



THE GOLD STAR
MEMORIAL HIGHWAY

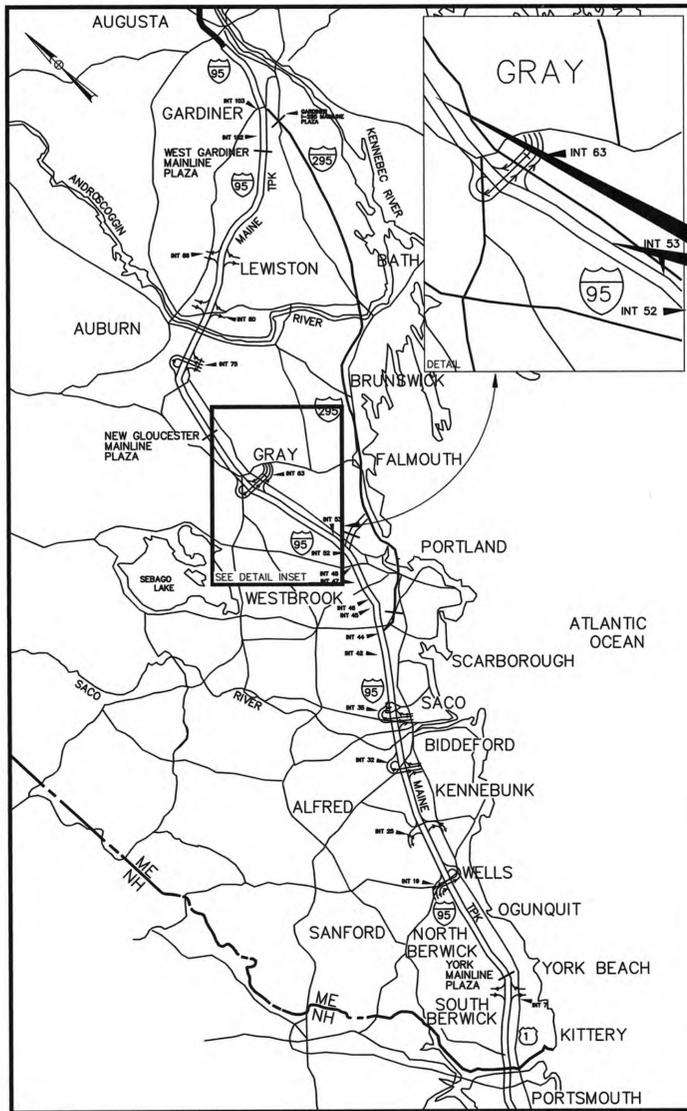
MAINE TURNPIKE AUTHORITY

DANIEL E. WATHEN, CHAIR
JAMES F. CLOUTIER, VICE CHAIR
GERARD P. CONLEY, SR., MEMBER
JOHN E. DORITY, MEMBER
ROBERT D. STONE, MEMBER
FREEMAN R. GOODRICH, MEMBER
KAREN S. DOYLE, MEMBER EX-OFFICIO

S. PETER MILLS, EXECUTIVE DIRECTOR

CONTRACT 2016.01 PAVEMENT REHABILITATION CLEAR ZONE IMPROVEMENTS MM 54.5 TO MM 64.4

EAGLES NEST BRIDGE REPAIRS MM 60.8 HUNTS HILL BRIDGE REPAIRS MM 61.6



LOCATION MAP

CONTRACT 2016.01
PAVEMENT REHABILITATION
CLEAR ZONE IMPROVEMENTS
MM 54.5 TO MM 64.4

EAGLES NEST BRIDGE REPAIRS
MM 60.8
HUNTS HILL BRIDGE REPAIRS
MM 61.6

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CONTRACT 2016.01



SHEETS
35-77

Norman L. Baker
NORMAN L. BAKER, P.E.
PROJECT MANAGER

1/26/16
DATE



SHEETS
2-24

Joseph Ryan Leavitt
JOSEPH RYAN LEAVITT, P.E.
SENIOR RESIDENT ENGINEER



1/27/16
DATE



SHEETS
25-34

Brian A. Taddeo
BRIAN A. TADDEO, P.E.
PROJECT ENGINEER



1/27/16
DATE

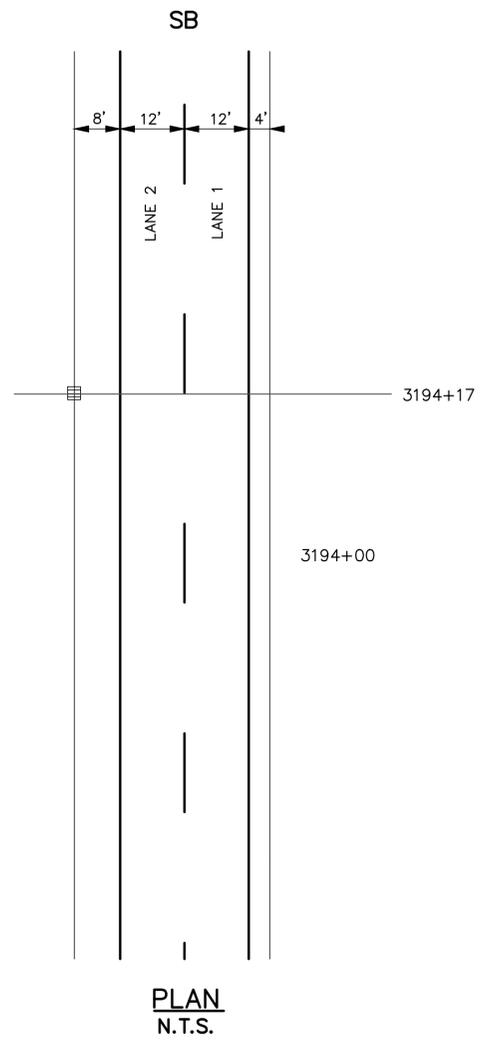
APPROVED:
MAINE TURNPIKE AUTHORITY

Peter S. Merfeld
PETER S. MERFELD, P.E. - CHIEF OPERATIONS OFFICER
DATE *1/27/16*

Stephen R. Tarrere
STEPHEN R. TARRERE, P.E. - DIRECTOR OF ENGINEERING &
BUILDING MAINTENANCE
DATE *1/27/16*

ESTIMATED QUANTITIES														
ITEM NO.	DESCRIPTION	UNIT	QUANTITY TOTAL	Mill & Fill		Shim & Overlay		Clear Zone		Bridge		Snow Fence		Ramps Exit 63
				Loc. 1 NB	Loc. 2 SB	Loc. 3 NB	Loc. 4 SB	Loc. 5 NB	Loc. 6 SB	Loc. 7 EAGLE	Loc. 8 HUNTS	Loc. 9 BENNETT	Loc. 10 KITTY	
202.12	Removing Existing Structural Concrete	CY	34											
202.127	Removing Existing Bituminous Pavement (675SY)	LS	1											
202.202	Removing Pavement Surface - Mainline	SY	121,164	41,167	41,167	16,327	22,503							
202.2026	Removing Pavement Surface - Drainage Paths	SF	4,200	1,432	1,432	668	668							
202.205	Rumble Strips	EA	155,510	22,230	22,230	43,600	67,450							
203.20	Common Excavation	CY	625					294	294	37				
203.21	Rock Excavation	CY	50						50					
203.241	Common Borrow, Truck Measure	CY	450					225	225					
203.25	Granular Borrow	CY	136					68	68					
205.51	Widening of Existing Shoulder	LF	265					130	135					
205.53	Widening of Existing Shoulder Pavement	LF	525					525						
304.10	Aggregate Subbase Coarse - Gravel	CY	150					70	53	27				
403.208	Hot Mix Asphalt, 12.5mm- RAP	Ton	7,150	3,575	3,575									
403.2081	Hot Mix Asphalt, 12.5mm (Polymer Modified)- RAP	Ton	30,190	4,150	4,150	8,354	12,897			50	74			515
403.2084	Hot Mix Asphalt, 12.5mm Nominal Maximum Size (sidewalks, drives, islands & incidentals)	Ton	261			130	131							
403.212	Hot Mix Asphalt 4.75mm Nominal Maximum Size	Ton	7,227			2,877	4,350							
403.213	Hot Mix Asphalt, 12.5mm	Ton	88							14	74			
407.08	Berm Dropoff Correction-Grindings	Ton	3,100			1,550	1,550							
407.081	Berm Correction	LF	35,195			17,597	17,598							
409.15	Bituminous Tack Coat - Applied	GAL	26,119	3,623	3,623	7,783	11,091							491
409.151	Bituminous Tack Coat - Applied, Bridge	GAL	59							26	33			
419.30	Sawing Bituminous Pavement	LF	2,415											
419.301	Sawing and Sealing Bituminous Pavement	LF	320											
424.323	Asphalt Rubber Fiber Crack Filler-Applied	Pound	22,000			11,000	11,000							
427.09	Pavement Crack Repair	LF	39,500	12,335	27,165									
429.341	Grid/Fabric Composite Pavement Interlayer	SF	2,400		2,400									
459.06	Bituminous Concrete Waterway - Type I	EA	3		3									
459.061	Bituminous Concrete Waterway - Type II	EA	9		9									
502.21	Structural Concrete, Abutments & Retaining Walls	CY	38							38				
502.26	Structural Concrete Super. Slab on St Br (2CY)	LS	1							1				
503.14	Epoxy-Coated Reinforcing Steel, Fab & Delivered	LB	6,400							6,300	100			
503.15	Epoxy-Coated Reinforcing Steel, Placing	LB	6,400							6,300	100			
506.99	Clean and Paint Existing Bearings	EA	20							20				
507.095	Aluminum Bridge Railing - Splice Modification	EA	56									24	32	
508.141	High Performance Waterproofing Membrane (Hunts Hill Rd)(660 SY)	LS	1							0	1			
515.201	Pigmented Protective Coating for Concrete Surfaces	SY	710							390	320			
515.202	Clear Protective Coating for Concrete Surfaces	SY	620							340	280			
518.39	Granite Curb Joint Mortar and Bedding Mortar Repair	LF	615							310	305			
518.40	Epoxy Injected Crack Repairs	LF	205							10	195			
518.43	Parapet Joint Repair	LF	260								260			
518.51	Repair of Upward Facing Concrete Surfaces - Below Reinforcing Steel-8 Inches	SF	360							40	320			
518.511	Full Depth Concrete Repair	SF	60								60			
518.60	Repair of Vertical Surfaces < 8 inches	SF	245							80	165			
518.70	Repair of Overhead Surfaces < 8 Inches	SF	95							5	90			
520.2211	Expansion Device Modifications (47.5FT)	EA	2							2				
520.231	Expansion Device - Asphalt Plug for Crack Control	LF	95								95			
520.232	Expansion Device - Asphalt Plug Joint	LF	145							145				
523.521	Bearing Rehabilitation, Rocker Bearings	EA	20							20				
526.306	Temporary Concrete Barrier, Type I - Supplied by Authority(660 LF)	LS	1							1				
527.303	Energy Absorbing System (CAT) - Remove and Reset	EA	1					1						
527.341	Work Zone Crash Cushions - TL-3	UNIT	2								2			
603.155	12 inch Reinforced Concrete Pipe - Class III	LF	78			78								
603.209	30 inch Culvert Pipe Option III	LF	20					20						
603.229	42 inch Culvert Pipe Option III	LF	20					20						
603.28	Concrete Collar	EA	4			2	2							
604.184	Rebuild Catch Basin to Grade - Type II	EA	32			32								
604.186	Rebuild Catch Basin to Grade - Type IV	EA	8			8								
606.1723	Bridge Transition - Type III	EA	6							6				
606.1724	Bridge Transition - Type III Modified	EA	1							1				
606.178	Guardrail Beam	LF	650					650						
606.24	Guardrail Type 3d Single Rail	LF	200					200						
606.2401	Guardrail Type 3d Double Rail	LF	450					500						
606.2652	Terminal End - Remove and Stack	EA	31					30		1				
606.278	Terminal End - Anchored End	EA	31					30		1				
606.279	Terminal End - Anchored End, Thrie Beam	EA	1					1						
606.352	Reflectorized Beam Guardrail Delineator	EA	1,500					1,315		185				
606.353	Delineator Post	EA	120					118		2				
606.355	Delineator Post - Remove and Stack	EA	120					120						
606.3605	Guardrail - Remove, Modify and Reset Type 3d Single Guardrail	LF	25							25				
606.3606	Guardrail - Remove, Modify and Reset Type 3d Double Guardrail	LF	545					272		273				
606.3621	Guardrail Adjust - Single Rail	LF	29,850					29,825		25				
606.3622	Guardrail Adjust - Double Rail	LF	37,915					37,915						
606.3631	Guardrail - Remove and Dispose	LF	130					130						
606.47	Single Wooden Post	EA	10					10						
606.471	Single Offset Block - W Beam	EA	90					90						
606.472	Single Offset Block - Thrie Beam	EA	10					10						
606.48	Single Galvanized Steel Post	EA	100					100						
606.63	Thrie Beam Rail Beam	LF	50					50						
606.65	Guardrail Thrie Beam - Single Rail	LF	25							25				
606.701	Assymetrical Thrie Beam Transition	EA	5					5						
606.754	Widen Shoulder for Guardrail 350 Flared Terminal	EA	2					2						
606.755	Modify Widen Shoulder for Guardrail 350 Flared Terminal	EA	19					19						
606.80	Guardrail - 350 Flared Terminal	EA	4					4						
606.82	Guardrail - Remove and Stack Existing Crash End	EA	4					4						
606.83	Remove, Modify, and Reset Existing Crash End	EA	1					1						
607.431	Snow Fence	LF	676							180		232	264	

ITEM NO.	DESCRIPTION	UNIT	QUANTITY TOTAL	Mill & Fill		Shim & Overlay		Clear Zone		Bridge		Snow Fence		Ramps Exit 63	
				Loc. 1 NB	Loc. 2 SB	Loc. 3 NB	Loc. 4 SB	Loc. 5 NB	Loc. 6 SB	Loc. 7 EAGLE	Loc. 8 HUNTS	Loc. 9 BENNETT	Loc. 10 KITTY	Loc. 11	
609.13	Vertical Bridge Curb, Type 1	LF	130											130	
609.191	Concrete Curb, Type 2	LF	84											84	
610.08	Plain Riprap	CY	75							75					
610.18	Stone Ditch Protection	CY	72												
613.319	Erosion Control Blanket	SY	200												
615.07	Loam	CY	73												
618.1401	Seeding Method Number 2, Plan Quantity	UNIT	146										146		
619.1201	Mulch, Plan Quantity	UNIT	176										176		
619.1202	Temporary Mulch	LS	1							1					
620.58	Erosion Control Geotextile	SY	200												
627.712	White or Yellow Pavement Marking Line	LF	179,100	24,700	24,700	51,600	78,100								
627.713	Broken White Pavement Marking Line	LF	89,550	12,350	12,350	25,800	39,050								
627.77	Removing Existing Pavement Markings	SF	1,150											1,150	
627.78	Temporary Pavement Marking Line, Yellow or White	LF	320,550	49,400	49,400	88,050	127,800							2,950	2,950
627.812	Temporary Raised Pavement Markers	EA	24,663	3,397	3,397	7,095	10,738							36	
627.94	Pavement Marking Tape	LF	3,363	465	465	969	1,464								
627.941	Pavement Marking Tape, Dotted White Lane Line, 6-inch Width	LF	906			906									
629.05	Hand Labor, Straight Time	HR	240							200				20	20



DRAINAGE SUMMARY INCLUDES LOCATIONS 1 THROUGH LOCATION 8

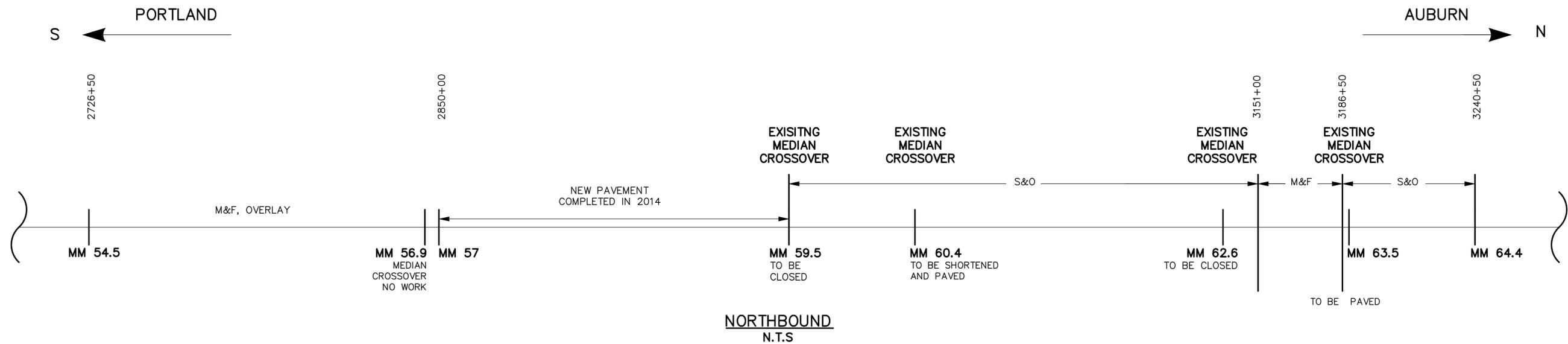
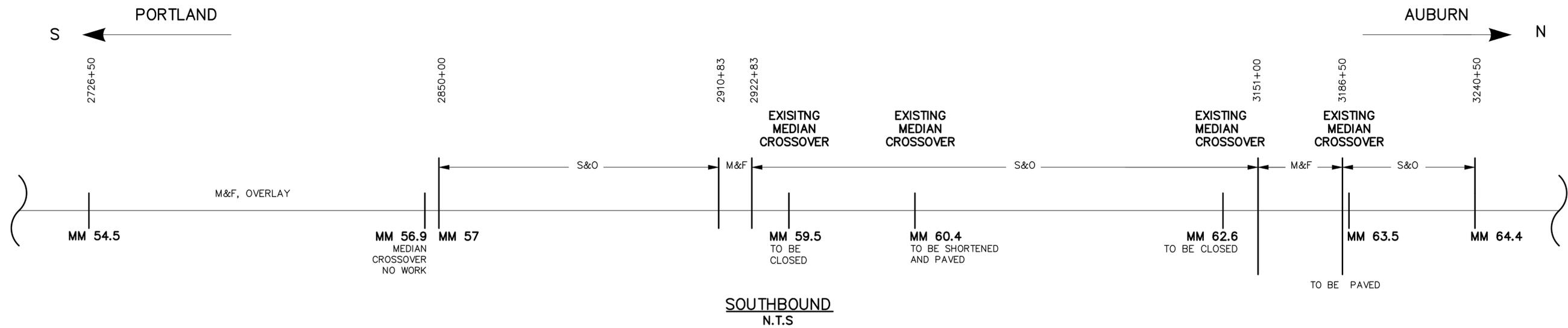
Location	ITEM NAME	RDWY	BIT. CONC. WATER-WAY Type I	BIT. CONC. WATER-WAY Type II	12" REINFORCED CONCRETE PIPE - CLASS III		12" REINFORCED CONCRETE PIPE - CLASS III		REBUILD CATCH BASIN TO GRADE - TYPE II				REBUILD CATCH BASIN TO GRADE - TYPE IV	CATCH BASIN TYPE F3	EROSION CONTROL BLANKET	BALED HAY IN PLACE	30 inch TEMP. SILT FENCE	CATCH BASIN OUTLETS ON RDWY SLOPE	REMARKS	
			GL	CIG	LF	LF	LF	LF	Ea.	Ea.	Ea.	Ea.	Ea.	SY	Ea.	LF.				
	Item No.		459.06	459.061	603.155		603.155		604.184				604.186	604.242	613.319	656.50	656.632			
	Unit		Ea.	Ea.	LF	LF	LF	LF	Ea.	Ea.	Ea.	Ea.	Ea.		SY	Ea.	LF.			
	Station				45' Lt	45' Lt	9' Lt to 0'	0' to 9' Rt.	45' Lt.	9' Lt.	9' Rt.	45' Rt.	0'							
	2740+80	NB																		30" HDPE with Concrete Collar
	2744+00	SB		1									1		56	10	20			
	2750+00	SB		1									1		56	10	20			
	2756+00	SB		1									1		56	10	20			
	2761+00	SB		1									1		56	10	20			
	2764+88																			
	2781+50*	NB							1	1	1	1								
	2784+50*	NB																		
	2790+24*	NB																		
	2792+64*	NB																		
	2804+80*	NB																		
	2812+00	NB																		
	2813+00	SB		1									1		56	10	20		42" HDPE with Concrete Collar	
	2832+50								1	1	1	1								
	2935+72								1	1										
	2980+50								1	1										
	3037+21	SB	1				9	9	1	1	1	1	1		56	10	20			
	3077+21								1	1	1	1								
	3092+00	SB		1											56	10	20			
	3128+34								1	1	1	1								
	3194+17	SB	1		12	12	9	9	1	1	1	1	1		56	10	20			
	3222+35*	NB																		
	3224+00	SB		1									1		56	10	20			
	3228+87	SB	1				9	9	1	1	1	1	1		56	10	20			
	TOTALS		3	7	12	12	27	27	9	9	7	7	9	0	560	100	200			

* See Plan Sheet 31 for Details

NOTES:

- INSTALL 12' - 12" RCP IN SLOPE.
- INSTALL 12' - 12" RCP WITH A CONCRETE COLLAR ON THE ROADWAY SIDE OF CB.
- REPLACE PAVEMENT AND GRAVEL TO MATCH EXISTING TEMPLATE AND IS INCIDENTAL TO ITEM 603.155.
- A TEMPORARY THREE DAY LANE 2 CLOSURE WILL BE ALLOWED FOR THIS WORK.

<p>Scale: NOT TO SCALE</p> <table border="1" style="width:100%; border-collapse: collapse;"> <thead> <tr> <th>No.</th> <th>Revision</th> <th>By</th> <th>Date</th> </tr> </thead> <tbody> <tr><td> </td><td> </td><td> </td><td> </td></tr> <tr><td> </td><td> </td><td> </td><td> </td></tr> <tr><td> </td><td> </td><td> </td><td> </td></tr> </tbody> </table>	No.	Revision	By	Date													<p>Designed by:</p> <div style="text-align: center;"> </div> <table border="1" style="width:100%; border-collapse: collapse;"> <thead> <tr> <th>By</th> <th>Date</th> <th>Checked</th> <th>By</th> <th>Date</th> </tr> </thead> <tbody> <tr> <td>Designed</td> <td>JRL 01/26/2016</td> <td>Checked</td> <td>GJO</td> <td>01/28/2016</td> </tr> <tr> <td>Drawn</td> <td>BMB 01/26/2016</td> <td>In Charge of</td> <td>SRT</td> <td>01/28/2016</td> </tr> </tbody> </table>	By	Date	Checked	By	Date	Designed	JRL 01/26/2016	Checked	GJO	01/28/2016	Drawn	BMB 01/26/2016	In Charge of	SRT	01/28/2016	<p>MAINE TURNPIKE AUTHORITY 2360 Congress Street Portland, ME 04102 TEL (207) 871-7771 FAX (207) 879-5567</p>	<div style="text-align: center;"> <h2 style="margin: 0;">THE GOLD STAR MEMORIAL HIGHWAY</h2> </div> <p>MTA PROJECT MANAGER: Joseph R. Leavitt, P.E.</p>	<p style="text-align: center;">PAVEMENT REHABILITATION CLEAR ZONE IMPROVEMENTS</p> <p style="text-align: center;">DRAINAGE SUMMARY</p> <p style="text-align: right;">SHEET NUMBER: C-2</p> <p>CONTRACT: 2016.01 3 of 77</p>
No.	Revision	By	Date																																
By	Date	Checked	By	Date																															
Designed	JRL 01/26/2016	Checked	GJO	01/28/2016																															
Drawn	BMB 01/26/2016	In Charge of	SRT	01/28/2016																															



MAINLINE WORK LIMITS

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No.	Revision	By	Date

Designed by:



	By	Date		By	Date
Designed	JRL	01/26/2016	Checked	GJO	01/28/2016
Drawn	BMB	01/26/2016	In Charge of	SRT	01/26/2016

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**THE GOLD STAR
 MEMORIAL HIGHWAY**

MTA PROJECT MANAGER: Joseph R. Leavitt, P.E.

PAVEMENT REHABILITATION
 CLEAR ZONE IMPROVEMENTS
 PAVING LIMITS OVERVIEW

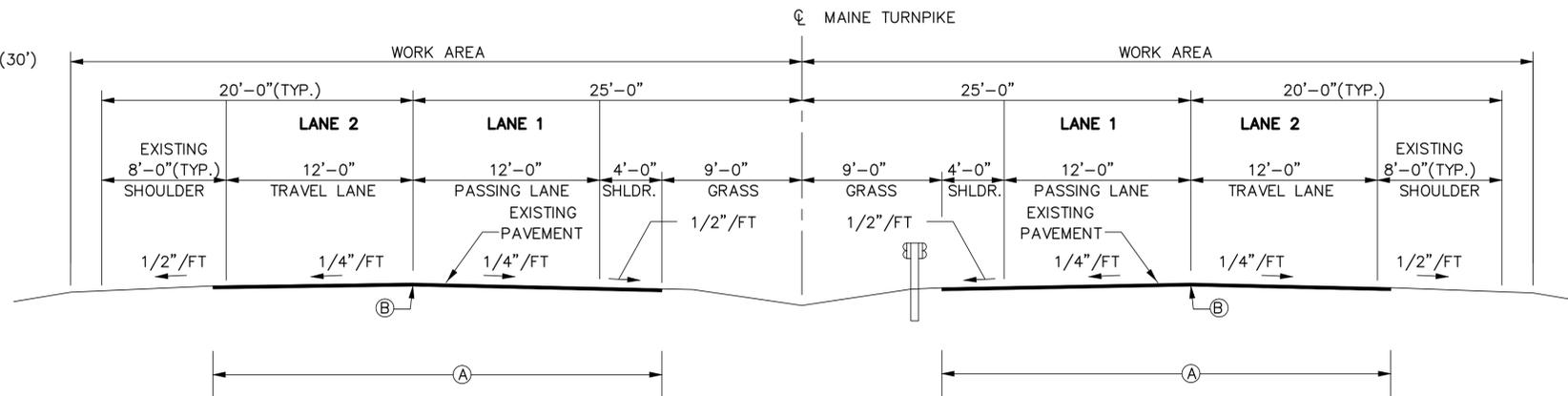
CONTRACT: 2016.01

SHEET NUMBER: C-3

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NB & SB 54.5-57.0
 1 1/2" M&F
 PAVEMENT LEGEND

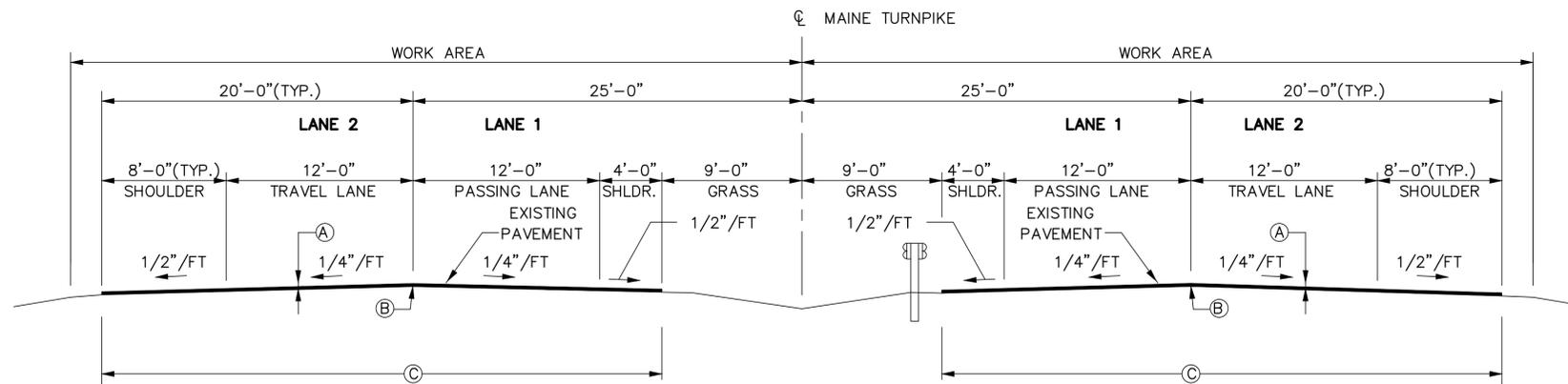
- Ⓐ = LIMITS OF 1 1/2" MILL AND 1 1/2" 12.5mm FILL (30')
- Ⓑ = DOUBLE LAYER OF TACK ON NOTCH WEDGE



PAVEMENT MILL & FILL LOCATIONS 1 & 2
 MM 54.5 - MM 57.0

NB & SB 54.5-57.0
 1 1/2" OVERLAY
 PAVEMENT LEGEND

- Ⓐ = 1 1/2" HOT MIX ASPHALT, 12.5mm NOMINAL MAXIMUM SIZE
- Ⓑ = DOUBLE LAYER OF TACK ON NOTCH WEDGE
- Ⓒ = LIMITS OF OVERLAY (36'-0")

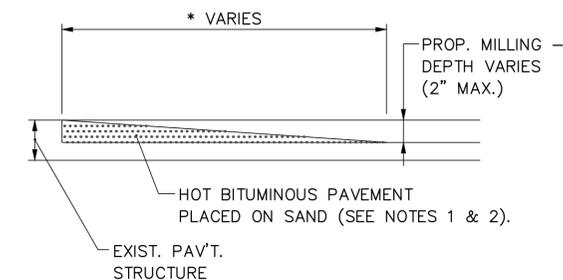


PAVEMENT OVERLAY LOCATIONS 1 & 2
 MM 54.5 - MM 57.0

REFERENCE:

- MM 64.4 = STA 3240+50
- MM 59.5 = STA 2982+70
- MM 57.0 = STA 2850+00
- MM 54.5 = STA 2726+50

* 2" = 35'
 1/2" = 29'



TEMPORARY BITUMINOUS RAMP
 N.T.S.

NOTES:

1. HOT MIX ASPHALT FOR TEMPORARY RAMPS WILL NOT BE MEASURED FOR PAYMENT, BUT SHALL BE INCIDENTAL TO HOT MIX ASPHALT 12.5mm.
2. REMOVAL OF TEMP. BITUMINOUS RAMPS WILL NOT BE MEASURED FOR PAYMENT, BUT SHALL BE INCIDENTAL TO HOT MIX ASPHALT 12.5mm.

NOTES:

1. A COATING OF HOT RUBBERIZED ASPHALT (SS-S-1401C) SHALL BE APPLIED TO ALL TRANSVERSE BUTT JOINTS AND LONGITUDINAL JOINTS EXCEPT WHERE THE NOTCHED WEDGE IS USED.
2. BITUMINOUS TACK COAT IS REQUIRED BETWEEN EXISTING PAVEMENT AND HMA, 12.5 MM, 9.5 MM OR SHIM.
3. REFERENCE PLAN SHEET 12 FOR ADDITIONAL CRACK REPAIRS.

Scale:			
NOT TO SCALE			
No.	Revision	By	Date

Designed by:					
					
	By	Date		By	Date
	JRL	01/26/2016	Checked	GJO	01/26/2016
	Drawn	BMB	01/26/2016	In Charge of	SRT
					01/26/2016

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THE GOLD STAR MEMORIAL HIGHWAY

MTA PROJECT MANAGER: Joseph R. Leavitt, P.E.

PAVEMENT REHABILITATION
CLEAR ZONE IMPROVEMENTS

TYPICAL SECTIONS
LOCATIONS 1 & 2

SHEET NUMBER: P-1

CONTRACT: 2016.01

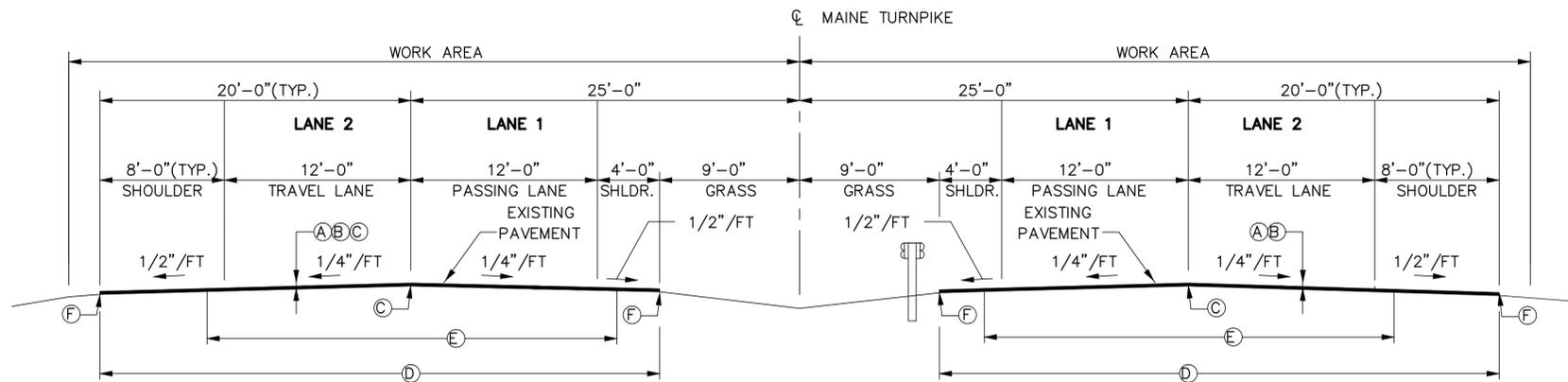
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REFERENCE:

MM 64.4 = STA 3240+50
 MM 59.5 = STA 2982+70
 MM 57.0 = STA 2850+00
 MM 54.5 = STA 2726+50

SOUTHBOUND
 MM64.4-3186+50, 3151+00-2922+83,
 2910+83-MM57.0
NORTHBOUND
 MM59.5-3151+00, 3186+00-MM64.4
2" OVERLAY
PAVEMENT LEGEND

- (A) = 1 1/2" HOT MIX ASPHALT, 12.5mm NOMINAL MAXIMUM SIZE
- (B) = 1/2" HOT MIX ASPHALT SHIMMING
- (C) = DOUBLE LAYER OF TACK ON NOTCH WEDGE
- (D) = LIMITS OF OVERLAY (36'-0")
- (E) = LIMITS OF SHIM (28'-0")
- (F) = BERM DROPOFF CORRECTION OR BERM CORRECTION

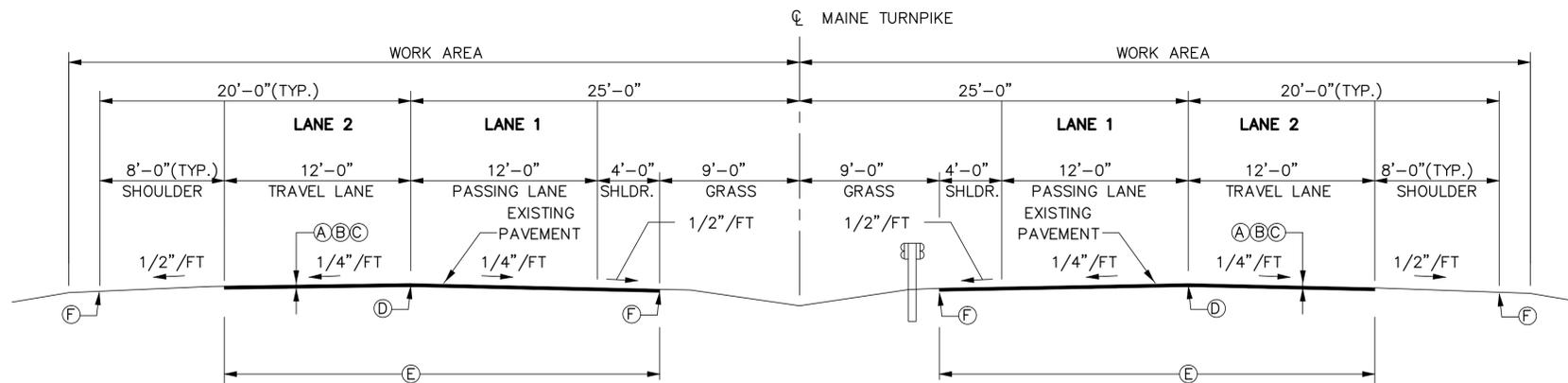


SOUTHBOUND PAVEMENT OVERLAY
LOCATIONS 3 & 4
 MM64.4-3186+50, 3151+00-2922+83,
 2910+83-MM57.0

NORTHBOUND PAVEMENT OVERLAY
LOCATIONS 3 & 4
 MM59.5-3151+00, 3186+00-MM64.4

NB & SB
 3151+00-3186+50, 2910+83-2922+83 SB ONLY
2" M&F
PAVEMENT LEGEND

- (A) = 1 1/2" HOT MIX ASPHALT, 12.5mm NOMINAL MAXIMUM SIZE
- (B) = 1/2" HOT MIX ASPHALT SHIMMING
- (C) = MILLING DEPTH VARIES AS CONTROLLED BY CROSS SLOPE
- (D) = DOUBLE LAYER OF TACK ON NOTCH WEDGE
- (E) = LIMITS OF MILL AND FILL (28'-0")
- (F) = BERM DROPOFF CORRECTION OR BERM CORRECTION



NORTHBOUND & SOUTHBOUND
PAVEMENT MILL & FILL LOCATIONS 3 & 4
 3151+00-3186+50, 2910+83-2922+83

NOTES:

1. A COATING OF HOT RUBBERIZED ASPHALT (SS-S-1401C) SHALL BE APPLIED AT THE TOP SURFACE LIFT TO ALL TRANSVERSE BUTT JOINTS AND LONGITUDINAL JOINTS EXCEPT WHERE THE NOTCHED WEDGE IS USED.
2. BITUMINOUS TACK COAT IS REQUIRED BETWEEN EXISTING PAVEMENT AND HMA, 12.5 MM, 9.5 MM OR SHIM.

Scale: NOT TO SCALE			
No.	Revision	By	Date

Designed by:					
	By	Date		By	Date
	JRL	01/26/2016	Checked	GJO	01/26/2016
	BMB	01/26/2016	In Charge of	SRT	01/26/2016

MAINE TURNPIKE AUTHORITY
 2360 Congress Street
 Portland, ME 04102
 TEL (207) 871-7771
 FAX (207) 879-5567



THE GOLD STAR
MEMORIAL HIGHWAY

MTA PROJECT MANAGER: Joseph R. Leavitt, P.E.

