

MAINE TURNPIKE AUTHORITY

ADDENDUM NO. 2

CONTRACT 2015.11

WEST GARDINER BARRIER TOLL PLAZA
OPEN ROAD TOLLING CONVERSION
MILE 100.2

The bid opening date is Thursday 3/19/15 at 11am.

The following changes are made to the Specifications and Plans.

SPECIFICATIONS

- Specification 719, SP-305 has been deleted and replaced in its entirety. Revised specification attached hereto.

PLANS

Sheet S-16 Sheet 91 of 118 ORT Slab Plan is deleted and replaced, attached hereto.

Sheet S-17 Sheet 92 of 118 ORT Slab Foundation Details is deleted and replaced, attached hereto.

QUESTIONS

The following questions have been raised to the Maine Turnpike Authority. Answers to the questions are noted. Bidders shall utilize this information in preparing their bid.

Question 1: All items have note E for hot rubber joint seal. Sheet 5 of 18 note 6 indicates joint seal on surface courses only. Could you please clarify

Answer: Complimentary Note E references Section 401.17 Joints, this section applies to all joints for all lifts. Sheet 5 of 118 Note 6 and the fifth paragraph in Section 401.17 indicate hot rubber sealant on the surface course or as directed by the Resident.

Question 2: Can the milling and paving of the existing pavement be completed during the day?

Answer: Milling and paving operations can occur within any allowable lane closure or when the work area is behind barrier.

Question 3: Can a 24 inch rock socket be used in place of a 36 inch rock socket within the 36 inch drilled shaft?

Answer: Yes, a 24 inch rock socket is acceptable following the attached alternate detail Figure 1 for the Structure Foundation on S-09 sheet 84 of 118.

Question 4: Per Note #6 on drawing S-07 (p.82) bars marked B413 are to be coupled. Can you point out where this bar mark is located?

Answer: Note 6 has been deleted and not replaced.

Question 5: Can epoxy coated #6 hoops be substituted for the epoxy coated #6 spirals in the drilled shafts on drawing S-09 (p.84)?

Answer: Yes, hoop bars may be substituted for spirals in the 36 inch diameter drilled shafts.

Question 6: Are there 8 drilled shafts that have epoxy coated bars and spirals?

Answer: Yes, there are eight (8) 36 inch diameter drilled shafts with epoxy coated bars and spirals.

ATTACHMENTS

- Specification Sheets (1 page)
- Plan Sheets (2 pages)
- Figure Sheet (1 page)

Notes: The above items shall be considered as part of the bid submittal.

The total number of pages included with this addendum is Seven (7).

All bidders are requested to acknowledge the receipt of the Addendum No. 2 by signing below and faxing this sheet to Nathaniel Carll, Purchasing Department, Maine Turnpike Authority at 207-871-7739. Bidders are also required to acknowledge receipt of this Addendum No. 2 on Page P-22 of the bid package.

Business Name

Print Name and Title

Signature

Date

March 17, 2015

Very truly yours,

MAINE TURNPIKE AUTHORITY

Nathaniel Carll
Purchasing Department
Maine Turnpike Authority

SPECIAL PROVISION

SECTION 719

SIGNING MATERIAL

Delete Section 719.01 Reflective Sheeting and replace with the following:

Section 719.01 Reflective Sheeting

All Reflective Sheeting shall be manufactured by 3M.

Reflective sheeting for all signs shall meet at a minimum the requirements for, ASTM 4956 – Type XI (Full cubed).

Reflective sheeting, used in sign construction, shall have been manufactured within the six months immediately prior to the fabrication of each sign. Upon delivery at the job site of each shipment of signs, a letter of certification shall be provided that the reflective sheeting conforms to the requirements.

For Type 1 Guide Signs, all reflective sheeting shall be color matched on each sign unit.

All warning signs shall be fluorescent yellow except for Ramp Advisory Speed signs which shall be yellow. Yellow on Type 1 Guide Signs shall be Yellow.

All Construction Series signs shall be fluorescent orange.

