

**MAINE TURNPIKE AUTHORITY**

**ADDENDUM NO. 1**

**REQUEST FOR PROPOSALS S2009.55**

**WEST GARDINER TRAVEL PLAZA**  
**TRUCK STOP ELECTRIFICATION SYSTEM**

**The following changes are made to the Specifications and Plans:**

**SPECIFICATIONS**

Page R-1, the first sentence of the first paragraph is deleted and replaced with the following:

The Maine Turnpike Authority is soliciting Proposals at the office of the Maine Turnpike Authority, 2360 Congress Street, Portland, ME until 1:00 pm, prevailing time as determined by the Authority on October 30, 2009 for the installation of a Truck Stop Electrification System for 30 trucks at the Authority's West Gardiner Service Plaza.

Page P-2 under S2009.55 Truck Stop Electrification System (TSE System), the third line item has been deleted and replaced with the following:

October 30, 2009                      Proposals due to MTA

Page P-6 the following has been added to **Section 4 - Technical Proposal**, subsection a):

- x) Unit lighting

Page P-7 and P-8 **Section 4 - Technical Proposal**, subsections i) and j) have been deleted and replaced with the following:

- i) System Extras.
  - i) Some examples.
    - (1) Internet.
    - (2) Video service.
    - (3) Cable TV.
    - (4) UV treated air.
    - (5) CO and/or smoke detectors.
    - (6) Identification of trucks idling (sensors).

- (7) Video surveillance for security (web service or monitor in plaza).
- j) System Monitoring for the POS system to control operations. Vendor needs to describe how the system will work in order to assure compliance with payment and reporting usage. Options to do this might include:
  - i) Presence detectors or similar system
  - ii) Usage reports. Provide details on how they will be generated and level of detail in reports
  - iii) Video monitoring of truck parking area to determine which truck is in which lane given potential remote nature of the POS (not in view of the parking area).

Page P-8 **Section 5 - System and Project Costs**

The following is added: As noted in the request for proposal, the Authority intends to enter negotiations with the proposer whose Proposal is determined, upon initial review, to present the best apparent value, based on the product and services proposed and the fee proposed.

Page P-8 **Section 5 - System and Project Costs**, subsection a) item i) has been deleted and replaced with the following:

- i) Cost to furnish and install the TSE system. Cost shall include all optional, supplemental, enhanced, or additional items that the vendor proposes to install.

Page SP-8, is deleted and replaced with Revised SP -8 October 14<sup>th</sup>, 2009 included in this addendum.

**The following questions have been submitted to the Maine Turnpike Authority in writing. Answers to the questions are noted. Bidders shall utilize this information in preparing their bid.**

Question 1: Can the Maine Turnpike discuss its expectation or preference regarding ownership of the TSE equipment once it is installed? Once the system is installed and paid for by the Turnpike using the grant funds, does the Turnpike desire to own the system outright; or does the Turnpike prefer a concession arrangement with the TSE provider with a revenue sharing model? We would also appreciate on what constitutes ownership of equipment, or what constitutes a fixed asset for the Turnpike e.g. any permanent or fixed structure etc. Our TSE Dual Service Towers are bolted but removable items, as is our communication cabinet. We appreciate any clarification regarding this aspect of the project to assist in providing a detailed response for the business model.

Answer: Vendor proposal needs to assume that the Maine Turnpike will own & operate the TSE system after contract completion (other than the 1 year warrantee provided by the successful vendor). Proposals should include all cost to install the complete system. The TSE equipment would be considered a fixed asset by the Authority. The Authority is a tax exempt entity under Maine Statute, please refer to Section 108.6 Taxes, Fees, Allowances, and Notices on SP-19. Per Section 5 of the RFP on SP-8, The Turnpike will consider separate proposals for the operation & maintenance of the MTA owned TSE equipment under a long-term contractual agreement. Proposals for operation & maintenance could include a revenue sharing model.

Question 2: Our system has been designed to comply with the draft proposal of the NEC (National Electrical Code) article 626 (Electrified Truck Parking Spaces). Based on this requirement and our resulting system design, 480v 3p power with an anticipated load of 375kva. Can the existing panel the service plaza utility room support this requirement? If not, should the service be drawn from another source (e.g. existing transformer, existing utility pole, etc)? Can identify the location of the power source, whether that source is the panel, a transformer, or a utility pole?

Answer: The TSE system will shall not connect to an existing Maine Turnpike panel or transformer. The TSE vendor is responsible for coordinating and contracting with the public utilities for service. All costs associated with providing utilities for the TSE system shall be the responsibility of the TSE vendor. Overhead public utilities (electric and telephone) are located in a utility easement on the east side of the site. The TSE vendor may access these utilities at the utility pole located at approximately 409+40 LT on the southeast side of the existing truck parking area received. Cable TV (including high speed internet) is located on Route 126 adjacent to the site and does not currently extend onto the MTA property.