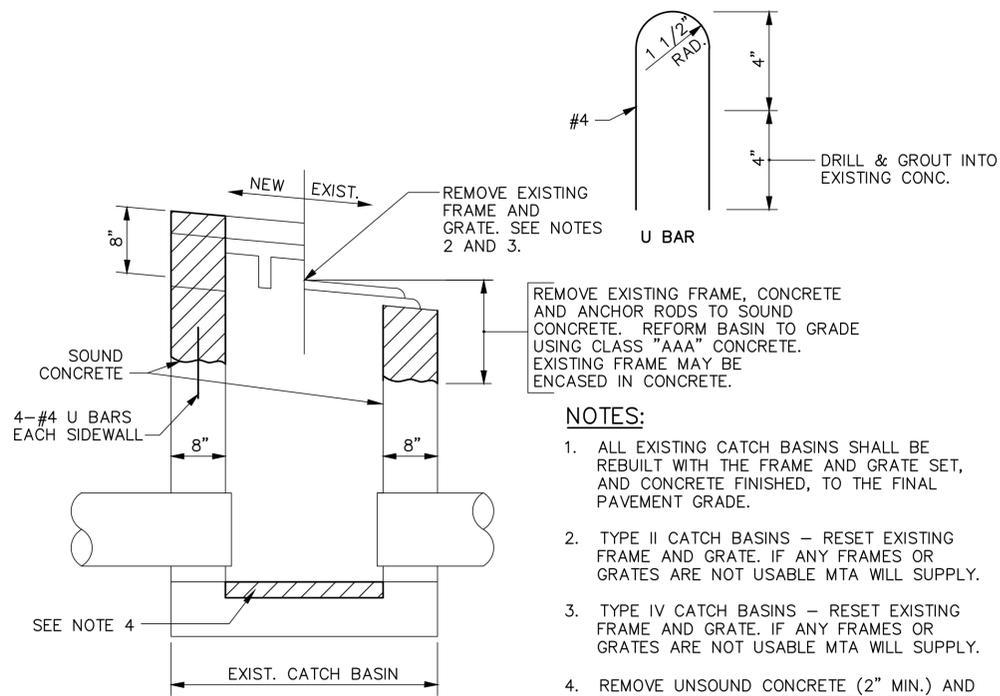
PAVEMENT REHABILITATION
 CLEAR ZONE IMPROVEMENTS

TYPICAL SECTIONS
 LOCATIONS 3 & 4

SHEET NUMBER: P-2

CONTRACT: 2016.01

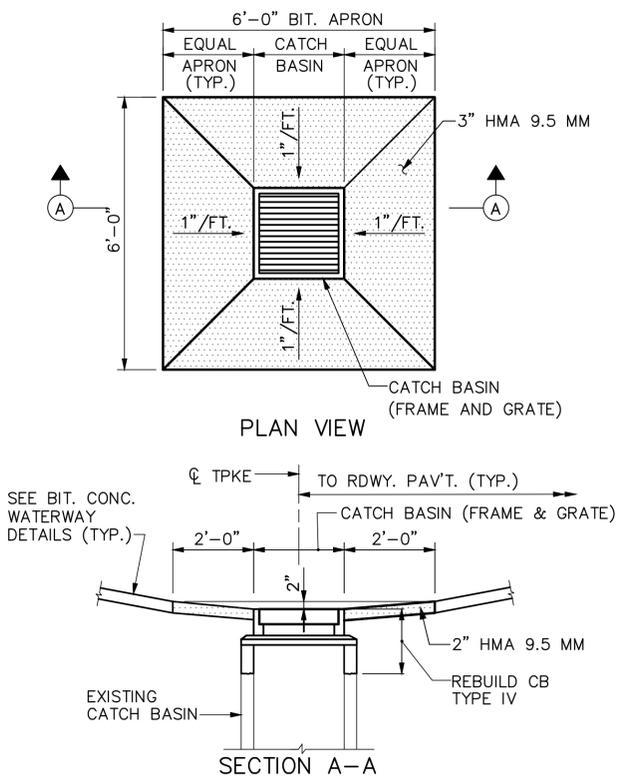
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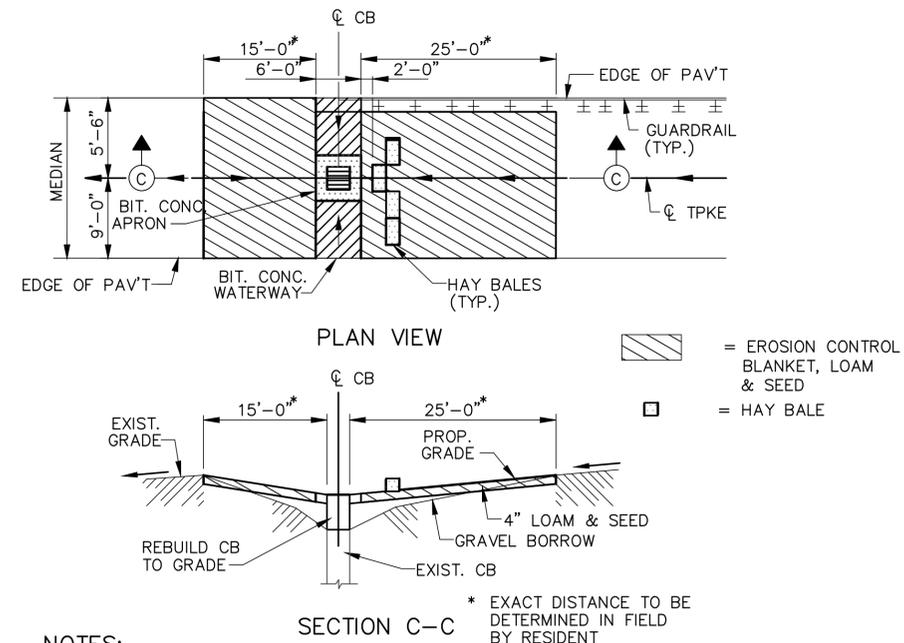
NOTES:

1. ALL EXISTING CATCH BASINS SHALL BE REBUILT WITH THE FRAME AND GRATE SET, AND CONCRETE FINISHED, TO THE FINAL PAVEMENT GRADE.
2. TYPE II CATCH BASINS - RESET EXISTING FRAME AND GRATE. IF ANY FRAMES OR GRATES ARE NOT USABLE MTA WILL SUPPLY.
3. TYPE IV CATCH BASINS - RESET EXISTING FRAME AND GRATE. IF ANY FRAMES OR GRATES ARE NOT USABLE MTA WILL SUPPLY.
4. REMOVE UNSOUND CONCRETE (2" MIN.) AND REPLACE WITH NEW CONCRETE IF DIRECTED BY THE RESIDENT.

REBUILD CATCH BASIN TO GRADE - TYPE IV
N.T.S.



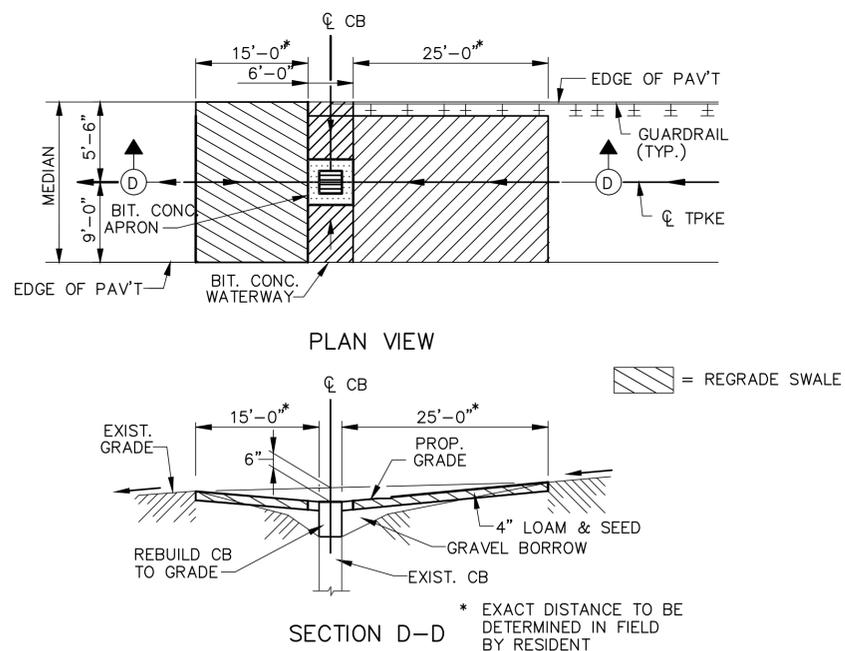
REBUILD CATCH BASIN IN MEDIAN
N.T.S.



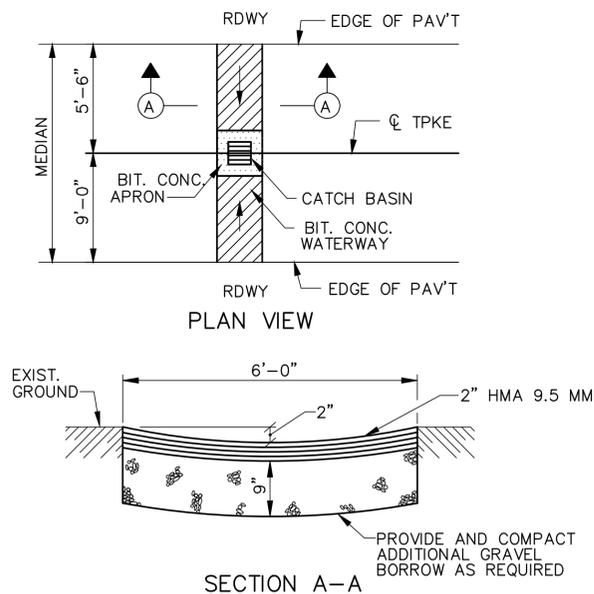
NOTES:

1. INSTALL MINIMUM OF 10 HAY BALES (5 EACH SIDE OF CATCH BASIN) AT LOW POINTS.
2. INSTALL MINIMUM OF 5 HAY BALES UP STREAM OF CATCH BASIN ON GRADE.
3. THE EXACT LOCATION AND NUMBER OF BALES SHALL BE AS DIRECTED BY THE RESIDENT.
4. LOAM AND SEEDING METHOD NUMBER 2 WILL NOT BE MEASURED SEPARATELY FOR PAYMENT, BUT SHALL BE CONSIDERED INCIDENTAL TO ITEMS 459.06 OR 459.061

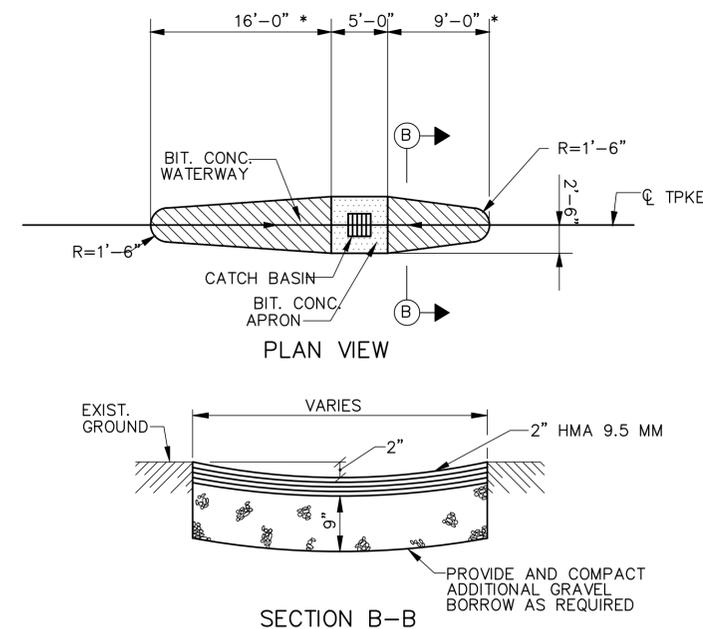
PROPOSED EROSION CONTROL AT MEDIAN
N.T.S.



RIM ELEVATION FOR TYPE IV CATCH BASIN
N.T.S.



BITUMINOUS CONCRETE WATERWAY, TYPE I
N.T.S.



BITUMINOUS CONCRETE WATERWAY, TYPE II
N.T.S.

Scale: NOT TO SCALE

No.	Revision	By	Date

Designed by:

	By	Date	
	JRL	01/26/2016	
	Checked	GJO	01/26/2016
	By	Date	
	Drawn	BMB	01/26/2016
	In Charge of	SRT	01/26/2016

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**THE GOLD STAR
MEMORIAL HIGHWAY**

MTA PROJECT MANAGER: Joseph R. Leavitt, P.E.

PAVEMENT REHABILITATION
CLEAR ZONE IMPROVEMENTS

DRAINAGE DETAILS

SHEET NUMBER: P-3

CONTRACT: 2016.01

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407.081 BERM CORRECTION LOCATIONS:
NB OUTSIDE SHOULDER--45' RIGHT

	STA	TO	STA	Length
NB	2737+00		2743+50	650
NB	2747+00		2749+00	200
NB	2756+00		2756+50	50
NB	2785+00		2785+50	50
NB	2728+50		2835+50	700
NB	2862+00		2866+00	400
NB	2917+00		2918+50	250
NB	2940+00		2943+00	300
NB	2987+50		3000+50	1300
NB	3011+00		3012+00	100
NB	3060+00		3065+00	500
NB	3074+00		3076+00	200
NB	3090+00		3092+50	250
NB	3159+00		3160+50	150

407.081 BERM CORRECTION LOCATIONS:
NB INSIDE SHOULDER--9' LEFT

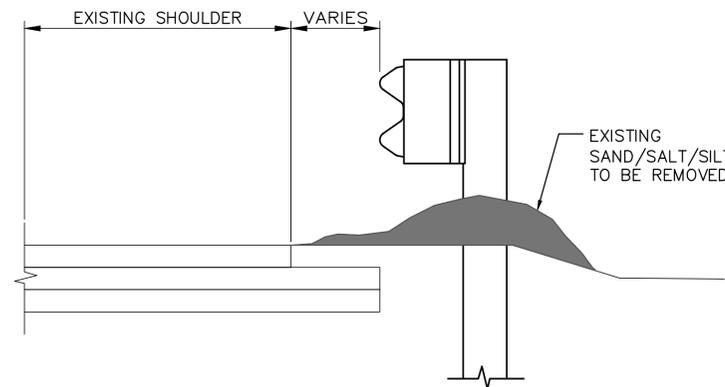
	STA	TO	STA	Length
NB	2780+00		2828+50	850
NB	2846+00		2848+00	200
NB	2911+00		2918+50	750
NB	3002+00		3004+00	200
NB	3009+00		3013+00	400
NB	3015+00		3029+00	1400
NB	3033+00		3045+50	1250
NB	3046+50		3048+50	200
NB	3090+00		3092+50	250
NB	3135+00		3143+00	835
NB	3154+00		3160+50	650
NB	3194+50		3197+50	300
NB	3232+00		3233+50	150

407.081 BERM CORRECTION LOCATIONS:
SB INSIDE SHOULDER--9' LEFT

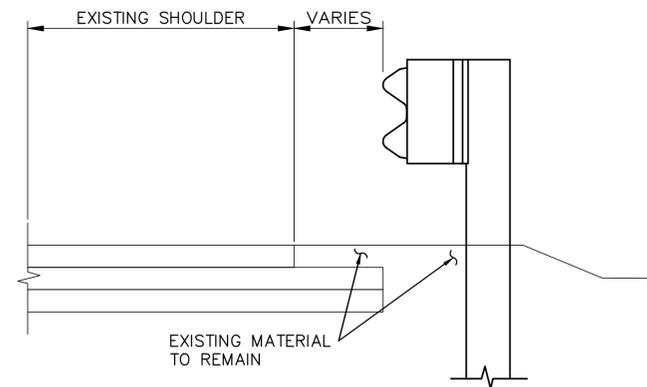
	STA	TO	STA	Length
SB	3185+00		3182+00	300
SB	3180+50		3179+00	150
SB	3175+00		3168+00	700
SB	3162+00		3159+00	300
SB	3152+00		3144+00	800
SB	3120+00		3093+50	2650
SB	3089+00		3079+00	1000
SB	3060+00		3053+00	700
SB	3051+00		3049+00	200
SB	3033+50		3031+00	250
SB	3008+00		3003+00	500
SB	2985+00		2981+00	400
SB	2932+00		2917+00	1500
SB	2910+00		2849+00	6100

407.081 BERM CORRECTION LOCATIONS:
SB OUTSIDE SHOULDER--45' RIGHT

	STA	TO	STA	Length
SB	3235+00		3234+50	150
SB	3226+50		3226+75	25
SB	3213+75		3211+00	275
SB	3210+50		3208+50	200
SB	3207+25		3203+00	425
SB	3195+00		3189+50	550
SB	3174+60		3174+00	60
SB	3172+00		3169+75	225
SB	3160+75		3158+75	200
SB	3132+25		3129+75	250
SB	3126+00		3124+50	150
SB	3123+00		3120+50	250
SB	3119+50		3117+50	200
SB	2986+00		2984+00	200
SB	2947+00		2944+50	250
SB	2937+50		2936+00	150
SB	2928+00		2925+50	250
SB	2920+00		2917+50	250
SB	2887+00		2884+00	300
SB	2837+00		2835+00	200
SB	2832+00		2830+00	200
SB	2821+00		2811+00	1000
SB	2786+50		2785+50	100
SB	2761+50		2749+00	1250

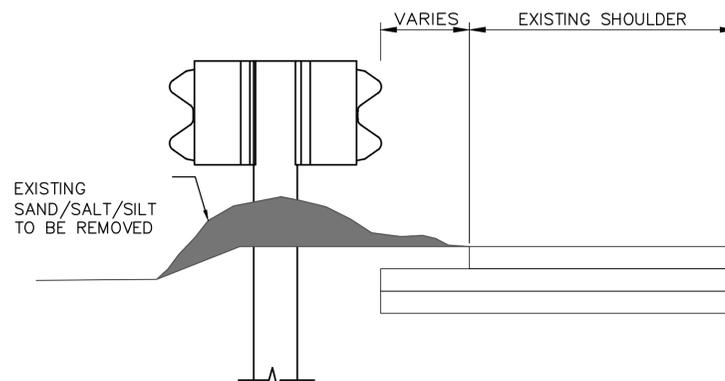


EXISTING SECTION

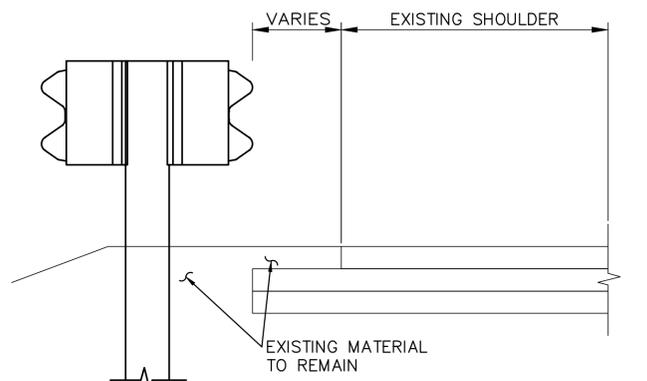


PROPOSED SECTION

**BERM CORRECTION WITH GUARDRAIL
RIGHT OUTSIDE SHOULDER
NOT TO SCALE**

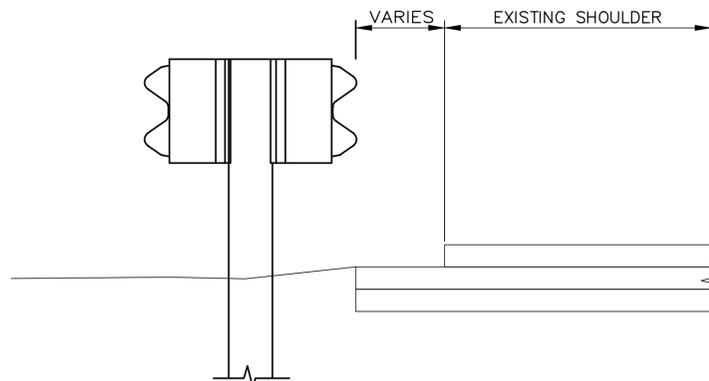


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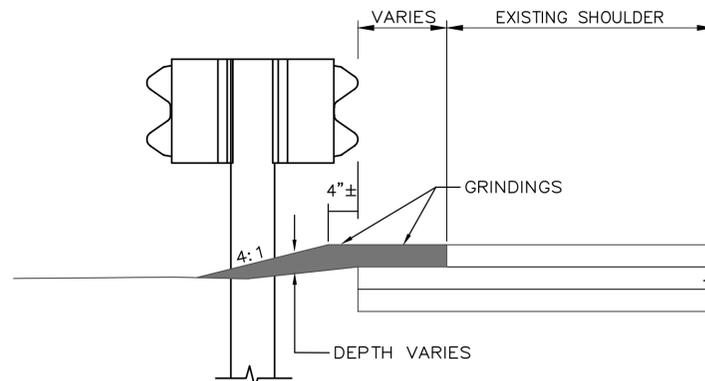


PROPOSED SECTION

**BERM CORRECTION WITH GUARDRAIL
MEDIAN GUARDRAIL
NOT TO SCALE**

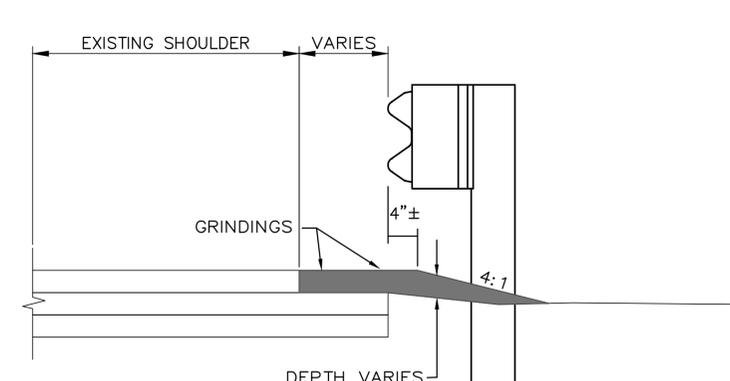


EXISTING SECTION



PROPOSED SECTION

**BERM DROPOFF CORRECTION WITH GUARDRAIL
MEDIAN GUARDRAIL
NOT TO SCALE
DETAIL B**

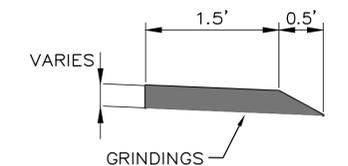


DEPTH VARIES

**BERM DROPOFF CORRECTION WITH GUARDRAIL
RIGHT OUTSIDE SHOULDER
NOT TO SCALE**

NOTES:

1. BERM DROPOFF CORRECTION WILL BE PLACED AT ALL LOCATIONS EXCEPT WHERE THE BERM CORRECTION WORK OCCURS
2. LEFT AND RIGHT ARE SPECIFIC TO WHETHER TRAVELING NB OR SB.
3. SEE BERM CORRECTION SPREADSHEETS FOR LOCATIONS.
4. BERM CORRECTION NEEDS TO BE COMPLETED PRIOR TO MILLING UNLESS THE HIGH EXISTING SECTIONS ARE OPENED FOR DRAINAGE PATHS.



**DETAIL A
(WITHOUT GUARDRAIL)**

Scale: NOT TO SCALE			
No.	Revision	By	Date

Designed by:			
			
	By	Date	
	JRL	01/26/2016	
	Checked	GJO	01/26/2016
	Drawn	BMB	01/26/2016
	In Charge of	SRT	01/26/2016

MAINE TURNPIKE AUTHORITY
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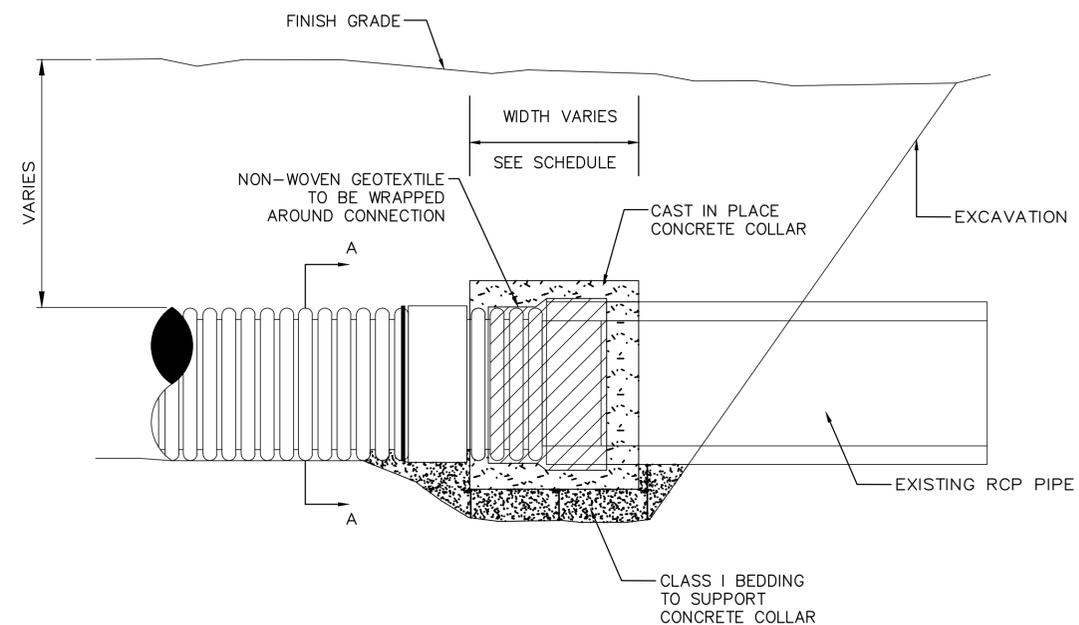
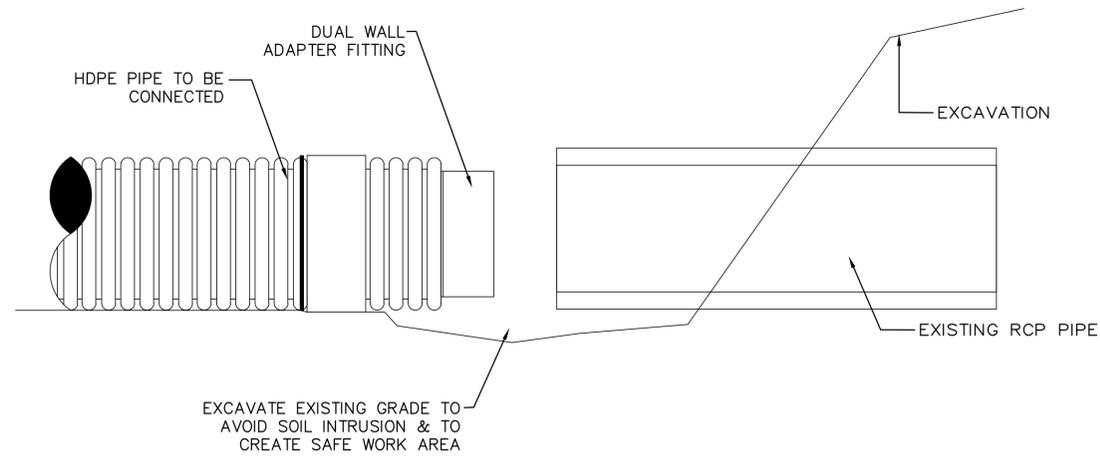
THE GOLD STAR MEMORIAL HIGHWAY

MTA PROJECT MANAGER: Joseph R. Leavitt, P.E.

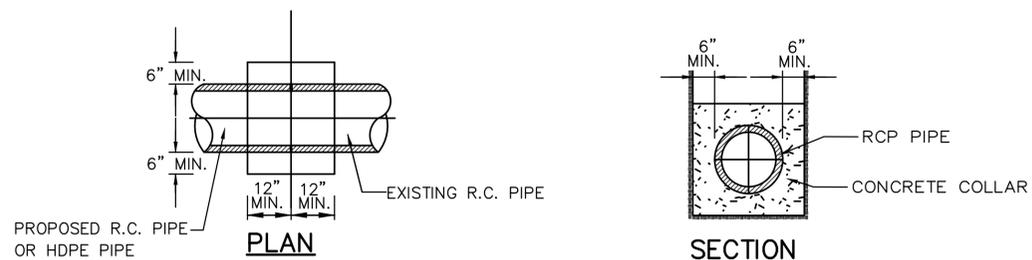
PAVEMENT REHABILITATION
CLEAR ZONE IMPROVEMENTS

BERM DROPOFF
CORRECTION DETAILS

SHEET NUMBER: P-4
CONTRACT: 2016.01
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CONCRETE COLLAR WIDTH SCHEDULE	
PIPE DIA. (INCHES)	WIDTH OF CONC. (INCHES)
12	24
15	24
18	24
24	24
30	30
36	36
42	48
48	48



CONCRETE COLLAR
N.T.S.

NOTES:

1. CONNECTION AND PIPE TO BE BACKFILLED PER ASTM D2321.
2. CONCRETE SHALL BE CLASS AAA FIBER REINFORCED.
3. SEE CONCRETE COLLAR WIDTH SCHEDULE FOR DIMENSIONS.

Scale:			
NOT TO SCALE			
No.	Revision	By	Date

Designed by:					
					
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THE GOLD STAR MEMORIAL HIGHWAY

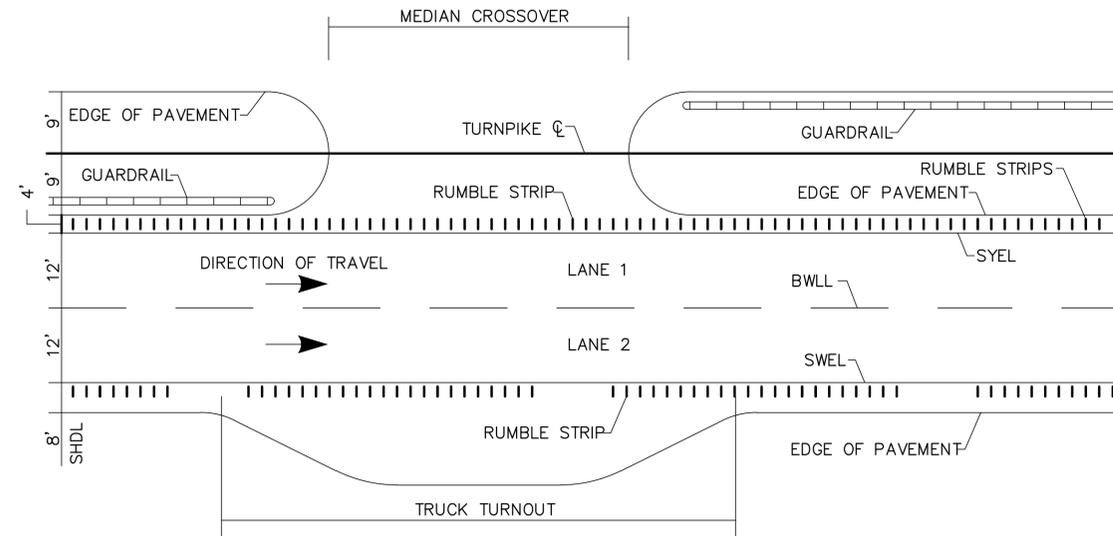
MTA PROJECT MANAGER: Joseph R. Leavitt, P.E.

PAVEMENT REHABILITATION
CLEAR ZONE IMPROVEMENTS

MISCELLANEOUS DETAILS 1

SHEET NUMBER: P-5

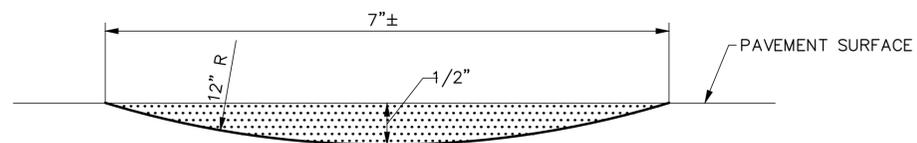
CONTRACT: 2016.01 9 of 77



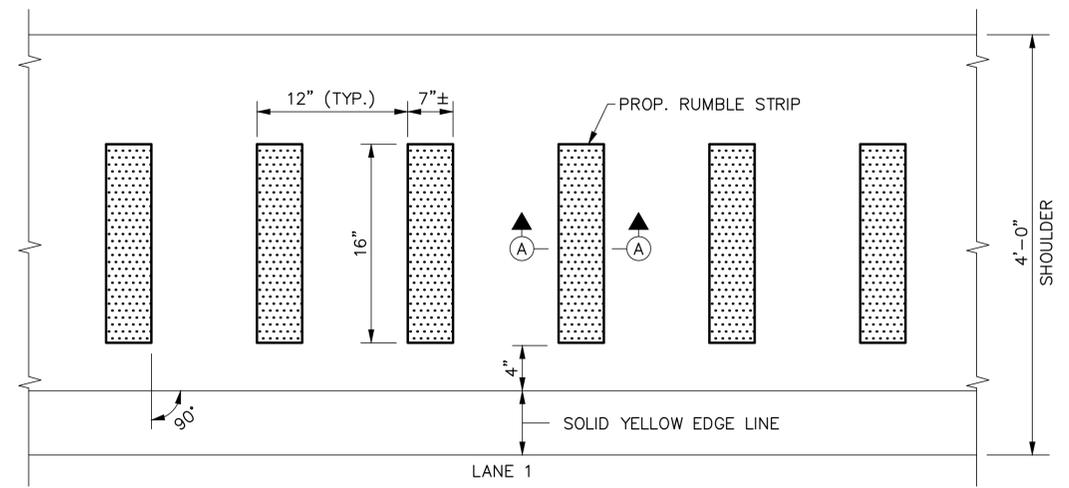
**LOCATION OF RUMBLE STRIPS
CROSSOVERS & TRUCK TURNOUTS**
N.T.S.

NOTES:

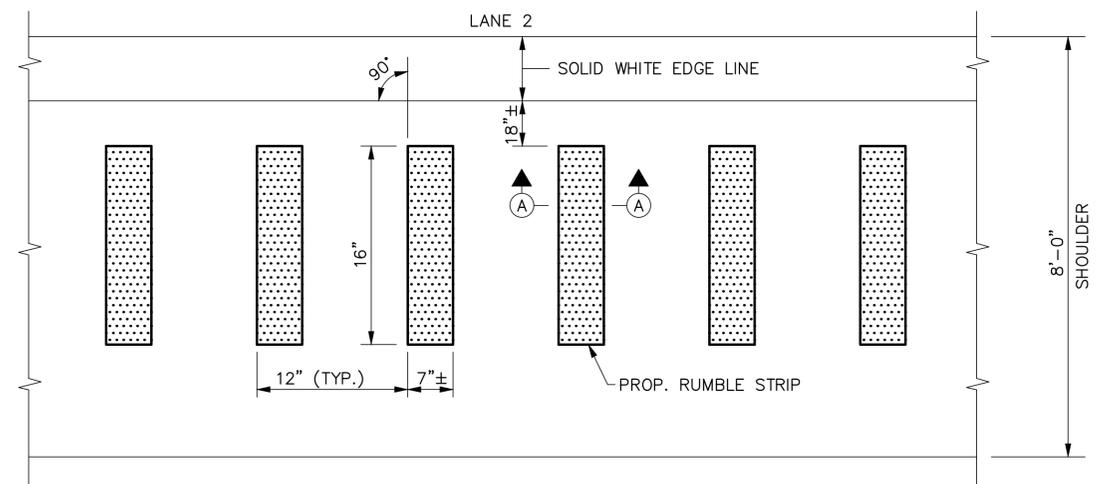
1. INSTALL LANE 2 RUMBLE STRIPS WITH A 20' GAP EVERY 100'. (I.E. 80' OF RUMBLE STRIP AND 20' OF NO RUMBLE STRIP; REPEAT...)
2. LANE 1 RUMBLE STRIPS NEEDED FROM SB MM 54.5 TO MM 64.4
3. LANE 2 RUMBLE STRIPS NEEDED FROM NB MM 54.5 TO MM 57.0
4. LANE 2 RUMBLE STRIPS NEEDED FROM NB MM 59.5 TO 3151+00, 3186+50 TO MM 64.4
5. LANE 2 RUMBLE STRIPS NEEDED FROM SB MM 64.4 TO 3186+50, 3151+00 TO 2922+83, 2910+83 TO MM 57.0
6. LANE 1 RUMBLE STRIPS NOT REPLACED FROM STATION 2910+83 TO 2922+83



SECTION A-A
N.T.S.



PLAN VIEW - LANE 1
N.T.S.



PLAN VIEW - LANE 2
N.T.S.

RUMBLE STRIPS DETAILS

Scale: NOT TO SCALE			
No.	Revision	By	Date

Designed by:					
					
	By	Date		By	Date
	JRL	01/26/2016	Checked	GJO	01/26/2016
	BMB	01/26/2016	In Charge of	SRT	01/26/2016

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**THE GOLD STAR
MEMORIAL HIGHWAY**

MTA PROJECT MANAGER: Joseph R. Leavitt, P.E.

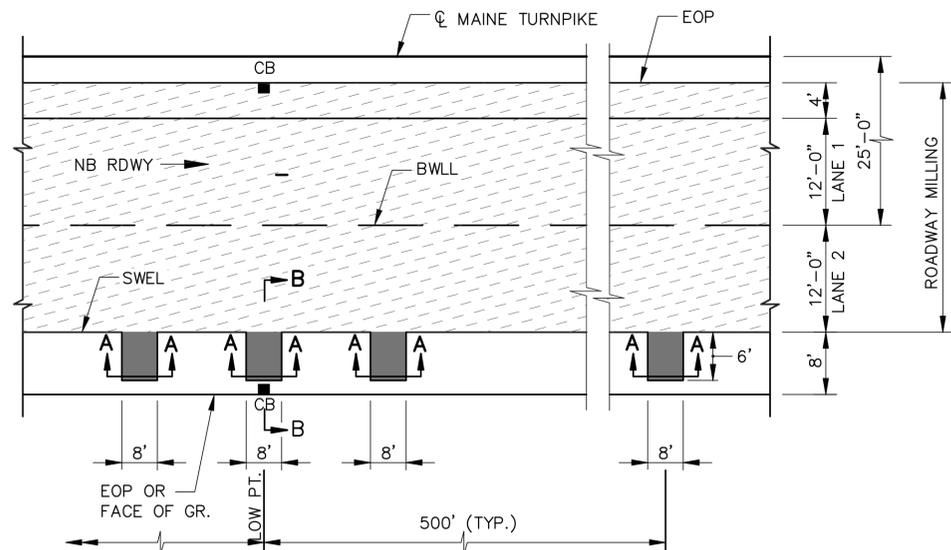
PAVEMENT REHABILITATION
CLEAR ZONE IMPROVEMENTS

MISCELLANEOUS DETAILS 2

CONTRACT: 2016.01

SHEET NUMBER: P-6

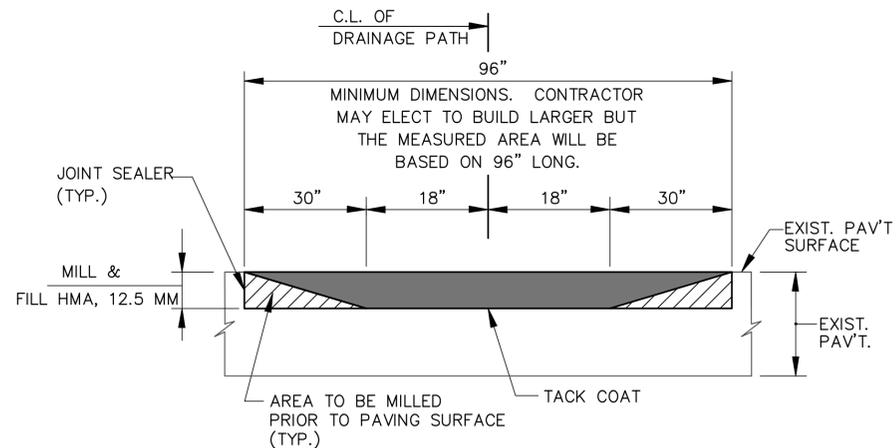
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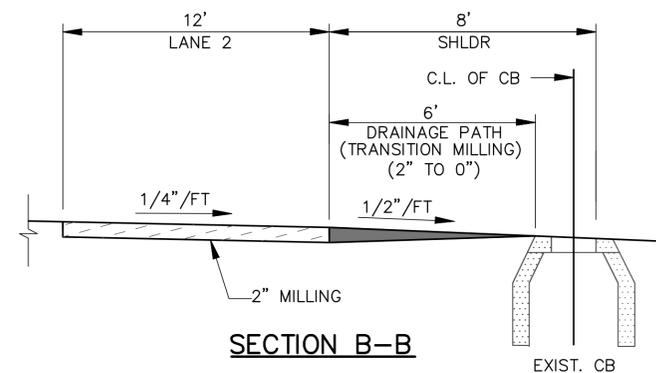
PLAN VIEW
MM68.5 TO MM74.9

NOTES:

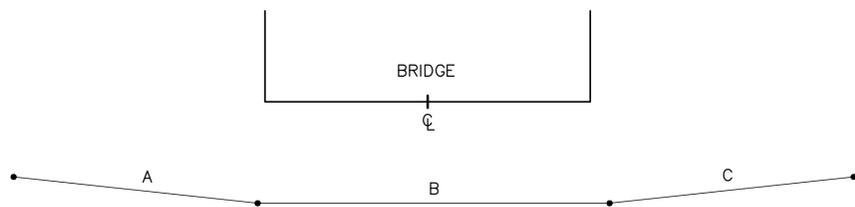
1. THE CATCH BASINS IN THE FOUR FOOT MEDIAN SHOULDERS SHALL HAVE A TEN FOOT TEMPORARY BITUMINOUS RAMP ON BOTH SIDES PARALLEL WITH THE MAINE TURNPIKE AND SHALL BE INCIDENTAL TO HOT MIX ASPHALT 12.5mm.



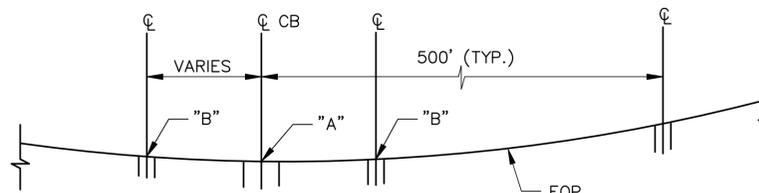
SECTION A-A



SECTION B-B



MILLING TRANSITIONS AT BRIDGE UNDERPASSES					
			A	B	C
HURRICANE RD	MM 56.6	SB	105'	90'	105'
DUTTON HILL RD	MM 59.9	NB/SB	210'	80'	210'
ROUTE 26	MM 64.3	NB	140'	70'	140'



PROFILE

DRAINAGE PATH DETAILS

LEGEND

- = DRAINAGE PATH MILLING
- = ROADWAY MILLING

NOTES:

1. POINT "B" IS 1" HIGHER THAN POINT "A".
2. DRAINAGE PATH REQUIRED AT ALL LOW POINTS.
3. MEDIAN LOW POINTS JUST INSIDE THE FOUR FOOT SHOULDER THAT IS MILL SHALL BE OPENED FOR WATER FLOW IF THE EXISTING MATREIAL IS HIGH. THIS SHALL BE INCIDENTAL TO MILLING.
4. DRAINAGE PATHS SHALL BE PAVED BACK IN FLUSH WITH EXISTING PAVEMENT

Scale: NOT TO SCALE

No.	Revision	By	Date

Designed by:



	By	Date	Checked	By	Date
Designed	JRL	01/26/2016	Checked	GJO	01/26/2016
Drawn	BMB	01/26/2016	In Charge of	SRT	01/26/2016

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**THE GOLD STAR
MEMORIAL HIGHWAY**

MTA PROJECT MANAGER: Joseph R. Leavitt, P.E.