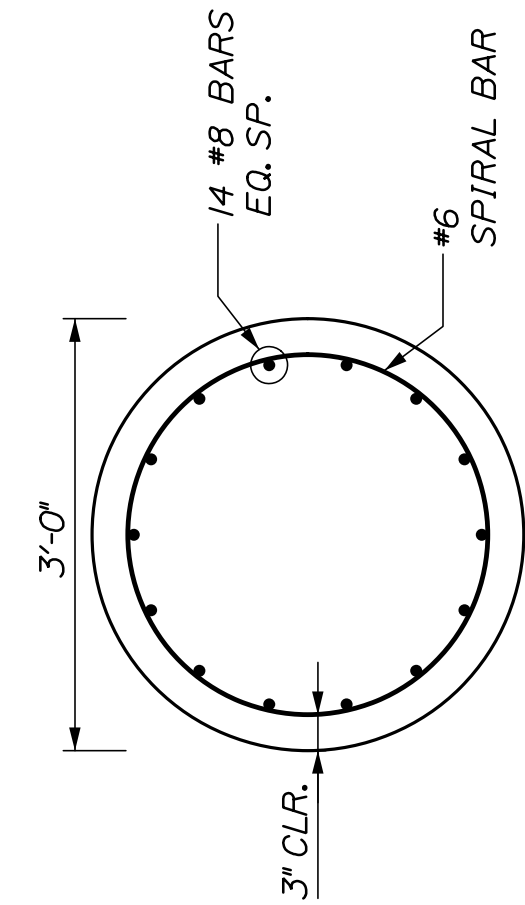
All Pedestrian Signs shall be fluorescent yellow-green.

EZ-PASS Purple shall conform to the FHWA Purple color block.

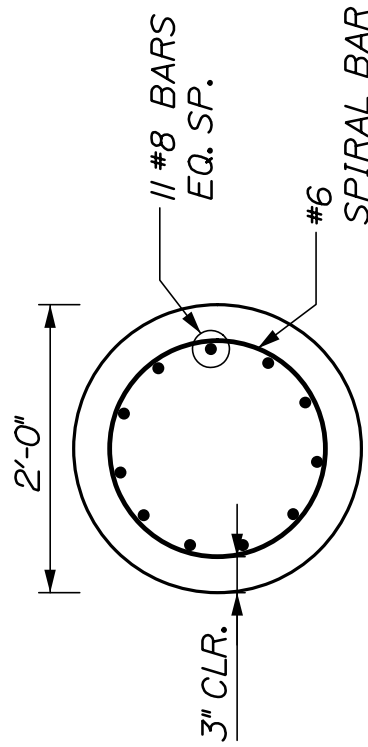
Delete Section 719.02 Demountable High Intensity Reflectorized Letters, Numerals, Symbols, and Borders and replace with the following:

719.02 Direct Applied Reflectorized Letters, Numerals, Symbols, and Borders Direct Applied reflectorized letters, numerals, symbols and borders shall consist of cut out sheeting shall meet at a minimum the requirements for ASTM 4956 – Type XI sheeting.

