

**MAINE TURNPIKE AUTHORITY**

**ADDENDUM NO. 1**

**CONTRACT 2014.11**

**BRIDGE REPAIRS**

**STROUDWATER RIVER BRIDGES (MILE 46.7)**

**MAINE CENTRAL RAILROAD BRIDGES (MILE 47.9)**

**The following changes are made to the Proposal, Plans, and Specifications.**

**PROPOSAL**

Proposal Sheet P-7 is deleted and replaced with Proposal Sheet P-7 (Revised 01/10/14) attached hereto. The revision modifies the unit price and bid amount of Item 652.45 Truck Mounted Attenuator to \$100.00 per calendar day.

**PLANS**

Sheet 2 of 32, General Notes Number 8 shall be added.

8. A protective shield shall be installed on the existing bridge rails, between Piers 1 and 2 on Stroudwater River NB and SB and MCRR NB. The shield shall be a minimum of 3 feet high to keep construction debris out of the river and off of the railroad right-of-way. The shielding system shall be submitted to the Resident for review and approval prior to installation. This work shall be incidental to Item 659.10 Mobilization.

Sheets 3, 20 and 21 of 32 are deleted and replaced with the attached revised plan sheets.

The construction joint locations on phase 2 and phase 3 have been adjusted to permit wide loads to pass through the work zone during permitted lane closure hours.

On Sheet 29 of 32 the following Note 5 shall be added:

5. The heads of the  $\frac{3}{4}$  diameter A449 hex head bolts shall not protrude above the steel plate. The top plates may be cored to recess the heads A449 hex head bolts.

Note: There are a total of 36 sheets in the plan set: 32 design sheets and 4 as-built sheets at the end.

## **SPECIFICATIONS**

### Special Provision 403 Hot Bituminous Pavements

Pages SP-53 and SP -54 are deleted in their entirety and replaced with the attached pages SP-53 (Revised 01/10/2014) and SP-54 (Revised 01/10/2014).

### Special Provision 523 Bearings

The following paragraphs are added to Section 523.09 Installation of Bearings:

The selected bearings shall be removed, cleaned, painted and reset; or removed and replaced prior to the, and in their final positions before the replacement locking compression seal joint systems can be installed.

At the Contractor's option, the Contractor may temporarily support the bridge deck in its' final position to undertake the removal, cleaning, painting and resetting; or removal and replacement of the bearings while concurrently installing replacing the replacement locking compression seal joint systems. The Contractor shall submit, for approval, his proposed methods to perform the work concurrently, and provide a completed joint where the deck and backwall are in the same plane, and the bridge beams are solidly supported on the properly set, or reset, bearings.

### Special Provision 524 Temporary Structural Support

The fifth paragraph, referencing operating equipment weighing more than 5000 pounds on the bridge while the bridge is temporarily supported, is deleted and not replaced.

## **QUESTIONS**

**The following are questions asked at the pre-bid meeting held on January 6, 2014 or submitted to the Maine Turnpike Authority in writing. Answers to the questions are noted. Bidders shall utilize this information in preparing their bid.**

Question 1: Can the allotted price for the truck mounted attenuator be increased? The current contract price does not even allow for the payment of the rental fee, the price should be market value.

Answer: The contract unit price has been revised. See the revised p-sheet included in this addendum.

Question 2: The contract schedule seems very tight? Is there any chance of an extension on the completion date or a delay on the notice to proceed with the duration remaining the same?

Answer: The notice to proceed and project completion dates remain as noted in the specifications.

Question 3: Butt joint pavement is incidental to the temporary steel plate special detour item but is the pavement paid per ton at the milled joints?

Answer: Yes, all final pavement required will be paid at the contract unit price.

Question 4: The plate detail has a 1 inch cushion under a 2 inch plate so that is a 3 inch height to calculate the length of the butt joint? That is a lot of asphalt.

Answer: Yes, that is correct. Also note the asphalt needs to be full width so that there is no longitudinal joint, the cost of furnishing the temporary plate detail asphalt is included in the Temporary Steel Plate Special Detour pay item.

Question 5: Has the Turnpike constructed a joint detail like this before?

Answer: No, however a similar detail was used by MaineDOT on I-295 Southbound over the Presumpscot River in Falmouth in 2010.

Question 6: If the railroad insisted the Contractor needed liability protection insurance would this cost be on the Contractor to pay?

Answer: There is no project work that requires accessing the railroad right-of-way or fouling the rail lines. Should the Contractor be required to obtain Railroad Liability Protection Insurance through no fault of the Contractor's actions the Authority will reimburse the Contractor's insurance premium cost (only, no mark up ) as an extra work item.

Question 7: Is high performance membrane required where concrete is replaced during the new joint construction?

Answer: Yes, the specifications and plan details call for high performance membrane to lap with existing membrane on the deck.

Question 8: Specification 403 complimentary notes, note A and I are both noted, which is correct?

Answer: The references to complimentary note I have been remove from the Table, see the attached pages SP-53 (Revised 01/10/2014) and SP-54 (Revised 01/10/2014).

Question 9: During compression seal installation will we be able to stop traffic to install the seal at once?

Answer: Yes, a short duration traffic stoppage with trooper assistance will be allowed to switch traffic to the opposite side of the bridge.

Question 10: On plan sheet 23, please describe what is new and what is existing?