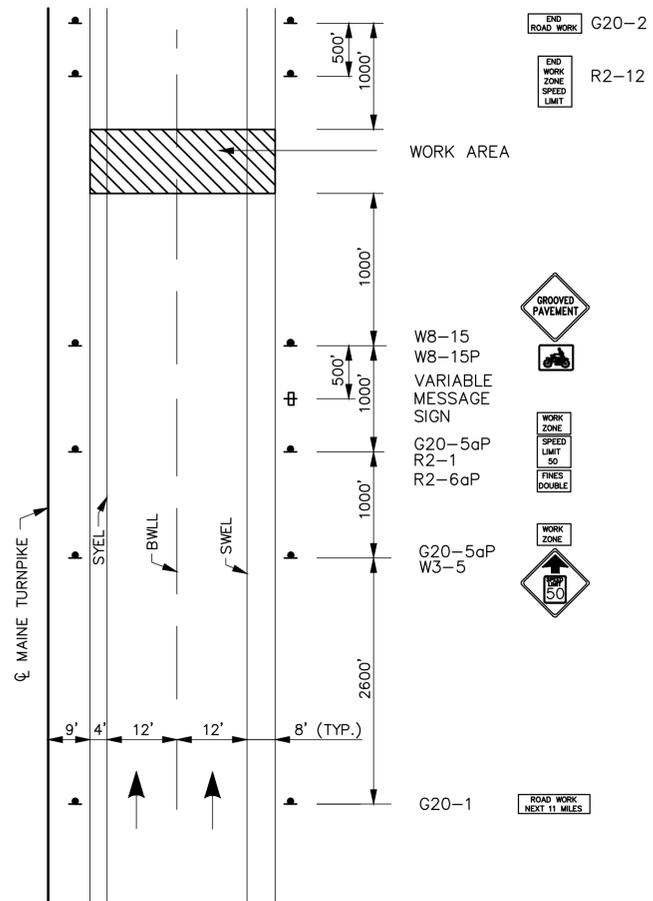
PAVEMENT REHABILITATION
CLEAR ZONE IMPROVEMENTS

MISCELLANEOUS DETAILS 3

SHEET NUMBER: P-7

CONTRACT: 2016.01

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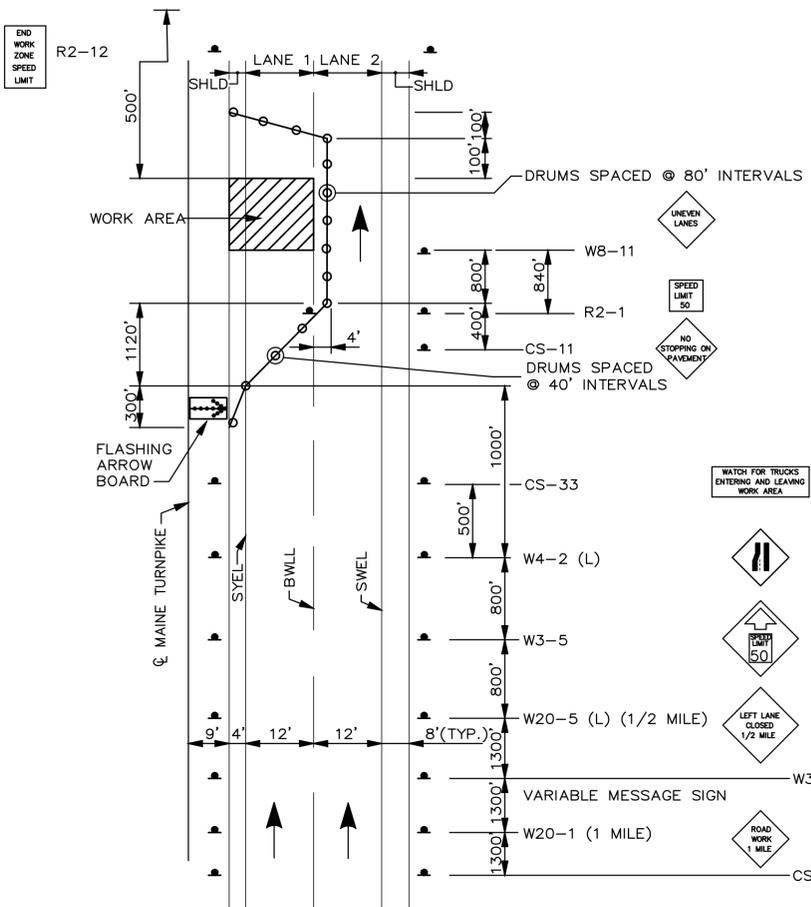


**MAINLINE PAVING
ADVANCED PROJECT SIGNING**

NOTES:

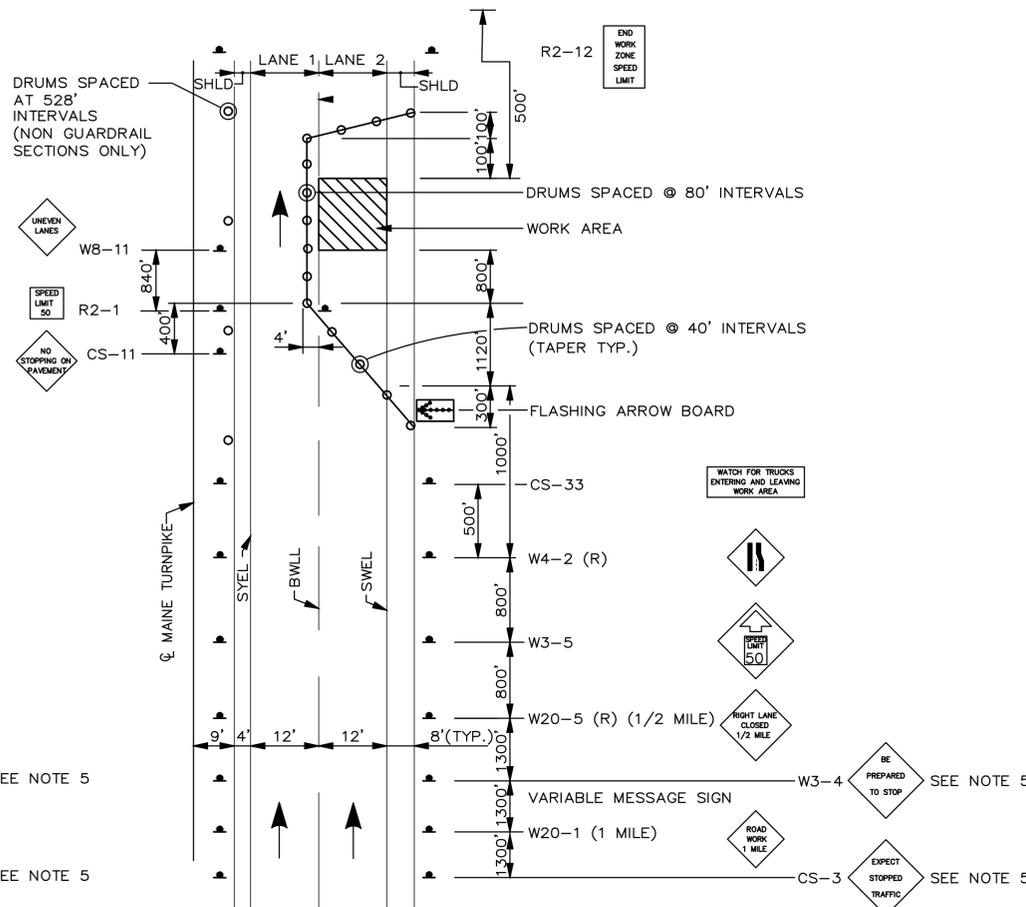
1. FOR SIGN DETAILS, SEE SIGN SUMMARY SHEETS.
2. ALL SIGNS ARE TO BE MOUNTED ON NCHRP 350 APPROVED POSTS.
3. UNCOVER W8-15 AND W8-15P SIGNS WHEN PAVEMENT IS MILLED OR AS DIRECTED BY THE RESIDENT.
4. PROVIDE W8-15 AND W8-15P AT ALL MILLED LOCATIONS.

Scale: NOT TO SCALE		Designed by: 		MAINE TURNPIKE AUTHORITY 2360 Congress Street Portland, ME 04102 TEL (207) 871-7771 FAX (207) 879-5567		THE GOLD STAR MEMORIAL HIGHWAY		PAVEMENT REHABILITATION CLEAR ZONE IMPROVEMENTS TRAFFIC CONTROL LANE CLOSURE DETAILS 1	
No.	Revision	By	Date					SHEET NUMBER: T-1	
				Designed	JRL	01/26/2016	Checked	GJO	01/28/2016
				Drawn	BMB	01/26/2016	In Charge of	SRT	01/28/2016
							MTA PROJECT MANAGER: Joseph R. Leavitt, P.E.		CONTRACT: 2016.01
									13 of 77

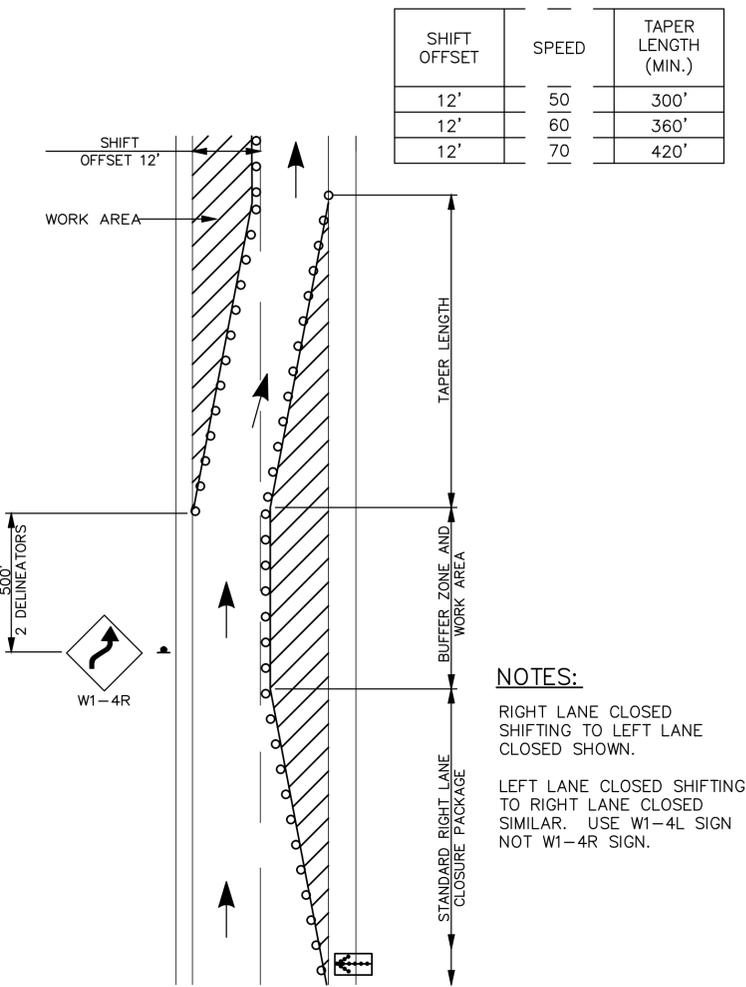


**LANE CLOSURE FOR MILLING AND PAVING OPERATIONS
LANE 1**

**LANE CLOSURE FOR PAVEMENT MARKINGS
LANE 1**



**LANE CLOSURE FOR MILLING AND PAVING OPERATIONS
LANE 2**



**SINGLE LANE CLOSURE
LANE SHIFT TO ADJACENT LANE**

NOTES:
 RIGHT LANE CLOSED SHIFTING TO LEFT LANE CLOSED SHOWN.
 LEFT LANE CLOSED SHIFTING TO RIGHT LANE CLOSED SIMILAR. USE W1-4L SIGN NOT W1-4R SIGN.

- NOTES:**
- FOR SIGN DETAILS, SEE SIGN SUMMARY SHEETS.
 - ALL TEMPORARY LANE CLOSURE SIGNS ARE TO BE MOUNTED ON EASELS.
 - STANDARD SINGLE LANE CLOSURES FOR LANES 1 AND 2 SHALL BE USED FOR DRAINAGE WORK, GUARDRAIL WORK, CLEAR ZONE IMPROVEMENTS, MEDIAN CROSSOVER WORK, EQUIPMENT MOVES, BERM DROP OFF CORRECTION WORK, PAINTING OF SOLID WHITE LANE LINES, AND ALL OTHER WORK NOT REQUIRING ENCROACHMENT INTO THE ADJACENT LANE.
 - SEE SHEET T-3 FOR STANDARD SINGLE LANE CLOSURE DETAILS.
 - ERECT CS-3 AND W3-4 SIGNS IMMEDIATELY PRIOR TO SCHEDULED STOPPAGES FOR EQUIPMENT MOVES AND REMOVE THE CS-3 AND W3-4 SIGNS IMMEDIATELY AFTER THE SCHEDULED STOPPAGES ARE COMPLETE.

Scale: NOT TO SCALE

No.	Revision	By	Date

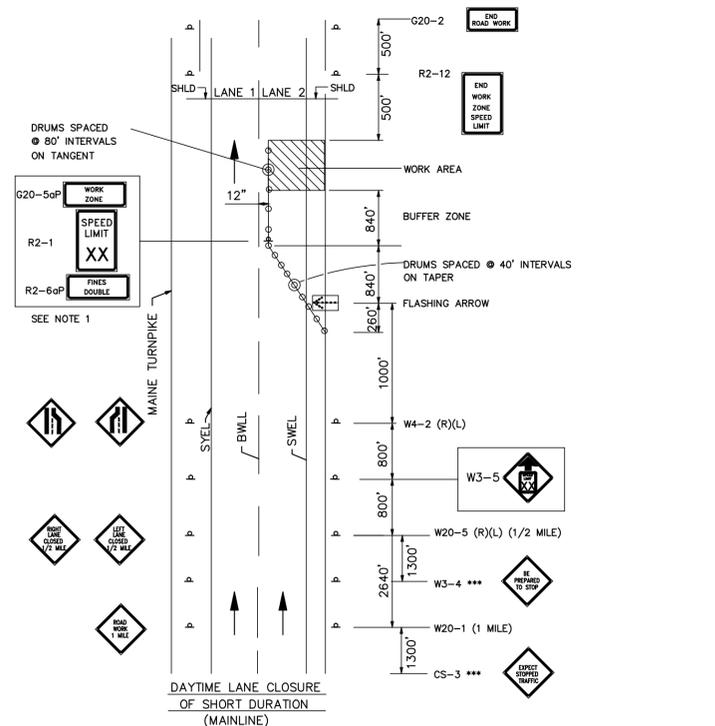
Designed by:

	By	Date	By	Date	
	JRL	01/26/2016	GJO	01/28/2016	
Drawn	BMB	01/26/2016	In Charge of	SRT	01/26/2016

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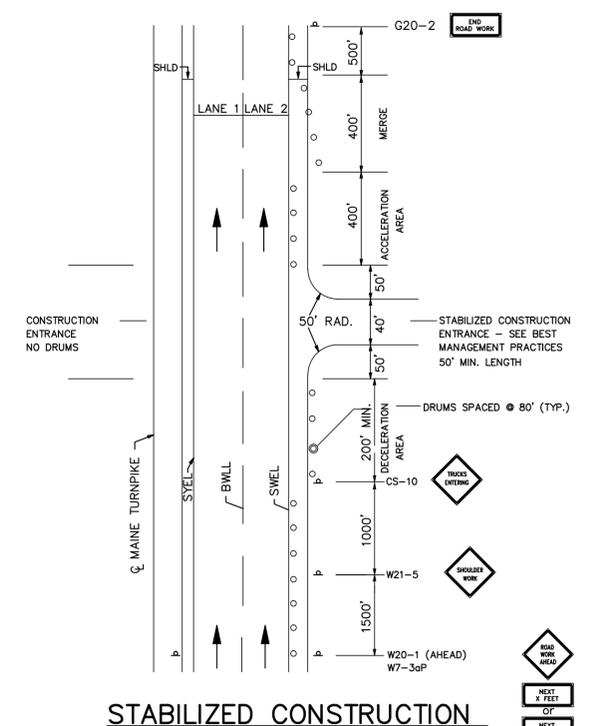
THE GOLD STAR MEMORIAL HIGHWAY

MTA PROJECT MANAGER: Joseph R. Leavitt, P.E.



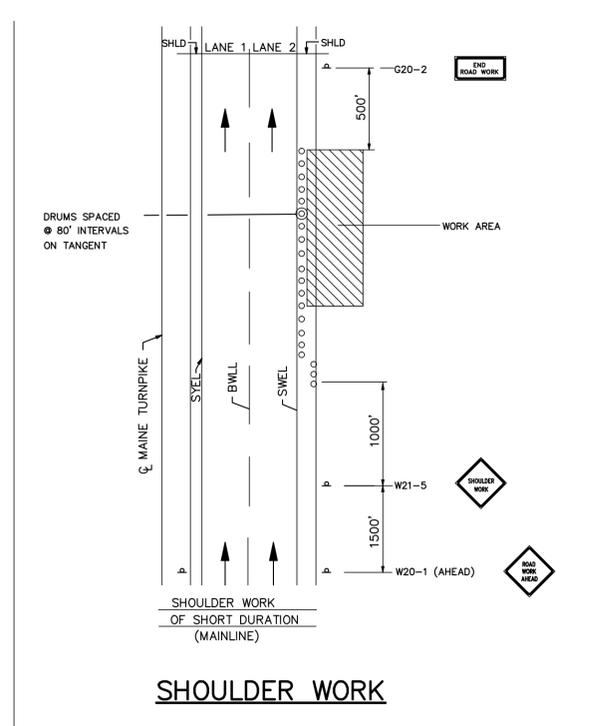
SINGLE LANE CLOSURE SIGNS ON EASELS

1. FOR EASEL SET UP ONLY, THIS SIGN IS NOT BRACKETED. ONE SIGN ASSEMBLY IS PLACED AT THE END OF THE TAPER.



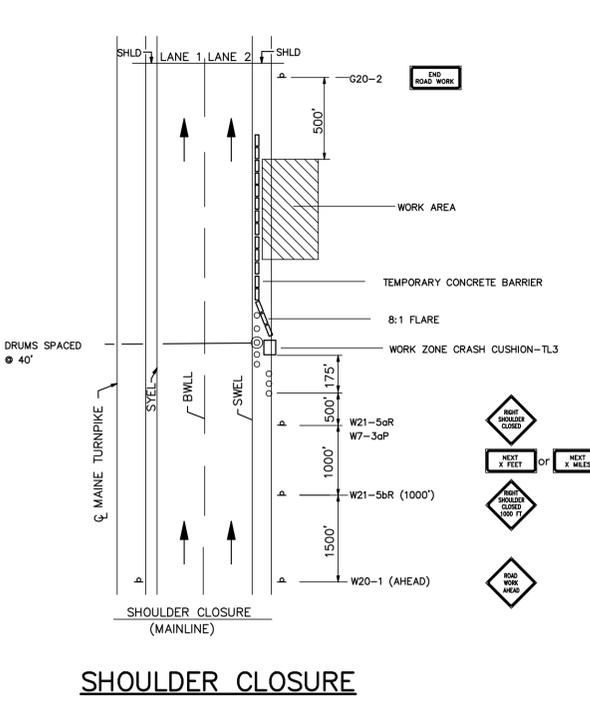
STABILIZED CONSTRUCTION ENTRANCE

1. SIGN G20-2 IS NOT REQUIRED IF ENTRANCE IS WITHIN THE WORK ZONE.



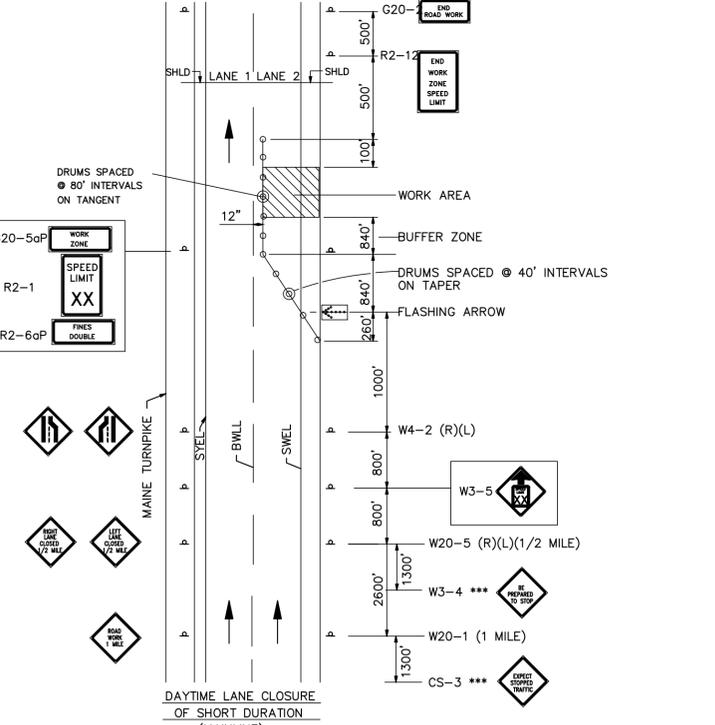
SHOULDER WORK

1. THE W20-1 AND G20-2 SIGNS ARE NOT REQUIRED IF THE SHOULDER CLOSURE IS WITHIN A PREVIOUSLY ESTABLISHED WORK ZONE.

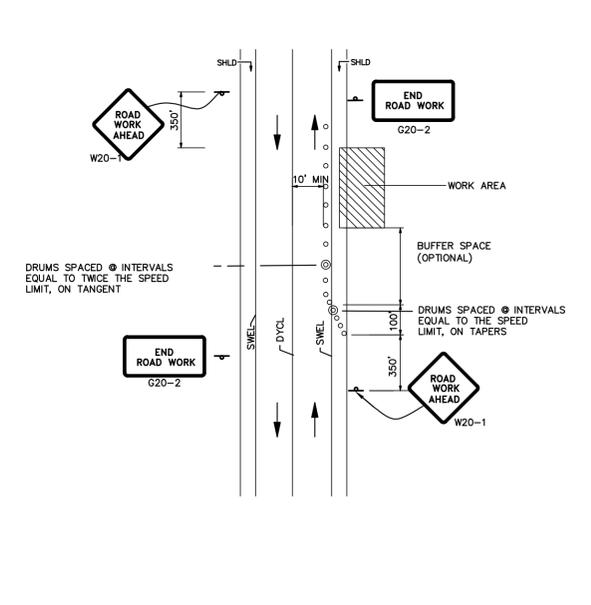


SHOULDER CLOSURE

1. THE W20-1 AND G20-2 SIGNS ARE NOT REQUIRED IF THE SHOULDER CLOSURE IS WITHIN A PREVIOUSLY ESTABLISHED WORK ZONE.

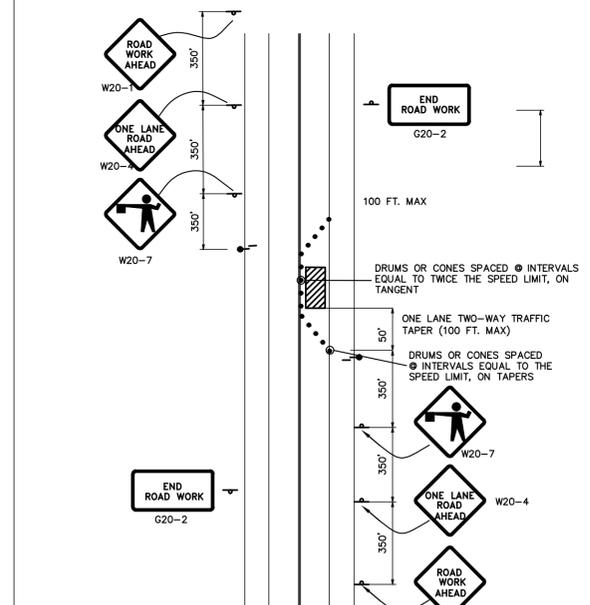


SINGLE LANE CLOSURE - POST MOUNTED SIGN SETUP



TEMP. LOCAL ROAD SHOULDER CLOSURE

NOTES:
1. FOR SHORT DURATION WORK (<1 HOUR), THE TAPER AND CHANNELIZATION DEVICES MAY BE OMITTED IF SHADOW VEHICLE IS PRESENT WITH ACTIVATED HIGH-INTENSITY ROTATING, FLASHING, OSCILLATING OR STROBE LIGHTS.



LOCAL ROAD SINGLE LANE CLOSURE

NOTES:
1. ROAD WORK AHEAD & END ROAD WORK SIGNS MAY BE OMITTED FOR SHORT DURATION OPERATIONS (LESS THAN 1 HOUR).
2. FLAGGERS SHALL BE LOCATED SO THEY ARE CLEARLY VISIBLE TO APPROACHING DRIVERS.
3. SIGN SPACING ASSUMES LOCAL ROAD SPEED IS 40 MPH OR LESS.

GENERAL MAINTENANCE OF TRAFFIC NOTES:

- ALL WORK TO CONFORM TO MAINE DEPARTMENT OF TRANSPORTATION STANDARD SPECIFICATIONS FOR HIGHWAY AND BRIDGES 2014, EXCEPT AS MODIFIED BY THE MAINE TURNPIKE AUTHORITY'S GENERAL AND SPECIAL PROVISIONS.
- ALL PAVEMENT STRIPING & SIGNING SHALL BE IN ACCORDANCE WITH THE "MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES", U.S.D.O.T., F.H.W.A., LATEST EDITION. NOTE THERE IS A 2009 EDITION OF MUTCD.
- THESE PLANS SHOW THE GENERAL CONDITION FOR TURNPIKE MAINLINE TRAFFIC CONTROL. DURING CONSTRUCTION, SLIGHT MODIFICATIONS IN CONSTRUCTION PROCEDURE MAY OCCUR AND MAY REQUIRE SOME MINOR ADJUSTMENTS TO BE MADE IN THE FIELD, ALL PROCEDURES MUST BE APPROVED BY THE RESIDENT.
- THE CONTRACTOR SHALL REMOVE ALL PAVEMENT MARKINGS THAT CONFLICT WITH PROPOSED PAVEMENT MARKINGS IN ACCORDANCE WITH THE SPECIFICATIONS AND MUTCD. PAYMENT SHALL BE MADE UNDER ITEM 627.77 - REMOVING PAVEMENT MARKINGS.
- EXPOSED BARRIER ENDS SHALL BE PROTECTED BY A WORK ZONE CRASH CUSHION. PAYMENT WILL BE UNDER ITEM 527.341 - WORK ZONE CRASH CUSHION - TL-3.

ABBREVIATIONS FOR ALL M.O.T. PLANS

- BWLL = BROKEN WHITE LANE LINE
- SWEL = SOLID WHITE EDGE LINE
- SYEL = SOLID YELLOW EDGE LINE
- TBWLL = TEMPORARY BROKEN WHITE LANE LINE
- TSWEL = TEMPORARY SOLID WHITE EDGE LINE
- TSYEL = TEMPORARY SOLID YELLOW EDGE LINE

Scale: NOT TO SCALE

No.	Revision	By	Date

Designed by:

By	Date	By	Date
JRL	01/26/2016	GJO	01/26/2016
Drawn	BMB	In Charge of	SRT
	01/26/2016		01/26/2016

MAINE TURNPIKE AUTHORITY
2360 Congress Street
Portland, ME 04102
TEL (207) 871-7771
FAX (207) 879-5567

MAINE TURNPIKE

THE GOLD STAR MEMORIAL HIGHWAY

MTA PROJECT MANAGER: Joseph R. Leavitt, P.E.

PAVEMENT REHABILITATION
CLEAR ZONE IMPROVEMENTS

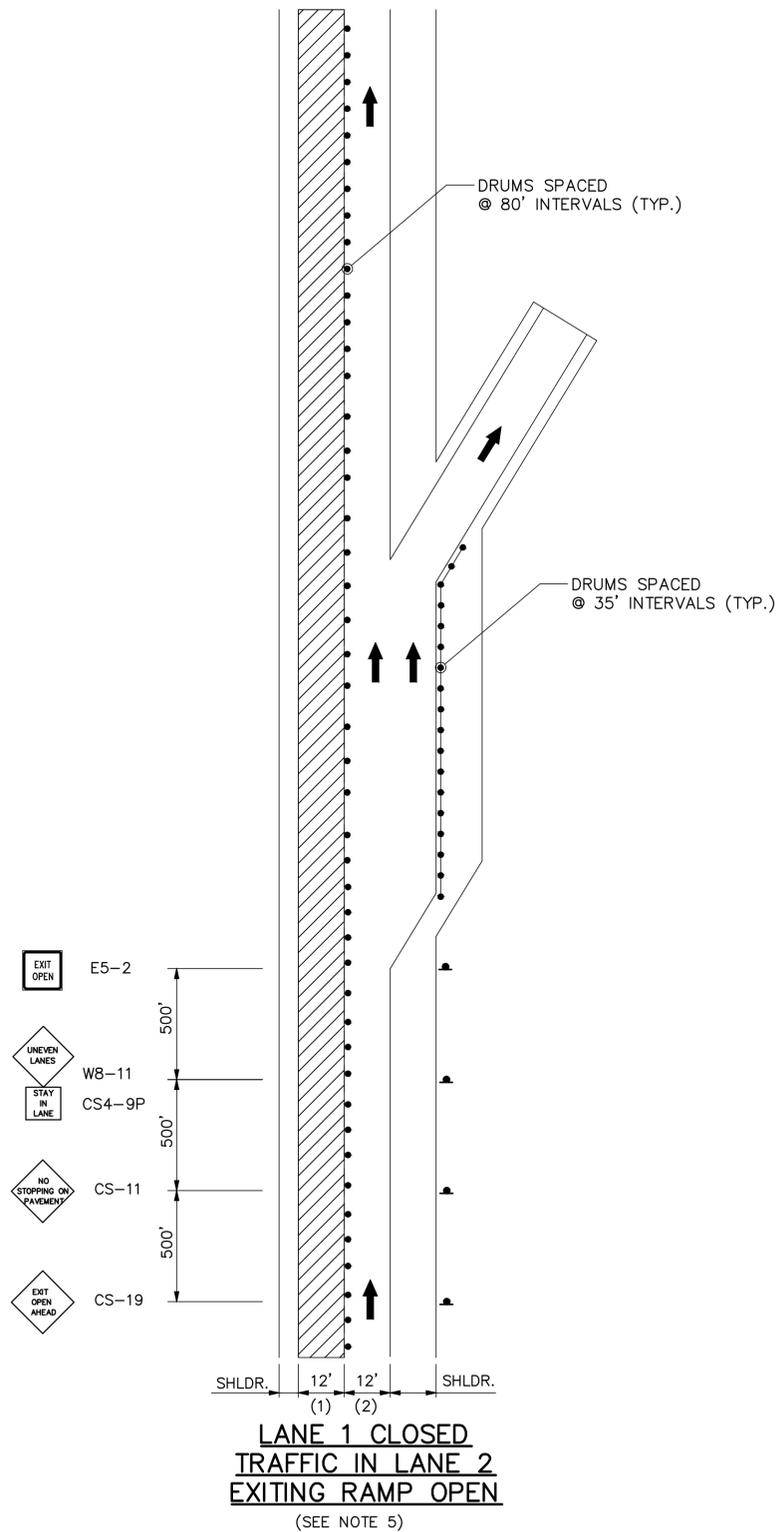
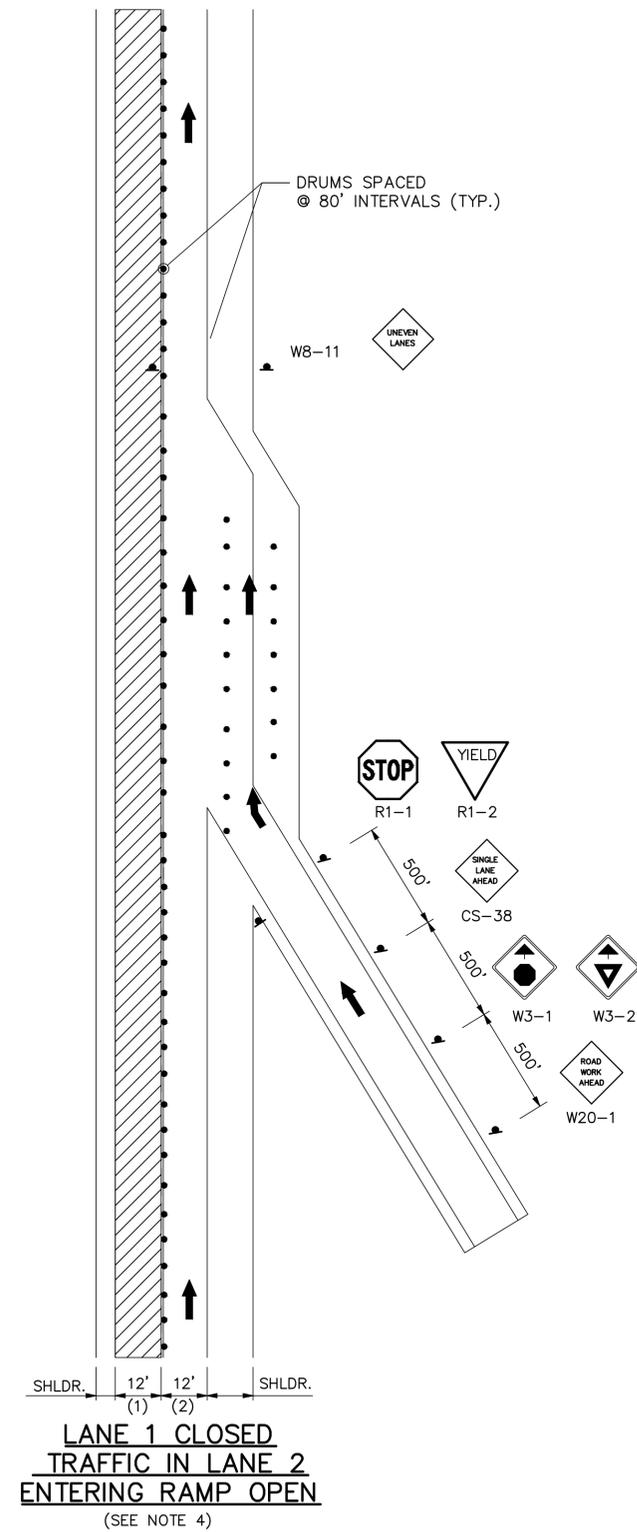
TRAFFIC CONTROL
LANE CLOSURE DETAILS 3

SHEET NUMBER: T-3

CONTRACT: 2016.01

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- CUMBERLAND SERVICE PLAZA
- EXIT 63



LEGEND

- ▨ = WORK AREA
- ▲ = CONSTRUCTION SIGN
- = DRUM
- ⊕ = PORTABLE - CHANGEABLE MESSAGE SIGN

NOTES:

1. FOR SIGN DETAILS, SEE SIGN SUMMARY SHEETS.
2. ALL SIGNS ARE TO BE MOUNTED ON EASELS.
3. LANE CLOSURES AT ENTRANCE AND EXIT RAMP MAY BE COMBINED IF APPROVED BY THE RESIDENT.
4. WITH LANE 1 CLOSED, AND LANE 2 AND ENTERING RAMP OPEN, USE THIS DETAIL WITH YIELD SIGN.
5. WITH LANE 1 CLOSED, AND LANE 2 AND EXITING RAMP OPEN, USE THIS DETAIL.

Scale: NOT TO SCALE

No.	Revision	By	Date

Designed by:



	By	Date		By	Date
Designed	JRL	01/26/2016	Checked	GJO	01/28/2016
Drawn	BMB	01/26/2016	In Charge of	SRT	01/26/2016

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**THE GOLD STAR
MEMORIAL HIGHWAY**

MTA PROJECT MANAGER: Joseph R. Leavitt, P.E.

PAVEMENT REHABILITATION
CLEAR ZONE IMPROVEMENTS

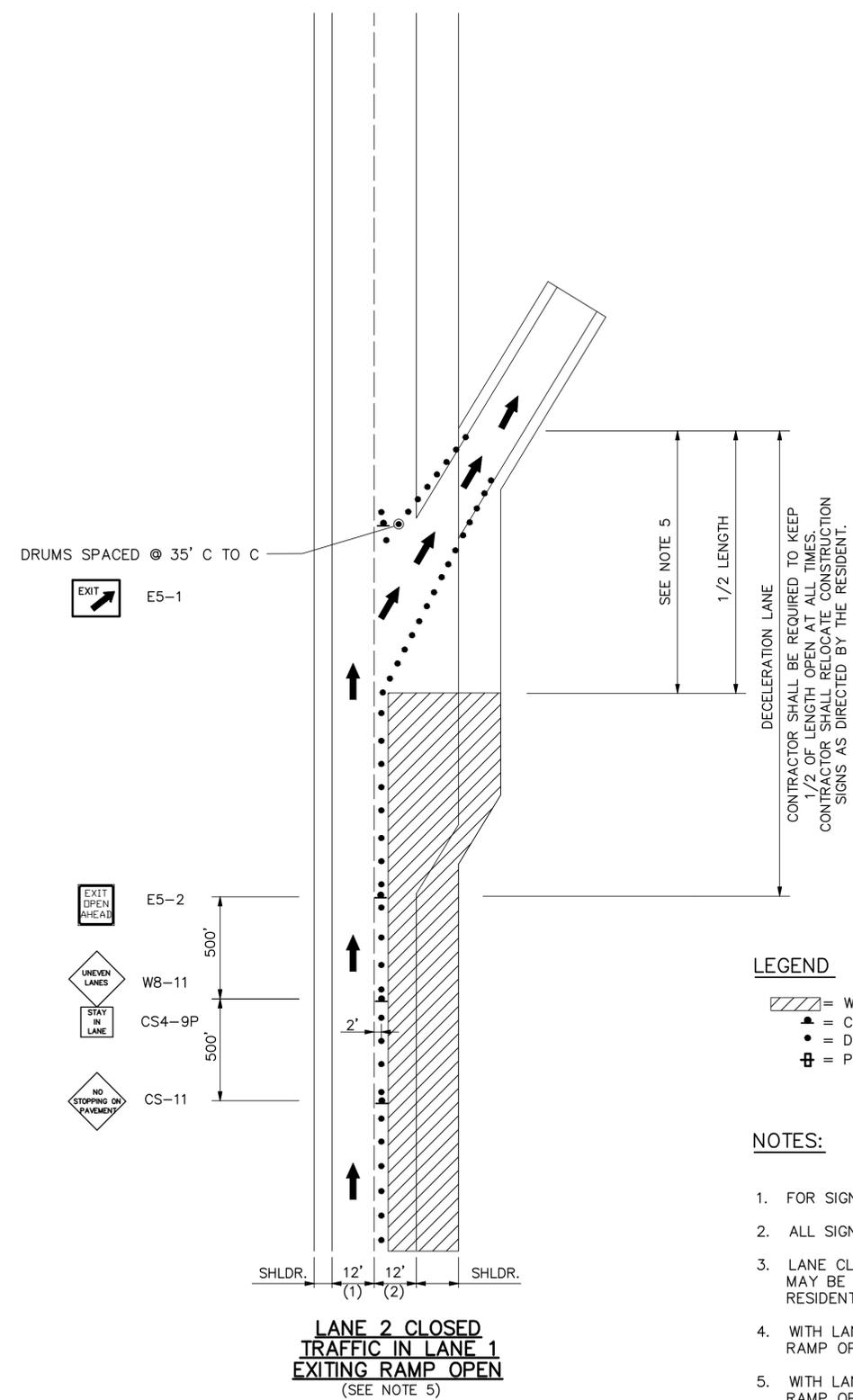
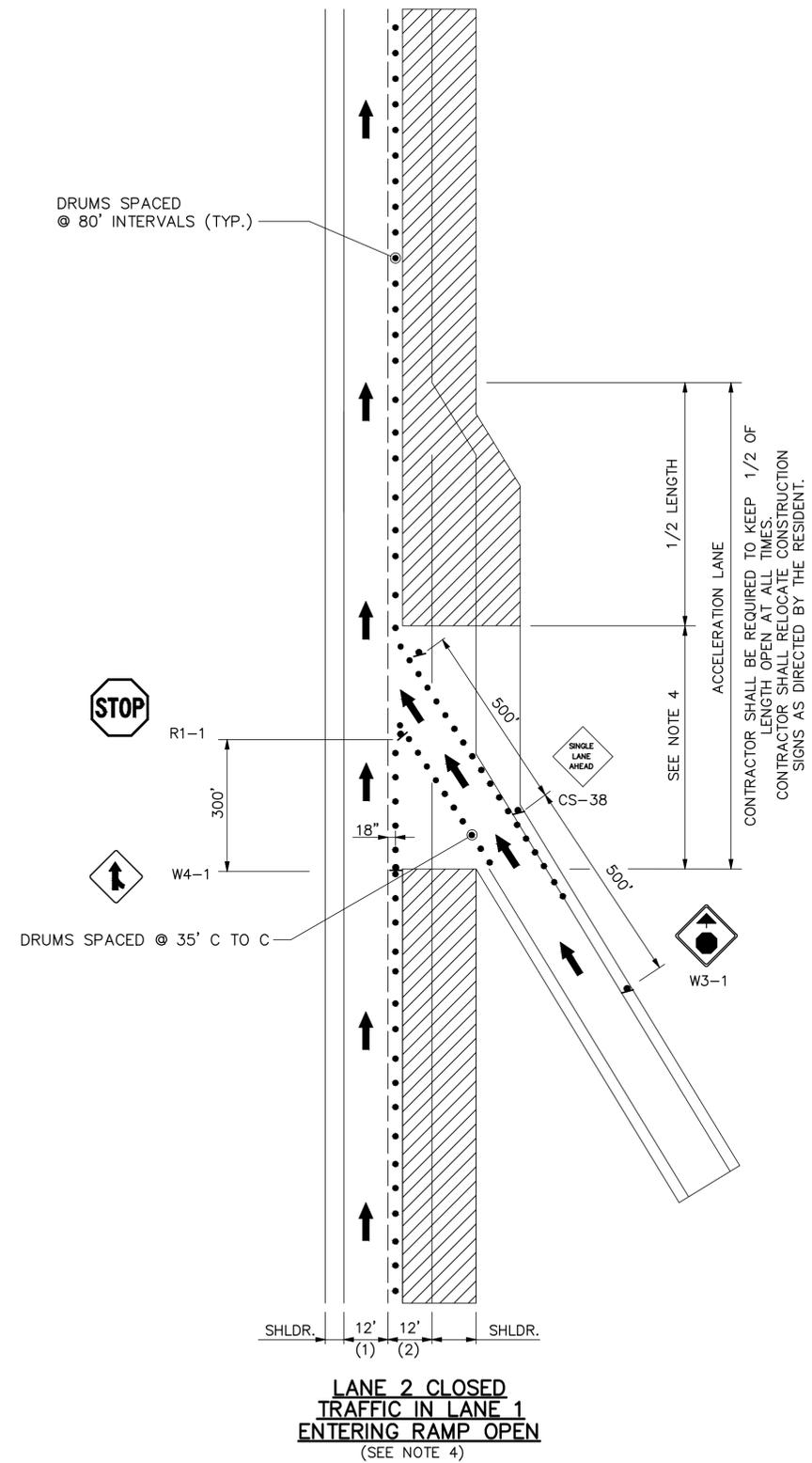
TRAFFIC CONTROL
LANE CLOSURE DETAILS 4

SHEET NUMBER: T-4

CONTRACT: 2016.01

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- CUMBERLAND SERVICE PLAZA
- EXIT 63



LEGEND

- ▨ = WORK AREA
- ▲ = CONSTRUCTION SIGN
- = DRUM
- ⊕ = PORTABLE - CHANGEABLE MESSAGE SIGN

NOTES:

1. FOR SIGN DETAILS, SEE SIGN SUMMARY SHEETS.
2. ALL SIGNS ARE TO BE MOUNTED ON EASELS.
3. LANE CLOSURES AT ENTRANCE AND EXIT RAMP MAY BE COMBINED IF APPROVED BY THE RESIDENT.
4. WITH LANE 2 CLOSED, AND LANE 1 AND ENTERING RAMP OPEN, USE THIS DETAIL WITH STOP SIGN.
5. WITH LANE 2 CLOSED, AND LANE 1 AND EXITING RAMP OPEN, USE THIS DETAIL.