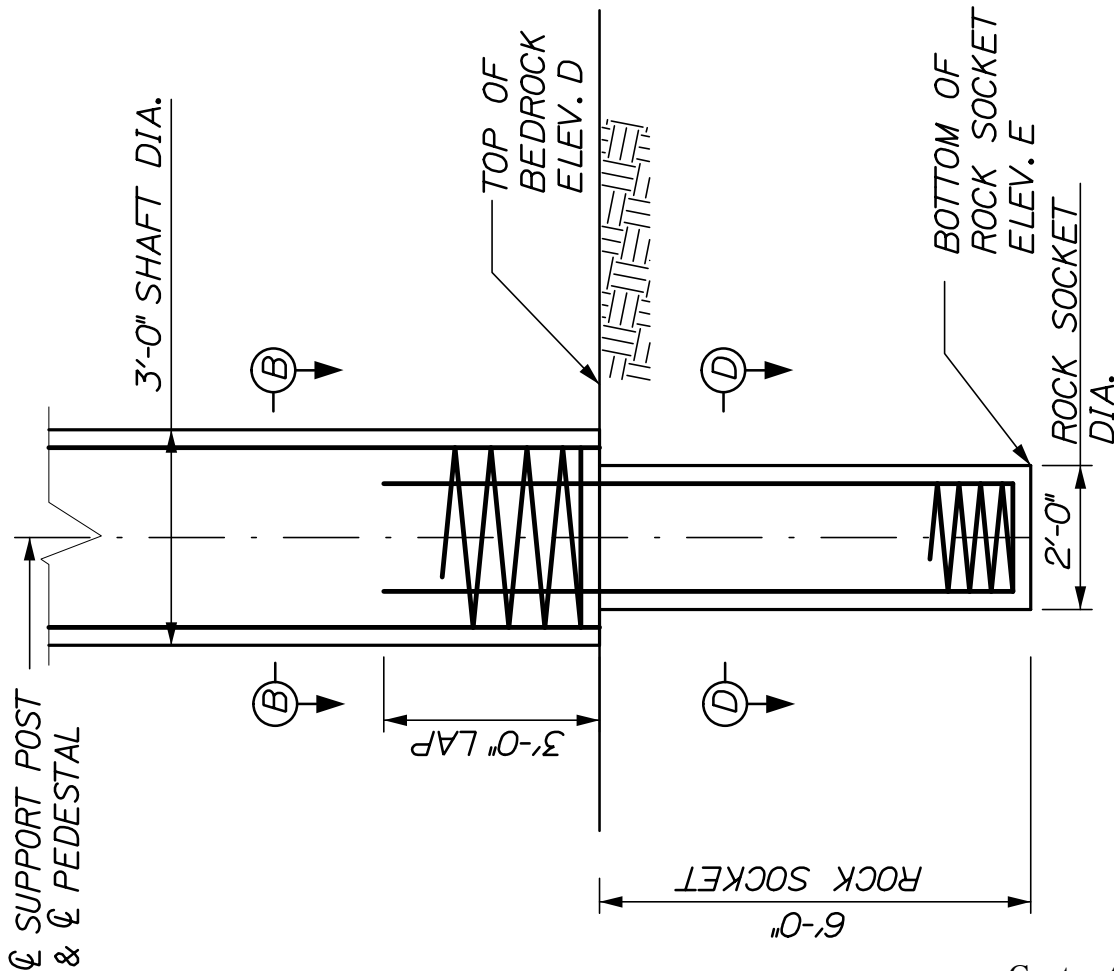
All Type 1 Guide Signs shall meet at a minimum the requirements for ASTM 4956 –Type XI (Full Cubed) sheeting.



SECTION B-B
SCALE: $\frac{3}{4}'' = 1'-0''$



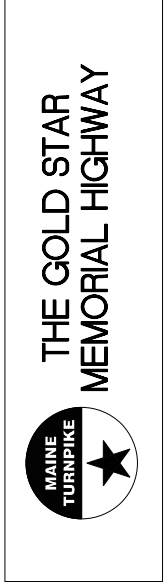
SECTION D-D
SCALE: $\frac{3}{4}'' = 1'-0''$