Answer: Section F-F applies to the Stroudwater River southbound existing bridge joints, The 1" x 4 1/2" plates are proposed, the L8"x4"x 1/2" angles and extrusions are existing and remain. The armor grouting detail applies only when required and approved by the Resident. Payment for using the grouting detail, if required, is via time and materials. The required replacement seal is not shown in Section F-F.

The neoprene compression seal with steel edge beam detail applies to the new Stroudwater River northbound and Maine Central Railroad northbound joint

replacements; and is the existing detail for the Stroudwater River southbound bridge joints.

Question 11: Are the extrusions for the seals new or existing?

Answer: The extrusions are new for the northbound bridges; the existing extrusions remain on for the southbound bridges.

Question 12: On plan sheet 23, the notes do not seem to pertain to this detail.

Answer: Expansion joint notes apply as follows;

Note 1 applies to full joint replacements.

Note 2 applies to full joint replacements.

Note 3 applies to full joint replacements and joint seal replacements.

Note 4 applies to full joint replacements.

Note 5 applies to full joint replacements.

Note 6 applies to full joint replacements and joint seal replacements.

Note 7 applies to full joint replacements and joint seal replacements.

Note 8 applies to Stroudwater River Bridge Southbound only if grout is required.

Note 9 applies to full joint replacements

Note 10 applies to full joint replacements and joint seal replacements.

Question 13: Is the 1" x 4 1/2" plate paid for under 520.2234?

Answer: Yes the 1" x 4 1/2" plate is paid for under 520.2234; see Basis of Payment for that item.

Question 14: How is the Stroudwater Southbound deck repair done given the short work windows?

Answer: The Contractor is responsible for coordinating the removal of existing membrane and pavement and the deck patching so the roadway can be opened to two lanes of traffic daily. Should the Contractor not be able to properly prep, place and cure the deck repair areas exposed during the shift the Contractor may infill the damaged or partially prepped area with temporary bituminous asphalt patch or temporary quick set mortar to open the roadway up to traffic on schedule. The installation and removal of temporary bituminous asphalt patch or temporary quick set mortar will not be paid separately, but shall be incidental to the deck repair pay items.

Question 15: How is the Stroudwater Southbound deck membrane and paving done given the short work windows?

Answer: The Contractor is responsible for coordinating the deck membrane and paving so the roadway can be opened to two lanes of traffic daily. Should the Contractor not be able to properly membrane and pave a lane during the shift Contractor may elect to install a membrane and pavement construction joint at 1/3 or 1/2 the deck length, and complete the lane in the next shift.

Question 16: Will the Authority allow RAP in the 403.2081 and 403.208 and if so what percentage will they allow?

Answer: RAP will be allowed in item 403.208, percentage per complimentary note B. RAP will not be allowed in 403.2081 per complimentary note J. See the attached pages SP-53 (Revised 01/10/2014) and SP-54 (Revised 01/10/2014).

Question 17: Is the depth of the Stroudwater NB surface depth of 1 1/8" the correct depth of HMA? If the depth is correct is the type of HMA the correct stone size for the application? Also the plans state you are placing an 1 1/4" on sheet 16 of 32 and not the 1 1/8", which is correct?

Answer: The correct overlay depth is 1 1/4" for Stroudwater NB and MCRR NB, see revised Specifications 403 SP-53 and SP-54, included in this addendum.

Question 18: The shim item states that the 403.211 item is going to be a 12.5 mm HMA, is this correct? Does the authority plan to feather the shim areas to nothing?

Answer: Yes, limits of shim will be milled to approximately 1 1/4" minimum depth. It is not intended to feather the shim areas to nothing.

Question 19: Does the Authority have any aesthetic concerns of placing a 1.25" depth of 0.5" stone?

Answer: No.

**Notes:** The above items shall be considered as part of the bid submittal.

The total number of pages included with this addendum is Twelve (12).

All bidders are requested to acknowledge the receipt of the Addendum No. 1 by signing below and faxing this sheet to Nathaniel Carll, Purchasing Department, Maine Turnpike Authority at 207-871-7739. Bidders are also required to acknowledge receipt of this Addendum No. 1 on Page P-9 of the bid package.