Scale: NOT TO SCALE

No.	Revision	By	Date

Designed by:

MAINE TURNPIKE

	By	Date		By	Date
Designed	JRL	01/26/2016	Checked	GJO	01/26/2016
Drawn	BMB	01/26/2016	In Charge of	SRT	01/26/2016

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MAINE TURNPIKE

THE GOLD STAR MEMORIAL HIGHWAY

MTA PROJECT MANAGER: Joseph R. Leavitt, P.E.

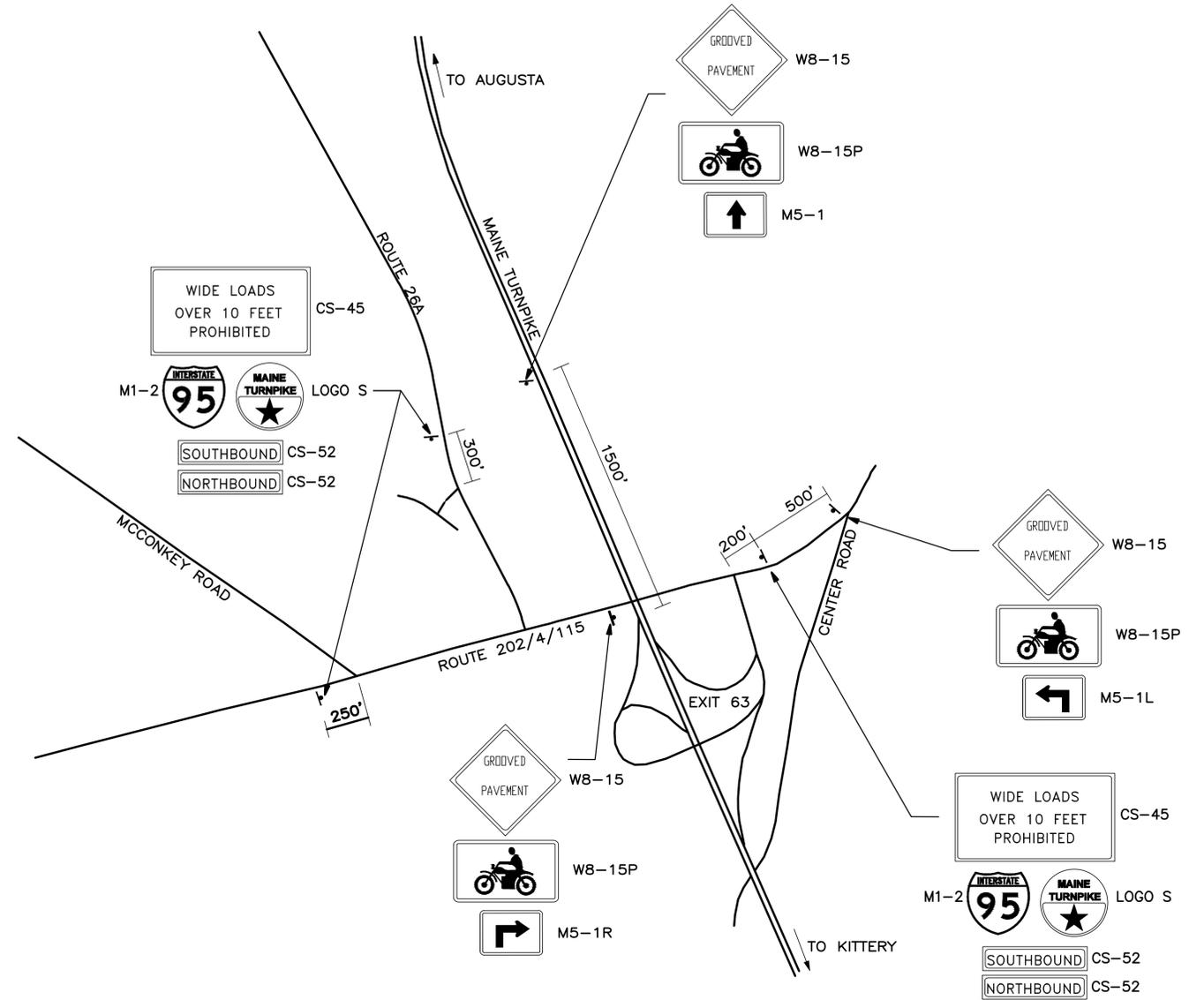
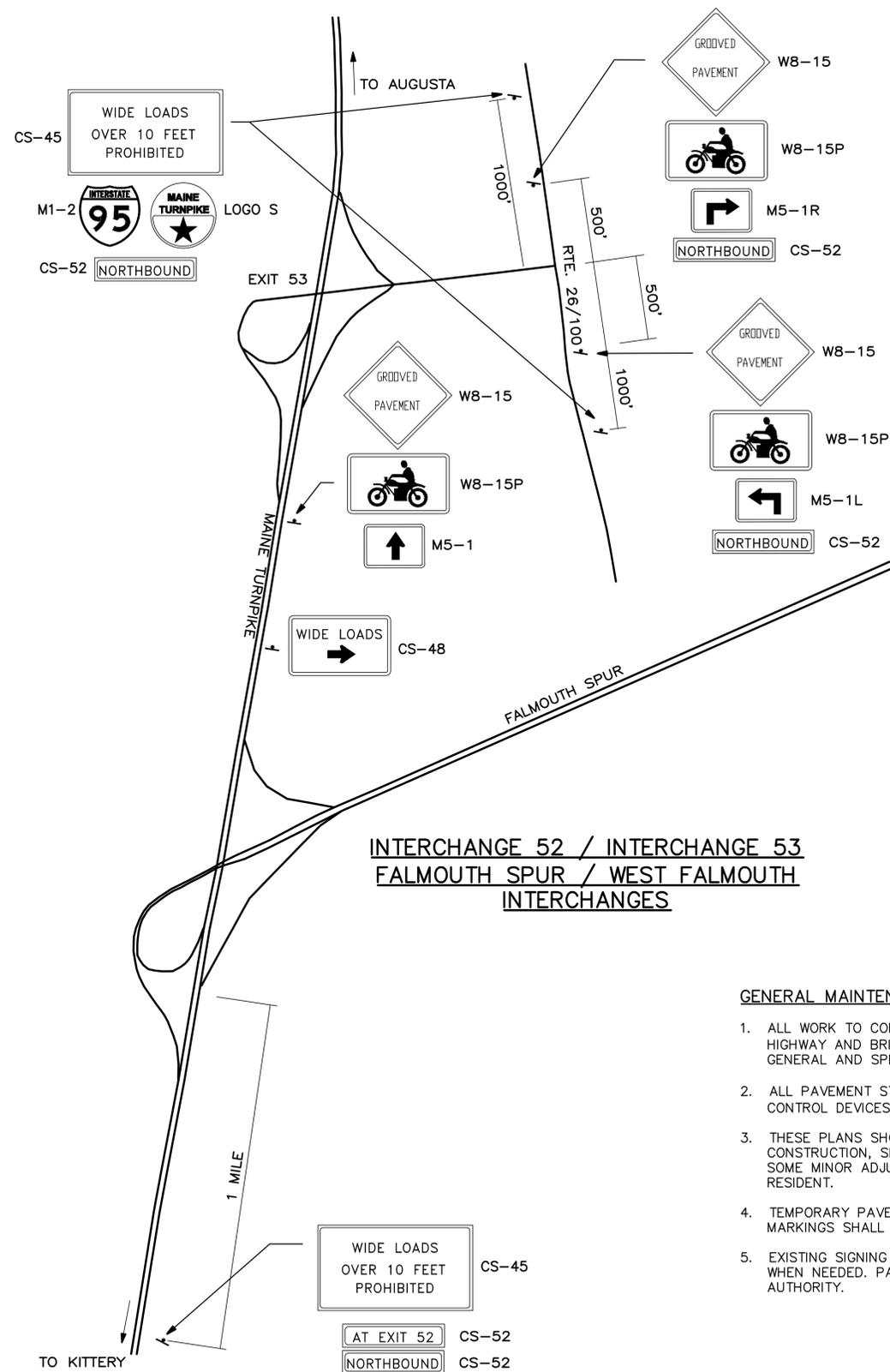
PAVEMENT REHABILITATION
CLEAR ZONE IMPROVEMENTS

TRAFFIC CONTROL
LANE CLOSURE DETAIL 5

SHEET NUMBER: T-5

CONTRACT: 2016.01

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GENERAL MAINTENANCE OF TRAFFIC NOTES:

1. ALL WORK TO CONFORM TO MAINE DEPARTMENT OF TRANSPORTATION STANDARD SPECIFICATIONS FOR HIGHWAY AND BRIDGES 2014 EDITION, EXCEPT AS MODIFIED BY THE MAINE TURNPIKE AUTHORITY'S GENERAL AND SPECIAL PROVISIONS.
2. ALL PAVEMENT STRIPING & SIGNING SHALL BE IN ACCORDANCE WITH THE "MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES", U.S.D.O.T., F.H.W.A., LATEST EDITION.
3. THESE PLANS SHOW THE GENERAL CONDITION FOR TURNPIKE MAINLINE TRAFFIC CONTROL. DURING CONSTRUCTION, SLIGHT MODIFICATIONS IN CONSTRUCTION PROCEDURE MAY OCCUR AND MAY REQUIRE SOME MINOR ADJUSTMENTS TO BE MADE IN THE FIELD, ALL PROCEDURES MUST BE APPROVED BY THE RESIDENT.
4. TEMPORARY PAVEMENT MARKINGS SHALL BE PAINTED, UNLESS OTHERWISE NOTED. PAYMENT FOR MARKINGS SHALL BE UNDER ITEM 627.761 - TEMPORARY PAVEMENT MARKING LINE, YELLOW OR WHITE.
5. EXISTING SIGNING HAS BEEN INSTALLED UNDER A SEPARATE CONTRACT. SIGNS SHALL BE UNCOVERED WHEN NEEDED. PAYMENT WILL BE MADE UNDER ITEM 652.351 - CONSTRUCTION SIGNS - SUPPLIED BY AUTHORITY.

**ADVANCED SIGNING LOCATIONS
FOR OVERSIZED LOAD RESTRICTIONS**

NOTE:

1. ALL SIGNS TO BE MOUNTED ON NCHRP 350 APPROVED POSTS UNLESS BEHIND GUARDRAIL

Scale: NOT TO SCALE

No.	Revision	By	Date

Designed by:

By	Date
JRL	01/26/2016
Checked	GJO
01/28/2016	
By	Date
Drawn	BMB
01/26/2016	
In Charge of	SRT
01/26/2016	

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**THE GOLD STAR
MEMORIAL HIGHWAY**

MTA PROJECT MANAGER: Joseph R. Leavitt, P.E.

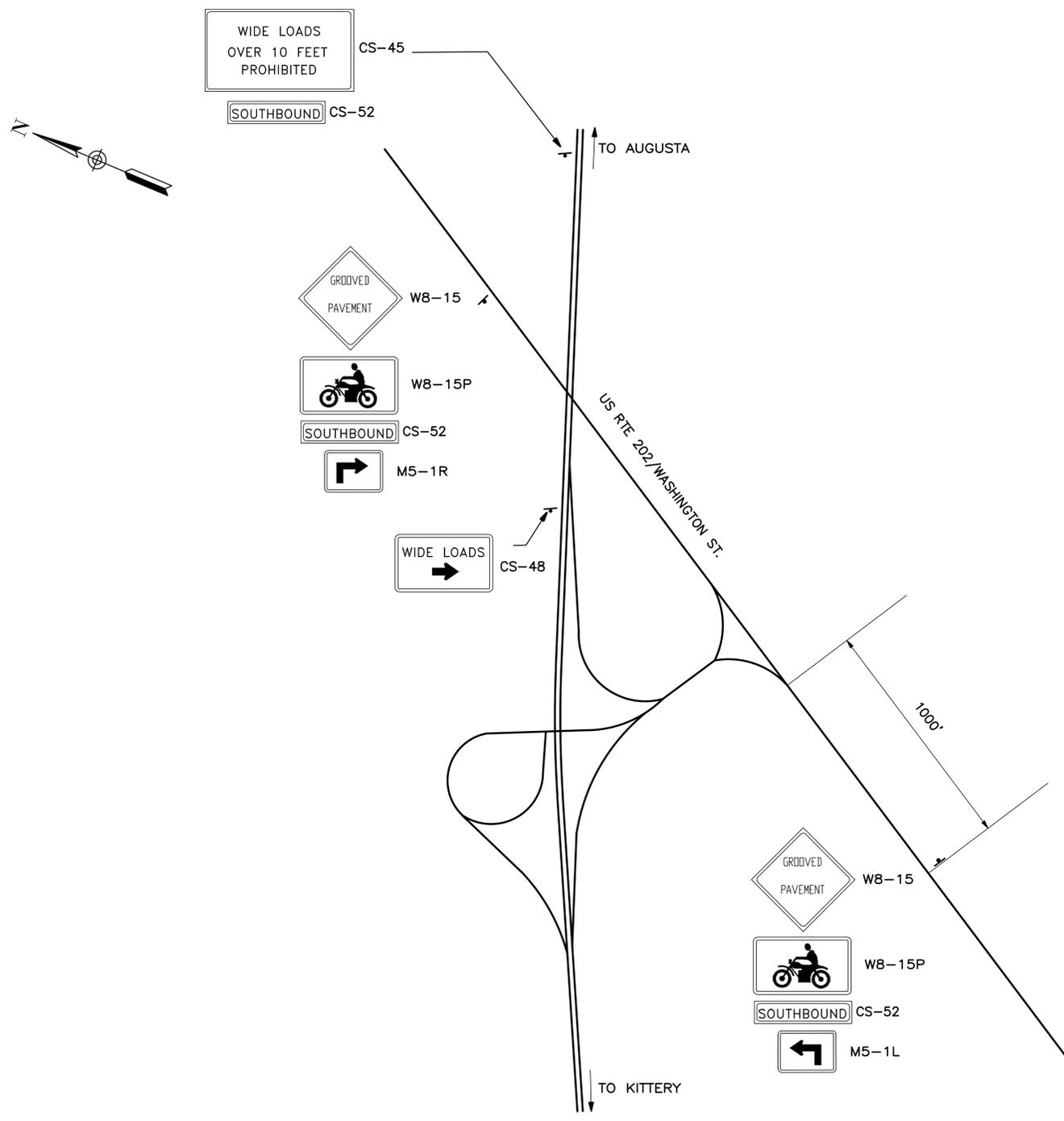
PAVEMENT REHABILITATION
CLEAR ZONE IMPROVEMENTS

MAINTENANCE OF TRAFFIC
NOTES AND MISCELLANEOUS DETAILS

CONTRACT: 2016.01

SHEET NUMBER: T-6

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INTERCHANGE 75 AUBURN
N.T.S.

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No.	Revision	By	Date

Designed by:



	By	Date		By	Date
Designed	BAT	01/26/2016	Checked	BAT	01/26/2016
Drawn	BMB	01/26/2016	In Charge of	SRT	01/26/2016

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**THE GOLD STAR
MEMORIAL HIGHWAY**

MTA PROJECT MANAGER: Joseph R. Leavitt, P.E.

PAVEMENT REHABILITATION
CLEAR ZONE IMPROVEMENTS

MAINTENANCE OF TRAFFIC

SHEET NUMBER: T-7

CONTRACT: 2016.01

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IDENTIFICATION NUMBER	SIZE OF SIGN		TEXT	TEXT DIMENSIONS (INCHES)			NUMBER OF SIGNS REQUIRED	COLOR		BORDER RADIUS	AREA IN SQUARE FEET
	WIDTH	HEIGHT		LETTER HEIGHT	VERTICAL SPACING	ARROW RTE. MKR.		BACK-GROUND	LEGEND BORDER		
G20-1	48"	24"		TEXT DIMENSIONS SHALL CONFORM TO "2004 EDITION - STANDARD HIGHWAY SIGNS - 2012 SUPPLEMENT"			4	COLORS SHALL CONFORM TO "2004 EDITION - STANDARD HIGHWAY SIGNS - 2012 SUPPLEMENT"			8.00 (32)
G20-2	48"	24"					4				8.00 (32)
G20-5aP	48"	24"					52				8.00 (416)
R2-1 (50)	48"	60"					52				20.00 (1040)
R2-6aP	48"	24"		6"	4.5"		48				8.00 (384)
M5-1L	36"	24"					3				6.00 (18)
M5-1R	36"	24"					3				6.00 (18)
M5-1	36"	24"					2				6.00 (12)
R1-1	48"	48"					4	RED	WHITE		16.00 (64)
R1-2	60"	60"					4	WHITE	RED		12.50 (50)
R2-12	36"	54"		6"	4.5"		16	COLORS SHALL CONFORM TO "2004 EDITION - STANDARD HIGHWAY SIGNS - 2012 SUPPLEMENT"			13.50 (216)
E5-1	72"	60"		TEXT DIMENSIONS SHALL CONFORM TO "2004 EDITION - STANDARD HIGHWAY SIGNS - 2012 SUPPLEMENT"			4	GREEN	WHITE		30.00 (120)
CS5-2 (OPEN) (OPEN AHEAD)	60"	48"					4	ORANGE	BLACK		20.00 (80) (80)
W1-4L	48"	48"					4	COLORS SHALL CONFORM TO "2004 EDITION - STANDARD HIGHWAY SIGNS - 2012 SUPPLEMENT"			16.00 (64)

IDENTIFICATION NUMBER	SIZE OF SIGN		TEXT	TEXT DIMENSIONS (INCHES)			NUMBER OF SIGNS REQUIRED	COLOR		BORDER RADIUS	AREA IN SQUARE FEET
	WIDTH	HEIGHT		LETTER HEIGHT	VERTICAL SPACING	ARROW RTE. MKR.		BACK-GROUND	LEGEND BORDER		
W1-4R	48"	48"		TEXT DIMENSIONS SHALL CONFORM TO "2004 EDITION - STANDARD HIGHWAY SIGNS - 2012 SUPPLEMENT"			4	COLORS SHALL CONFORM TO "2004 EDITION - STANDARD HIGHWAY SIGNS - 2012 SUPPLEMENT"			16.00 (64)
W3-1	48"	48"					4	ORANGE	BLACK		16.00 (64)
W3-2a	48"	48"					4	ORANGE	BLACK		16.00 (64)
W3-4	48"	48"					8	COLORS SHALL CONFORM TO "2004 EDITION - STANDARD HIGHWAY SIGNS - 2012 SUPPLEMENT"			16.00 (128)
W3-5 (50)	48"	48"					12				16.00 (192)
W4-2L	48"	48"					8				16.00 (128)
W4-2R	48"	48"					8				16.00 (128)
W5-4	48"	48"					4				16.00 (64)
W8-1	48"	48"					8				16.00 (128)
W8-11	48"	48"					44				16.00 (704)

Scale: NOT TO SCALE

No.	Revision	By	Date

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By	Date	By	Date
Designed	BAT 01/26/2016	Checked	BAT 01/26/2016
Drawn	BMB 01/26/2016	In Charge of	SRT 01/26/2016

MAINE TURNPIKE AUTHORITY
2360 Congress Street
Portland, ME 04102
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**THE GOLD STAR
MEMORIAL HIGHWAY**

MTA PROJECT MANAGER: Joseph R. Leavitt, P.E.

PAVEMENT REHABILITATION
CLEAR ZONE IMPROVEMENTS

SIGN SUMMARY 1

CONTRACT: 2016.01

SHEET NUMBER: T-8

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IDENTIFICATION NUMBER	SIZE OF SIGN		TEXT	TEXT DIMENSIONS (INCHES)			NUMBER OF SIGNS REQUIRED	COLOR		BORDER RADIUS	AREA IN SQUARE FEET
	WIDTH	HEIGHT		LETTER HEIGHT	VERTICAL SPACING	ARROW RTE. MKR.		BACK-GROUND	LEGEND BORDER		
W8-15	48"	48"	GROOVED PAVEMENT	TEXT DIMENSIONS SHALL CONFORM TO "2004 EDITION - STANDARD HIGHWAY SIGNS - 2012 SUPPLEMENT"			12	COLORS SHALL CONFORM TO "2004 EDITION - STANDARD HIGHWAY SIGNS - 2012 SUPPLEMENT"			16.00 (192)
W8-15P	30"	24"					12				5.00 (60)
W20-1 (1 MILE) (AHEAD)	48"	48"	ROAD WORK XXX				8 8				16.00 (128) (128)
W20-5L (1/2 MILE) (AHEAD)	48"	48"	LEFT LANE CLOSED XXX				8 1				16.00 (128) (16)
W20-5R (1/2 MILE) (AHEAD)	48"	48"	RIGHT LANE CLOSED XXX				8 1				16.00 (128) (16)
W21-5aR	48"	48"	RIGHT SHOULDER CLOSED				4				16.00 (64)
W21-5aL	48"	48"	LEFT SHOULDER CLOSED				4				16.00 (64)
W21-5bR (1000 FT)	48"	48"	RIGHT SHOULDER CLOSED 1000 FT				4				16.00 (64)
W21-5bL (1000 FT)	48"	48"	LEFT SHOULDER CLOSED 1000 FT				4				16.00 (64)

IDENTIFICATION NUMBER	SIZE OF SIGN		TEXT	TEXT DIMENSIONS (INCHES)			NUMBER OF SIGNS REQUIRED	COLOR		BORDER RADIUS	AREA IN SQUARE FEET
	WIDTH	HEIGHT		LETTER HEIGHT	VERTICAL SPACING	ARROW RTE. MKR.		BACK-GROUND	LEGEND BORDER		
W24-1L	48"	48"		TEXT DIMENSIONS SHALL CONFORM TO "2004 EDITION - STANDARD HIGHWAY SIGNS - 2012 SUPPLEMENT"			2	COLORS SHALL CONFORM TO "2004 EDITION - STANDARD HIGHWAY SIGNS - 2012 SUPPLEMENT"			16.00 (32.00)
W24-1R	48"	48"					2				16.00 (32.00)
CS-3	48"	48"	EXPECT STOPPED TRAFFIC	6" 6" 6"	4" 4"		8	ORANGE	BLACK		16.00 (128)
CS-10	48"	48"	TRUCKS ENTERING	7" 7"	6"		4	ORANGE	BLACK		16.00 (64)
CS-11	48"	48"	NO STOPPING ON PAVEMENT	6" 6" 6"	4" 4"		8	ORANGE	BLACK		16.00 (128)
CS-33	96"	36"	WATCH FOR TRUCKS ENTERING AND LEAVING WORK AREA	6" 6" 6"	4" 4"		4	ORANGE	BLACK		24.00 (96)
CS-52	96"	18"	NORTHBOUND SOUTHBOUND				5 4				12.00 (60) 12.00 (48)
CS-53L	48"	48"		TEXT DIMENSIONS SHALL CONFORM TO "2004 EDITION - STANDARD HIGHWAY SIGNS - 2012 SUPPLEMENT"			4	COLORS SHALL CONFORM TO "2004 EDITION - STANDARD HIGHWAY SIGNS - 2012 SUPPLEMENT"			16.00 (64)
CS-53R	48"	48"					4				16.00 (64)
CS4-9P	36"	48"	STAY IN LANE				24	ORANGE	BLACK		12.00 (480)

Scale: NOT TO SCALE

No.	Revision	By	Date

Designed by:



By	Date	By	Date
Designed	BAT 01/26/2016	Checked	BAT 01/28/2016
Drawn	BMB 01/26/2016	In Charge of	SRT 01/26/2016

MAINE TURNPIKE AUTHORITY
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THE GOLD STAR
MEMORIAL HIGHWAY

MTA PROJECT MANAGER: Joseph R. Leavitt, P.E.

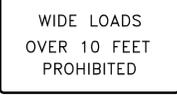
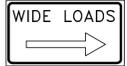
PAVEMENT REHABILITATION
CLEAR ZONE IMPROVEMENTS

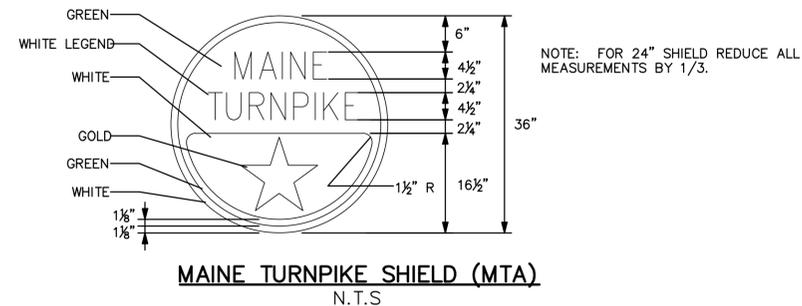
SIGN SUMMARY 2

SHEET NUMBER: T-9

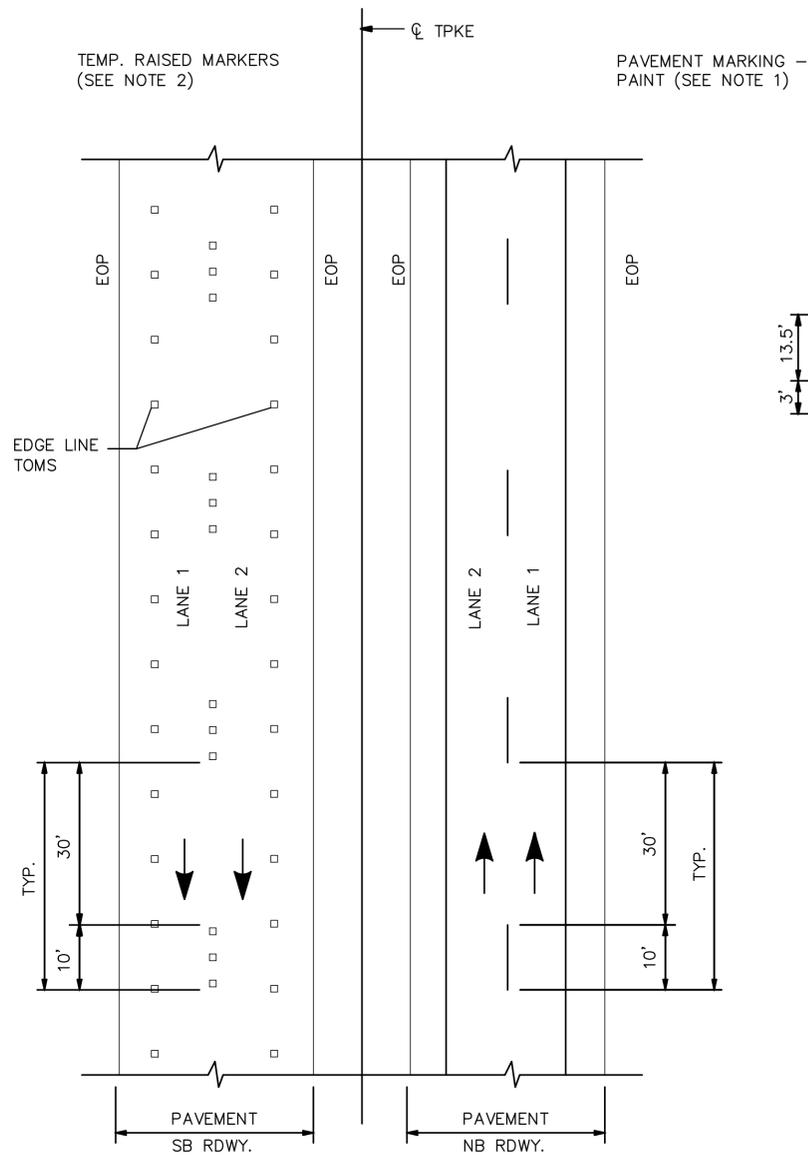
CONTRACT: 2016.01

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IDENTIFICATION NUMBER	SIZE OF SIGN		TEXT	TEXT DIMENSIONS (INCHES)			NUMBER OF SIGNS REQUIRED	COLOR		BORDER RADIUS	AREA IN SQUARE FEET
	WIDTH	HEIGHT		LETTER HEIGHT	VERTICAL SPACING	ARROW RTE. MKR.		BACK-GROUND	LEGEND BORDER		
CS-38	48"	48"		TEXT DIMENSIONS SHALL CONFORM TO "2004 EDITION - STANDARD HIGHWAY SIGNS - 2012 SUPPLEMENT"			4	ORANGE	BLACK		16.00 (64)
W24-lal	48"	48"					4	ORANGE	BLACK		16.00 (64)
W4-1	48"	48"					4	ORANGE	BLACK		16.00 (64)
CS-45	96"	48"		8" C	6"		4	ORANGE	BLACK		32.00 (128)
CS-48	60"	36"		8" D		9"x36" @ 0"	2	ORANGE	BLACK		15.00 (30)
LOGO S	24"	24"		SEE DETAILS BELOW			3	SEE DETAILS BELOW			4.00 (12)



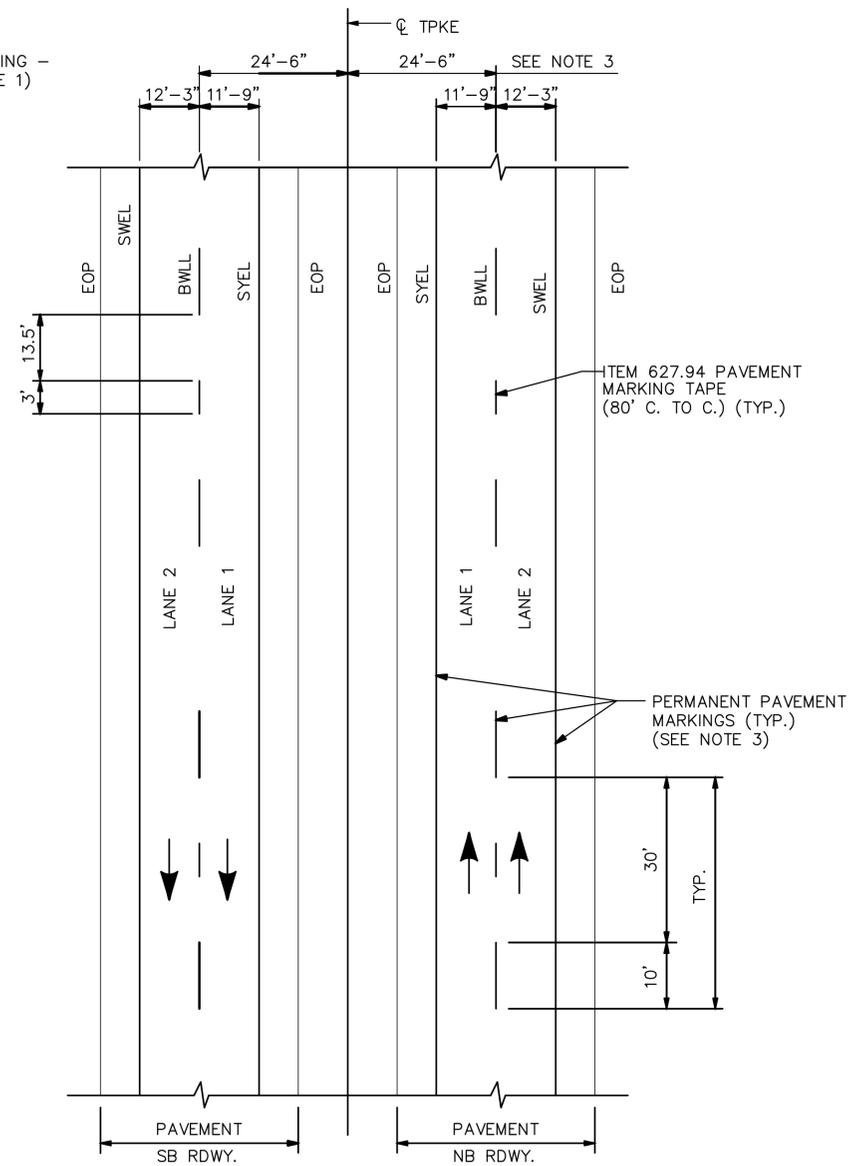
Scale: NOT TO SCALE		Designed by: 		MAINE TURNPIKE AUTHORITY 2360 Congress Street Portland, ME 04102 TEL (207) 871-7771 FAX (207) 879-5567		 THE GOLD STAR MEMORIAL HIGHWAY		PAVEMENT REHABILITATION CLEAR ZONE IMPROVEMENTS PAVEMENT MARKING DETAILS 1	
No.	Revision	By	Date	By	Date	MTA PROJECT MANAGER: Joseph R. Leavitt, P.E.		SHEET NUMBER: T-10	
				Designed	BAT 01/26/2016	Checked	BAT 01/26/2016	CONTRACT: 2016.01	
				Drawn	BMB 01/26/2016	In Charge of	SRT 01/26/2016	22 of 77	



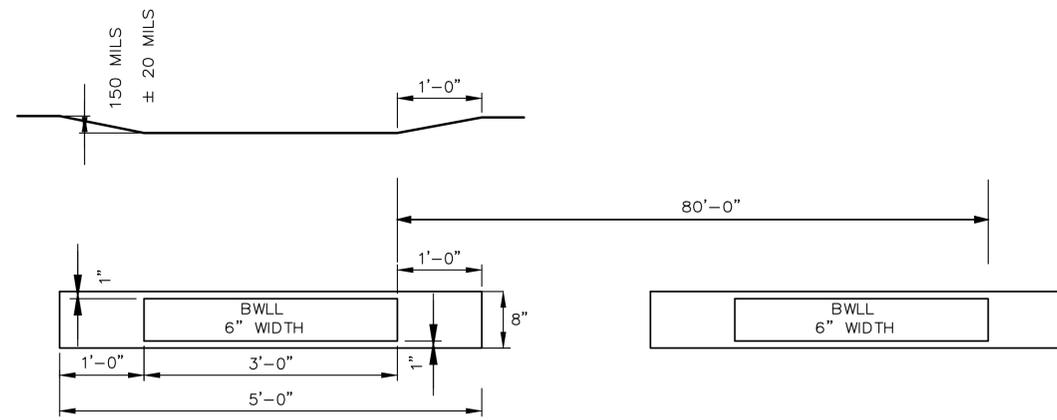
**TEMP. PAVEMENT MARKINGS
2 LANE RDWY**

NOTES:

- TEMPORARY PAVEMENT MARKINGS ON MILLED PAVEMENT SHALL BE 6" SOLID WHITE, BROKEN WHITE, AND SOLID YELLOW PAINT LINES.
- TEMPORARY PAVEMENT MARKINGS ON NEW HMA 12.5MM, OR SHIM MATERIAL, SHALL BE TEMPORARY RAISED PAVEMENT MARKERS. "SOLID" WHITE AND "SOLID" YELLOW LANE LINES SHALL BE TEMPORARY RAISED PAVEMENT MARKERS SPACED AT 10'-0" C. TO C. "BROKEN" WHITE LANE LINES SHALL BE TEMPORARY RAISED PAVEMENT MARKERS SPACED 5'-0" C. TO C. EVERY 40'-0".



**PERMANENT PAVEMENT MARKINGS
2 LANE RDWY**



**ITEM 627-94
PERMANENT PAVEMENT
MARKINGS WITH TAPE**

GROOVING DETAIL
N.T.S.

LEGEND

- SYEL = SOLID YELLOW EDGE LINE (6" PAINT)
- SWEL = SOLID WHITE EDGE LINE (6" PAINT)
- BWLL = BROKEN WHITE LANE LINE (6" PAINT)
- EOP = EDGE OF PAVEMENT

NOTES:

- PERMANENT PAVEMENT MARKINGS SHALL BE 6" WIDE LINES.
- PERMANENT PAVEMENT MARKINGS SHALL BE INSTALLED ON BOTH ROADWAYS.
- THE OFFSET DISTANCE SHOWN ARE MEASURED FROM THE CENTER OF THE 6" PAINT LINES.
- ALL EXISTING PAINT MARKINGS SHALL BE REMOVED PRIOR TO INSTALLATION OF ITEM 627.941 PAVEMENT MARKING TAPE AND SHALL BE INCIDENTAL TO INSTALLATION OF THE TAPE.
- ON DECELERATION RAMP, THE DWLL SHALL BEGIN AT THE BEGINNING OF THE FULL WIDTH DECELERATION LANE AND EXTEND TO THE THEORETICAL GORE MARKINGS.
- ITEM 627.941 PAVEMENT MARKING TAPE FOR DWLL SHALL BE INSTALLED IN AN "IN-AND-OUT" GROOVING PATTERN. SEE DETAIL THIS SHEET.

Scale: NOT TO SCALE			
No.	Revision	By	Date

Designed by:					
					
	By	Date		By	Date
	BAT	01/26/2016	Checked	BAT	01/26/2016
	BMB	01/26/2016	In Charge of	SRT	01/26/2016

MAINE TURNPIKE AUTHORITY
2360 Congress Street
Portland, ME 04102
TEL (207) 871-7771
FAX (207) 879-5567



**THE GOLD STAR
MEMORIAL HIGHWAY**

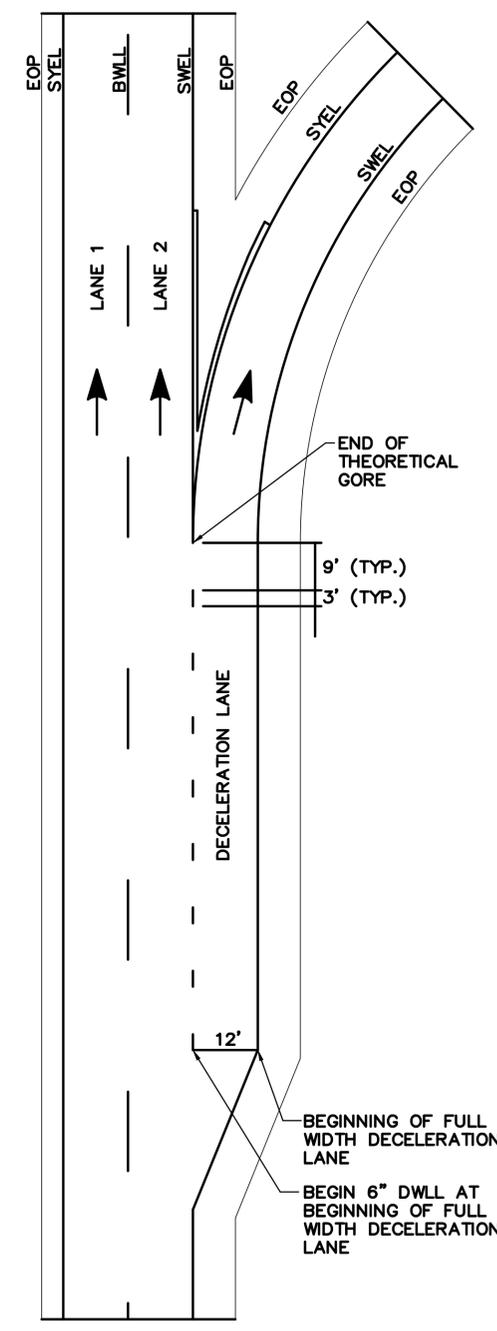
MTA PROJECT MANAGER: Joseph R. Leavitt, P.E.