ORT STRUCTURE FOUNDATION
SCALE: $\frac{3}{8}'' = 1'-0''$

MAINE TURNPIKE AUTHORITY
DRILLED SHAFT ALTERNATE
WITH 2'-0" DIA. ROCK SOCKET

THE GOLD STAR
MEMORIAL HIGHWAY



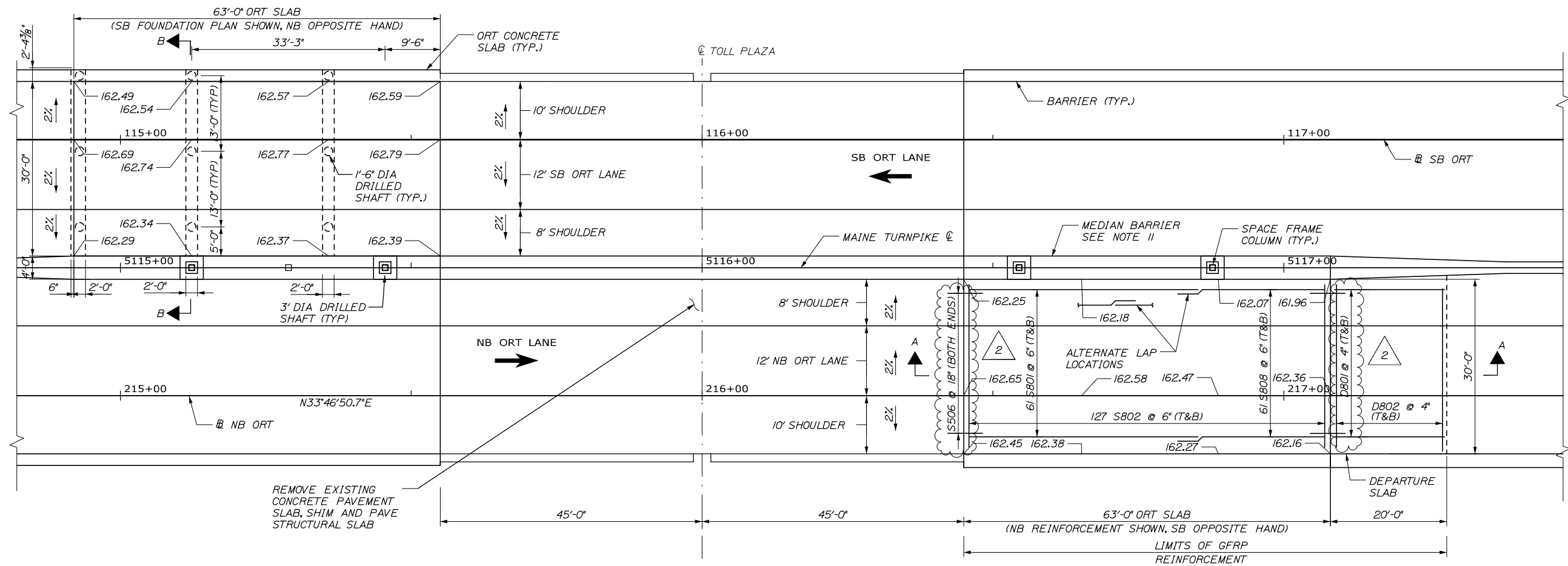
HNTB

DATE: 03/17/15

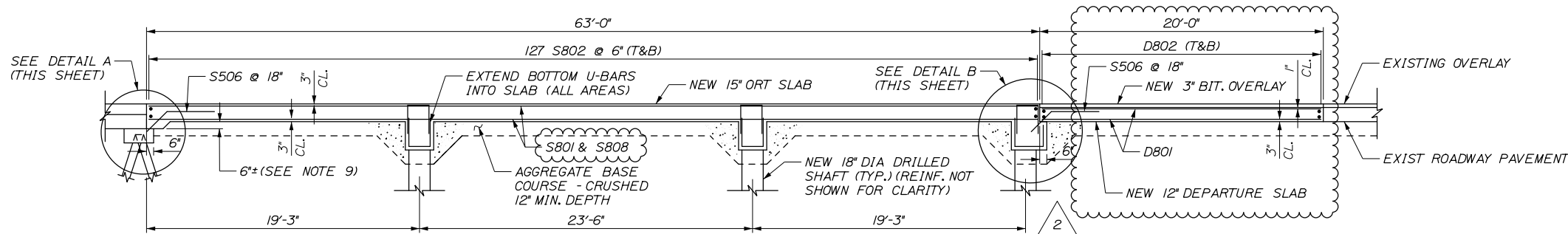
CONTRACT: 2015.11

FIGURE 1 OF 1

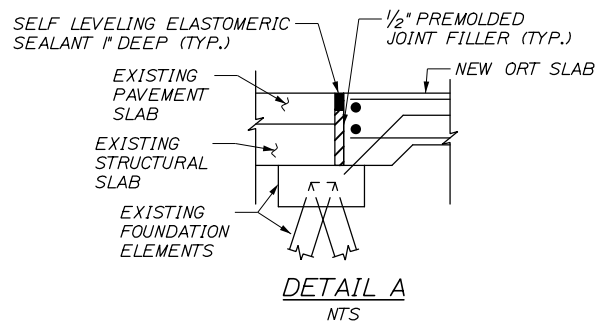
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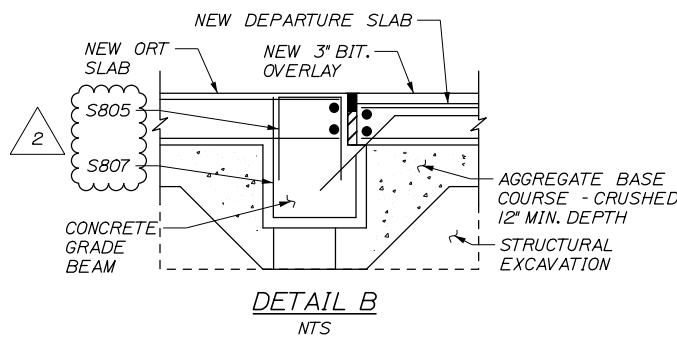
ORT SLAB PLAN
SCALE: 1" = 10'-0"