---

Business Name

---

Print Name and Title

---

Signature

---

Date

January 10, 2014

Very truly yours,

MAINE TURNPIKE AUTHORITY

---

Nathaniel Carll  
Purchasing Department  
Maine Turnpike Authority

SPECIAL PROVISION

SECTION 403

HOT BITUMINOUS PAVEMENTS

| <b>Desc. of Course</b>   | <b>Grad. Design</b> | <b>Item Number</b> | <b>Bit Cont. % of Mix</b> | <b>Total Thick</b> | <b>No. Of Layers</b> | <b>Comp. Notes</b> |
|--|---------------------|--------------------|---------------------------|--------------------|----------------------|--------------------|
| <b><u>Bridge Decks</u></b><br><b><u>Stroudwater NB / MCRR NB</u></b>   |                     |                    |                           |                    |                      |                    |
| Wearing  | 12.5 mm             | 403.2081           | 5.2 min to 6.40           | 1-1/4 in.          | 1                    | A, C, E, G, H & J  |
| Repair   | 12.5 mm             | 403.2081           | 5.2 min to 6.40           | 1-1/2 in.          | 1                    | A, C, E, G, H & J  |
| <b><u>Bridge Deck</u></b><br><b><u>Stroudwater SB</u></b>              |                     |                    |                           |                    |                      |                    |
| Wearing  | 12.5 mm             | 403.2081           | 5.2 min to 6.40           | 2-1/2 in.          | 2                    | A, C, E, G, H & J  |
| <b><u>Mainline Mill, Shim and Overlay - All</u></b>                    |                     |                    |                           |                    |                      |                    |
| Wearing  | 12.5 mm             | 403.208            | 5.2 min to 6.40           | 1-1/2 in.          | 1                    | A, B, C, E, G & H  |
| Shim   | 12.5 mm             | 403.211            | 5.2 min to 6.40           | varies             |                      | A, B, C, E, F & G  |
| <b><u>Mainline Shoulder Repair - NB</u></b>                            |                     |                    |                           |                    |                      |                    |
| Wearing  | 12.5 mm             | 403.213            | 5.2 min to 6.40           | 2 in.              | 1                    | A, B, C, E, G & H  |
| Base   | 12.5 mm             | 403.213            | 5.2 min to 6.40           | 4 in.              | 2                    | A, B, C, E & G     |
| <b><u>Mainline Approach / Departure Structural Excavation - NB</u></b> |                     |                    |                           |                    |                      |                    |
| Base   | 12.5 mm             | 403.213            | 5.2 min to 6.40           | 8.5 in.            | 4                    | A, B, C, E, F & G  |

SP-53 (Revised 01/10/2014)

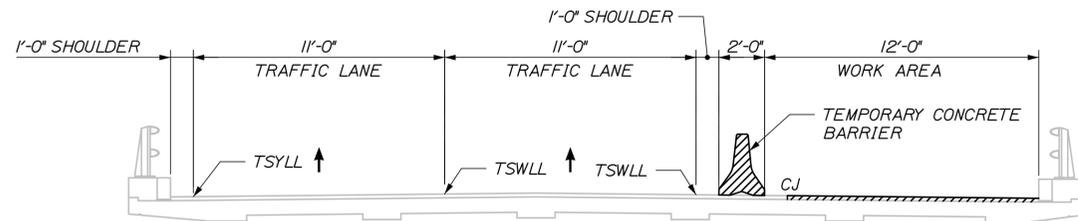
### COMPLEMENTARY NOTES

- A. The required minimum PGAB for Items 403.208, 403.2081, 403.211 and 403.213 shall be a **PG 64-28** grading.
- B. A maximum of 15 percent RAP shall be used in the Hot Mix Asphalt.
- C. The MaineDOT will conduct the job mix verification. The aggregate qualities shall meet the design traffic level of 3 to < 10 million ESAL for mix placed under this Contract. The design, verification, Quality Control, and Acceptance tests for this mix will be performed at **75 gyrations**.
- D. A material transfer vehicle (transfer box) shall be used for the placement of Hot Mix Asphalt, 12.5 mm Nominal Maximum size, wearing surface, on both the northbound and southbound roadways including acceleration and deceleration lanes and all ramps.
- E. No vehicular traffic or loads shall be permitted on newly completed pavement until adequate stability has been attained and the material has cooled sufficiently to prevent distortion or loss of fines. The newly paved area may be opened to traffic after the internal temperature of the pavement has cooled to 120°F. The Resident will test the internal temperature of the pavement and shall be the sole judge as to the opening to traffic. The period of time before opening to traffic may be extended at the discretion of the Resident. The lane closure may not be removed until the internal temperature has cooled to 120°F.
- F. Tack coat shall be applied to all milled pavement at a rate of 0.03 G/SY prior to placing the surface course.
- G. Joints shall conform to Subsection 401.17.
- H. Tack coat shall be applied to the pavement at a rate of 0.03G/SY prior to placing the surface course. Tack coat is required between all lifts of pavement, or as directed by the Resident.
- I. Asphalt cement shall conform to the provisions of 403.02 – Polymer or Liquid Latex Modified PGAB for HMA.
- J. **Bridge decks:** No RAP is allowed in the bridge mix design. The use of an oscillating steel roller shall be used to compact all hot mix asphalts placed on bridge decks.