PAVEMENT REHABILITATION
CLEAR ZONE IMPROVEMENTS
PAVEMENT MARKING DETAILS 1

CONTRACT: 2016.01

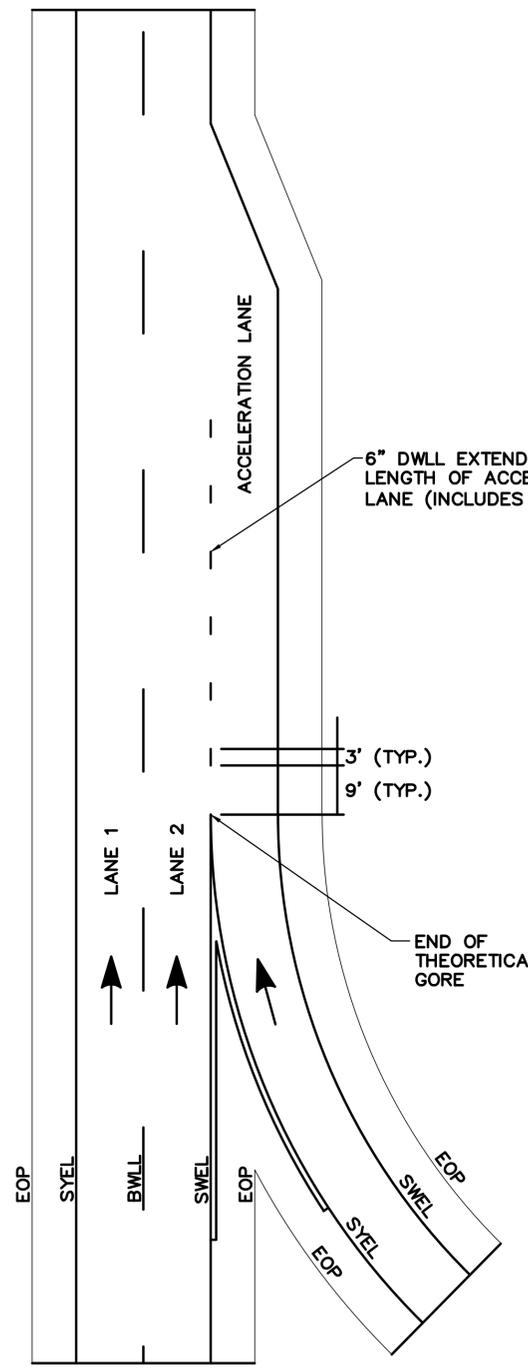
SHEET NUMBER: T-11

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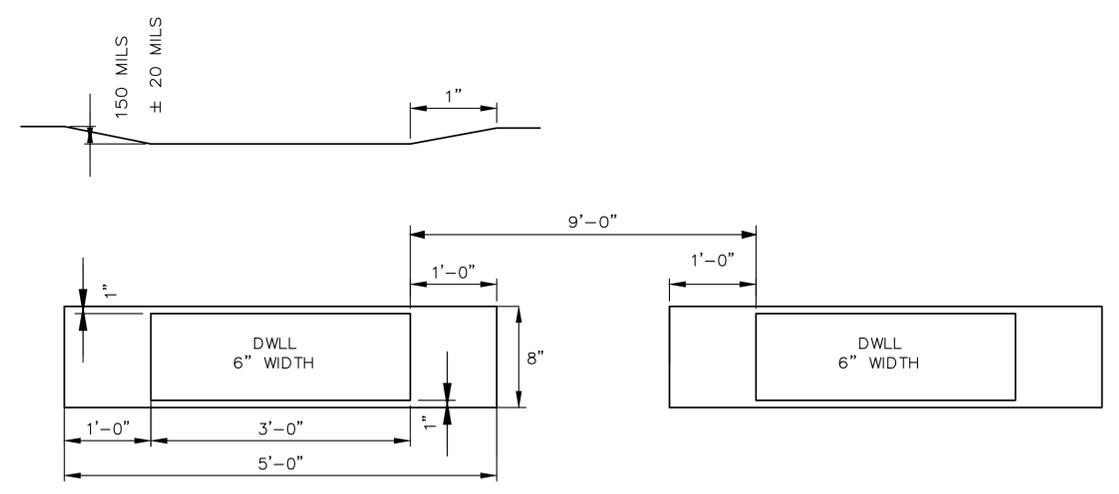


DEPARTING RAMP

PERMANENT PAVEMENT MARKINGS WITH TAPE
(ITEM 627.941 PAVEMENT MARKING TAPE)



ENTERING RAMP



ITEM 627.941
DWLL PERMANENT PAVEMENT MARKINGS WITH TAPE
GROOVING DETAIL
N.T.S.

CONTRACT 2016.01
PAVEMENT MARKING TAPE-DOTTED WHITE LANE LINE INSTALLATION LOCATIONS

NORTH BOUND		ITEM NUMBER 627.941 PAVEMENT MARKING-DWLL					
Location	Lane Type		Begin Station	End Station	Length of DWLL (LF)	Marking Count (EA)	Tape (LF)
	Decel	Accel					
Exit 63			3153+00	3162+25	925	78	234
Exit 63			3180+20	3186+75	655	55	165
Total NB							399

SOUTH BOUND		ITEM NUMBER 627.941 PAVEMENT MARKING-DWLL					
Location	Lane Type		Begin Station	End Station	Length of DWLL (LF)	Marking Count (EA)	Tape (LF)
	Decel	Accel					
Exit 63			3187+50	3177+55	695	58	174
Exit 63			3163+10	3158+10	500	42	126
MM 58.8, Cumberland Service Plaza			2947+00	2938+75	825	69	207
Total SB							507

LEGEND

- SYEL = SOLID YELLOW EDGE LINE (6" PAINT)
- SWEL = SOLID WHITE EDGE LINE (6" PAINT)
- BWLL = BROKEN WHITE LANE LINE (6" PAINT)
- EOP = EDGE OF PAVEMENT

NOTES:

1. ALL EXISTING PAINT MARKINGS SHALL BE REMOVED PRIOR TO INSTALLATION OF ITEM 627.941 PAVEMENT MARKING TAPE AND SHALL BE INCIDENTAL TO INSTALLATION OF THE TAPE.
2. ON DEPARTING RAMPS, THE DWLL SHALL BEGIN AT THE BEGINNING OF THE FULL WIDTH DECELERATION LANE AND EXTEND TO THE THEORETICAL GORE MARKINGS.
3. ON ENTERING RAMPS, THE DWLL SHALL BEGIN AT THE END OF THE THEORETICAL GORE MARKINGS AND SHALL EXTEND HALF THE TOTAL LENGTH OF THE RAMP INCLUDING THE TAPER.
4. ITEM 627.941 PAVEMENT MARKING TAPE FOR DWLL SHALL BE INSTALLED IN AN "IN-AND-OUT" GROOVING PATTERN. SEE DETAIL THIS SHEET.
5. CONTRACTOR SHALL FOLLOW MANUFACTURERS RECOMMENDATIONS FOR INSTALLATION OF THE TAPE. ANY CHANGES TO INSTALLATION PROCEDURES SHALL BE APPROVED THE RESIDENT ENGINEER.

Scale: NOT TO SCALE

No.	Revision	By	Date

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	By	Date		By	Date
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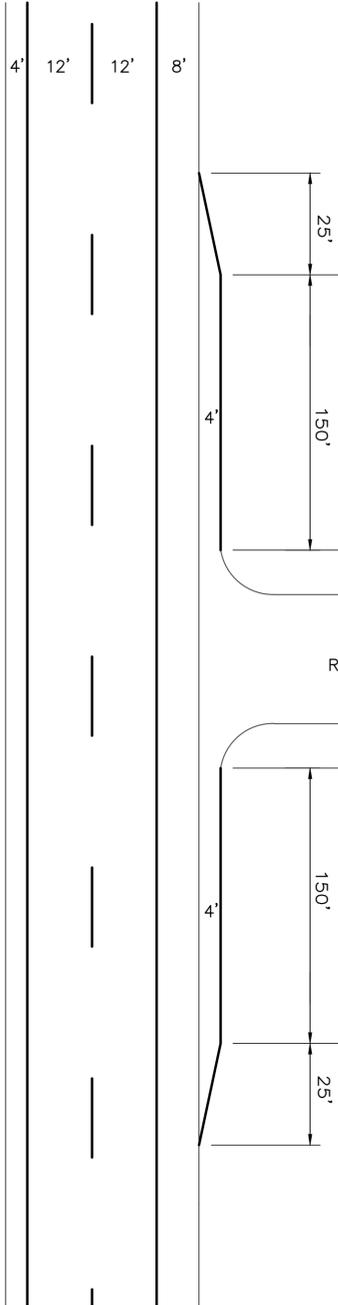


THE GOLD STAR MEMORIAL HIGHWAY

MTA PROJECT MANAGER: Joseph R. Leavitt, P.E.

PAVEMENT REHABILITATION CLEAR ZONE IMPROVEMENTS
PAVEMENT MARKINGS DETAILS 2

NB



TRUCK TURN OUT NOTES:

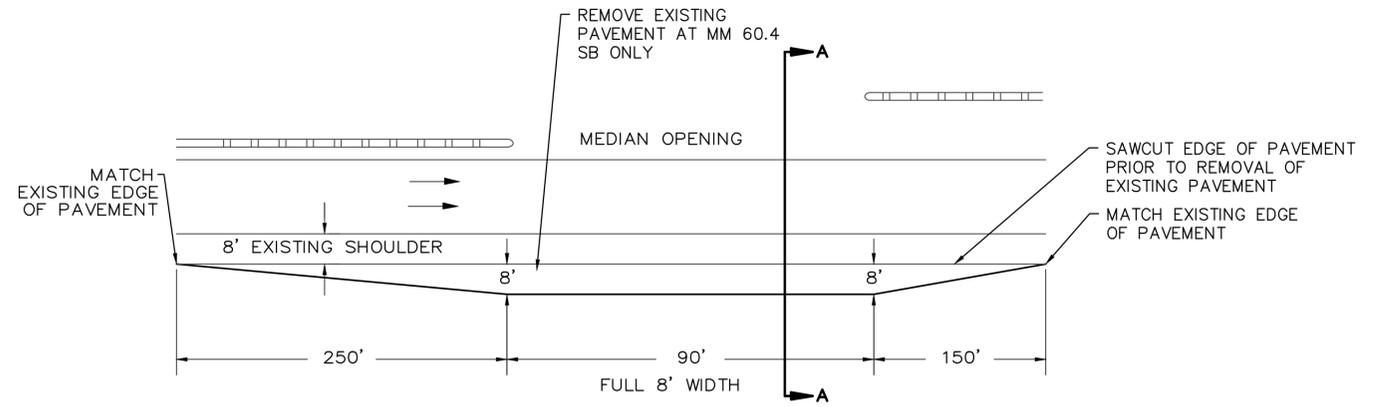
1. EXCAVATION NECESSARY TO MAINTAIN DRAINAGE WAYS OUTSIDE THE LIMITS OF THE TRUCK TURNOUTS SHALL BE PAID FOR UNDER ITEM 631.12 ALL PURPOSE EXCAVATOR AS APPROVED BY THE RESIDENT. EXCAVATED MATERIAL MAY BE USED TO GRADE ANY PROPOSED SLOPE AREA OUTSIDE OF 1:1 SLOPE LIMIT AS APPROVED BY THE RESIDENT OR USED TO FLATTEN SLOPES OUTSIDE THE WORK AREA AS APPROVED BY THE RESIDENT. PLACEMENT AND COMPACTION OF THIS EXCAVATED MATERIAL WILL NOT BE MEASURED SEPARATELY FOR PAYMENT.
2. SLOPES CONSTRUCTED FROM EXCAVATED MATERIAL SHALL BE LOAMED, SEEDED AND MULCHED. LOAM, SEED AND MULCH WILL NOT BE MEASURED FOR PAYMENT AND SHALL BE INCIDENTAL TO THE CONTRACT.
3. EXISTING PAVEMENT ON THE TRUCK TURNOUT AT MEDIAN OPENING MM 60.4 SB IS TO BE REPLACED AS PER LINES, GRADES AND DIMENSIONS SHOWN ON THIS PLAN. ALL WORK ASSOCIATED WITH REPAVING TRUCK TURNOUTS AT THIS LOCATION SHALL BE PAID FOR AT CONTRACT UNIT PRICES.
4. EXISTING PAVEMENT ON TRUCK TURNOUTS AT MM 60.4 NB AND MM 63.6 NB AND SB SHALL BE OVERLAID WITH 2" HMA 12.5mm NOMINAL MAX SIZE. ADDITION PAVEMENT TO BE ADDED AT THESE LOCATIONS SHALL BE CONSTRUCTED AS PER LINES AND GRADES AS PER THIS PLAN.

SIGN SHOP AND ROUTE 26 SLIP RAMP

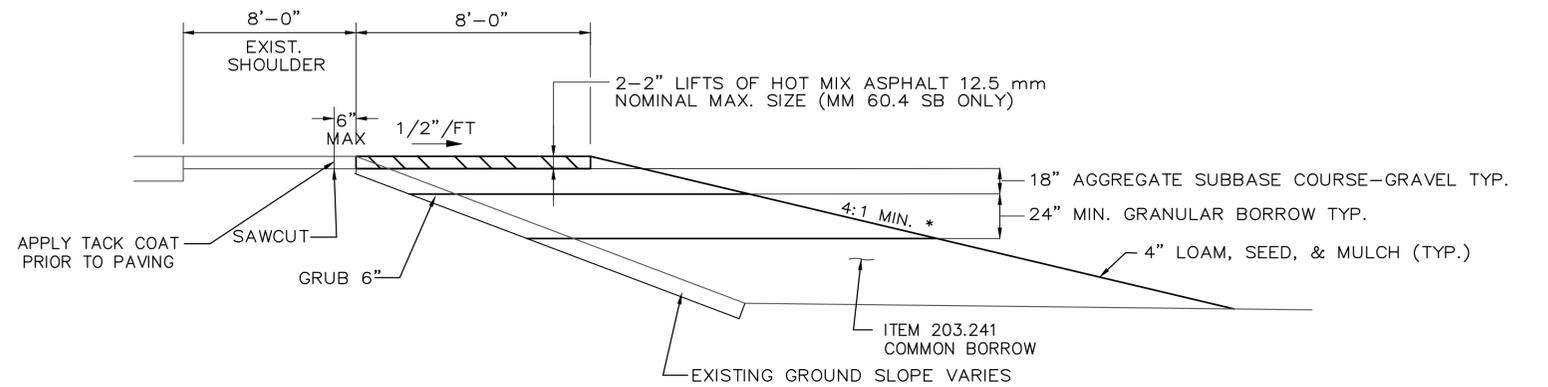
PLAN
N.T.S.

NOTES:

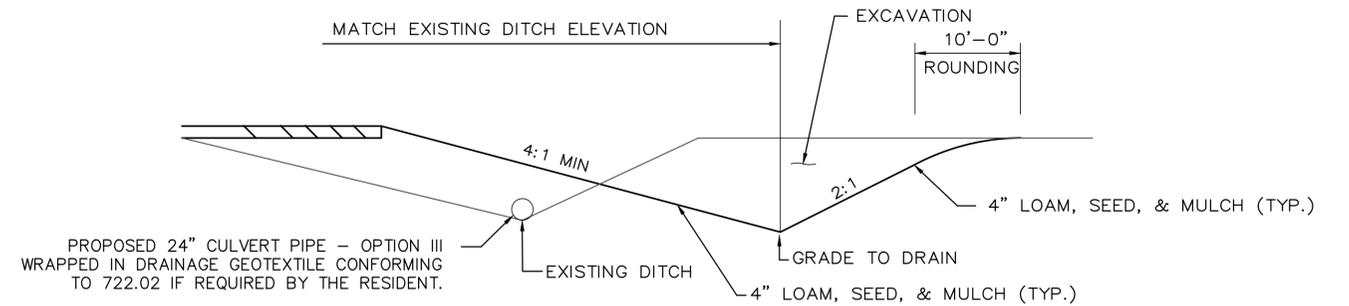
1. ADD 4' PAVED SHOULDER NORTH AND SOUTH OF SIGN SHOP.
2. ADD 4' PAVED SHOULDER NORTH OF ROUTE 26 SLIP RAMP.
3. REFERENCE TTO SECTION FOR MATERIALS.
4. SAWCUT ALONG EXISTING 8' SHOULDER.



TYPICAL PLAN VIEW



SECTION A-A * 4:1 SLOPE OR MATCH SURROUNDING EXISTING SLOPES IF THEY ARE FLATTER.
FILL DETAIL



SECTION A-A
DITCH DETAIL

TRUCK TURNOUT DETAIL

Scale:			
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**THE GOLD STAR
MEMORIAL HIGHWAY**

MTA PROJECT MANAGER: Joseph R. Leavitt, P.E.

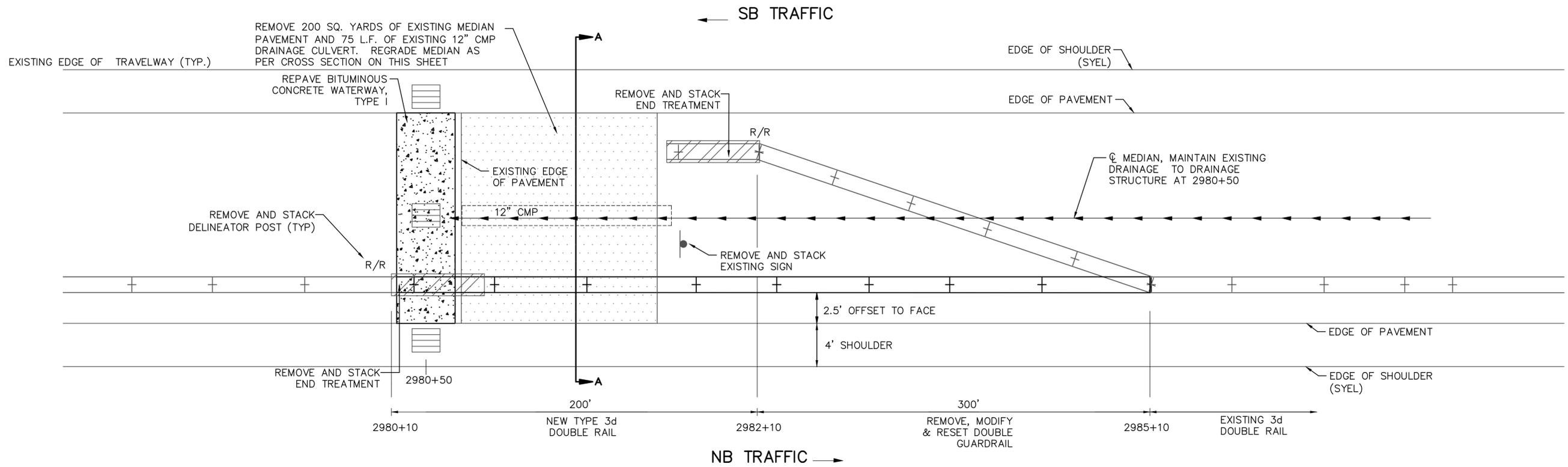
PAVEMENT REHABILITATION
CLEAR ZONE IMPROVEMENTS

WIDEN SHOULDER DETAIL
FOR TRUCK TURNOUT

CONTRACT: 2016.01

SHEET NUMBER: G-1

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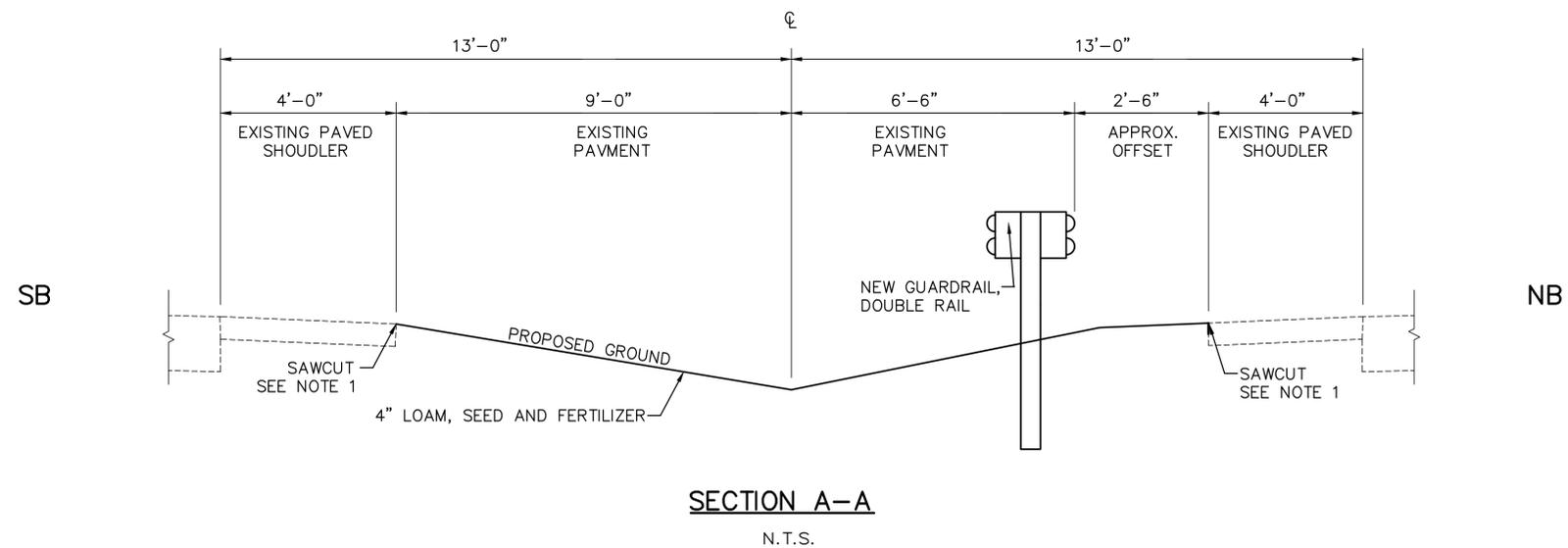


NOTES:

1. REMOVE EXISTING END TREATMENTS. CLOSE MEDIAN OPENING BY CONNECTING EXISTING TYPE 3d DOUBLE RAIL ENDS WITH NEW TYPE 3d DOUBLE RAIL
2. SAWCUT EXISTING PAVEMENT FULL DEPTH PRIOR TO REMOVAL.
3. MAINTAIN POSITIVE DRAINAGE TO CATCHBASIN AT STA. 2980+50.

CLOSE EXISTING MEDIAN OPENING MM 59.5

N.T.S.



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THE GOLD STAR MEMORIAL HIGHWAY

MTA PROJECT MANAGER: Joseph R. Leavitt, P.E.

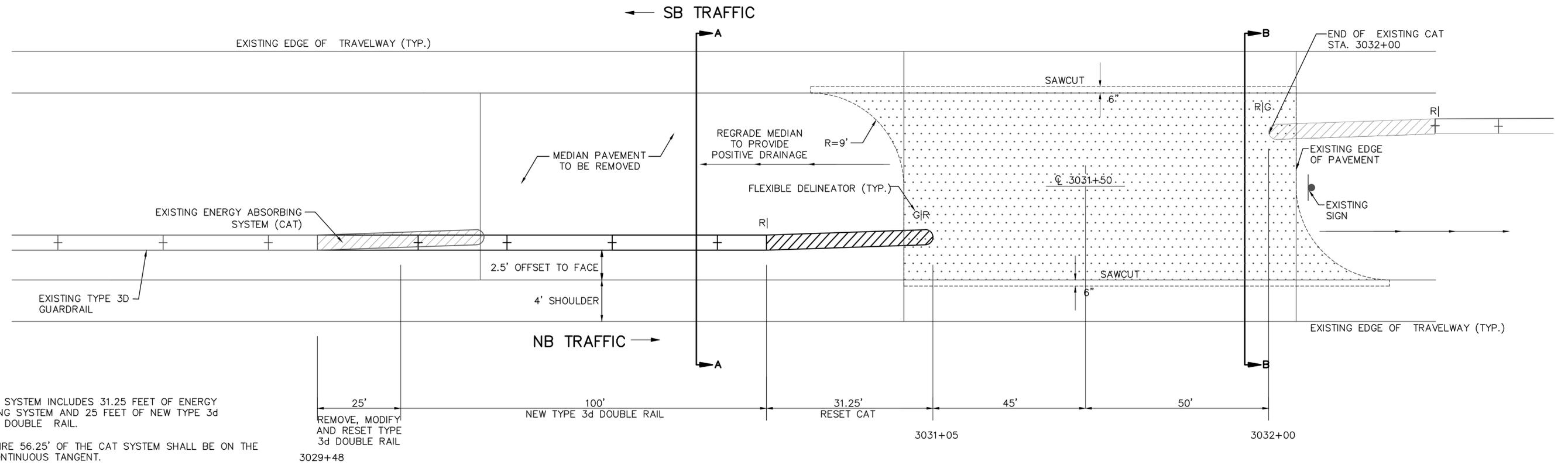
PAVEMENT REHABILITATION
 CLEARZONE IMPROVEMENTS

GUARDRAIL MM 59.5 MEDIAN

SHEET NUMBER: G-2

CONTRACT: 2016.01

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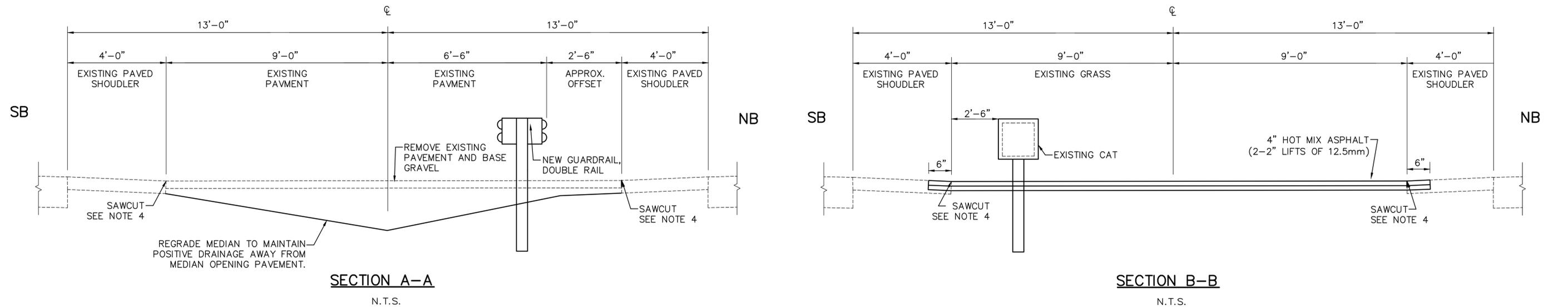


NOTES:

1. THE CAT SYSTEM INCLUDES 31.25 FEET OF ENERGY ABSORBING SYSTEM AND 25 FEET OF NEW TYPE 3d MODIFIED DOUBLE RAIL.
2. THE ENTIRE 56.25' OF THE CAT SYSTEM SHALL BE ON THE SAME CONTINUOUS TANGENT.
3. THE LEADING END OF THE CAT SHALL BE NO CLOSER THAN 1 FOOT FROM THE TANGENT LINE CREATED BY THE DOUBLE RAIL PRIOR TO THE CAT SYSTEM.
4. SAW CUT PAVEMENT PRIOR TO REMOVING, EXTEND SAW CUT INTO SHOULDER PAVEMENT 6" ALONG WIDTH OF NEW MEDIAN OPENING WIDTH.

**GUARDRAIL ADJUSTMENTS FOR
MM 60.4 MEDIAN OPENING**

N.T.S.



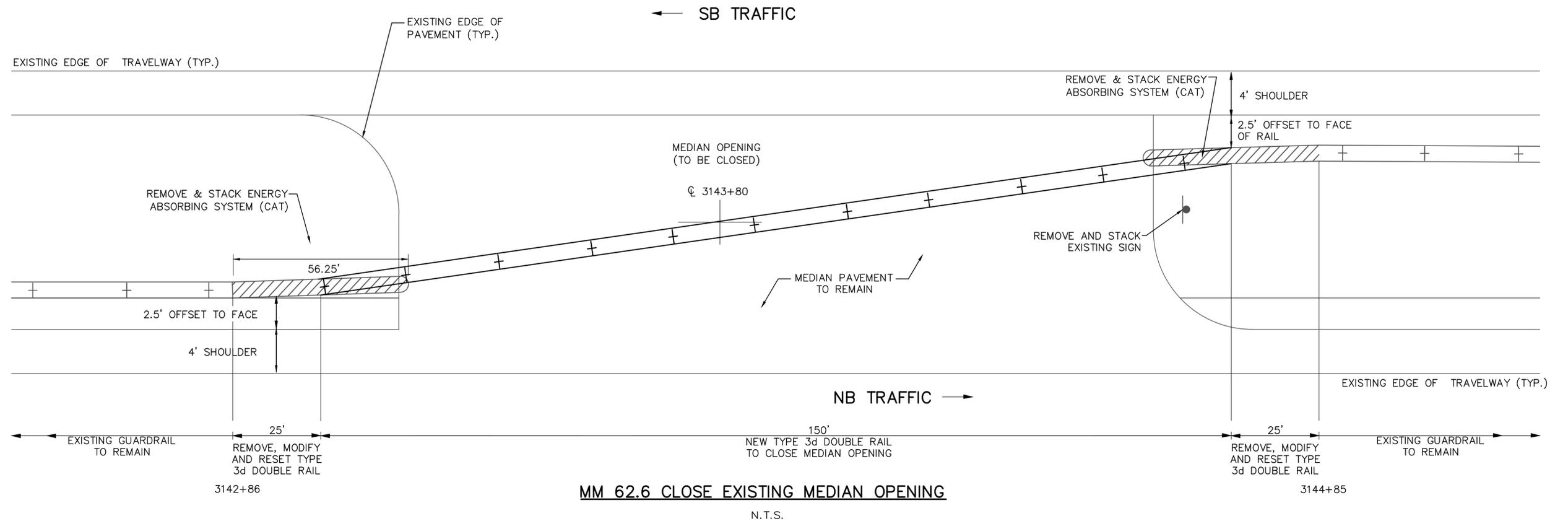
SECTION A-A

N.T.S.

SECTION B-B

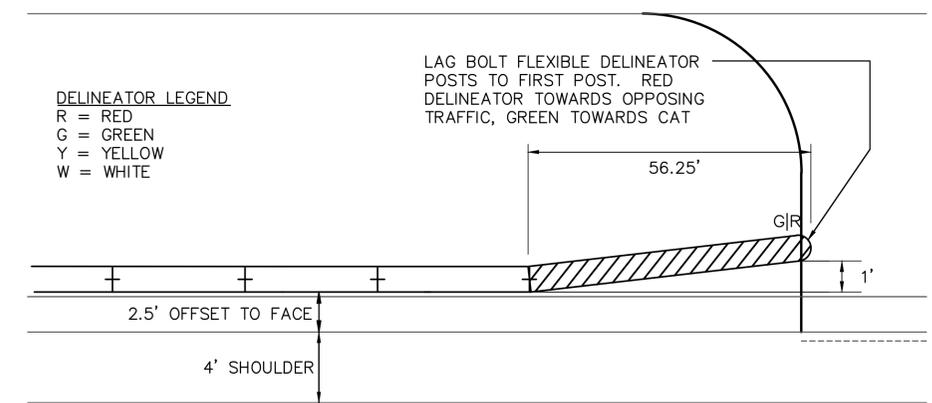
N.T.S.

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MM 62.6 CLOSE EXISTING MEDIAN OPENING

N.T.S.



DELINEATOR LEGEND
 R = RED
 G = GREEN
 Y = YELLOW
 W = WHITE

CAT INSTALLATION DETAIL

NOTES:

1. THE RESET CAT SYSTEM INCLUDES 31.25 FEET OF ENERGY ABSORBING SYSTEM AND 25 FEET OF TYPE 3D MODIFIED DOUBLE RAIL.
2. THE ENTIRE 56.25' OF THE CAT SYSTEM SHALL BE ON THE SAME CONTINUOUS TANGENT.
3. THE LEADING END OF THE CAT SHALL BE NO CLOSER THAN 1 FOOT FROM THE TANGENT LINE CREATED BY THE DOUBLE RAIL PRIOR TO THE CAT SYSTEM.

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THE GOLD STAR MEMORIAL HIGHWAY

MTA PROJECT MANAGER: Joseph R. Leavitt, P.E.

PAVEMENT REHABILITATION
 CLEARZONE IMPROVEMENTS

MEDIAN OPENING DETAILS MM 62.6

SHEET NUMBER: G-4

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GUARDRAIL EARTHWORK AND PAVEMENT REPAIR NOTES

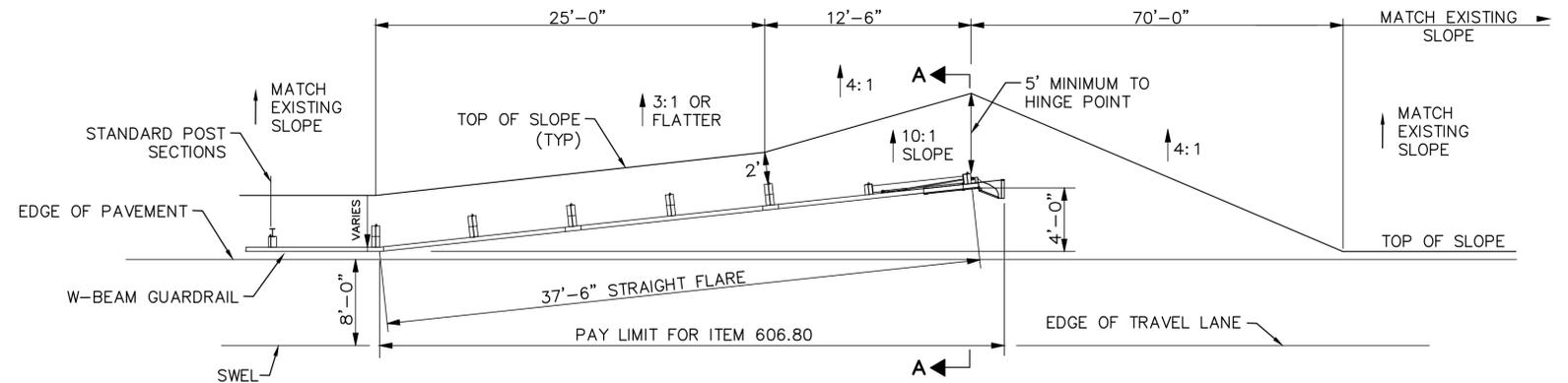
1. ALL EARTHWORK AND PAVEMENT REPAIRS ASSOCIATED WITH GUARDRAIL INSTALLATIONS AND/OR MODIFICATIONS FOR THIS CONTRACT SHALL BE INCIDENTAL TO THE GUARDRAIL ITEMS UNLESS OTHERWISE NOTED.
2. ALL DISTURBED AREAS DUE TO THE WORK PERFORMED UNDER THIS CONTRACT FOR GUARDRAIL REPAIRS, MODIFICATION AND/OR INSTALLATIONS SHALL BE REPAIRED AND RESTORED TO ORIGINAL CONDITION. GRASSED AREAS SHALL RECEIVE 4" OF LOAM, SEED, FERTILIZER AND MULCH. IN PAVED AREAS, SUBBASE GRAVEL SHALL BE REPLACED AND RECOMPACTED WITH MAINE DOT TYPE-D GRAVEL AND HOT MIX ASPHALT TO A THICKNESS TO MATCH SURROUNDING PAVEMENT.
3. COMPACTION OF EARTH AND/OR SUBBASE GRAVEL AROUND GUARDRAIL POSTS DISTURBED DURING THE WORK IS REQUIRED. ALL POSTS SHALL BE PLUMB AFTER COMPACTION.
4. EXCAVATION NECESSARY TO MAINTAIN DRAINAGE WAYS OUTSIDE OF THE FLEAT 350 WIDENING OR THE SHOULDER WIDENING WORK AREA SHALL BE PAID FOR UNDER ITEM 631.12 ALL PURPOSE EXCAVATOR AS APPROVED BY THE RESIDENT ENGINEER. EXCAVATED MATERIAL SHALL BE USED TO GRADE ANY PROPOSED SLOPE AREA OUTSIDE OF 1:1 SLOPE LIMIT OR USED TO FLATTEN SLOPES OUTSIDE THE WORK AREA AS APPROVED BY THE RESIDENT. PLACEMENT AND COMPACTION OF THE EXCAVATED MATERIAL WILL NOT BE MEASURED SEPARATELY FOR PAYMENT.
5. SLOPES CONSTRUCTED FROM THE ABOVE NOTED EXCAVATED MATERIAL SHALL BE LOAMED, SEEDED AND MULCHED. LOAM, SEED AND MULCH WILL NOT BE MEASURED FOR PAYMENT AND SHALL BE INCIDENTAL TO THE CONTRACT.
6. SLOPES CONSTRUCTED FOR ITEM 205.51 SHALL BE 3:1 WITHIN 50 FEET OF THE TERMINAL END AND CAN BE 2:1 IN ALL OTHER AREAS.

GUARDRAIL INSTALLATION AND MODIFICATION NOTES

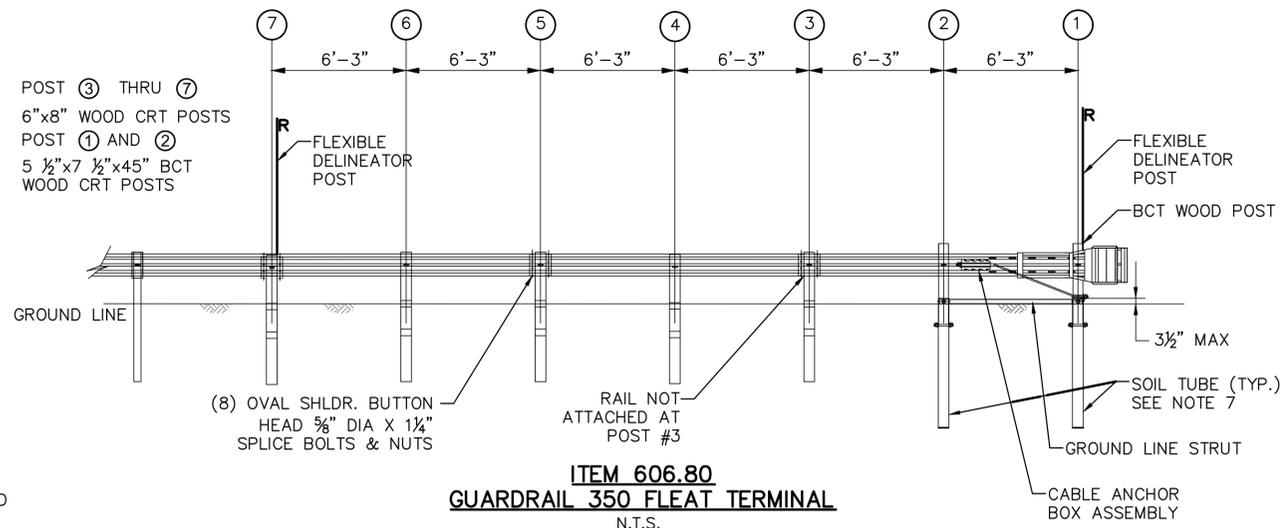
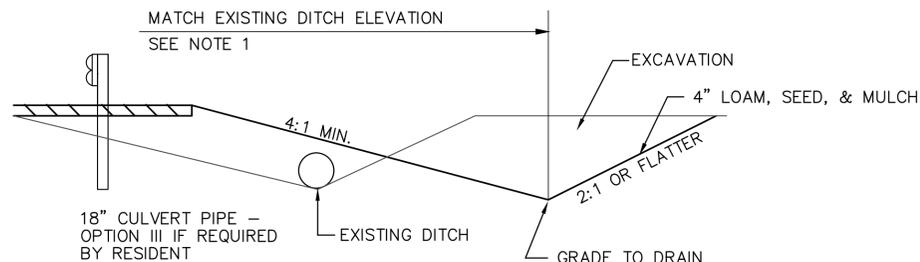
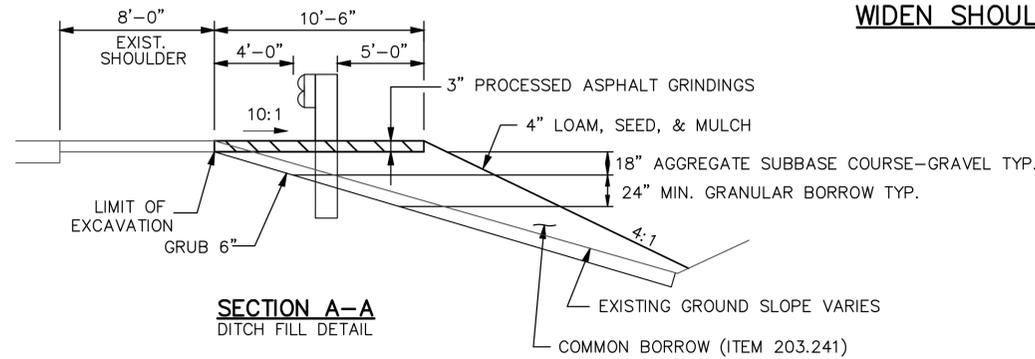
1. THE CONTRACTOR SHALL FOLLOW ALL MANUFACTURER'S INSTALLATION INSTRUCTIONS FOR THE INSTALLATION AND/OR MODIFICATION OF ANY AND ALL GUARDRAIL COMPONENTS TO BE INSTALLED UNDER THIS CONTRACT. THE CONTRACTOR SHALL NOTIFY THE RESIDENT ENGINEER IF THE CONTRACTOR DISCOVERS CONFLICTS OR IRREGULARITIES OF ANY SORT BETWEEN THE MANUFACTURER'S INSTALLATION INSTRUCTIONS AND THESE CONTRACT DOCUMENTS.
2. THE CONTRACTOR IS REQUIRED TO HAVE AN APPROVED CRASH END TREATMENT ON ALL GUARDRAIL ENDS IN ALL WORK AREAS AT THE END OF EACH DAY. THE CONTRACTOR SHALL ENSURE THAT ENOUGH TIME EXISTS IN THE WORK DAY TO COMPLETE ALL MODIFICATIONS AND/OR INSTALLATIONS TO END TERMINALS. THE CONTRACTOR SHALL ENSURE THAT ALL PARTS AND MANPOWER ARE ON-SITE PRIOR TO UNDERTAKING ANY GUARDRAIL MODIFICATIONS.
3. GUARDRAIL HEIGHT SHALL BE INSTALLED OR ADJUSTED TO 30" +0", -1/2" HEIGHT. GUARDRAIL HEIGHT SHALL BE MEASURED FROM THE EDGE OF PAVEMENT WHEN WITHIN 2 FEET. IF GUARDRAIL IS OFFSET FROM EDGE OF PAVEMENT GREATER THAN 2 FEET, THEN HEIGHT OF RAIL SHALL BE MEASURED AT GROUND ELEVATION ADJACENT TO EACH POST BELOW THE RAIL TO THE TOP OF GUARDRAIL AND AFTER FINAL GRADING OF SHOULDER.
4. THE CONTRACTOR SHALL REMOVE AND DISPOSE OF ALL CUT BOLTS, DISCARDED PARTS, TRASH, PAVEMENT DEBRIS, ETC., AFTER COMPLETING GUARDRAIL MODIFICATIONS AND/OR INSTALLATIONS.
5. CONNECTIONS FOR PROPOSED RAIL TO EXISTING RAIL SHALL BE INCIDENTAL TO THE INSTALLATION OF PROPOSED GUARDRAIL.
6. FLEAT 350 TERMINALS INSTALLED UNDER THIS CONTRACT SHALL BE INSTALLED AT A HEIGHT OF 27.5" AT THE TERMINAL HEAD AND TRANSITIONING UP TO THE REQUIRED HEIGHT OF RAIL OF 30" +0", -1/2" OVER A 25 LINEAR FOOT LENGTH. HEIGHT OF FLEAT 350 RAIL SHALL BE MEASURED FROM THE GROUND ELEVATION UNDER THE FLEAT RAIL PANELS TO THE TOP OF RAIL.
7. SOIL ANCHOR TUBES INSTALLED AS PART OF CAT, FLEAT OR TRAILING TERMINAL ENDS SHALL NOT PROTRUDE FROM THE GROUND MORE THAN 3-1/2". ADDITIONAL SITE GRADING MAY BE REQUIRED TO ADJUST SOIL TUBE REVEAL AND IF REQUIRED SHALL NOT DIRECT STORMWATER SURFACE DRAINAGE TOWARDS THE SHOULDER OR TRAVEL WAY. ADDITIONAL SITE GRADING SHALL BE INCIDENTAL TO THE END TERMINAL INSTALLATION.
8. TRANSITION RAIL HEIGHT ON TERMINAL END-ANCHORED ENDS FROM 27-1/2" AT THE END OF THE RAIL TO 30" HEIGHT OVER A 25 LINEAR FOOT LENGTH.