SECTION A-A
1" = 5'-0"
(NB ORT LANE SHOWN, SB ORT LANE SIMILAR)



DETAIL A
NTS



DETAIL B
NTS

- ORT SLAB NOTES:**
1. THE CONTRACTOR SHALL CONSTRUCT AN ORT SLAB IN THE NORTHBOUND AND SOUTHBOUND LANES AS SHOWN ON THE PLANS. EACH SLAB WILL HAVE A CROWN LINE BETWEEN THE TRAVEL LANE AND THE 10 FT SHOULDER AS PER THE SLOPES SHOWN ON THE PLANS. THE PROPOSED APPROACH AND DEPARTURE 8 FT SHOULDER PAVEMENT SHALL BE WARPED TO MATCH THE ORT SLAB 8 FT SHOULDER SLOPES. NO SHOULDER BREAK SHALL BE CONSTRUCTED ON THE ORT SLAB BETWEEN THE TRAVEL LANES AND THE 8 FT SHOULDERS.
 2. EACH ORT SLAB SHALL BE PLACED IN ONE CONTINUOUS PLACEMENT. CONSTRUCTION JOINTS ARE NOT ALLOWED. SOFT CUT JOINTS SHALL BE CONSTRUCTED AT APPROXIMATE 10-FT SPACING, AS DIRECTED BY THE ENGINEER.
 3. FINISHING OF ORT SLABS SHALL ADHERE TO WEARING SURFACE TOLERANCES PER 502 SPECIAL PROVISION (502.14).
 4. CONCRETE FOR ORT SLABS SHALL BE CLASS AAA DECK WITH 5LBS/CY OF SYNTHETIC FIBER REINFORCING.
 5. ORT SLAB SHALL HAVE A TRANSVERSED BROOMED FINISH.
 6. ALL REINFORCING BAR IN ORT SLAB, GRADE BEAMS, AND 18" DIA. DRILLED SHAFTS SHALL BE GFRP.
 7. ALL REINFORCING BAR SUPPORTS AND TIES FOR GFRP BARS SHALL BE NON-METALLIC.
 8. DEPARTURE SLAB CROSS SLOPE SHALL FOLLOW THE CROSS SLOPE OF THE ORT SLAB.
 9. HAUNCH HEIGHT SHALL BE ADJUSTED AS NEEDED TO MATCH FIELD CONDITIONS.
 10. FOR ORT AND DEPARTURE SLAB REINFORCING SCHEDULE, SEE SHEET S-17.
 11. FOR ADDITIONAL REINFORCEMENT IN MEDIAN BARRIER AT ORT SLAB, SEE SHEET BAR-05.
 12. ELEVATION INFORMATION REPRESENTS FINISHED SURFACE.

Scale: AS NOTED

No.	Revision	By	Date
2	ADDENDUM #2 - REBAR MOD.	GLS	3/15

Designed by:

HNTB

CONSULTANT PROJECT MANAGER: R. Bruce Munger, P.E.

	By	Date	By	Date	
Designed	AJF	02/15	Checked	GLS	02/15
Drawn	MPC	02/15	In Charge of	RAL	02/15