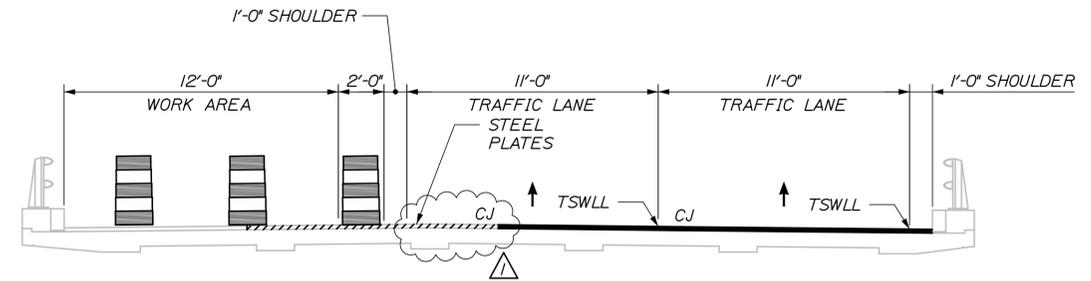
| Item No                 | Item Description                       | Units        | Approx. Quantities | Unit Prices in Numbers |       | Bid Amount in Numbers |       |
|-------------------------|--|--------------|--------------------|------------------------|-------|-----------------------|-------|
|                         |  |              |                    | Dollars                | Cents | Dollars               | Cents |
| <b>BROUGHT FORWARD:</b> |  |              |                    |                        |       |                       |       |
| 631.36                  | Foreman                                | Hour         | 40                 |                        |       |                       |       |
| 631.40                  | Welder (including operator)            | Hour         | 40                 |                        |       |                       |       |
| 652.30                  | Flashing Arrow Board                   | Each         | 2                  |                        |       |                       |       |
| 652.312                 | Type III Barricades                    | Each         | 8                  |                        |       |                       |       |
| 652.33                  | Drum                                   | Each         | 260                |                        |       |                       |       |
| 652.35                  | Construction Signs                     | Square Foot  | 2,450              |                        |       |                       |       |
| 652.361                 | Maintenance of Traffic Control Devices | Lump Sum     | 1                  |                        |       |                       |       |
| 652.41                  | Portable - Changeable Message Sign     | Each         | 2                  |                        |       |                       |       |
| 652.451                 | Truck Mounted Attenuator               | Calendar Day | 160                |                        |       |                       |       |
|                         |  |              |                    | \$100                  | 00    | \$16,000              | 00    |
| 656.50                  | Baled Hay, in place                    | Each         | 20                 |                        |       |                       |       |
| 656.60                  | Temporary Berms                        | Linear Foot  | 300                |                        |       |                       |       |
| 656.62                  | Temporary Slope Drains                 | Linear Foot  | 160                |                        |       |                       |       |

**CARRIED FORWARD:**

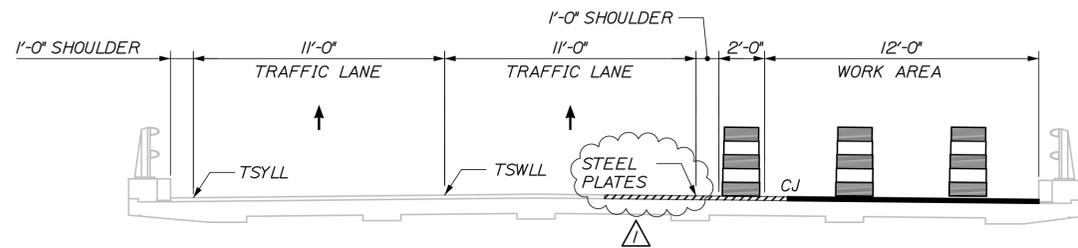
Date: 1/9/2014



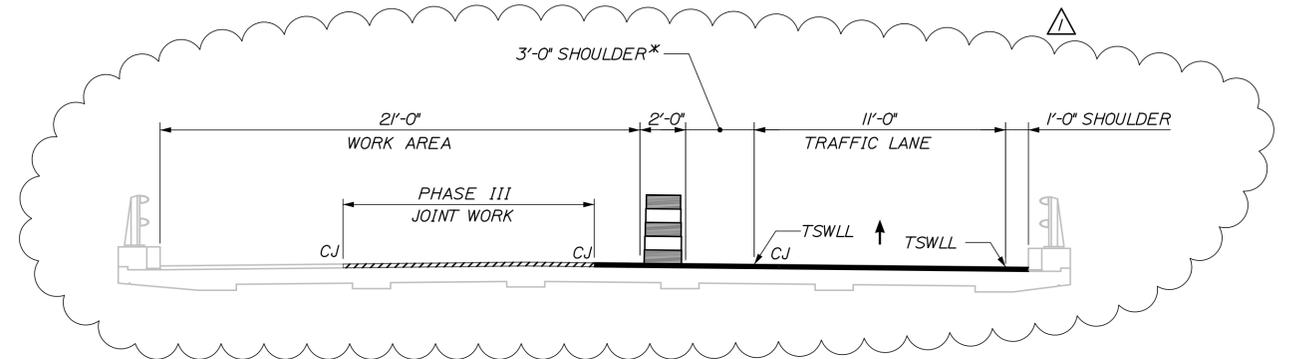
**PHASE I - DAYTIME WORK ZONE**  
1/4" = 1'-0"



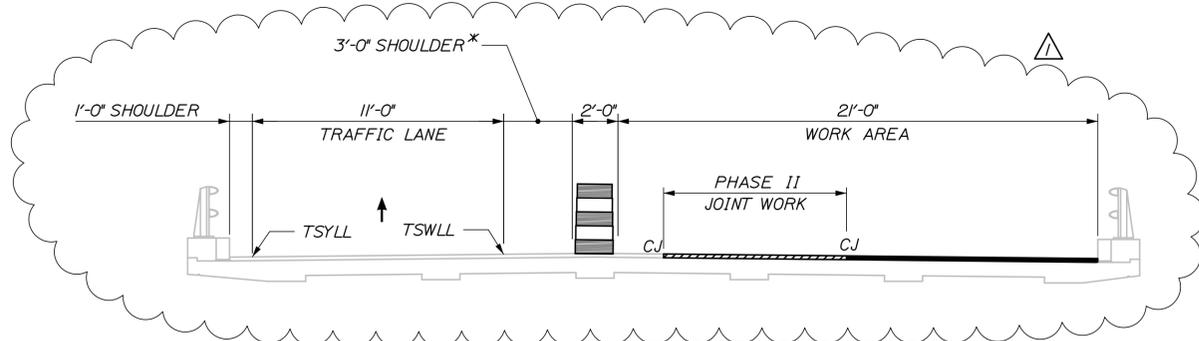
**PHASE III - DAYTIME - NO WORK PERMITTED**  
1/4" = 1'-0"