EROSION

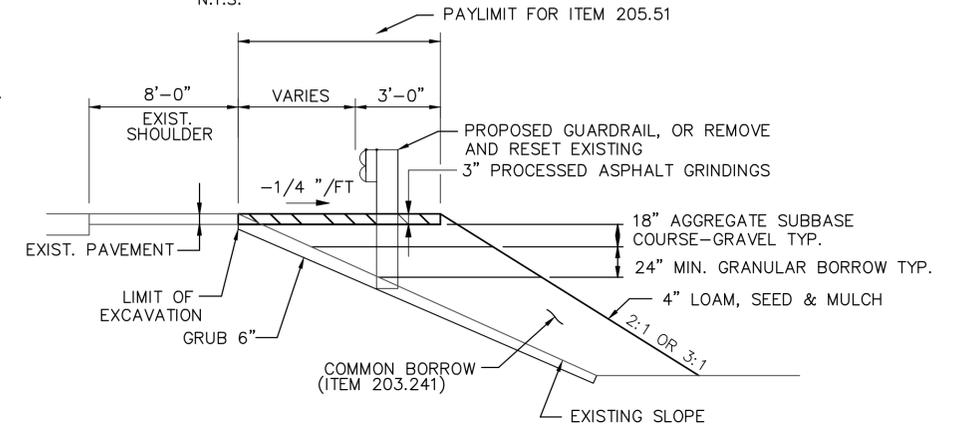
1. EROSION CONTROL BLANKET SHALL BE INSTALLED ON ALL DISTURBED 2:1 SLOPES AND IN FLOW LINE OF ALL DISTURBED DITCHES AFTER LOAM AND SEED HAVE BEEN INSTALLED.



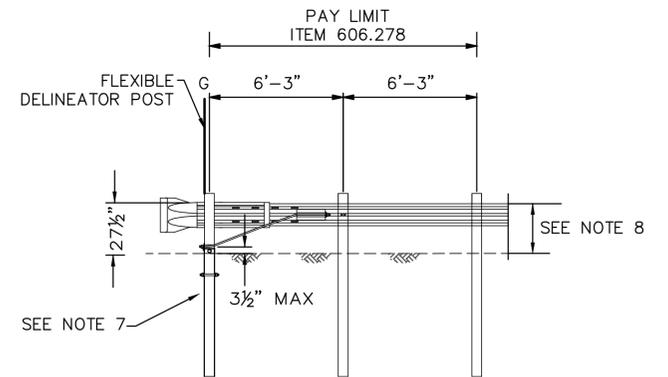
ITEM 606.754
WIDEN SHOULDER FOR GUARDRAIL 350 FLARED TERMINAL
PLAN
N.T.S.



ITEM 606.80
GUARDRAIL 350 FLEAT TERMINAL
N.T.S.



SHOULDER WIDENING DETAIL
ITEM 205.51



NOTES:

1. TRANSITION TOP OF GUARDRAIL RUN OVER 25' PRIOR TO INSTALLED TERMINAL END - ANCHORED END.
2. HEIGHT OF GUARDRAIL TERMINAL END SHALL BE 27-1/2"

ITEM 606.278
TERMINAL END - ANCHORED END
N.T.S.

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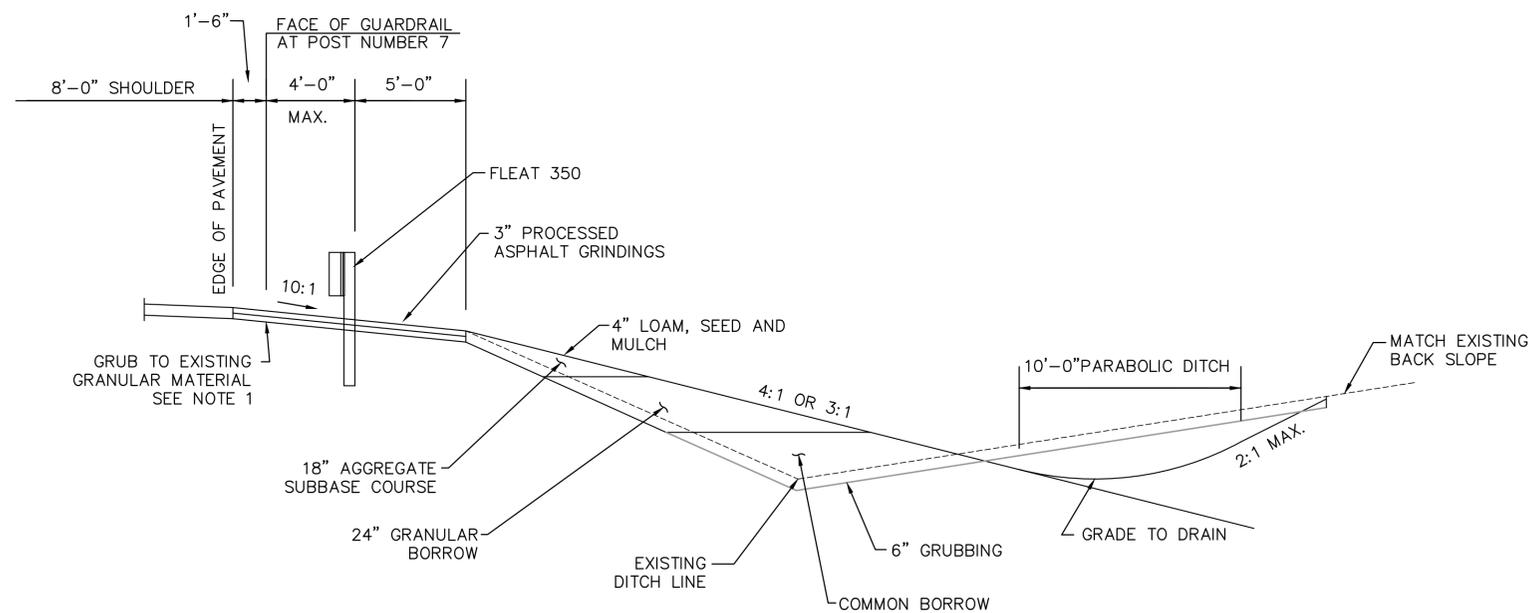
THE GOLD STAR MEMORIAL HIGHWAY

MTA PROJECT MANAGER: Joseph R. Leavitt, P.E.

PAVEMENT REHABILITATION
CLEARZONE IMPROVEMENTS

GUARDRAIL AND SHOULDER
WIDENING DETAILS

SHEET NUMBER: G-5
CONTRACT: 2016.01
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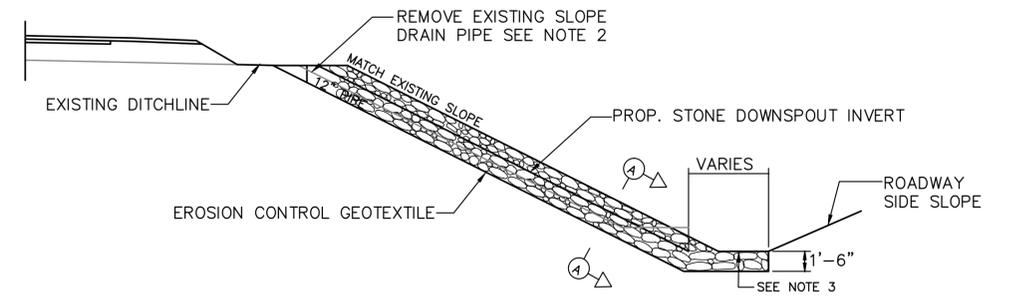
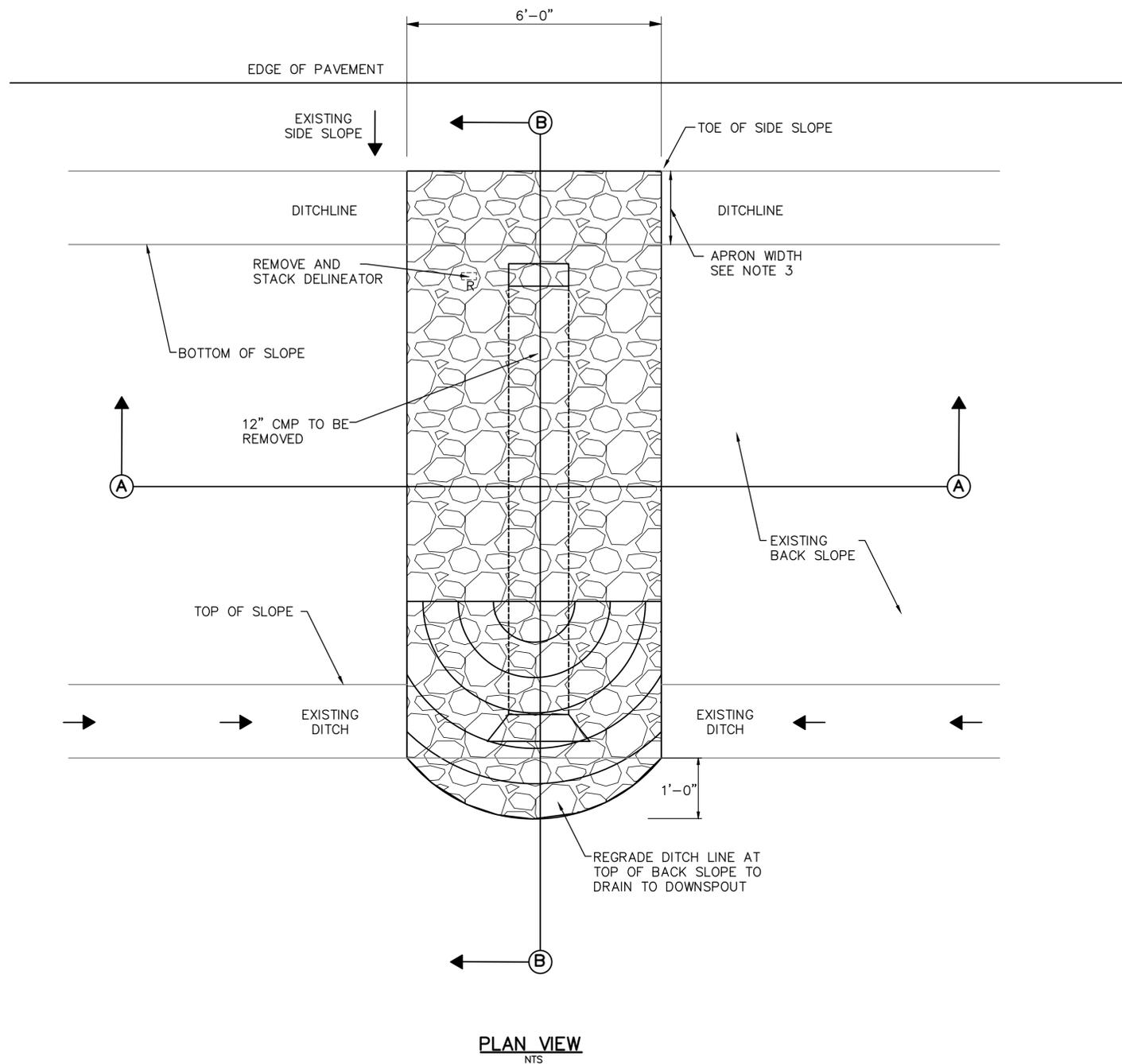


**MODIFY SHOULDER WIDENING FOR GUARDRAIL 350 FLARED TERMINAL
(ITEM 606.755)**
N.T.S.

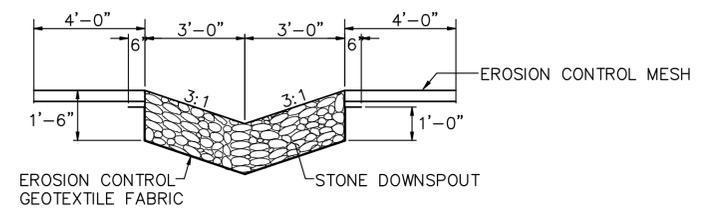
NOTES:

- GRUB TOP OF EXISTING SHOULDER WIDENING DOWN TO GRANULAR SUBBASE MATERIAL MINIMUM 3" BELOW EDGE OF PAVEMENT AND COMPACT. PLACE ADDITIONAL AGGREGATE SUBBASE COURSE AND COMPACT IF GRUBBING MORE THAN 3" BELOW EDGE OF PAVEMENT.

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SECTION B-B
NTS



SECTION A-A
NTS

1 LF OF DOWNSPOUT = .33 CY OF STONE DITCH PROTECTION
1 LF OF DOWNSPOUT = 1.04 SY OF EROSION CONTROL GEOTEXTILE FABRIC

NOTES:

1. STONE FOR STONE DOWNSPOUTS SHALL MEET THE REQUIREMENTS OF MDOT SPECIFICATION 703.29 AND WILL BE PAID FOR UNDER ITEM 610.18 STONE DITCH PROTECTION.
2. REMOVAL OF 12" SLOPE DRAIN PIPE IS INCIDENTAL TO CONSTRUCTION OF DOWNSPOUT.
3. EXTEND RIPRAP APRON TO TOE OF ROADWAY SIDE SLOPE.
4. SEE DRAINAGE SCHEDULE FOR LOCATIONS.

Scale:			
No.	Revision	By	Date

Designed by:					
					
	By	Date		By	Date
Designed	BAT	01/26/2016	Checked	BAT	01/26/2016
Drawn	BMB	01/26/2016	In Charge of	SRT	01/26/2016

MAINE TURNPIKE AUTHORITY
2360 Congress Street
Portland, ME 04102
TEL (207) 871-7771
FAX (207) 879-5567



**THE GOLD STAR
MEMORIAL HIGHWAY**

MTA PROJECT MANAGER: Joseph R. Leavitt, P.E.

PAVEMENT REHABILITATION
CLEARZONE IMPROVEMENTS
DOWN SPOUT DRAIN DETAILS

SHEET NUMBER: G-7

CONTRACT: 2016.01

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MEDIAN SCHEDULE OF GUARDRAIL IMPROVEMENTS																					
MILE MARKER	LOCATION DESCRIPTION	LOCATION OF WORK				ITEM 606.178 GUARDRAIL BEAM	ITEM 606.2401 GUARDRAIL TYPE 3d DOUBLE RAIL	ITEM 606.2652 TERMINAL END - REMOVE & STACK	ITEM 606.278 TERMINAL END- ANCHORED END	ITEM 606.353 DELINEATOR POST	ITEM 606.355- DELINEATOR POST - REMOVE AND STACK	ITEM 606.3601 GUARDRAIL REMOVE, MODIFY AND RESET DOUBLE RAIL	ITEM 606.3621 GUARDRAIL ADJUST - SINGLE RAIL	ITEM 606.3622 GUARDRAIL ADJUST - DOUBLE RAIL	ITEM 606.471 SINGLE OFFSET BLOCK - W BEAM	ITEM 606.48 SINGLE GALVANIZED STEEL POST	ITEM 606.701 ASYMMETRICAL THRIE BEAM TRANSITION	ITEM 606.82 GUARDRAIL - REMOVE AND STACK EXISTING CRASH END	ITEM 527.303 GUARDRAIL - REMOVE, MODIFY AND RESET EXISTING CRASH END	ITEM 645.109 REMOVE AND STACK SIGN	DESCRIPTION OF WORK
		APPROX. STATION	APPROX. STATION of TERMINAL END or ENDPOST	STATION LEADING END	STATION TRAILING END																
58.3	Blackstrap Road	2917+30	2918+75	2917+17.5	2918+75			1	1	1	1		112.5								See Note 1 and Note 2. ALL WORK LOCATED ON THE SOUTH BOUND ONLY.
59.5	Median Opening (TO BE CLOSED)	2980+65	2981+55	N/A	N/A	200.0			1	5	300.0						2		1		CLOSE MEDIAN OPENING. Remove and Stack existing Crash Ends, Delineators and Signage. R,M,R Median Double Rail From Station 2982+10 to 2985+00 to the North Bound Shoulder. Close Median with NEW Type 3d Double Rail.
59.9	Dutton Hill	3002+75	3004+20	3002+62.5	3004+32.5			2	2	4	4		250.0								See Note 1 and Note 2.
60.4	Median Opening	3030+05	3032+00	3030+05	3031+00	100.0			4	5	25.0							1			REDUCE MEDIAN OPENING WIDTH. Reset CAT located on North Bound to STA 3031+05. Install 100 LF of NEW Type 3d Double Rail.
60.8	Eagles Nest	3048+00	3050+50	N/A	N/A																SEE EAGLES NEST BRIDGE REPAIR PLANS FOR GUARDRAIL UPGRADES
61.6	Hunts Hill	3092+40	3092+80	3092+40	3092+80				2	2							2				See General Note 3. Replace existing Thrie Beam transition panels with Item 606.701 Asymmetrical Thrie Beam Transition
62.6	Median Opening (TO BE CLOSED)	3143+50	3144+35	N/A	N/A	150.0				2	50.0						2		1		CLOSE MEDIAN OPENING. REFER TO PLANS FOR CONFIGURATION.
62.9	Center Road	3159+05	3160+25	3158+92.5	3160+37.5			2	2	4	4		250.0								See Note 1 and Note 2.
63.3	Rte. 202	3178+40	3179+75	3178+27.5	3179+87.5			2	2	6	4										See Note 2.
63.5	Median Opening	3195+20	3196+25	3195+20	3196+25				4	4											Adjust Rail Height of CATs (2) TO 27.5-inches to Top of Rail.
64.3	Rte. 26	3231+95	3233+37.5	3231+82.5	3233+50			2	2	2	2			36	76						See Note 2 and Note 3. REINFORCE BOTH BOUNDS AT PIERS.
TOTAL						375.0	450.0	9	9	28	33	375	612.5	37,915	36	76	2	4	1	2	
UNIT						LF	LF	EA	EA	EA	EA	LF	LF	LF	EA	EA	EA	EA	EA	EA	

GENERAL NOTES:
1 - APPROXIMATE STATION IS THE LEADING END OF THE EXISTING TWISTED END, MELT, FLEAT 350 OR BRIDGE ENDPOST WHEN EXITING, PRIOR TO PROPOSED WORK.
2 - REPAIR DAMAGED GUARDRAIL BEAMS AS INDICATED OR AS DIRECTED BY THE RESIDENT ENGINEER.
3 - REPLACE ALL RIGID GUARDRAIL DELINEATOR POSTS WITH FLEXIBLE DELINEATORS.

INSTALLATION NOTES:
1 - ADJUST ALL GUARDRAIL TO 30-INCH ABOVE EDGE OF PAVEMENT AS REQUIRED OR AS DIRECTED BY THE RESIDENT.
2 - REMOVE EXISTING END SECTION AND STACK; INSTALL ANCHORED TRAILING END.
3 - INSTALL ADDITIONAL GALVANIZED POSTS AND NEW OFFSET BLOCKS TO RAIL ALONG FACE OF BRIDGE PIER AND A MINIMUM OF 25 FEET EITHER SIDE OF PIER TO REDUCE POST SPACING TO 3.125- FEET.
4 - BEGIN MEDIAN RAIL ADJUSTMENTS AT STATION 3384+00 AND END AT STATION 3798+60.

Scale:	Designed by:	 MAINE TURNPIKE AUTHORITY 2360 Congress Street Portland, ME 04102 TEL (207) 871-7771 FAX (207) 879-5567	 THE GOLD STAR MEMORIAL HIGHWAY	PAVEMENT REHABILITATION CLEARZONE IMPROVEMENTS GUARDRAIL MEDIAN SCHEDULE SHEET NUMBER: G-8 CONTRACT: 2016.01 32 of 77																											
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NORTHBOUND SCHEDULE OF SHOULDER GUARDRAIL IMPROVEMENTS																					
MILE MARKER	LOCATION DESCRIPTION	LOCATION OF WORK				ITEM 606.178 GUARDRAIL BEAM	ITEM 606.2652 TERMINAL END - REMOVE & STACK	ITEM 606.278 TERMINAL END- ANCHORED END	ITEM 606.353 DELINEATOR POST	ITEM 606.355 - DELINEATOR POST - REMOVE AND STACK	ITEM 606.3605 REMOVE, MODIFY and RESET SINGLE RAIL	ITEM 606.3621 GUARDRAIL ADJUST - SINGLE RAIL	ITEM 606.701 ASYMMETRICAL THRIE BEAM TRANSITION	ITEM 606.755 MODIFY WIDEN SHOULDER FOR GUARDRAIL 350 FLARED TERMINAL	ITEM 606.80 GUARDRAIL 350 FLARED TERMINAL	ITEM 606.82 REMOVE AND STACK EXISTING CRASH END	ITEM 205.51 WIDEN SHOULDER TRAILING END			DESCRIPTION OF WORK	
		APPROX. STATION	APPROX. STATION OF TERMINAL END OR ENDPOST	STATION LEADING END	STATION TRAILING END												BEGIN STATION	END STATION	QUANTITY		
55.7	NB Shoulder	2785+25	2787+85	2785+25	2787+97.5		1	1	3	4									See Note 1 and See Note 2. Remove and Stack White Delineator Post on FLEAT Platform.		
55.5	Piscataqua River	2828+70	2835+10	2828+70	2835+10				2	2									Remove non-tubular delineators and replace with Item 606.353 Tubular Delineators.		
57.1	NB Shoulder	2855+00	2889+40	2855+00	2889+40				1	1		3,440.0							See Note 1. Adjust FLEAT 350 height of rail and height of soil tube as per specifications.		
58.3	Blackstrap Road	2916+60	2918+55	2916+60	2918+42.5				1	1	25.0								Replace existing Delineators with Item 606.353 Tubular Delineators. RMR 25 LF of existing Guardrail on trailing end for installation of widened shoulder.		
58.7	NB Decel Ramp	2939+70	2942+90	2939+70	2942+90	12.5													See Note 1. Toe nail wood blocking, replace first panel in FLEAT 350 Crash End.		
59.0	Cumberland Service Plaza (Main Line Only)	2950+20	2958+60	2950+20	2959+75				1	1									See Note 1. Adjust Height of Rail on CAT to 27.5".		
59.4	NB Shoulder	2990+35	3018+25	2990+35	3018+25		1	1	3	3							3018+25	3018+40	15.0	See Note 1 and Note 2.	
60.4	NB Shoulder	3025+00	3028+25	3025+00	3028+37.5		1	1	3	4								3028+25	3028+40	15.0	See Note 1 and See Note 2.
60.5	Eagles Nest Road	3037+75	3084+90	N/A	N/A									1,000.0						See Note 1. Modify Widen Shoulder for FLEAT 350 AT sta 3037+75. SEE EAGLES NEST BRIDGE REPAIR PLANS FOR GUARDRAIL UPGRADES	
61.5	Hunts Hill Road	3089+35	3122+10	3089+35	3122+22.5		1	1	3	3				4,150.0						See Note 1 and Note 2. Adjust rail for height between STA 3089+65 and STA 3092+30 ONLY. Install Item 606.701 Asymmetrical Thrie Beam Transition panels to adjust rail height.	
63.5	NB Shoulder	3189+65	3190+80	3189+65	3190+92.5		1	1	3	3				125.0				3189+65	3189+80	15.0	See Note 1 and Note 2.
64.2	Rte. 26	3228+20	3232+50	3228+20	3232+62.5				3	3				300.0						See Note 1. ADJUST SINGLE RAIL ONLY.	
TOTAL						12.5	5	5	23	25	25.0	13,515.0	2	4	1	1					
UNIT						LF	EA	EA	EA	EA	LF	LF	EA	EA	EA	EA				45.0	

GENERAL NOTES:
1 - APPROXIMATE STATION IS THE LEADING END OF THE EXISTING TWISTED END, MELT, OR BRIDGE ENDPOST WHEN EXITING, PRIOR TO PROPOSED WORK.
2 - FOR ITEM 205.51 SHOULDER WIDENING, WIDEN SHOULDER AS INDICATED ON ABOVE SCHEDULE OR AS DIRECTED BY THE RESIDENT ENGINEER.
3 - REPAIR DAMAGED GUARDRAIL BEAMS AS INDICATED OR AS DIRECTED BY THE RESIDENT ENGINEER.
4 - REPLACE ALL RIGID GUARDRAIL DELINEATOR POSTS WITH FLEXIBLE DELINEATORS.
5 - INDICATED INSTALLATION LOCATION OF FLEAT UNITS REFERS TO LOCATION OF THE FLEAT CRASH HEAD.

INSTALLATION NOTES:
1 - ADJUST ALL GUARDRAIL TO 30-INCH ABOVE EDGE OF PAVEMENT AS REQUIRED OR AS DIRECTED BY THE RESIDENT.
2 - REMOVE EXISTING END SECTION AND STACK; INSTALL ANCHORED TRAILING END.
3 - INSTALL ADDITIONAL GALVANIZED POSTS AND NEW OFFSET BLOCKS TO RAIL ALONG FACE OF BRIDGE PIER AND A MINIMUM OF 25 FEET EITHER SIDE OF PIER TO REDUCE POST SPACING TO 37.5-INCHES.

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SOUTHBOUND SCHEDULE OF OUTSIDE SHOULDER WORK																										
MILE MARKER	LOCATION DESCRIPTION	LOCATION OF WORK				ITEM 606.178 GUARDRAIL BEAM	ITEM 606.24 GUARDRAIL TYPE 3d- SINGLE RAIL	ITEM 606.2652 TERMINAL END - REMOVE & STACK	ITEM 606.278 TERMINAL END- ANCHORED END	ITEM 606.353 DELINEATOR POST	ITEM 606.355 - DELINEATOR POST - REMOVE AND STACK	ITEM 606.3621 GUARDRAIL ADJUST - SINGLE RAIL	ITEM 606.471 SINGLE OFFSET BLOCK - W BEAM	ITEM 606.48 SINGLE GALVANIZED STEEL POST	ITEM 66.701 ASYMMETRICAL THRIE BEAM TRANSITION	ITEM 606.754 WIDEN SHOULDER FOR GUARDRAIL 350 FLARED TERMINAL	ITEM 606.755 MODIFY WIDEN SHOULDER FOR GUARDRAIL 350 FLARED TERMINAL	ITEM 606.80 GUARDRAIL 350 FLARED TERMINAL	ITEM 606.82 REMOVE AND STACK EXISTING CRASH END	ITEM 606.83 REMOVE AND RESET EXISTING CRASH END	ITEM 205.51 WIDEN SHOULDER TRAILING END			DESCRIPTION OF WORK		
		APPROX. STATION	APPROX. STATION of TERMINAL END or ENDPOST	STATION LEADING END	STATION TRAILING END																BEGIN STATION	END STATION	QUANTITY			
64.3	Route 26	3235+05	3226+50	3235+05	3226+37.5			1	1	3	3	817.5	48	18							3226+50	3226+35	15.0	See Note 1, Note 2 and Note 3.		
63.9	SB Shoulder	3213+70	3202+90	3213+70	3203+87.5			1	1	3	4	1,087.5					1					3202+90	3202+85	15.0	See Note 1 and Note 2. Remove and Stack White Delineator on FLEAT Platform.	
63.5	SB Shoulder	3195+00	3189+80	3195+00	3189+47.5			1	1	3	3	550.0					1					3189+60	3189+45	15.0	See Note 1 and Note 2.	
63.1	Exit 63 Decel Ramp	3180+50	3178+25	3180+50	3178+12.5			1	1	3	3	225.0														See Note 1 and Note 2.
62.5	SB Shoulder	3131+90	3128+80	3131+90	3128+77.5			1	1	3	3	275.0					1									See Note 1 and Note 2.
62.3	SB Shoulder	3122+80	3112+25	3122+80	3112+12.5			1	1	3	3	1,050.0					1									See Note 1 and Note 2. Adjust Height of Rail on FLEAT 350 to 27.5 inch.
61.6	Hurts Hill Road	3094+65	3094+00	3097+00	3094+00		200.0			2	2	50.0			1	1				1		3095+10	3095+65	55.0	See Note 1 for rail to remain in place. Remove and Reset FLEAT 350 at STA 3095+00. Install Item 606.701 to adjust rail height to 30-inch.	
61.2	SB Shoulder	3084+25	3079+70	3084+25	3079+57.5			1	1	3	3	450.0					1					3079+70	3079+55	15.0	See Note 1 and Note 2.	
60.8	Eagles Nest Road	3053+60	3048+05	N/A	N/A							425.0					1									See Note 1 and Note 2. Modify Widen Shoulder for FLEAT 350 at STA 3053+60. SEE EAGLES NEST BRIDGE REPAIR PLANS FOR ADDITIONAL GUARDRAIL UPGRADES.
60.7	SB Shoulder	3044+40	3039+90	3044+40	3039+77.5			1	1	3	3	450.0					1					3039+90	3039+75	15.0	See Note 1 and See Note 2.	
60.2	SB Shoulder	3021+50	2991+50	3021+50	2991+37.5	100.0		1	1	3	3	3,000.0					1					2991+50	2991+35	15.0	See Note 1 and See Note 2.	
59.5	SB Shoulder	2986+00	2982+90	2986+00	2982+77.5	100.0		1	1	3	3	310.0					1					2982+90	2982+75	15.0	See Note 1 and Note 2.	
58.8	Cumberland Service Plaza SB	2946+25	2939+00	2946+25	2939+00					3	3	725.0					1									See Note 1. Adjust Height of FLEAT 350 to 27.5 inches. Stop shoulder guardrail height adjustments at STA 2939+00.
58.7	Cumberland Service Plaza SB	2937+90	2935+90	2937+90	2935+90			1	1	3	3	200.0														See Note 1 and Note 2.
58.3	Blackstrap Road	2919+30	2917+00	2919+30	2916+87.5	37.5		1	1	3	3	225.0					1									See Note 1 and Note 2.
57.7	SB Shoulder	2886+90	2856+40	2886+90	2856+27.5			1	1	3	3	3,050.0					1					2856+40	2856+25	15.0	See Note 1 and Note 2.	
56.4	SB Shoulder	2820+70	2810+90	2820+70	2810+87.5	25.0		1	1	3	3	950.0					1	1	1			2810+90	2810+75	15.0	See Note 1 and Note 2.	
55.8	SB Shoulder	2791+50	2785+35	2791+50	2785+22.5			1	1	3	3	625.0					1	1	1			2785+35	2785+20	15.0	See Note 1 and Note 2.	
56.4	SB Shoulder	2761+00	2748+50	2761+00	2748+37.5			1	1	3	3	1,250.0					1	1	1			2748+50	2748+35	15.0	See Note 1 and Note 2.	
TOTAL						262.5	200.0	16	16	53	54	15,715.0	48	18	1	2	15	3	3	1				220.0		
UNIT						LF	LF	EA	EA	EA	EA	LF	EA	EA	EA	EA	EA	EA	EA	EA	EA				LF	

GENERAL NOTES:
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3 - REPAIR DAMAGED GUARDRAIL BEAMS AS INDICATED OR AS DIRECTED BY THE RESIDENT ENGINEER.
4 - REPLACE ALL RIGID GUARDRAIL DELINEATOR POSTS WITH FLEXIBLE DELINEATORS.
5 - INDICATED INSTALLATION LOCATION OF GUARDRAIL 350 FLEAT UNITS REFERS TO LOCATION OF THE FLEAT CRASH HEAD.

INSTALLATION NOTES (SITE SPECIFIC):
1 - ADJUST ALL GUARDRAIL TO 30-INCH ABOVE EDGE OF PAVEMENT AS REQUIRED OR AS DIRECTED BY THE RESIDENT.
2 - REMOVE EXISTING END SECTION AND STACK; INSTALL ANCHORED TRAILING END.
3 - INSTALL ADDITIONAL GALVANIZED POSTS AND NEW OFFSET BLOCKS TO RAIL ALONG FACE OF BRIDGE PIER AND A MINIMUM OF 25 FEET EITHER SIDE OF PIER TO REDUCE POST SPACING TO 37.5-INCHES.

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No.	Revision	By	Date																	
				CONTRACT: 2016.01	94 of 97															

Date: 1/26/2016

Filename: ... \BRIDGE\WSTA\035_Quantities.dgn

ITEM NO.	ITEM DESCRIPTION	UNIT	EAGLES NEST RD OVERPASS (MILE 60.8)	HUNTS HILL RD OVERPASS (MILE 61.6)	BENNET ROAD UNDERPASS (MILE 68.6)	KITTYHAWK AVE UNDERPASS (MILE 75.0)	TOTAL
202.12	REMOVING EXISTING STRUCTURAL CONCRETE	CY	34				34
202.127	REMOVING OF EXISTING BITUMINOUS PAVEMENT (660 SY)	LS	0.01	0.99			1
203.20	COMMON EXCAVATION	CY	37				37
304.10	AGGREGATE SUBBASE COURSE - GRAVEL	CY	27				27
403.2081	12.5MM POLYMER MODIFIED HOT MIX ASPHALT	TON	50	74			124
403.213	HOT MIX ASPHALT, 12.5MM NOMINAL MAXIMUM SIZE, BASE	TON	14	74			88
409.15	BITUMINOUS TACK COAT, APPLIED	GAL	26	33			59
502.21	STRUCTURAL CONCRETE, ABUTMENTS & RETAINING WALLS	CY	38				38
502.26	STRUCTURAL CONCRETE SUPER. SLAB ON ST BR (2 CY)	LS	1				1
503.14	EPOXY-COATED REINFORCING STEEL, FAB & DELIVERED	LB	6300	100			6400
503.15	EPOXY-COATED REINFORCING STEEL, PLACING	LB	6300	100			6400
506.99	CLEAN AND PAINT EXISTING BEARINGS	EA	20				20
507.095	ALUMINUM BRIDGE RAILING 13*3213*32 SPLICE MODIFICATION	EA			24	32	56
508.141	HIGH PERFORMANCE WATERPROOFING MEMBRANE (660 SY)	LS	0.01	0.99			1
515.201	PIGMENTED PROTECTIVE COATING FOR CONCRETE SURFACES	SY	390	320			710
515.202	CLEAR PROTECTIVE COATING FOR CONCRETE SURFACES	SY	340	280			620
518.39	GRANITE CURB JOINT MORTAR AND BEDDING MORTAR REPAIR	LF	310	305			615
518.40	EPOXY INJECTED CRACK REPAIRS	LF	10	195			205
518.43	PARAPET JOINT REPAIR	LF		260			260
518.51	REPAIR OF UPWARD FACING CONCRETE SURFACES - BELOW REINFORCING STEEL < 8 INCHES	SF	40	320			360
518.511	FULL DEPTH CONCRETE REPAIR	SF		60			60
518.60	REPAIR OF VERTICAL SURFACES < 8 INCHES	SF	80	165			245
518.70	REPAIR OF OVERHEAD SURFACES < 8 INCHES	SF	5	90			95
520.2211	EXPANSION DEVICE MODIFICATIONS (47.5 FT)	EA		2			2
520.231	EXPANSION DEVICE - ASPHALTIC PLUG FOR CRACK CONTROL	LF		95			95
520.232	EXPANSION DEVICE - ASPHALTIC PLUG JOINT	LF	145				145
523.5211	BEARING REHABILITATION, ROCKER BEARINGS	EA	20				20
526.306	TEMPORARY CONCRETE BARRIER, TYPE I - SUPPLIED BY AUTHORITY (660 LF)	LS	1				1
527.341	WORK ZONE CRASH CUSHIONS - TL-3	UN	2				2
606.1723	BRIDGE TRANSITION - TYPE III	EA	6				6
606.1724	BRIDGE TRANSITION - TYPE III MODIFIED	EA	1				1
606.2652	TERMINAL END - REMOVE AND STACK	EA	1				1
606.278	TERMINAL END - ANCHORED END	EA	1				1
606.279	TERMINAL END - ANCHORED END - THRIE BEAM	EA	1				1
606.352	REFLECTORIZED BEAM GUARDRAIL DELINEATOR	EA	185				185
606.353	DELINEATOR POST	EA	2				2
606.3606	GUARDRAIL - REMOVE, MODIFY, AND RESET - DOUBLE RAIL	LF	170				170
606.3621	GUARDRAIL ADJUST, SINGLE RAIL	LF	25				25
606.3631	GUARDRAIL - REMOVE AND DISPOSE	LF	130				130
606.65	GUARDRAIL THRIE BEAM - SINGLE RAIL	LF	25				25
607.431	SNOW FENCE	LF	180		232	264	676
609.13	VERTICAL BRIDGE CURB, TYPE I	LF	130				130
609.191	CONCRETE CURB TYPE 2	LF	84				84
627.77	REMOVING EXISTING PAVEMENT MARKING	SF	1150				1150
627.78	TEMPORARY PAVEMENT MARKING LINE, WHITE OR YELLOW	LF	5900				5900
627.812	TEMPORARY RAISED PAVEMENT MARKERS	EA	36				36
629.05	HAND LABOR, STRAIGHT TIME	HR	20	20			40
652.30	FLASHING ARROW BOARD	EA	2	2	2	2	8
652.331	DRUM (120EA EAGLES NEST, 120EA HUNTS HILL, 15EA BENNETT RD, 15EA KITTYHAWK AVE)	LS			1		1
652.34	CONE	EA	15	15	15	15	60
652.35	CONSTRUCTION SIGNS	SF	750	640	570	570	2530
652.38	FLAGGERS	HR	1200	650	130	130	2110
652.3911	MAINTENANCE OF TRAFFIC CONTROL DEVICES (EAGLES NEST)	LS	1				1
652.3912	MAINTENANCE OF TRAFFIC CONTROL DEVICES (HUNTS HILL)	LS		1			1
652.3913	MAINTENANCE OF TRAFFIC CONTROL DEVICES (BENNETT & KITTYHAWK)	LS			0.5	0.5	1
652.45	TRUCK MOUNTED ATTENUATOR	CD	2	120	10	10	142
656.632	30 INCH TEMPORARY SILT FENCE	LF	100	50			150
659.10	MOBILIZATION	LS			1		1

INDEX OF SHEETS		
SHEET NO.	SHEET LABEL	DESCRIPTION
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36	GN-01	GENERAL NOTES (EAGLES NEST & HUNTS HILL)
37	EN-01	LIMIT OF DISTURBANCE (EAGLES NEST)
38	EN-02	DECK PLAN & TYPICAL SECTION (EAGLES NEST)
39	EN-03	JOINT REPAIR DETAILS (EAGLES NEST)
40 - 43	EN-04 - EN-07	SUBSTRUCTURE MODIFICATIONS (EAGLES NEST)
44	EN-08	DECK DETAILS (EAGLES NEST)
45 - 47	EN-09 - EN-11	END POST DETAILS (EAGLES NEST)
48	EN-12	REINFORCEMENT SCHEDULE (EAGLES NEST)
49	EN-13	MAINTENANCE OF TRAFFIC (EAGLES NEST)
50	EN-14	GENERAL PLAN (EAGLES NEST)
51	EN-15	GUARDRAIL DETAILS (EAGLES NEST)
52	EN-16	BRIDGE TRANSITION DETAILS (EAGLES NEST)
53	HH-01	LIMIT OF DISTURBANCE (HUNTS HILL)
54	HH-02	DECK PLAN & TYPICAL SECTION (HUNTS HILL)
55	HH-03	JOINT REPAIR DETAILS (HUNTS HILL)
56 - 57	HH-04 - HH-05	SUBSTRUCTURE REPAIR (HUNTS HILL)
58	SF-01	SNOW FENCE DECK PLAN
59 - 60	SF-02 - SF-03	SNOW FENCE DETAILS
61 - 64	AB-01 - AB-04	AS-BUILT SHEETS (EAGLES NEST)
65 - 73	AB-05 - AB-13	AS-BUILT SHEETS (HUNTS HILL)
74 - 75	AB-14 - AB-15	AS-BUILT SHEETS (BENNETT)
76 - 77	AB-16 - AB-17	AS-BUILT SHEETS (KITTYHAWK)

Scale: NOT TO SCALE

No.	Revision	By	Date

Designed by:

TYLIN INTERNATIONAL

CONSULTANT PROJECT MANAGER: Norman L. Baker

By	Date	By	Date
Designed	BLT 1/2016	Checked	DSM 1/2016
Drawn	PJB 1/2016	In Charge of	NLB 1/2016

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MAINE TURNPIKE

THE GOLD STAR MEMORIAL HIGHWAY

MTA PROJECT MANAGER: R. NORWOOD

BRIDGE REPAIRS
 2 LOCATIONS (MILE 60.8 & MILE 61.6)

QUANTITIES

SHEET NUMBER: QT-01
 CONTRACT: 2016.01
 35 OF 77

Date: 1/26/2016

Filename: ... \BRIDGE\WSTA\036_Gen_Notes.dgn

SPECIFICATIONS:

DESIGN

AASHTO LRFD BRIDGE DESIGN SPECIFICATIONS
7TH EDITION

CONSTRUCTION

STATE OF MAINE, DEPARTMENT OF TRANSPORTATION
STANDARD SPECIFICATIONS NOVEMBER, 2014

STATE OF MAINE DEPARTMENT OF TRANSPORTATION
STANDARD DETAILS, NOVEMBER 2014, WITH LATEST
REVISIONS

AASHTO LRFD BRIDGE CONSTRUCTION SPECIFICATIONS,
THIRD EDITION WITH 2013 INTERIMS

DESIGN LOADING:

LIVE LOAD

HL-93

MATERIALS:

CONCRETE

CONCRETE END POSTS AND DECK EXTENSIONS SHALL
BE CLASS AAA.