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MAINE TURNPIKE

THE GOLD STAR MEMORIAL HIGHWAY

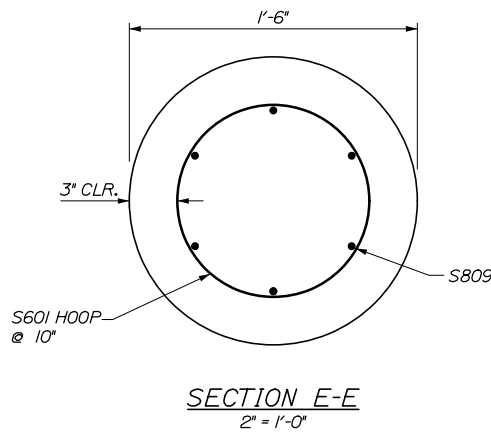
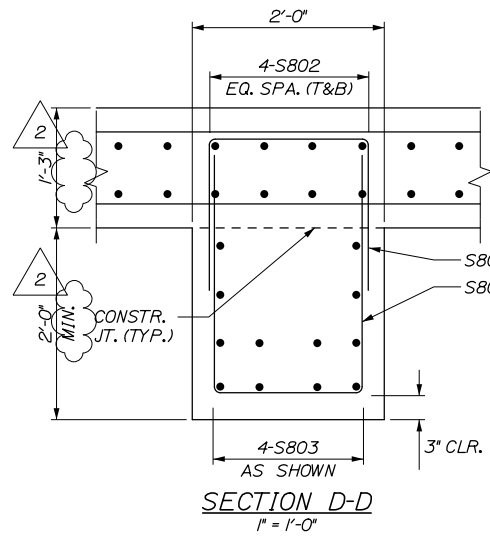
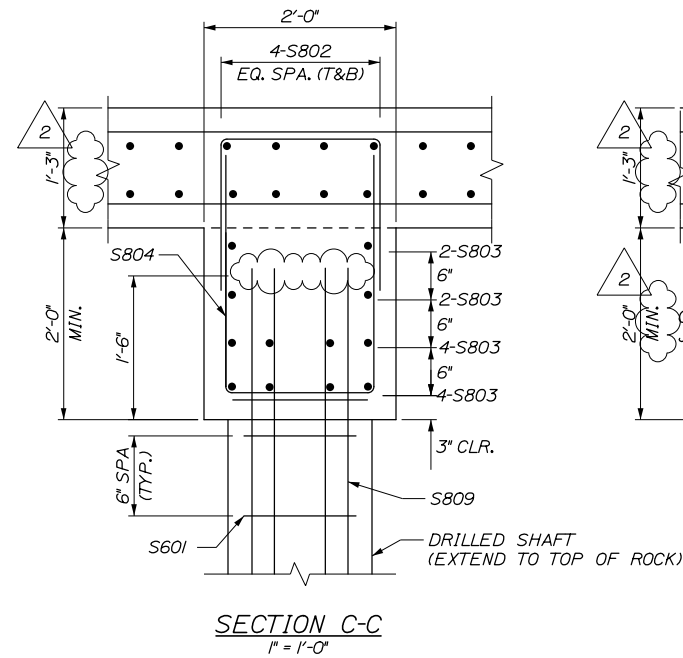
MTA PROJECT MANAGER: William Yates

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ORT CONVERSION Contract 2015.11
ORT SLAB PLAN Addendum No. 2
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SHEET NUMBER: S-16
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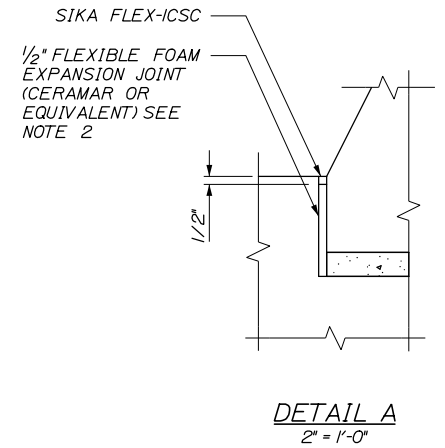
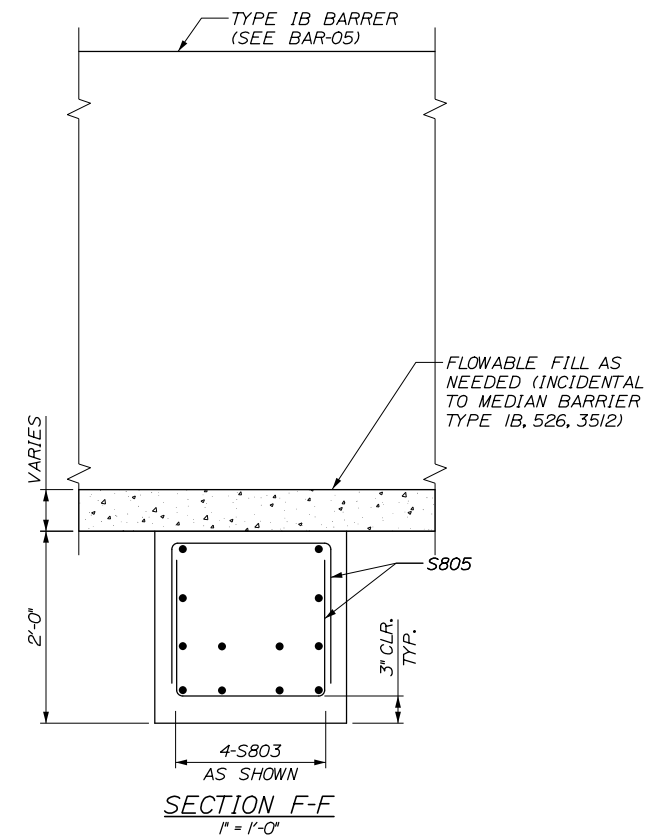
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Date: 3/17/2015



