURE

DATE: 05-19-2020