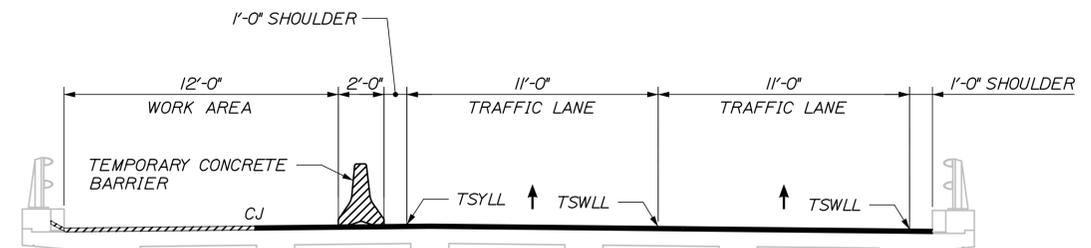
**PHASE II - DAYTIME - NO WORK PERMITTED**  
1/4" = 1'-0"



**PHASE III - NIGHTTIME WORK ZONE**  
1/4" = 1'-0"



**PHASE II - NIGHTTIME WORK ZONE**  
1/4" = 1'-0"



**PHASE IV - DAYTIME WORK ZONE**  
1/4" = 1'-0"

// // // // JOINT UNDER CONSTRUCTION  
 ——— JOINT COMPLETE  
 CJ = CONSTRUCTION JOINT  
 TSWLL = TEMPORARY SOLID WHITE LANE LINE  
 TSYLL = TEMPORARY SOLID YELLOW LANE LINE

NOTE:  
 FINAL LIFT OF DECK PAVEMENT AND APPROACH SHIMMING SHALL BE PLACED NIGHTS AFTER PHASE IV IS COMPLETE. PAVEMENT JOINTS SHALL MATCH AT 25'-0" AND 37'-0" RIGHT OF MTA CENTERLINE.

\*▲ TRAVEL WAY SHALL BE A MINIMUM OF 15' WIDE DURING WIDE LOAD PERMITTED HOURS; ONE HALF HOUR BEFORE SUNRISE AND ONE HALF HOUR AFTER SUNSET.

Contract 2014.11  
Addendum No. 1  
Page 10 of 12

Scale: AS NOTED

| No. | Revision    | By  | Date |
|-----|-------------|-----|------|
| ▲   | Addendum *1 | TMH | 1/14 |

Designed by:

**HNTB**

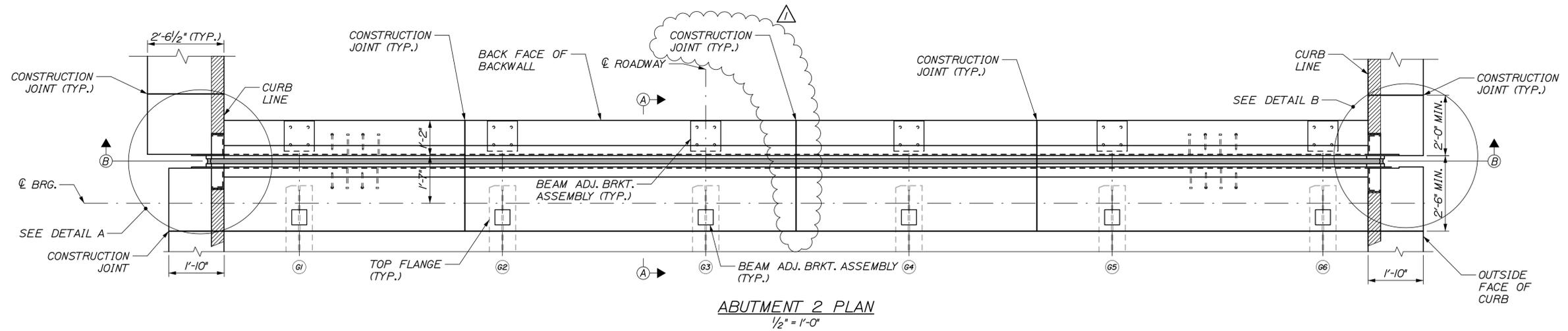
|  |           |              |           |
|--|-----------|--------------|-----------|
| CONSULTANT PROJECT MANAGER: Robert J. Driscoll, P.E. |           |              |           |
| By   | Date      | By           | Date      |
| Designed   | CAH 12/13 | Checked      | TMH 12/13 |
| Drawn  | JRD 12/13 | In Charge of | RAL 12/13 |

HNTB CORPORATION  
 340 County Road, Suite 6-C  
 Westbrook, ME 04092  
 TEL (207) 774-5155  
 FAX (207) 228-0909