ORT SLAB DRILLED SHAFT ELEVATIONS			
ROADWAY	STATION	TOP	BOTTOM
NB	216+64.25	158.92	147.50
	216+87.75	158.82	150.00
	217+07.00	158.71	150.00
SB	115+35.75	159.12	144.00
	115+12.25	159.09	144.00
	114+93.00	159.04	144.00

NOTE: BOTTOM OF DRILLED SHAFT IS AT TOP OF ROCK.



REINFORCING SCHEDULE									
MARK	SIZE	NO.	LENGTH	TYPE	A	B	C	D	REMARKS
DEPARTURE SLAB (GFRP BARS)									
D801	8	368	19'-6"	STR					INC. 1 DIA. BAR
D802	8	240	31'-0"	STR					FIELD CUT
ORT SLAB AND GRADE BEAMS (GFRP BARS)									
S801	8	240	40'-0"	STR					
S802	8	504	29'-6"	STR					
S803	8	72	31'-10"	STR					
S804	8	236	7'-0"	STR	1'-6"	2'-9"			
S805	8	402	4'-0"	STR	1'-6"	1'-3"			
S506	5	84	6'-6"	STR	3'-8"	2'-10"	2'-4"		
S807	8	118	6'-3"	STR	2'-0"	1'-6"	2'-9"	0'-0"	
S808	8	240	26'-0"	STR					
18" DIAMETER DRILLED SHAFTS (GFRP BARS)									
S809	8	108	MAX 18' MIN 11'	STR					
S601	6	288	4'-0"	STR					

STRUCTURAL CONCRETE, SPACE FRAME PEDESTAL/ MEDIAN BARRIER TYPE II

TOP OF DRILLED SHAFT

3'-0" DIAMETER SHAFT (TYP.) SEE SHEET S-09

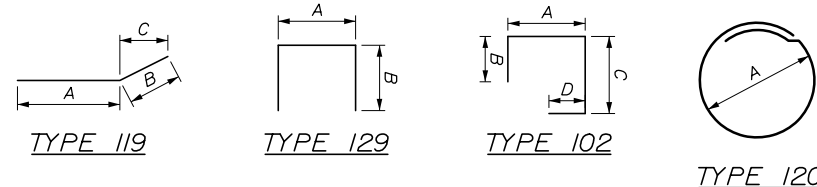
1'-6" DIAMETER SHAFT (TYP.)

SECTION B-B
1/2" = 1'-0"

NOTE: CONDUIT IN ORT SLAB NOT SHOWN FOR CLARITY. SEE SHEET E-10 FOR CONDUIT DETAILS.

NOTE: REINFORCING IN DRILLED SHAFTS NOT SHOWN FOR CLARITY

- NOTES:
- ALL BARS GFRP UNLESS NOTED OTHERWISE.
 - COST OF SIKAFLEX-IA AND FLEXIBLE FOAM EXPANSION JOINT SHALL BE INCIDENTAL TO ITEM 526, 3512, MEDIAN BARRIER TYPE IB.
 - TWO D801 BAR (T&B PER SLAB) WILL NEED TO BE PLACED PARALLEL TO BARRIER TRANSITION. FIELD CUT, D802 BARS TO ALLOW 3" COVER ON BOTH SIDES OF BAR.




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Scale: AS NOTED			
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2	ADDENDUM #2 - REBAR MOD.	AJF	3/15

Designed by:			
HNTB			
CONSULTANT PROJECT MANAGER: R. Bruce Munger, P.E.			
Designed	By	Date	Checked
Drawn	By	Date	In Charge of
	AJF	02/15	CRM
	MPC	02/15	RAL

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