Question 3: Who will be responsible for making primary service connection (TSE contractor, utility company, MTA)? If utility company makes the connection, who is responsible for coordinating this effort?

Answer: The TSE vendor is responsible for all efforts required to provide power to the site including coordination. All costs associated with providing utilities to the site shall be the responsibility of the TSE vendor. (Note that Central Maine Power is the electricity distributor at this location).

Question 4: Where will communications services (telephone, cable televisions, Internet) originate from (e.g., panel in service plaza building or external utility pole)? Who is responsible for coordinating the extension of these services to the TSE control panel?

Answer: No TSE required utilities will originate from the service plans building. Please refer to the answer for question 2 for additional information.

Question 5: The Turnpike's AutoCAD plans provided & sheet UP-1 of contract 2009.08 show 2-4" PVC conduits extending from near the back of the service plaza building to the northwest corner of the parking expansion. Do these conduits connect to the utility room in the service plaza building or somewhere else? Are these conduits be used for the TSE system? If so, are the conduits empty, do they contain pull strings or do they contain wires (and of so what type)?

Answer: The conduits are not for providing utilities to the TSE system. The MTA will likely allow the TSE vendor to utilize the conduits as part of the TSE controls if the TSE Vendor proposes to utilize an existing building for the point of sale. The conduits extend to the utility room in either the restaurant or the gas station building. Pull strings are located within the conduits.

Question 6: Is there an MTA or Maine DOT standard conduit trench detail that will be utilized for this project? If so, can we be provided with a copy of this or be directed to where we can obtain this detail? Are there any MTA or MaineDOT standards or restrictions regarding placing power and communications conduits within the same trench? If so what are they?

Answer: The attached MTA standard trench detail is included in this addendum for information only. It is the MTA's assumption that RJ Grondin (RJG) will be installing all conduits below finished grade. Power and communication conduits can occupy the same trench as long as it is in compliance with the NEC. Conduit spacing shall meet code requirements. MTA understands that the conduit location,

size, number, and construction details may be revised based on the selected vendor's design. MTA will coordinate this work with RJ Grondin as warranted.

Question 7: Our preference for construction sequence is that conduit installation occurs in unison with the installation of concrete foundation for TSE units. Does flexibility exist in the anticipated schedule that would allow TSE provider's contractor to work in unison with R.J. Grondin's installation of conduit? (Current schedule dictates all concrete foundation work will be completed by 4/15/10, with conduit installation to begin 4/30/10).

Answer: Flexibility is likely available in RJG's schedule to allow construction to be sequenced as negotiated with the vendor.

Question 8: RFP states that RJ, Grondin will "install all conduits beneath the pavement." Can CabAire assume R.J. Grondin's work to include excavation of trench for conduits, as well as backfill and compaction of trench fill material? Will RJ Grondin be pulling all wires through the conduit that they install or will they be providing pull strings in the conduits for the TSE contractor to pull wires at a later date?

Answer: RJ Grondin will be responsible for all the trenching and conduit installation below finished grade and all activities that go along with it. This includes excavation, backfill, and compaction. RJG will not be installing any wiring nor will they be installing pull strings. The TSE vendor will be responsible for the installation of the wires. For additional information, please refer to the answer for question 6.

Question 9: Our system design call for a point of sale (POS) terminal to be installed in a location that will be operated by a service area staff member. Is both the convenience store in the main service plaza building and the gas service station building staffed on a 24 hour basis? If so does the MTA have a preference as to which of these locations, (or both) that the POS should be placed?

Answer: The service plaza building and the Burger King are open 24 hours a day along with the gas service station. The MTA has requested (see submission requirements section 4-b-i-4) that the vendor provide the POS location and type. The MTA does not have a chosen or preferred location for the POS. In the event the vendor proposes a long-term contractual agreement to operate & maintain the TSE system for the Authority, the RFP response shall note if the vendor proposes a business arrangement with HMS Host or CN Brown and shall include a statement relative to the status of the business relationship.

Question 10: Will local or state permitting be required for this project or will all permitting and notifications to proceed be handled strictly through the MTA?

Answer: See specifications page SP-5 Section 104.2.2 for permitting requirements. Notification to proceed will be the responsibility of the Maine Turnpike Authority.

Question 11: Can we receive the electronic plans in AutoCAD 2000 format?

Answer: Yes. A CD with the requested format will be transmitted to the proposer which made the request.