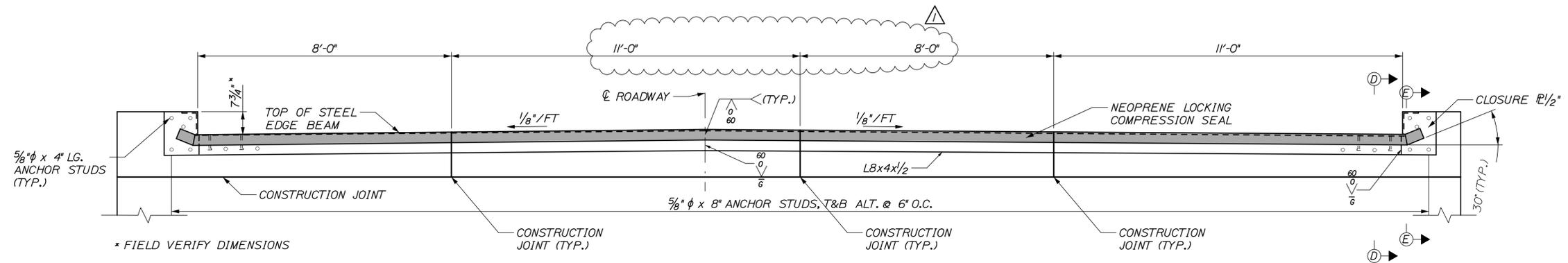

**THE GOLD STAR MEMORIAL HIGHWAY**  
 MTA PROJECT MANAGER: Ralph C. Norwood, IV, P.E., P.T.O.E.

BRIDGE REPAIRS - STROUDWATER RIVER & MAINE CENTRAL RAILROAD  
 NORTHBOUND TRAFFIC PHASING  
 SHEET NUMBER: 3  
 CONTRACT: 2014.11  
 3 OF 32

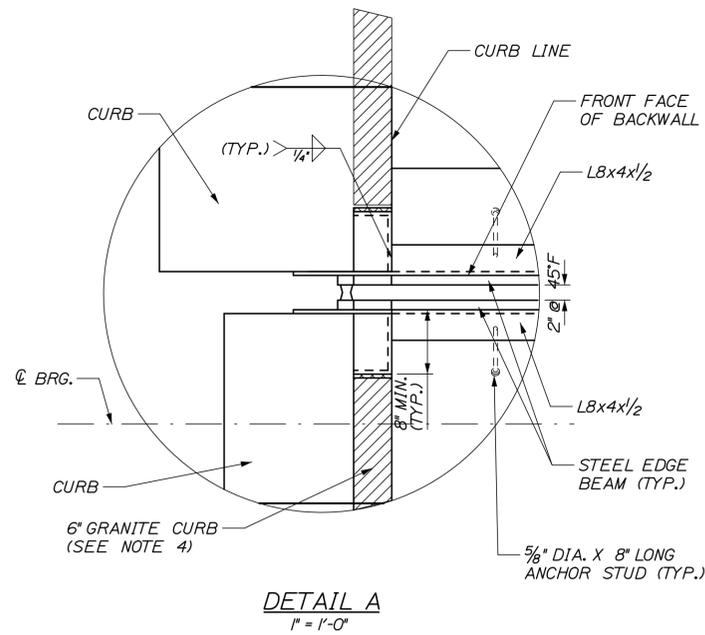
Date: 1/9/2014



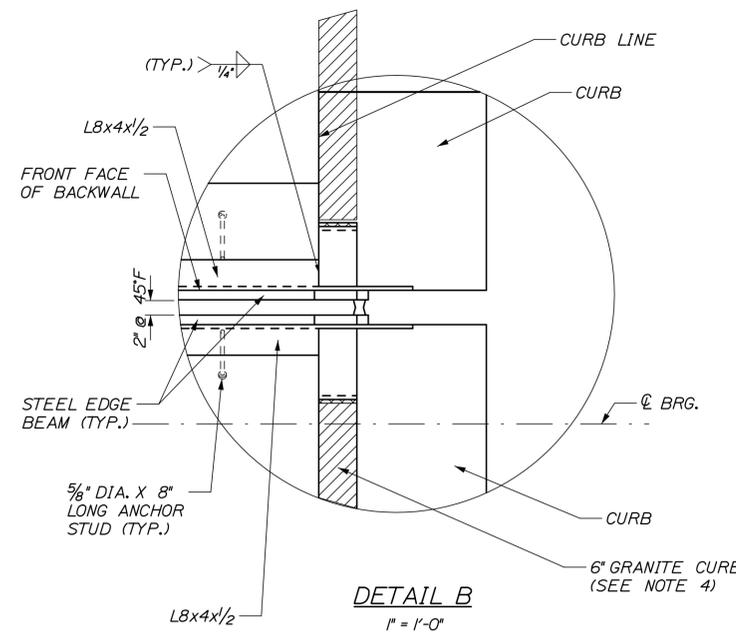
ABUTMENT 2 PLAN  
1/2" = 1'-0"



SECTION B-B  
1/2" = 1'-0"



DETAIL A  
1" = 1'-0"



DETAIL B  
1" = 1'-0"

NOTES:

1. ABUTMENT 2 EXPANSION JOINT ASSEMBLY SHOWN. ABUTMENT 1 JOINT ASSEMBLY SIMILAR.
2. FOR NOTES AND DETAILS NOT SHOWN SEE SHEETS 22 AND 23.
3. EXISTING GRANITE CURB TO BE SALVAGED. PAYMENT FOR REMOVING, STORING AND RESETTING THE GRANITE CURB SHALL BE INCIDENTAL TO PAY ITEM 520.2211.
4. ALUMINUM BRIDGE RAIL NOT SHOWN FOR CLARITY.
5. EXPANSION JOINT ASSEMBLIES SHALL BE FABRICATED AND HOT DIP GALVANIZED AS A UNIT; AND AFTER GALVANIZING FIELD SPLICES SHALL BE FABRICATED SHOP SAWN. ADEQUATE TEMPORARY ADJUSTMENT ANGLES SHALL BE INSTALLED ON THE EXPANSION JOINT ASSEMBLY DURING FABRICATION TO MAINTAIN THE JOINT GEOMETRY DURING GALVANIZING, SHOP SAWING, SHIPPING AND FIELD INSTALLATION.
6. PROVIDE TEMPORARY SUPPORT BARS, RODS, ETC., TO HOLD JOINT ARMOR IN SET POSITION WHILE DECK AND BACKWALL CONCRETE IS CAST AND CURED.
7. PROVIDE HOLES IN JOINT ARMOR TO VENT CONCRETE.