ALL OTHER PATCHING AND REPAIR AREAS SHALL
BE CLASS AAA-MODIFIED.

REINFORCING STEEL

AASHTO M31, GRADE 60 EPOXY-COATED.

BASIC DESIGN STRESSES:

CONCRETE

CLASS AAA, $f_c = 4,500$ P.S.I.

REINFORCING STEEL

$f_y = 60,000$ P.S.I.

GENERAL NOTES:

- FOR ADDITIONAL DETAILS REFERENCED OR NOT SHOWN IN THESE DRAWINGS, SEE THE STATE OF MAINE, DEPARTMENT OF TRANSPORTATION STANDARD DETAILS, HIGHWAYS AND BRIDGES, NOVEMBER 2014 WITH UPDATES.
- COPIES OF THE AS-BUILT PLANS ARE ON FILE AT THE MAINE TURNPIKE AUTHORITY. A PORTION OF THESE PLANS ARE INCLUDED IN THIS CONTRACT FOR THE CONTRACTOR'S CONVENIENCE. THE COMPLETENESS AND ACCURACY OF THESE PLANS IS NOT GUARANTEED.
- THE CONTRACTOR IS REQUIRED TO MAINTAIN ACCESS TO ALL DRIVEWAYS AND SIDE ROADS DURING CONSTRUCTION UNLESS NOTED OTHERWISE ON THE PLANS.
- REINFORCING STEEL SHALL HAVE A CLEAR COVER OF 2" MIN., UNLESS OTHERWISE NOTED.
- CHAMFER ALL EXPOSED CONCRETE EDGES $\frac{3}{4}$ " UNLESS OTHERWISE NOTED.
- ALL CONCRETE BRIDGE PARAPET FACES AND END POST CONCRETE INSIDE FACES AND TOP FACES SHALL HAVE A RUBBED FINISH PRIOR TO THE APPLICATION OF THE CLEAR PROTECTIVE COATING FOR CONCRETE SURFACE.
- SHIELDING REQUIRED DURING CONSTRUCTION SHALL NOT PROJECT BELOW THE BOTTOM FLANGES OF GIRDERS.
- ALL PAINT ON EXISTING STEEL, DAMAGED BY CONTRACTOR'S OPERATIONS, SHALL BE REPAIRED TO THE SATISFACTION OF THE RESIDENT AT THE CONTRACTOR'S EXPENSE. THE CONTRACTOR SHALL NOTE THAT THE EXISTING BRIDGE STRUCTURES CONTAIN LEAD BASED PAINT. THE CONTRACTOR SHALL INSTITUTE EVERY PRECAUTION WHEN WORKING WITH MATERIALS COATED WITH LEAD PAINTS.

PAVING NOTES:

- ANY NECESSARY CLEANING OF EXISTING PAVEMENT PRIOR TO PAVING SHALL BE INCIDENTAL TO THE RELATED PAVING ITEMS.
- RECYCLED BITUMINOUS PAVEMENT (RAP) SHALL NOT BE PLACED ON ANY PORTION OF THE EXISTING BRIDGE DECK.
- ALL COLD JOINTS WITH TEMPERATURES LESS THAN 120°F SHALL BE SEALED WITH JOINT SEALANT CONFORMING TO FEDERAL SPECIFICATION SS-S-1401C.

MAINTENANCE OF TRAFFIC NOTES:

- TRAFFIC CONTROL FOR HUNTS HILL OVERPASS WORK SHALL USE THE LANE CLOSURE FOR MILLING AND PAVING OPERATIONS DETAILS.

CONCRETE REPAIR PROCEDURAL NOTES:

- CONCRETE REPAIRS, INCLUDING THE ASSOCIATED REMOVALS, WILL BE PAID UNDER 518 ITEMS.
- ANY REINFORCING STEEL REQUIRED TO REPLACE DETERIORATED EXISTING REINFORCEMENT SHALL BE EPOXY COATED AND WILL BE PAID FOR UNDER ITEM 503.14, "EPOXY-COATED REINFORCING STEEL, FAB & DELIVERED" AND ITEM 503.15, "EPOXY-COATED REINFORCING STEEL, PLACING."

REMOVAL PROCEDURE:

- PRIOR TO THE START OF THE CONCRETE REMOVAL, THE RESIDENT AND THE CONTRACTOR SHALL SOUND ALL EXPOSED SURFACES OF CONCRETE AND AGREE ON THE REMOVAL LIMITS. SHOULD THE REMOVAL AREA LIMITS APPEAR TO CHANGE DURING THE DEMOLITION PROCESS, THE CONTRACTOR SHALL NOTIFY THE RESIDENT. THE RESIDENT AND CONTRACTOR SHALL AGREE ON THE REVISED PAY LIMITS PRIOR TO THE CONTRACTOR CONTINUING THE REMOVAL.
- PERFORM 1 INCH DEEP SAWCUTS ALONG LIMITS OF REMOVAL.
- CHIP CONCRETE TO DEPTH REQUIRED PER MAINE DOT STANDARD SPECIFICATION 518.

CONCRETE SURFACE PATCH/REPAIR PROCEDURE:

- PREPARE AND PATCH REPAIR AREAS WITH CLASS AAA MODIFIED CONCRETE. SEE SPECIFICATIONS FOR MATERIAL, PREPARATION, PLACEMENT, AND CURING REQUIREMENTS.
- PERFORM GENERAL FINISHING (SEE BELOW).

EPOXY INJECTION CRACK REPAIR:

- PREPARE CONCRETE SURFACE AND CRACK AREA BY CLEANING SUBSTRATE WITH WIRE BRUSH TO REMOVE LAITANCE AND CONTAMINANTS. BLAST CLEAN THE CRACK AND CONCRETE SURFACE; DO NOT USE WATER.
- SET INJECTION PORTS ALONG THE LENGTH OF THE CRACK USING A HIGH-MODULUS EPOXY ADHESIVE. ONCE SET, SEAL PORTS AND CRACK WITH SAME EPOXY ADHESIVE.
- WHEN THE EPOXY ADHESIVE SEAL HAS CURED, INJECT A HIGH-MODULUS, LOW-VISCOSITY EPOXY SUCH AS SIKADUR 35, SIKADUR 55, OR APPROVED EQUAL.

GENERAL FINISHING:

- CONTRACTOR SHALL REMOVE TECTYL COATING WHERE PRESENT PRIOR TO APPLYING PROTECTIVE CONCRETE COATING. WORK SHALL BE INCIDENTAL TO THE SPECIFIED ITEM IN S.P. 518.
- ALL EXPOSED CONCRETE SURFACES SHALL BE COATED WITH A PROTECTIVE COATING SUITABLE FOR CONCRETE SURFACES AFTER PATCHING IS COMPLETE AND PATCH MATERIALS HAVE CURED. PIGMENTED PROTECTIVE COATING SHALL BE APPLIED TO ALL PIER FACES EXCEPT THOSE ADJACENT TO ABUTMENTS, ALL ABUTMENT FACES, ALL WINGWALL FACES, AND ALL END POST OUTSIDE FACES. CLEAR PROTECTIVE COATING SHALL BE APPLIED TO ALL PARAPET AND CURB FACES, ALL FASCIAS AND FASCIA OVERHANGS, PIER FACES ADJACENT TO ABUTMENTS, AND ALL END POST INSIDE AND TOP FACES.
- JACKING OF GIRDERS AT THE ABUTMENTS AT EAGLES NEST ROAD WILL BE REQUIRED. REFER TO SP 523 FOR SPECIFIC REQUIREMENTS. THIS WORK SHALL BE COMPLETED AT EACH ABUTMENT PRIOR TO THE INSTALLATION OF THE ASSOCIATED APJ EXPANSION DEVICE.

LIST OF ABBREVIATIONS

ABUT.	ABUTMENT
ADDL.	ADDITIONAL
ASCG.	AGGREGATE SUBBASE COURSE GRAVEL
ALT.	ALTERNATE
APPROX.	APPROXIMATE
BOT.	BOTTOM
BRG.	BEARING
CL.	CLEAR
Ⓞ	CENTERLINE
CONC.	CONCRETE
CONSTR.	CONSTRUCTION
DEMO.	DEMOLITION
DIA.	DIAMETER
DIAPH.	DIAPHRAGM
EA.	EACH
EB	EASTBOUND
E.F.	EACH FACE
EL.	ELEVATION
EQ.	EQUAL
EXIST.	EXISTING
EXP.	EXPANSION
F.F.	FAR FACE
JT.	JOINT
MAX.	MAXIMUM
MIN.	MINIMUM
MTA	MAINE TURNPIKE AUTHORITY
NB	NORTHBOUND
N.F.	NEAR FACE
N.T.S.	NOT TO SCALE
O.H.W.	ORDINARY HIGH WATER
PED.	PEDESTAL
PGL	PROFILE GRADE LINE
Ⓟ	PLATE
PROP.	PROPOSED
RDWY.	ROADWAY
SHLD.	SHOULDER
SB	SOUTHBOUND
SP.	SPACES
STA.	STATION
T.&B.	TOP & BOTTOM
TPKE.	TURNPIKE
TYP.	TYPICAL
U.O.N.	UNLESS OTHERWISE NOTED
VERT.	VERTICAL
WB	WESTBOUND
W.P.	WORKING POINT

Scale:			
NOT TO SCALE			
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Designed by:					
TYLIN INTERNATIONAL					
CONSULTANT PROJECT MANAGER: Norman L. Baker					
	By	Date		By	Date
Designed	DSM	1/2016	Checked	NLB	1/2016
Drawn	PJB	1/2016	In Charge of	NLB	1/2016

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**THE GOLD STAR
MEMORIAL HIGHWAY**

MTA PROJECT MANAGER: R. NORWOOD

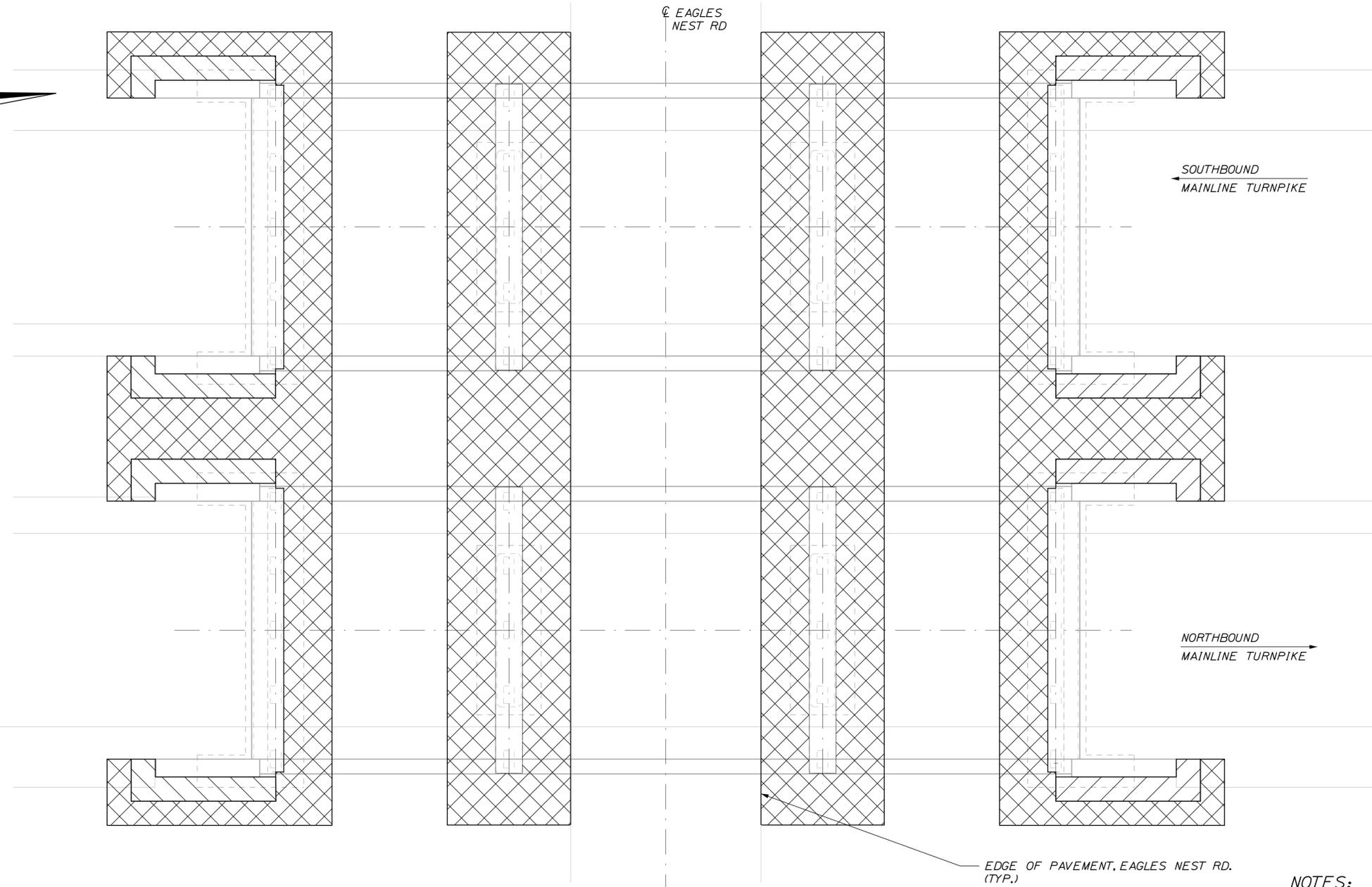
BRIDGE REPAIRS
2 LOCATIONS (MILE 60.8 & MILE 61.6)

GENERAL NOTES

SHEET NUMBER: GN-01
CONTRACT: 2016.01
36 OF 77

Date: 1/26/2016

Filename: ... \037_60.8_Limits_Of_Disturbance.dgn



LIMIT OF DISTURBANCE PLAN - EAGLES NEST ROAD
NOT TO SCALE

NOTES:

1. EROSION AND SEDIMENT CONTROL DEVICES REQUIRED FOR THE CONTRACTOR'S ACCESS LOCATIONS AND STORAGE AREAS WILL NOT BE MEASURED FOR PAYMENT.
2. THE CONTRACTOR WILL BE ALLOWED TO STORE MATERIAL AND EQUIPMENT ON GRAVEL AND PAVEMENT WITHIN THE CLOSED PORTIONS OF THE MAINLINE TURNPIKE AND EAGLES NEST ROAD. THESE AREAS ARE EITHER INCLUDED IN THE LIMITS OF DISTURBANCE OR HAVE BEEN PREVIOUSLY SURFACED AND DO NOT ADD TO THE LIMITS OF DISTURBANCE.
3. CONTRACTOR SHALL TAKE PRECAUTIONS TO AVOID IMPACTING OR DISTURBING ROADWAY DITCHES.

AREAS:

LOCATION	CONTRACT LIMIT OF DISTURBANCE	CONTRACTOR'S LIMIT OF DISTURBANCE AREA FOR ACCESS AND STORAGE
EAGLES NEST ROAD OVERPASSES (MILE 60.8)	0.01 ACRES	0.10 ACRES

LEGEND:

- CONTRACT LIMIT OF DISTURBANCE
- CONTRACTOR'S LIMIT OF DISTURBANCE AREA FOR ACCESS AND STORAGE

Scale: NOT TO SCALE			
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TY-LIN INTERNATIONAL

CONSULTANT PROJECT MANAGER: Norman L. Baker

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THE GOLD STAR MEMORIAL HIGHWAY

MTA PROJECT MANAGER: R. NORWOOD

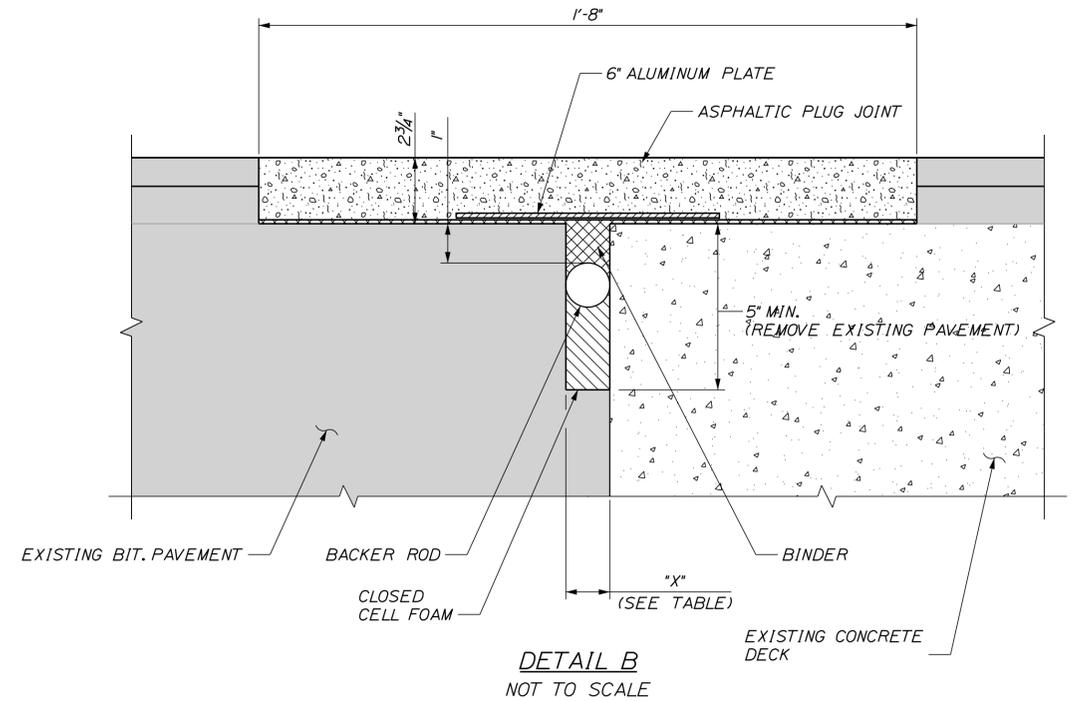
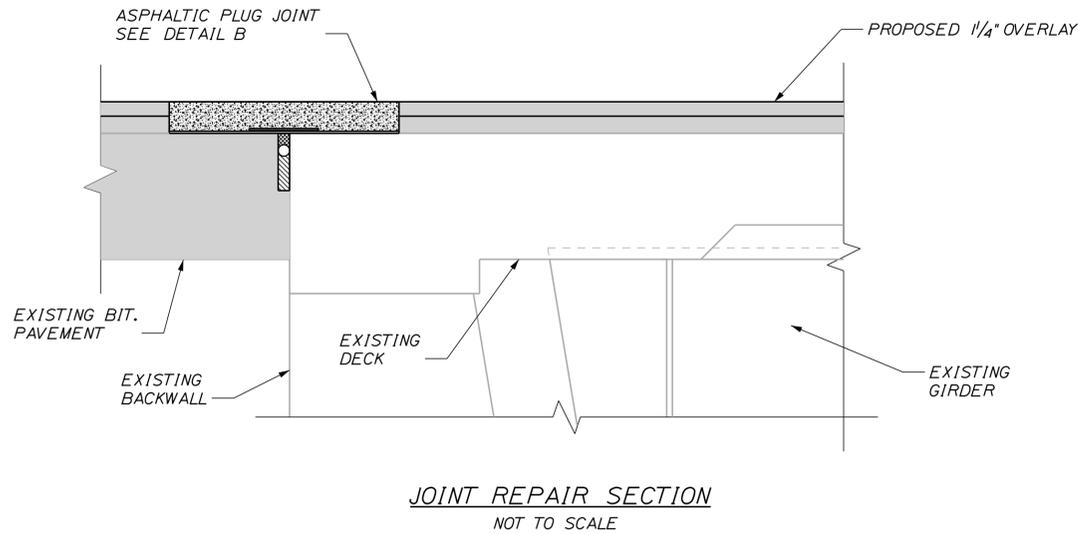
BRIDGE REPAIRS
2 LOCATIONS (MILE 60.8 & MILE 61.6)
EAGLES NEST OVERPASS (MILE 60.8)
LIMIT OF DISTURBANCE

CONTRACT: 2016.01

SHEET NUMBER: EN-01

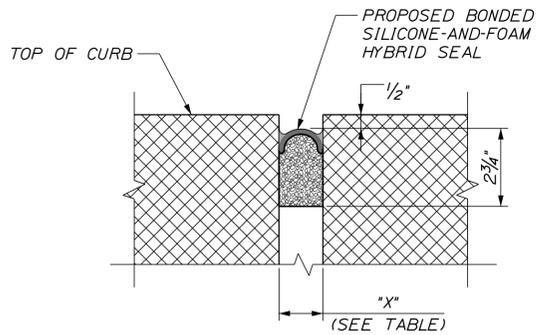
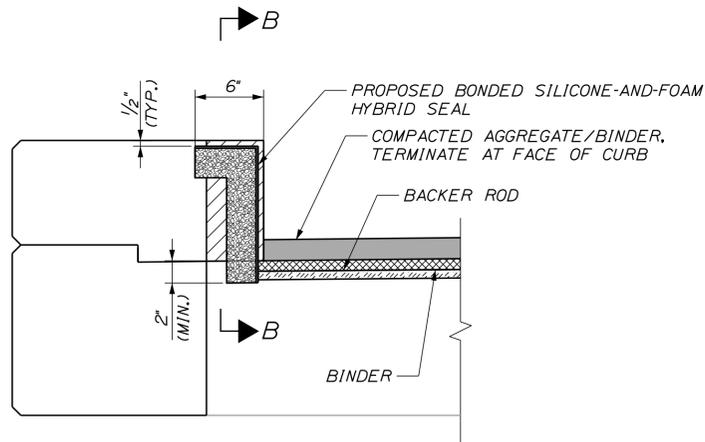
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Date: 1/26/2016



ASPHALTIC PLUG JOINT NOTES:

1. DECK AND BACKWALL REINFORCING NOT SHOWN FOR CLARITY.



JOINT INSTALLATION TABLE - NB AND SB BRIDGES								
NORTH ABUTMENT								
TEMP (°F)	20°	30°	40°	45°	50°	60°	70°	80°
"X" INCHES	1/16"	1/16"	1"	1"	1"	1 5/16"	1 5/16"	1 5/16"
SOUTH ABUTMENT								
TEMP (°F)	20°	30°	40°	45°	50°	60°	70°	80°
"X" INCHES	1/8"	1/16"	1"	1"	1"	1 5/16"	7/8"	1 3/16"

Filename: ...039_60.8_Joint_Repairs.dgn

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No.	Revision	By	Date

Designed by: TYLIN INTERNATIONAL					
CONSULTANT PROJECT MANAGER: Norman L. Baker					
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Designed	DSM	1/2016	Checked	NLB	1/2016
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**THE GOLD STAR
MEMORIAL HIGHWAY**

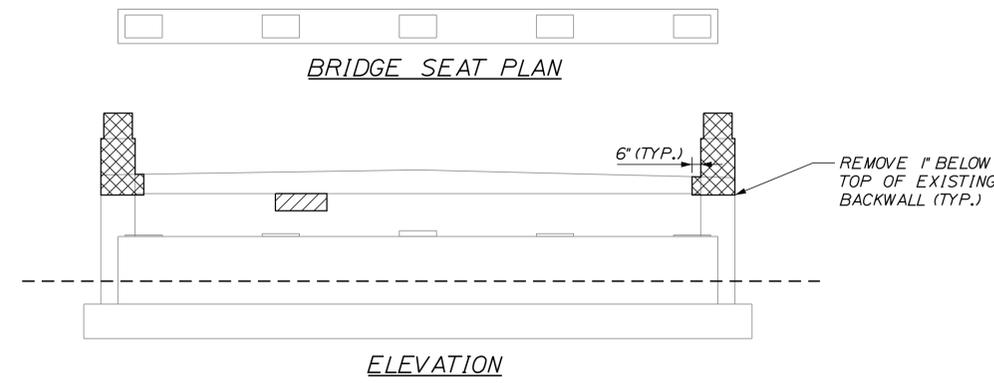
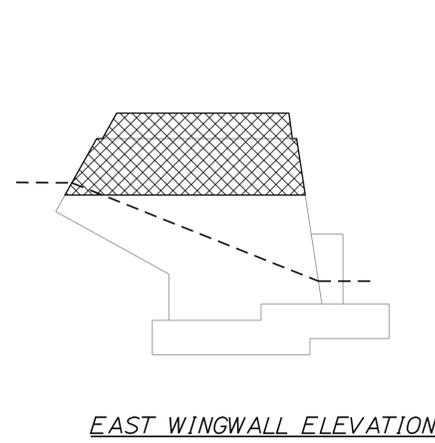
MTA PROJECT MANAGER: R. NORWOOD

BRIDGE REPAIRS
 2 LOCATIONS (MILE 60.8 & MILE 61.6)
 EAGLES NEST BRIDGED (MILE 60.8)
 JOINT REPAIR DETAILS

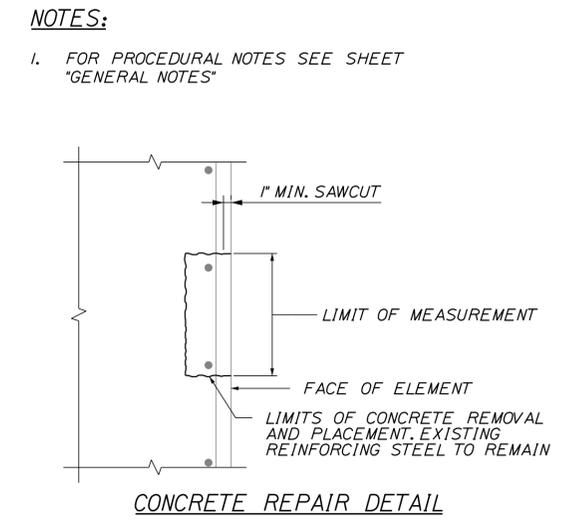
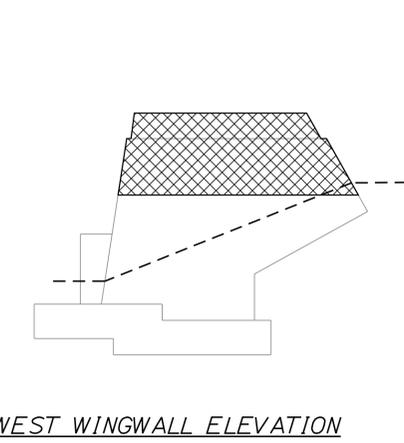
SHEET NUMBER: EN-03
 CONTRACT: 2016.01
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Date: 1/26/2016

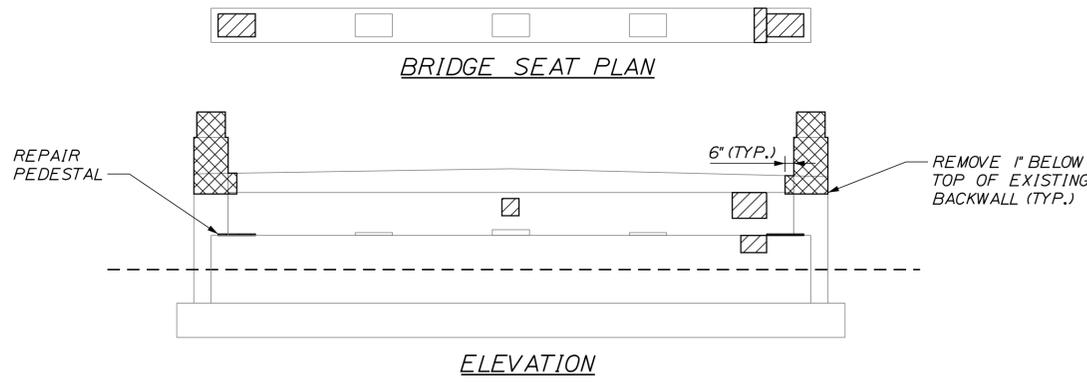
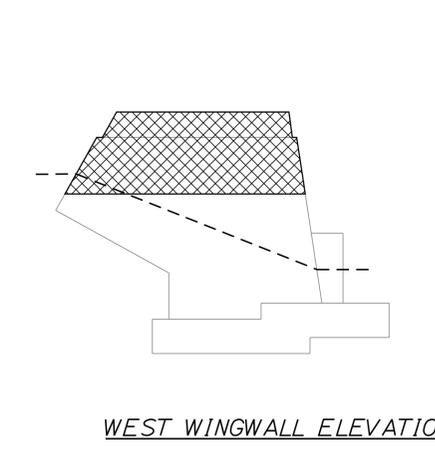
EAGLES NEST ROAD MILE (60.8 NB) BRIDGE ABUTMENTS ESTIMATED QUANTITY OF REPAIRS			
	NORTH ABUT.	SOUTH ABUT.	TOTAL
UPWARD FACING CONCRETE SURFACES	15 S.F.	0 S.F.	15 S.F.
VERTICAL SURFACES < 8 INCHES	10 S.F.	5 S.F.	15 S.F.
OVERHEAD SURFACES < 8 INCHES	0 S.F.	0 S.F.	0 S.F.
EPOXY INJECTED CRACK REPAIRS	5 L.F.	0 L.F.	5 L.F.



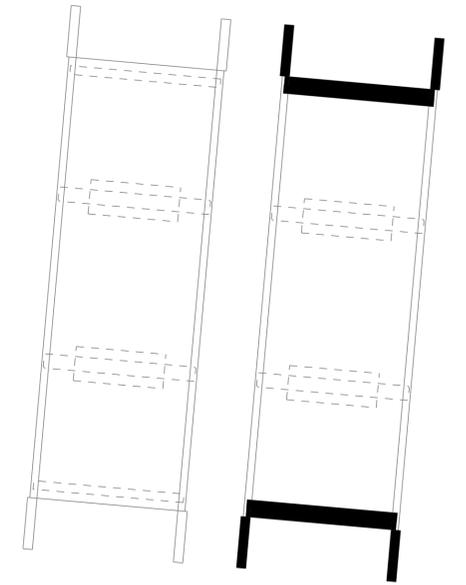
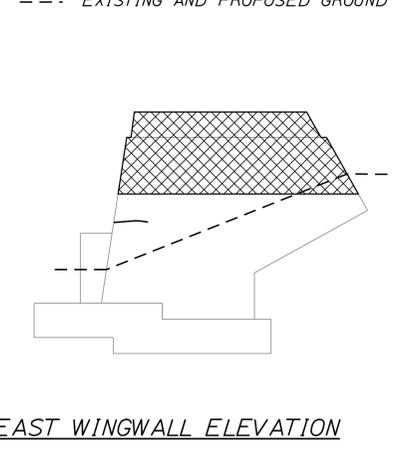
NORTHBOUND BRIDGE - SOUTH ABUTMENT



- LEGEND:**
- ▣ LIMIT OF CONCRETE REMOVAL
 - ▨ LIMIT OF CONCRETE SURFACE REPAIRS
 - LIMIT OF EPOXY INJECTED CRACK REPAIR
 - EXISTING CONCRETE
 - - - EXISTING AND PROPOSED GROUND



NORTHBOUND BRIDGE - NORTH ABUTMENT

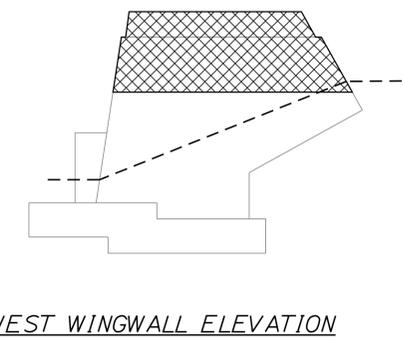
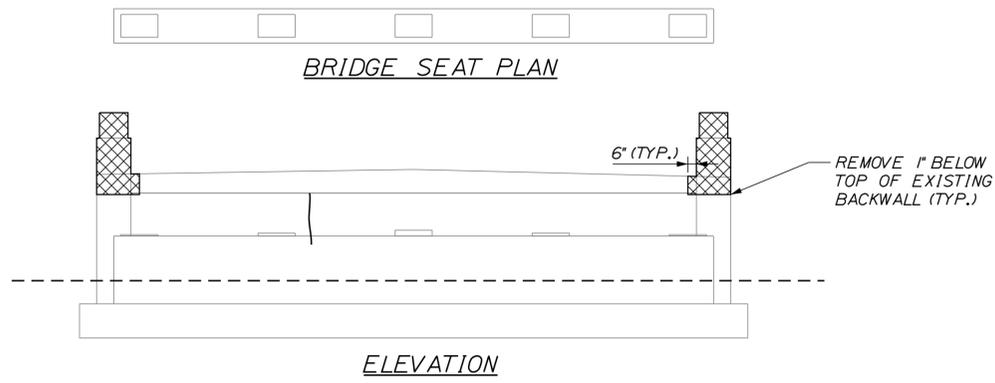
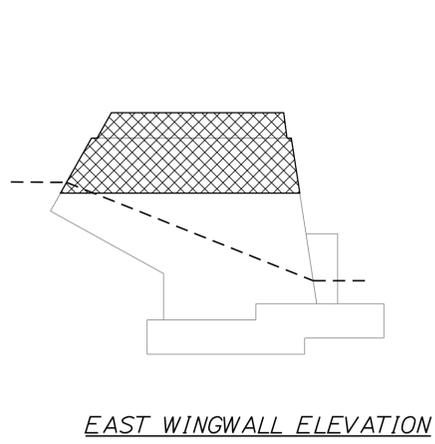


Filename: ... \MSTA\040_60.8_Sub_Mod_1.dgn

Scale: NOT TO SCALE		Designed by: TY-LIN INTERNATIONAL		T.Y. Lin International 12 Northbrook Drive Building A, Suite One Falmouth, Maine 04105 TEL: (207) 781-4721 FAX: (207) 781-4753		 THE GOLD STAR MEMORIAL HIGHWAY		BRIDGE REPAIRS 2 LOCATIONS (MILE 60.8 & MILE 61.6) EAGLES NEST OVERPASS (MILE 60.8) SUBSTRUCTURE MODIFICATIONS - 1 OF 4																													
<table border="1" style="width: 100%; border-collapse: collapse;"> <thead> <tr> <th>No.</th> <th>Revision</th> <th>By</th> <th>Date</th> </tr> </thead> <tbody> <tr><td> </td><td> </td><td> </td><td> </td></tr> <tr><td> </td><td> </td><td> </td><td> </td></tr> <tr><td> </td><td> </td><td> </td><td> </td></tr> </tbody> </table>		No.	Revision	By	Date													CONSULTANT PROJECT MANAGER: Norman L. Baker		<table border="1" style="width: 100%; border-collapse: collapse;"> <thead> <tr> <th>By</th> <th>Date</th> <th>By</th> <th>Date</th> </tr> </thead> <tbody> <tr> <td>Designed DSM</td> <td>1/2016</td> <td>Checked NLB</td> <td>1/2016</td> </tr> <tr> <td>Drawn PJB</td> <td>1/2016</td> <td>In Charge of NLB</td> <td>1/2016</td> </tr> </tbody> </table>		By	Date	By	Date	Designed DSM	1/2016	Checked NLB	1/2016	Drawn PJB	1/2016	In Charge of NLB	1/2016	MTA PROJECT MANAGER: R. NORWOOD		CONTRACT: 2016.01 SHEET NUMBER: EN-04 40 OF 77	
No.	Revision	By	Date																																		
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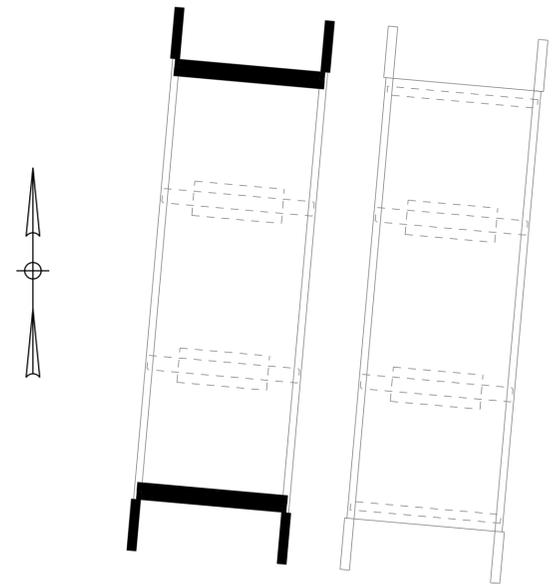
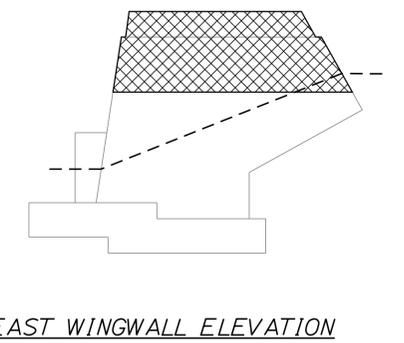
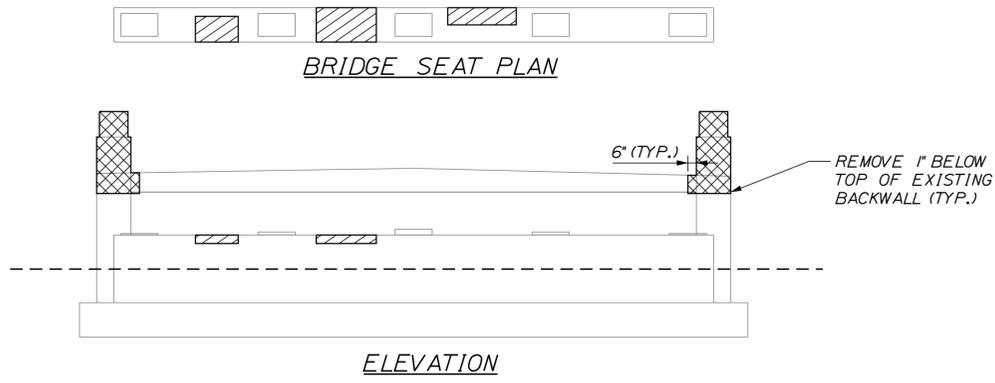
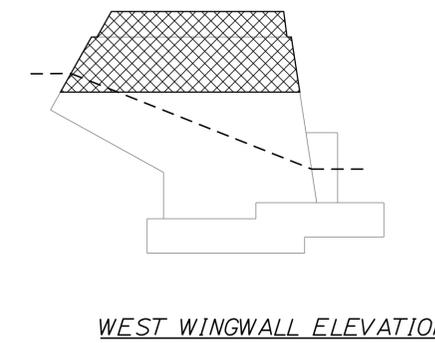
EAGLES NEST ROAD (MILE 60.8) SB BRIDGE ABUTMENTS ESTIMATED QUANTITY OF REPAIRS			
	NORTH ABUT.	SOUTH ABUT.	TOTAL
UPWARD FACING CONCRETE SURFACES	25 S.F.	0 S.F.	25 S.F.
VERTICAL SURFACES < 8 INCHES	5 S.F.	0 S.F.	5 S.F.
OVERHEAD SURFACES < 8 INCHES	0 S.F.	0 S.F.	0 S.F.
EPOXY INJECTED CRACK REPAIRS	0 L.F.	5 L.F.	5 L.F.