Question 12: Given that we have not received the answers to our questions, and in order to submit a detailed and competitive proposal, we are asking for extension of the proposal due date. We appreciate your consideration of our for a new submission date of Friday, October 30<sup>th</sup>.

Answer: The due date for proposals to the MTA has been revised to October 30, 2009.

Question 13: In order to provide a more detailed installation timeline, we would appreciate the Turnpike's detailed discussion on the permitting process for the West Gardiner project, specifically identifying each department within the Turnpike, the Turnpike approval process and estimated time to review and approve permits. Please provide any other external local or state agencies involved in plan reviews, approvals and permitting.

Answer: See response to Question 10 regarding permitting. The permits required for the construction of the service plaza were much more extensive than the anticipated permits required for the installation of the TSE system. Other than the state electrical permit, no additional state or local reviews and approvals are anticipated. MTA approval of the systems components and construction methods are anticipated to occur as part of the negotiations. Questions relative to the system components and construction of the system shall be addressed by Steve Tartre, PE, MTA Director of Engineering and Building Maintenance or his designee. Questions relative to the existing service plaza vendors shall be addressed by Greg J. Stone, MTA Director of Public Safety or his designee. Depending on the complexity of the issues, we anticipate that responses can be expected within 10 working days.

Question 14: In Section 4 Technical Proposal item i) there is a list of "System Extras." Are the items listed under j) options/extras? Item j) lists such items as lighting, video surveillance, sensors and usage reports. Are the items listed in j) optional or required features?

Answer: Refer to the Specification changes noted on the page 1 & 2 of this addendum. Subsections i and j have been revised. Subsection i lists extras (not required) and j lists options for the Point of Sale system. Vendor should describe how the POS would work in their case.

Question 15: Is signage a required feature to be included in the cost proposal or listed as an option? Please provide any Turnpike guidelines on size, dimensions, review and approval process.

Answer: Signage adjacent to and/or on the TSE equipment and within the Service Plaza buildings providing for the direction, operation and use of the TSE by truck drivers is required and shall be included in the proposal. Vendors need to provide details on their plan to communicate to the users. Site signage or highway signing will be the responsibility of the MTA.

Question 16: If awarded this project, will we be required to submit construction drawings for all aspects of our design that bear the sign & seal of a licensed professional engineer in the state of Maine? If so, what authority or officials will subsequently be conducting a review of the various aspects (e.g. mechanical, electrical, structural, etc.) our system and site design plans?

Answer: The Contractor shall comply with all Federal and State laws. The Maine Turnpike Authority cannot provide legal advice or interpretation of Federal or Maine law. The Contractor shall review the laws pertaining to this matter. For reference, refer to [www.maine.gov/pfr/professionallicensing/laws\\_rules.htm](http://www.maine.gov/pfr/professionallicensing/laws_rules.htm). As part of the negotiations with the vendor determined to have submitted the TSE system with the best value, the MTA or their agent will review the system components and construction methods. No other external reviews are anticipated.

**Notes:** The above items shall be considered as part of the bid submittal.

The total number of pages included with this addendum is 8.

All bidders are requested to acknowledge the receipt of the Addendum No. 1 by signing below and faxing this sheet to Susan Danforth, Purchasing Manager at 207-871-7739.

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Business Name

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Print Name and Title

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Signature

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Date

October 16, 2009

Very truly yours,

MAINE TURNPIKE AUTHORITY

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Susan Danforth  
Purchasing Manager  
Maine Turnpike Authority

**104.3.8 Wage Rates and Labor Laws**

The fair minimum hourly rates determined by the State of Maine Department of Labor for this Contract.

State of Maine  
 Department of Labor  
 Bureau of Labor Standards  
 Technical Services Division  
 Augusta, Maine 04333-0045  
 Telephone (207) 623-7906

Wage Determination - In accordance with 26 MRSA §1301 et. seq., this is a determination by the Bureau of Labor Standards, of the fair minimum wage rate to be paid laborers and workers employed on the below titled project.