Contract 2014.11  
Addendum No. 1  
Page 11 of 12

Scale: AS NOTED

| No. | Revision    | By  | Date |
|-----|-------------|-----|------|
| ▲   | Addendum #1 | TMH | 1/14 |

Designed by:

**HNTB**

CONSULTANT PROJECT MANAGER: Robert J. Driscoll, P.E.

|          | By  | Date  | Checked      | By  | Date  |
|----------|-----|-------|--------------|-----|-------|
| Designed | CAH | 12/13 | Checked      | JDW | 12/13 |
| Drawn    | JRD | 12/13 | In Charge of | RAL | 12/13 |

HNTB CORPORATION  
340 County Road, Suite 6-C  
Westbrook, ME 04092  
TEL (207) 774-5155  
FAX (207) 228-0909

MAINE TURNPIKE

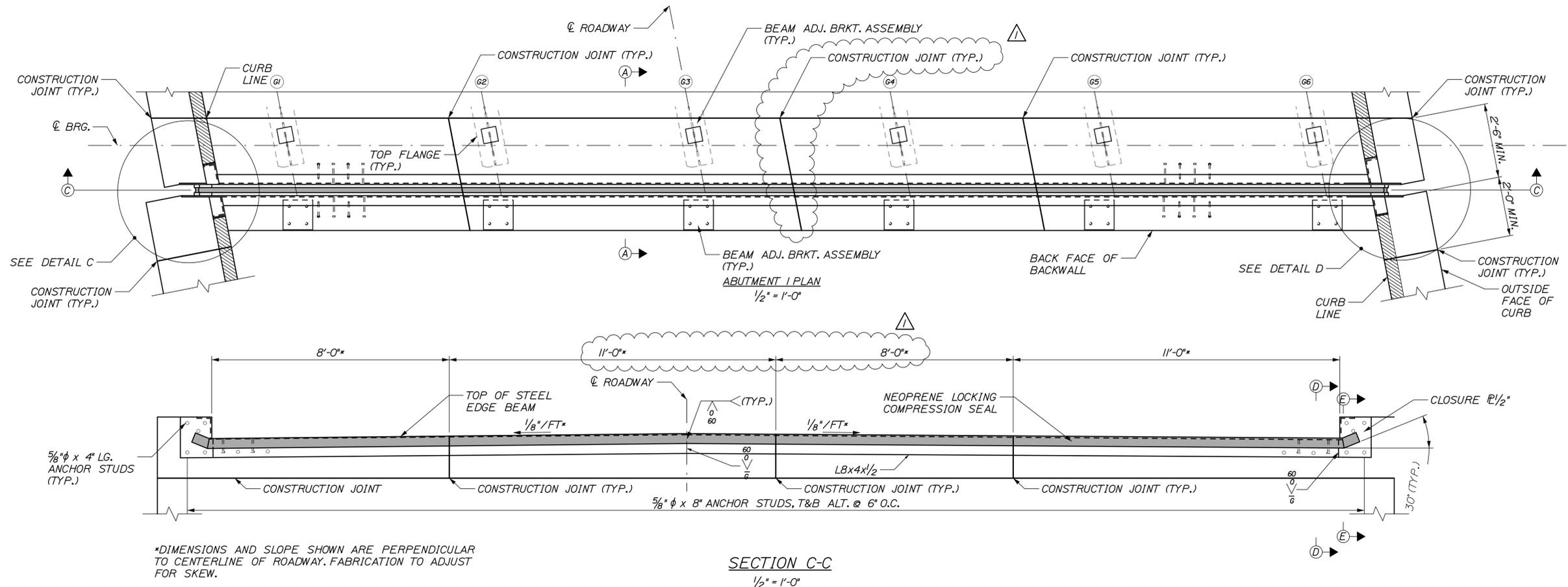
**THE GOLD STAR MEMORIAL HIGHWAY**

MTA PROJECT MANAGER: Ralph C. Norwood, IV, P.E., P.T.O.E.