Title of Project ----- West Gardiner Travel Plaza Truck Stop Electrification System

Location of Project -- West Gardiner, Maine in Kennebec County

**2009 Fair Minimum Wage Rates  
 Building 2 Kennebec County  
 (other than 1 or 2 family homes)**

Occupation Title	Minimum Wage	Minimum Benefit	Total	Occupation Title	Minimum Wage	Minimum Benefit	Total
Asbestos Abatement Wrkr	\$14.00	\$0.52	\$14.52	Ironworker - Ornamental	\$21.15	\$13.90	\$35.05
Assembler - Metal Bldg	\$14.25	\$1.36	\$15.61	Ironworker - Reinforcing	\$16.00	\$0.23	\$16.23
Backhoe Loader Operator	\$16.00	\$2.88	\$18.88	Ironworker - Structural	\$21.15	\$1.63	\$22.78
Boom Truck Operator	\$17.50	\$1.40	\$18.90	Laborers/Helper/Tender	\$12.50	\$0.48	\$12.98
Bricklayer	\$23.50	\$2.37	\$25.87	Laborer - Skilled	\$16.00	\$1.48	\$17.48
Bulldozer Operator	\$17.00	\$2.88	\$19.88	Loader Op - Front End	\$14.80	\$2.04	\$16.84
Carpenter	\$18.50	\$2.92	\$21.42	Mechanic - Automatic Door	\$20.50	\$6.77	\$27.27
Carpenter - Acoustical	\$13.75	\$1.98	\$15.73	Mechanic - Maintenance	\$22.83	\$3.36	\$26.19
Carpenter - Rough	\$14.75	\$2.28	\$17.03	Mechanic - Refrigeration	\$21.00	\$3.84	\$24.84
Cement Mason/Finisher	\$16.00	\$1.97	\$17.97	Millwright	\$22.60	\$13.80	\$36.40
Commun Equip Installer	\$22.00	\$4.41	\$26.41	Oil/Fuel Burner Serv & Instr	\$20.90	\$4.68	\$25.58
Concrete Mixing Plant Op	\$16.45	\$5.02	\$21.47	Painter	\$12.50	\$1.31	\$13.81
Concrete Pump Operator	\$20.75	\$3.23	\$23.98	Paver - Bituminous	\$16.92	\$1.93	\$18.85
Crane Operator <15 Tons	\$17.00	\$2.84	\$19.84	Pipe/Stm/Sprkler Fitter	\$21.65	\$5.66	\$27.31
Crane Oprtr =>15 Tons	\$20.38	\$4.94	\$25.32	Plumber (Licensed)	\$22.00	\$3.69	\$25.69
Crusher Plant Operator	\$14.54	\$2.29	\$16.83	Plumber Hlpr/Trainee (Lic)	\$18.00	\$3.68	\$21.68
Driller - Rock	\$13.50	\$3.79	\$17.29	Roller Operator - Pavement	\$15.67	\$4.42	\$20.09
Dry-Wall Applicator	\$20.00	\$0.00	\$20.00	Roofer	\$15.66	\$1.57	\$17.23
Dry-Wall Taper & Finisher	\$18.00	\$0.62	\$18.62	Sheet Metal Worker	\$18.00	\$3.04	\$21.04
Electrician	\$21.86	\$5.15	\$27.01	Sider	\$15.00	\$0.91	\$15.91
Electrician Hlpr (Licensed)	\$14.50	\$2.36	\$16.86	Stone Mason	\$18.95	\$1.34	\$20.29
Elevator Constrctr/Installer	\$46.20	\$17.14	\$63.34	Tile Setter	\$20.00	\$3.44	\$23.44
Excavator Operator	\$17.00	\$5.40	\$22.40	Truck Driver - Light	\$15.75	\$2.13	\$17.88
Fence Setter	\$13.13	\$1.39	\$14.52	Truck Driver - Medium	\$9.50	\$0.00	\$9.50
Floor Layer	\$16.50	\$0.69	\$17.19	Truck Driver - Heavy	\$13.50	\$0.25	\$13.75
Glazier	\$17.50	\$4.31	\$21.81	Truck Driver - Tractor Trailer	\$13.50	\$2.67	\$16.17
Insulation Installer	\$16.95	\$2.51	\$19.46				

The Laborer classifications include a wide range of work duties. Therefore, if any specific occupation to be employed on this project is not listed in this determination, call the Bureau of Labor Standards at the above number for further clarification.

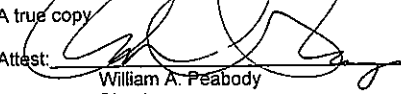
Welders are classified in the trade to which the welding is incidental.

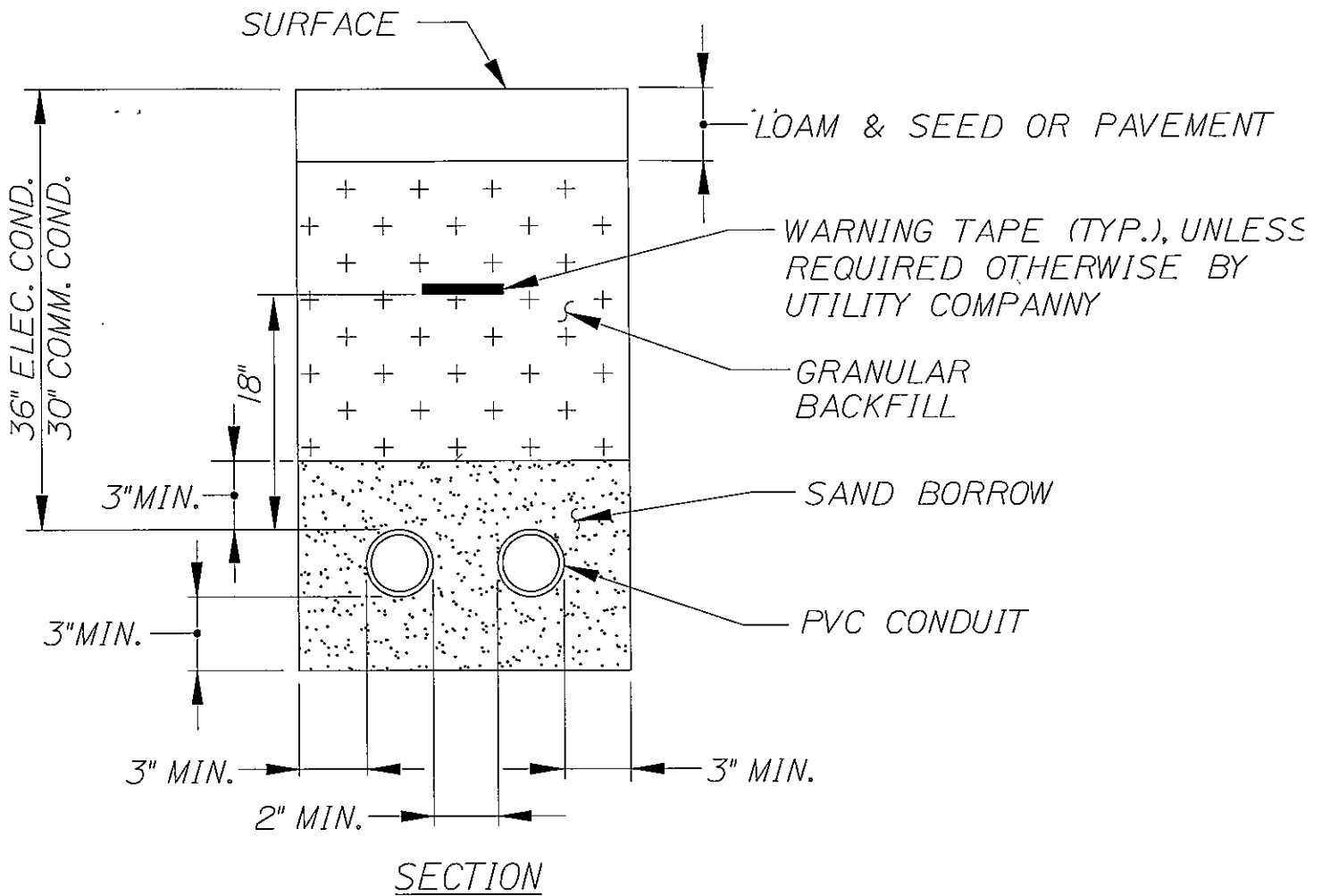
Apprentices - The minimum wage rate for registered apprentices are those set forth in the standards and policies of the Maine State Apprenticeship and Training Council for approved apprenticeship programs.

Posting of Schedule - Posting of this schedule is required in accordance with 26 MRSA §1301 et. seq., by any contractor holding a State contract for construction valued at \$50,000 or more and any subcontractors to such a contractor.

Appeal - Any person affected by the determination of these rates may appeal to the Commissioner of Labor by filing a written notice with the Commissioner stating the specific grounds of the objection within ten (10) days from the filing of these rates with the Secretary of State.

Determination No: B2-091-2009  
 Filing Date: September 21, 2009  
 Expiration Date: 12-31-2009  
 BLS 424BU (R2009) (Building 2 Kennebec County)

A true copy  
 Attest:   
 William A. Peabody  
 Director  
 Bureau of Labor Standards



NOTES

1. PVC CONDUIT IN UNPAVED AREAS SHALL BE SCHEDULE 40 AND SCHEDULE 80 UNDER PAVED AREAS.
2. CONDUIT CONFIGURATION SHALL BE AS REQUIRED TO SUIT CONDITIONS AS NECESSARY, AS APPROVED BY RESIDENT.
3. CONDUIT SPACING SHALL BE NEC REQUIREMENTS.

CONDUIT TRENCH DETAIL

NOT TO SCALE