BRIDGE REPAIRS - STROUDWATER RIVER & MAINE CENTRAL RAILROAD  
STROUDWATER RIVER NORTHBOUND  
BRIDGE JOINT MODIFICATIONS

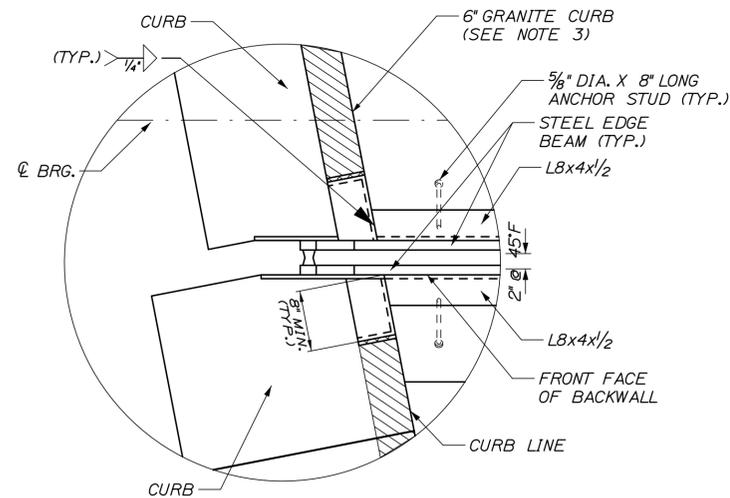
SHEET NUMBER: 20  
CONTRACT: 2014.11  
20 OF 32

Date: 1/9/2014

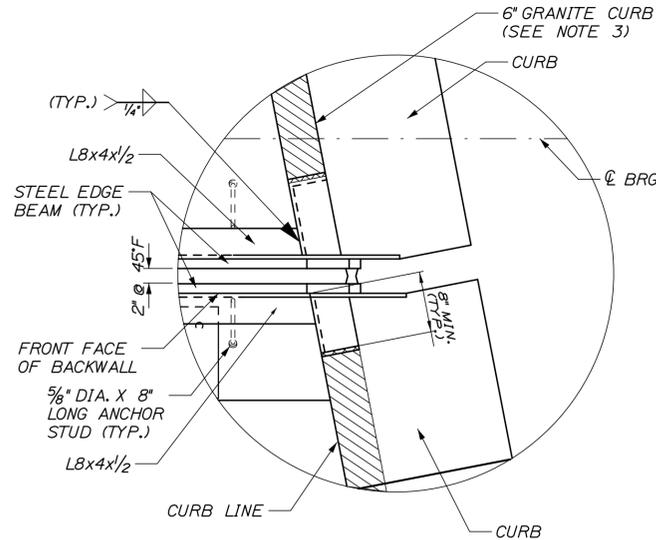


\*DIMENSIONS AND SLOPE SHOWN ARE PERPENDICULAR TO CENTERLINE OF ROADWAY. FABRICATION TO ADJUST FOR SKEW.

SECTION C-C  
1/2" = 1'-0"



DETAIL C  
1" = 1'-0"



DETAIL D  
1" = 1'-0"

NOTES:

1. ABUTMENT 1 EXPANSION JOINT ASSEMBLY SHOWN. ABUTMENT 2 JOINT ASSEMBLY SIMILAR.
2. FOR NOTES AND DETAILS NOT SHOWN SEE SHEETS 22 AND 23.
3. EXISTING GRANITE CURB TO BE SALVAGED. PAYMENT FOR REMOVING, STORING AND RESETTING THE GRANITE CURB SHALL BE INCIDENTAL TO PAY ITEMS 520.2225 AND 520.2227.
4. ALUMINUM BRIDGE RAIL NOT SHOWN FOR CLARITY.
5. EXPANSION JOINT ASSEMBLIES SHALL BE FABRICATED AND HOT DIP GALVANIZED AS A UNIT; AND AFTER GALVANIZING FIELD SPLICES SHALL BE FABRICATED SHOP SAWN. ADEQUATE TEMPORARY ADJUSTMENT ANGLES SHALL BE INSTALLED ON THE EXPANSION JOINT ASSEMBLY DURING FABRICATION TO MAINTAIN THE JOINT GEOMETRY DURING GALVANIZING, SHOP SAWING, SHIPPING AND FIELD INSTALLATION.
6. PROVIDE TEMPORARY SUPPORT BARS, RODS, ETC., TO HOLD JOINT ARMOR IN SET POSITION WHILE DECK AND BACKWALL CONCRETE IS CAST AND CURED.
7. PROVIDE HOLES IN JOINT ARMOR TO VENT CONCRETE.

Filename: 021\_MCRRL-joints1.dgn

|        |             |          |      |
|--------|-------------|----------|------|
| Scale: |             | AS NOTED |      |
| No.    | Revision    | By       | Date |
| 1      | Addendum #1 | TMH      | 1/14 |

|  |     |       |              |      |       |
|--|-----|-------|--------------|------|-------|
| Designed by:   |     |       |              |      |       |
| <b>HNTB</b>  |     |       |              |      |       |
| CONSULTANT PROJECT MANAGER: Robert J. Driscoll, P.E. |     |       |              |      |       |
|  | By  | Date  | By           | Date |       |
| Designed   | CAH | 12/13 | Checked      | JDW  | 12/13 |
| Drawn  | JRD | 12/13 | In Charge of | RAL  | 12/13 |

HNTB CORPORATION  
340 County Road, Suite 6-C  
Westbrook, ME 04092  
TEL (207) 774-5155  
FAX (207) 228-0909



## THE GOLD STAR MEMORIAL HIGHWAY

MTA PROJECT MANAGER: Ralph C. Norwood, IV, P.E., P.T.O.E.

BRIDGE REPAIRS - STROUDWATER  
RIVER & MAINE CENTRAL RAILROAD  
MAINE CENTRAL RAILROAD NORTHBOUND  
BRIDGE JOINT MODIFICATIONS

SHEET NUMBER: 21  
CONTRACT: 2014.11  
21 OF 32

Contract 2014.11  
Addendum No. 1  
Page 12 of 12