- NOTES:**
- FOR CONCRETE REPAIR DETAIL SEE SHEET EN-04.
 - FOR PROCEDURAL NOTES SEE SHEET "GENERAL NOTES"

- LEGEND:**
- LIMIT OF CONCRETE REMOVAL
 - LIMIT OF CONCRETE SURFACE REPAIRS
 - LIMIT OF EPOXY INJECTED CRACK REPAIR
 - EXISTING CONCRETE
 - EXISTING AND PROPOSED GROUND

SOUTHBOUND BRIDGE - SOUTH ABUTMENT



SOUTHBOUND BRIDGE - NORTH ABUTMENT

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TY-LIN INTERNATIONAL

CONSULTANT PROJECT MANAGER: Norman L. Baker

	By	Date	Checked	By	Date
Designed	DSM	1/2016		NLB	1/2016
Drawn	PJB	1/2016	In Charge of	NLB	1/2016

T.Y. Lin International
 12 Northbrook Drive
 Building A, Suite One
 Falmouth, Maine 04105
 TEL: (207) 781-4721
 FAX: (207) 781-4753

THE GOLD STAR MEMORIAL HIGHWAY

MTA PROJECT MANAGER: R. NORWOOD

BRIDGE REPAIRS
 2 LOCATIONS (MILE 60.8 & MILE 61.6)
 EAGLES NEST OVERPASS (MILE 60.8)
 SUBSTRUCTURE MODIFICATION - 2 OF 4

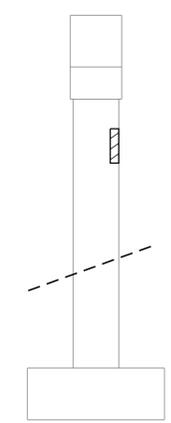
CONTRACT: 2016.01

SHEET NUMBER: EN-05
 41 OF 77

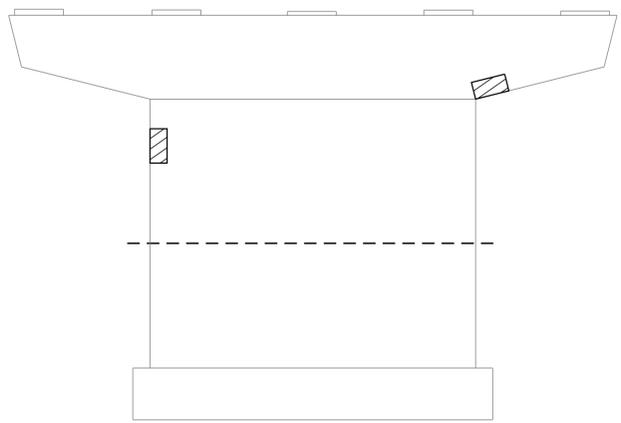
Date: 1/26/2016

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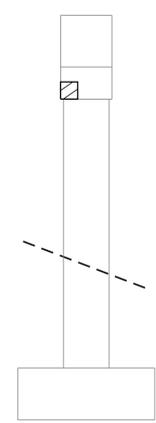
EAGLES NEST ROAD (MILE 60.8) NORTHBOUND PIERS ESTIMATED QUANTITY OF REPAIRS			
	PIER 1	PIER 2	TOTAL
UPWARD FACING CONCRETE SURFACES	0 S.F.	0 S.F.	0 S.F.
VERTICAL SURFACES < 8 INCHES	10 S.F.	0 S.F.	10 S.F.
OVERHEAD SURFACES < 8 INCHES	5 S.F.	0 S.F.	5 S.F.
EPOXY INJECTED CRACK REPAIRS	0 L.F.	0 L.F.	0 L.F.



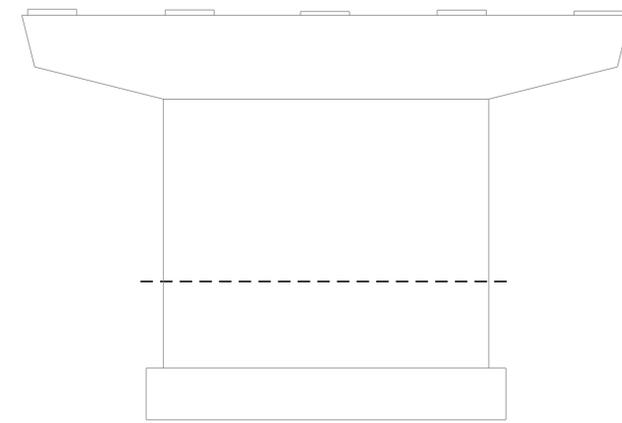
ELEVATION D



ELEVATION C



ELEVATION B



ELEVATION A

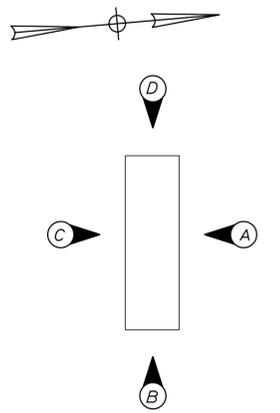
NORTHBOUND PIER 1 (SOUTH)

NOTES:

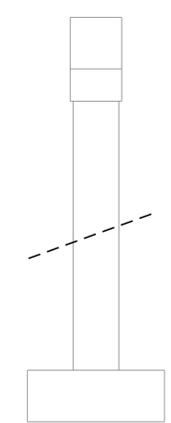
- FOR CONCRETE REPAIR DETAIL SEE SHEET EN-04.
- FOR PROCEDURAL NOTES SEE SHEET "GENERAL NOTES"

LEGEND:

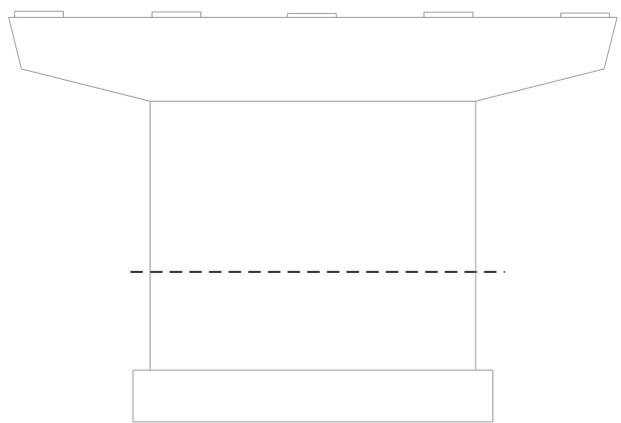
- LIMIT OF CONCRETE SURFACE REPAIRS
- LIMIT OF EPOXY INJECTED CRACK REPAIR
- EXISTING CONCRETE
- EXISTING AND PROPOSED GROUND



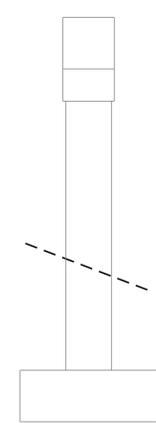
PIER ELEVATIONS



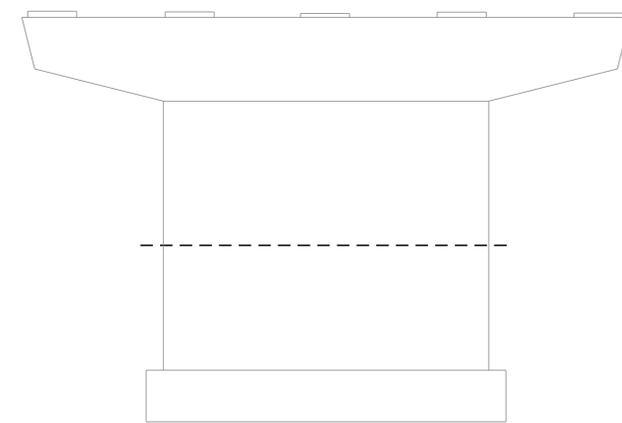
ELEVATION D



ELEVATION C

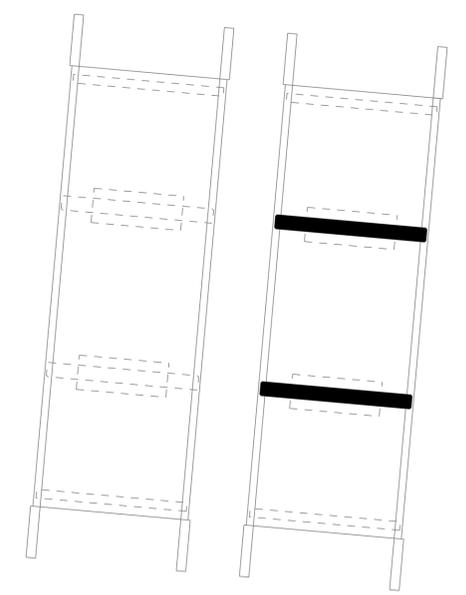


ELEVATION B



ELEVATION A

NORTHBOUND PIER 2 (NORTH)



LOCATION PLAN

Scale: NOT TO SCALE

No.	Revision	By	Date

Designed by:

TYLIN INTERNATIONAL

CONSULTANT PROJECT MANAGER: Norman L. Baker

	By	Date	By	Date	
Designed	DSM	1/2016	Checked	NLB	1/2016
Drawn	PJB	1/2016	In Charge of	NLB	1/2016

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THE GOLD STAR MEMORIAL HIGHWAY

MTA PROJECT MANAGER: R. NORWOOD

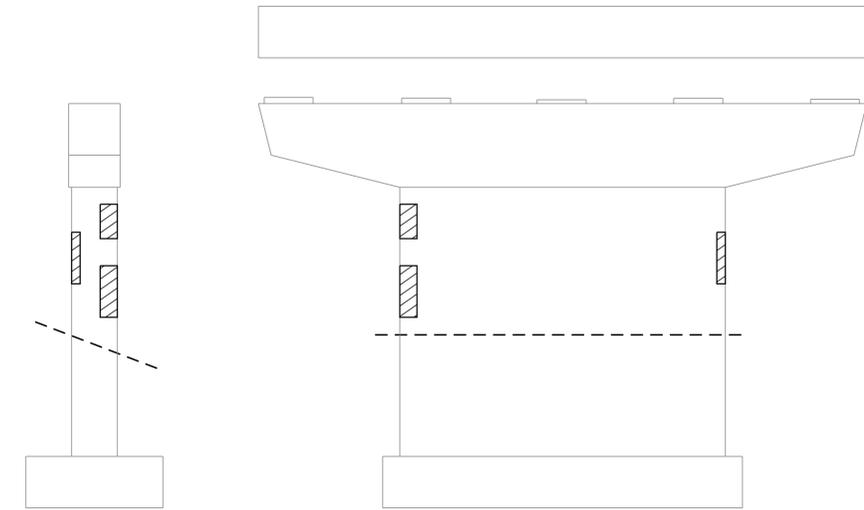
BRIDGE REPAIRS
 2 LOCATIONS (MILE 60.8 & MILE 61.6)
 EAGLES NEST OVERPASS (MILE 60.8)
 SUBSTRUCTURE MODIFICATIONS - 3 OF 4

CONTRACT: 2016.01

SHEET NUMBER: EN-06
 42 OF 77

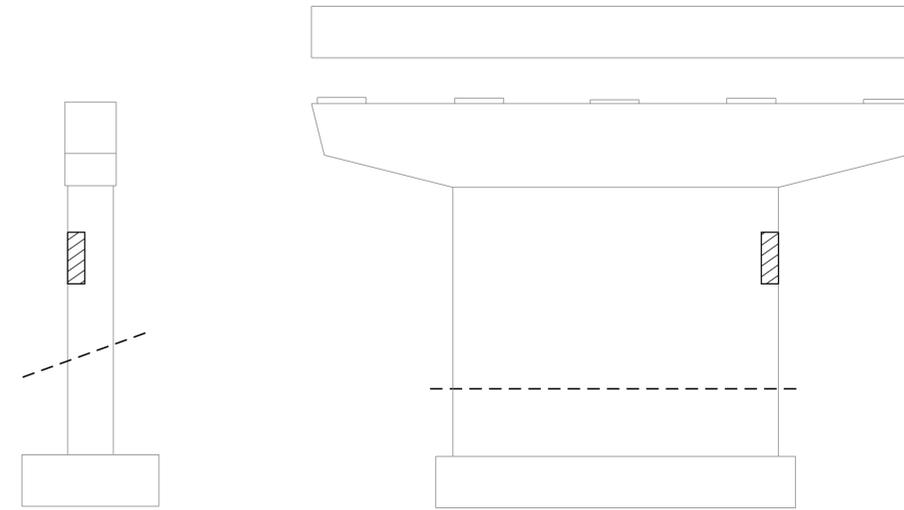
Date: 1/26/2016

Filename: ... \MSTA\043_60.8_Sub_Mod_4.dgn



ELEVATION D

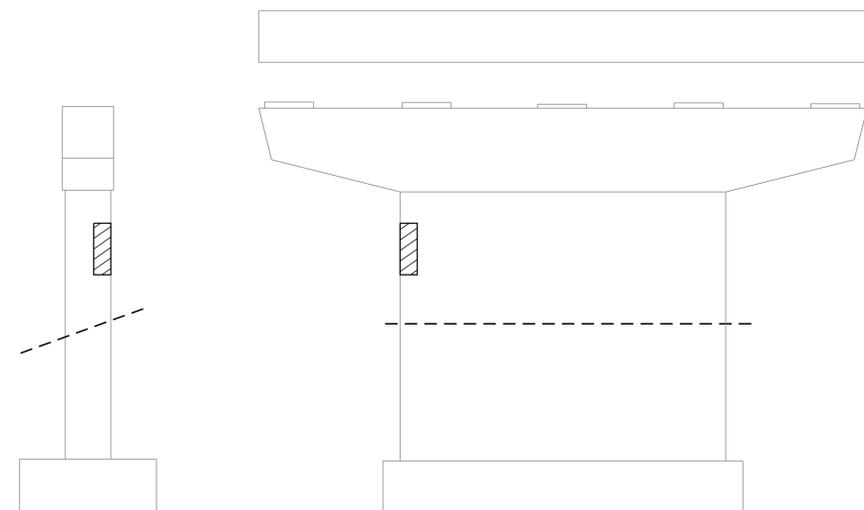
ELEVATION C



ELEVATION B

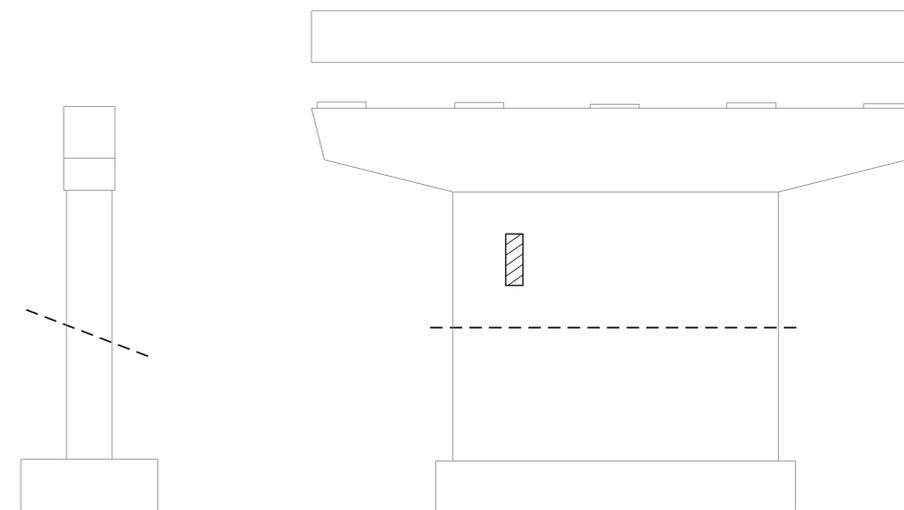
ELEVATION A

SOUTHBOUND PIER 1 (SOUTH)



ELEVATION D

ELEVATION A



ELEVATION B

ELEVATION A

SOUTHBOUND PIER 2 (NORTH)

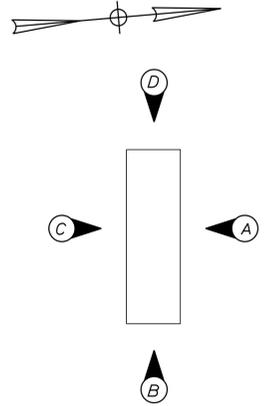
EAGLES NEST ROAD (MILE 60.8) NORTHBOUND PIERS ESTIMATED QUANTITY OF REPAIRS			
	PIER 1	PIER 2	TOTAL
UPWARD FACING CONCRETE SURFACES	0 S.F.	0 S.F.	0 S.F.
VERTICAL SURFACES < 8 INCHES	25 S.F.	15 SF.	40 S.F.
OVERHEAD SURFACES < 8 INCHES	0 S.F.	0 S.F.	0 S.F.
EPOXY INJECTED CRACK REPAIRS	0 L.F.	0 L.F.	0 L.F.

NOTES:

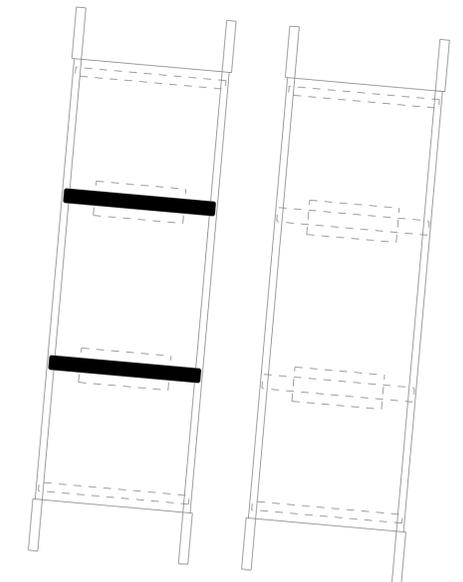
- FOR CONCRETE REPAIR DETAIL SEE SHEET EN-04.
- FOR PROCEDURAL NOTES SEE SHEET "GENERAL NOTES"

LEGEND:

- LIMIT OF CONCRETE SURFACE REPAIRS
- LIMIT OF EPOXY INJECTED CRACK REPAIR
- EXISTING CONCRETE
- EXISTING AND PROPOSED GROUND



PIER ELEVATIONS



LOCATION PLAN

Scale: NOT TO SCALE

No.	Revision	By	Date

Designed by:

TYLIN INTERNATIONAL

CONSULTANT PROJECT MANAGER: Norman L. Baker

	By	Date	By	Date	
Designed	DSM	1/2016	Checked	NLB	1/2016
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THE GOLD STAR MEMORIAL HIGHWAY

MTA PROJECT MANAGER: R. NORWOOD

BRIDGE REPAIRS
 2 LOCATIONS (MILE 60.8 & MILE 61.6)
 EAGLES NEST OVERPASS (MILE 60.8)
 SUBSTRUCTURE MODIFICATIONS - 4 OF 4

CONTRACT: 2016.01

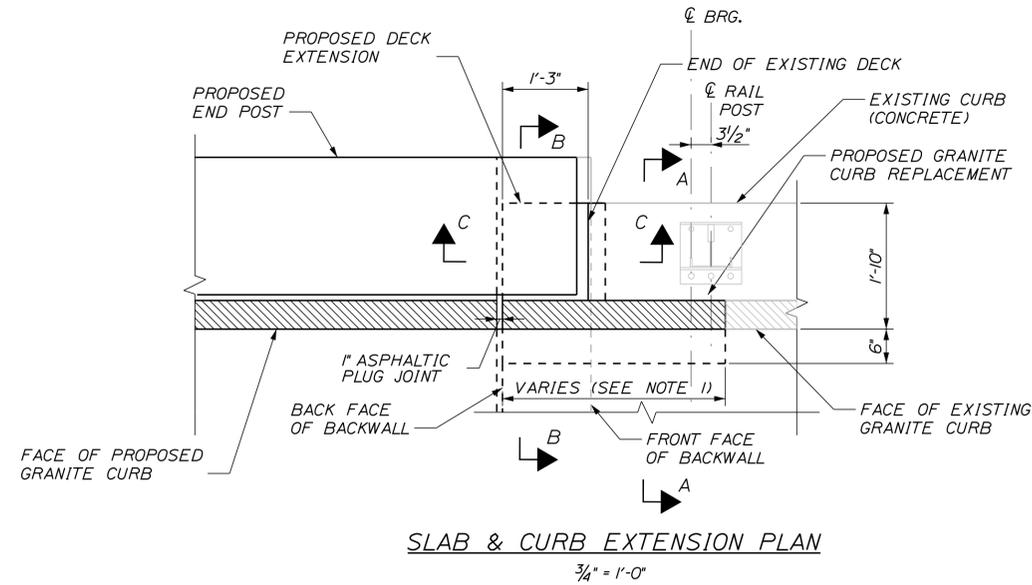
SHEET NUMBER: EN-07
 43 OF 77

Date: 1/26/2016

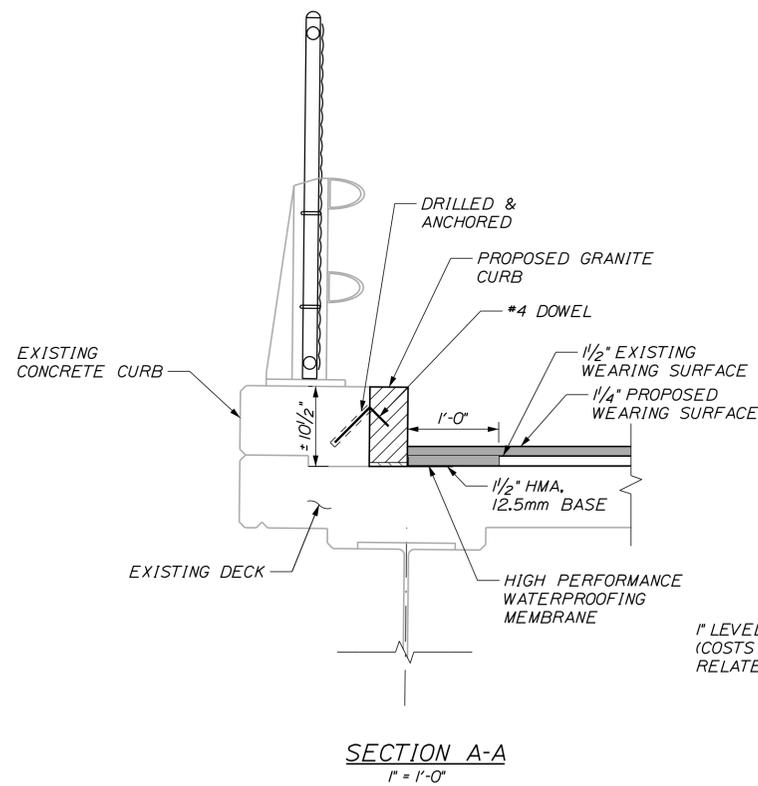
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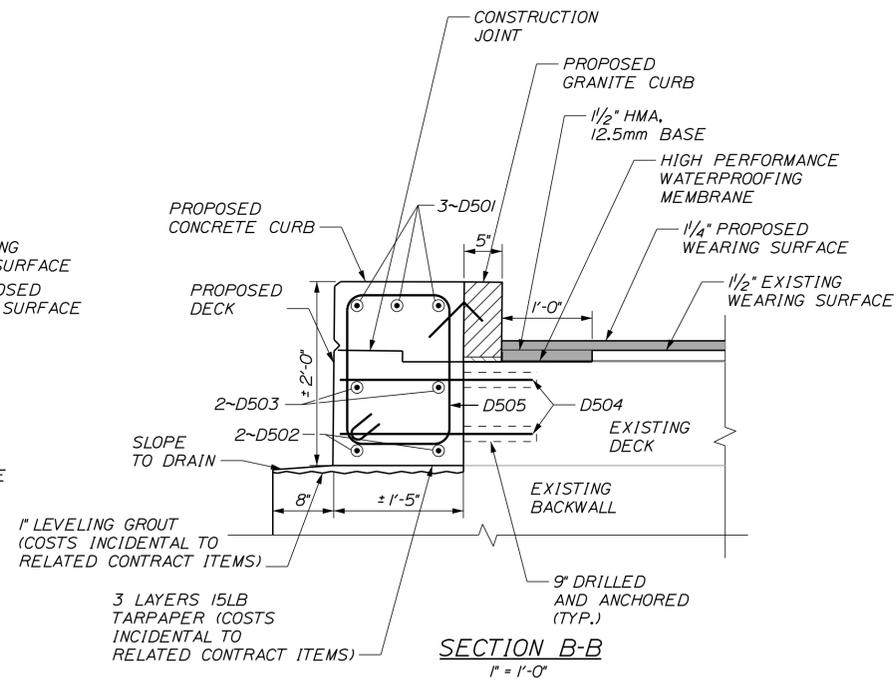
1. EXTENT OF GRANITE CURB REPLACEMENT AT EACH LOCATION TO BE DETERMINED BY THE RESIDENT.



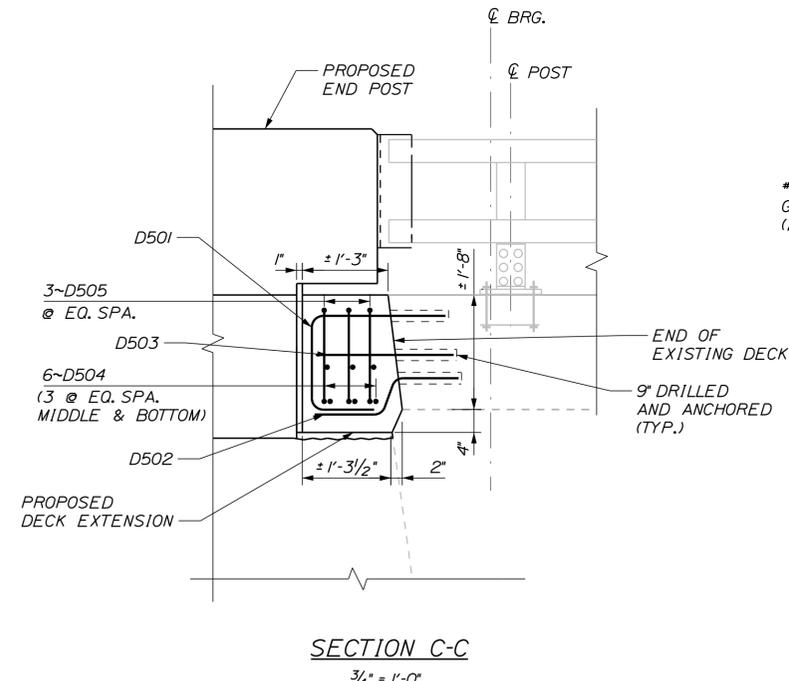
SLAB & CURB EXTENSION PLAN
3/4" = 1'-0"



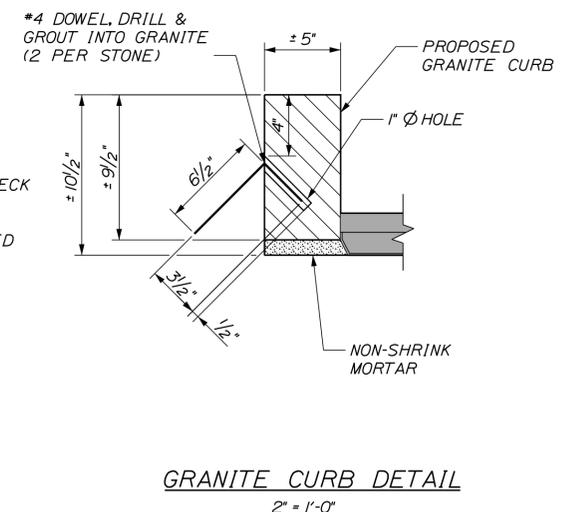
SECTION A-A
1" = 1'-0"



SECTION B-B
1" = 1'-0"



SECTION C-C
3/4" = 1'-0"



GRANITE CURB DETAIL
2" = 1'-0"

DECK AND CURB EXTENSION DETAILS

Scale: AS NOTED			
No.	Revision	By	Date

Designed by: TYLIN INTERNATIONAL					
CONSULTANT PROJECT MANAGER: Norman L. Baker					
	By	Date		By	Date
Designed	DSM	1/2016	Checked	NLB	1/2016
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THE GOLD STAR MEMORIAL HIGHWAY

MTA PROJECT MANAGER: R. NORWOOD

BRIDGE REPAIRS
2 LOCATIONS (MILE 60.8 & MILE 61.6)
MILE 60.8 OVERPASS
DECK DETAILS

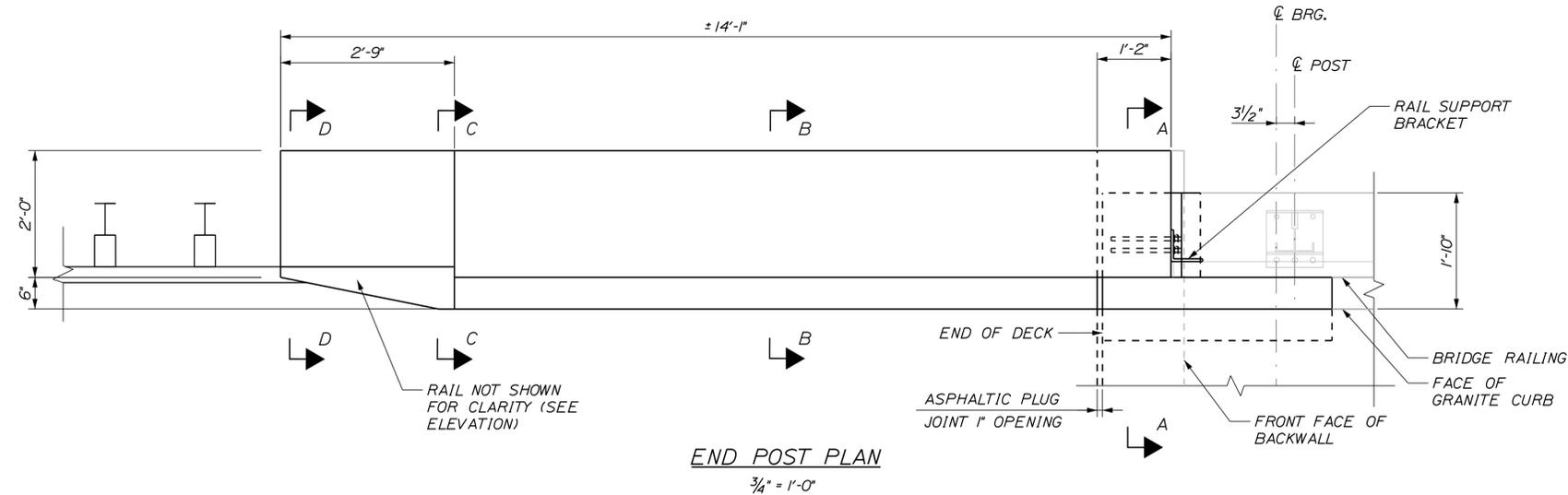
CONTRACT: 2016.01

SHEET NUMBER: EN-08

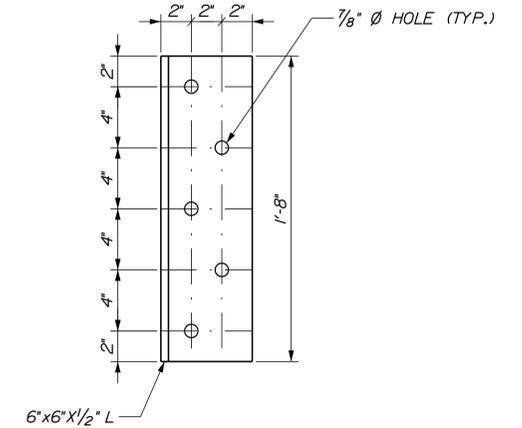
44 OF 77

Date: 1/26/2016

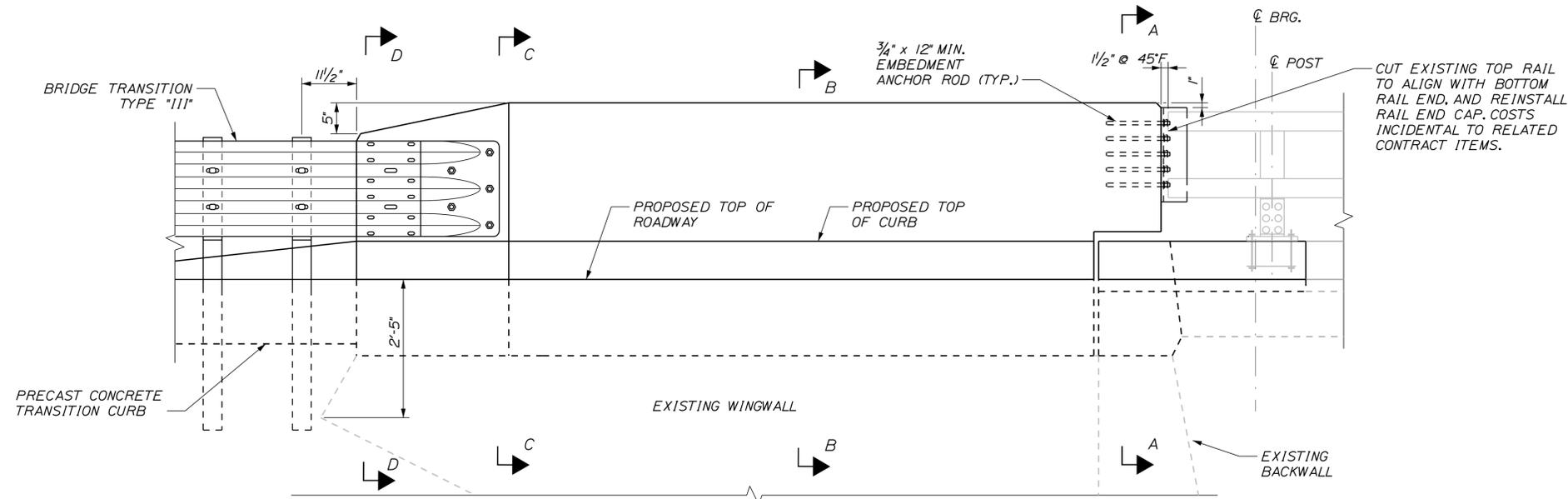
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END POST PLAN
3/4" = 1'-0"



RAIL SUPPORT BRACKET
2" = 1'-0"
ASTM A36 GALVANIZED



INSIDE END POST ELEVATION
3/4" = 1'-0"

NOTES:

1. FOR SECTIONS A-A, B-B, C-C, AND D-D, SEE SHEET "END POST DETAILS 2 OF 3"
2. COSTS FOR FABRICATION, DELIVERY, AND INSTALLATION OF RAIL SUPPORT BRACKETS AND ASSOCIATED HARDWARE SHALL BE INCIDENTAL TO RELATED CONTRACT ITEMS.
3. RAIL SUPPORT BRACKET SHALL MEET THE REQUIREMENTS OF ASTM 36, GALVANIZED. ANCHOR RODS AND HARDWARE SHALL MEET THE REQUIREMENTS OF ASTM F1554 GRADE 36, GALVANIZED.

Scale: AS NOTED			
No.	Revision	By	Date

Designed by:					
TYLIN INTERNATIONAL					
CONSULTANT PROJECT MANAGER: Norman L. Baker					
	By	Date		By	Date
Designed	DSM	1/2016	Checked	NLB	1/2016
Drawn	PJB	1/2016	In Charge of	NLB	1/2016

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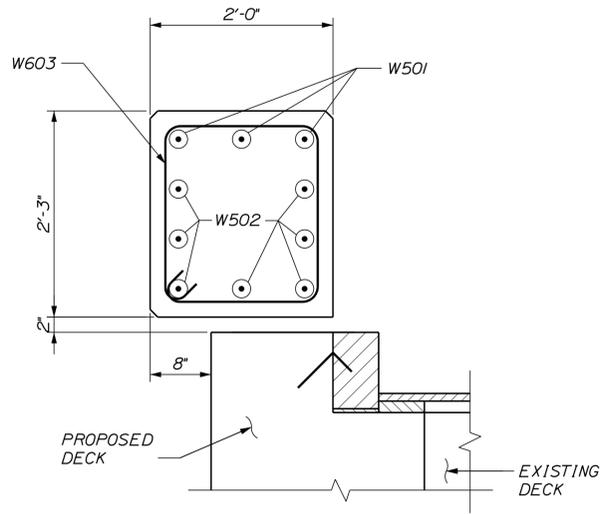
THE GOLD STAR MEMORIAL HIGHWAY

MTA PROJECT MANAGER: R. NORWOOD

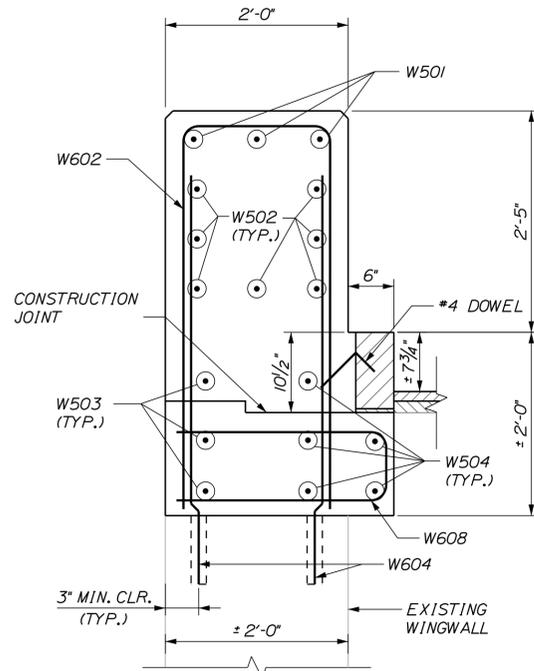
BRIDGE REPAIRS
2 LOCATIONS (MILE 60.8 & MILE 61.6)
EAGLES NEST OVERPASS (MILE 60.8)
END POST DETAILS - 1 OF 3

SHEET NUMBER: EN-09
CONTRACT: 2016.01
45 OF 77

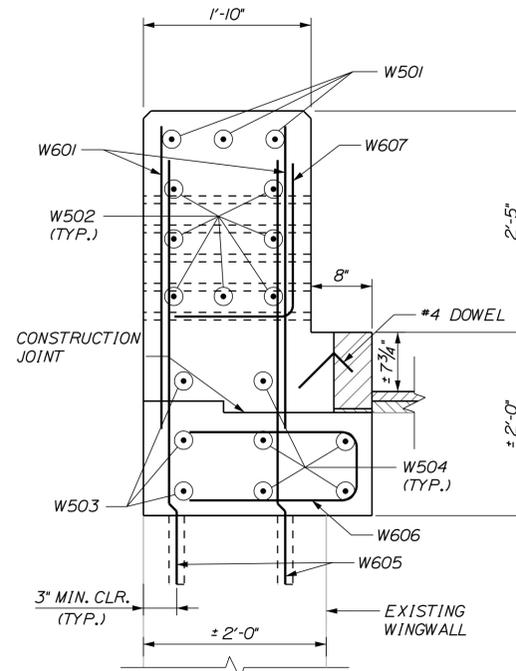
Date: 1/26/2016



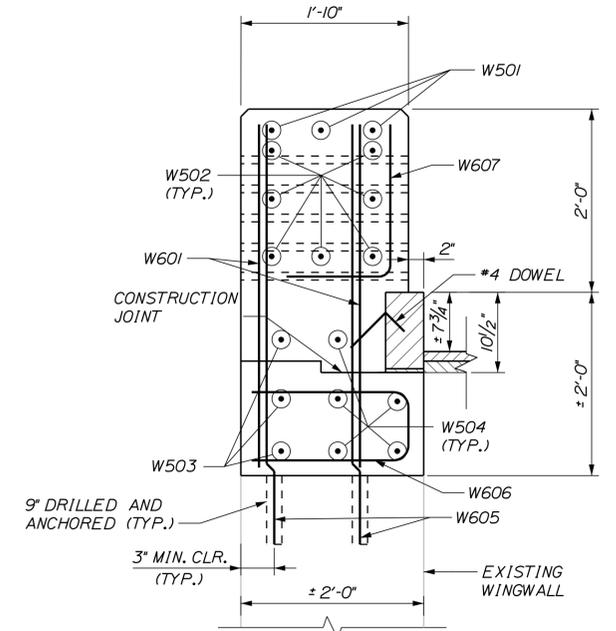
SECTION A-A
1" = 1'-0"



SECTION B-B
1" = 1'-0"



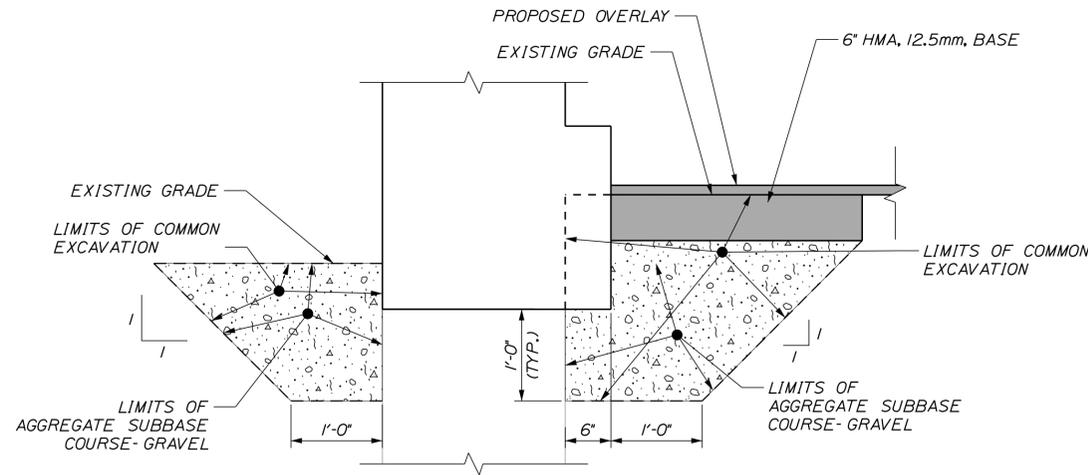
SECTION C-C
1" = 1'-0"



SECTION D-D
1" = 1'-0"

NOTES:

1. FOR LOCATIONS OF SECTIONS A-A, B-B, C-C AND D-D, SEE SHEET "ENDPOST DETAILS - 1 OF 3"
2. FOR GRANITE CURB DETAIL, SEE SHEET "DECK DETAILS."



TYPICAL EARTHWORK LIMITS
1" = 1'-0"

Filename: ... \046_60.8_EndPost_Details_2.dgn

Scale: AS NOTED			
No.	Revision	By	Date

Designed by:

TYLIN INTERNATIONAL

CONSULTANT PROJECT MANAGER: Norman L. Baker

	By	Date		By	Date
Designed	DSM	1/2016	Checked	NLB	1/2016
Drawn	PJB	1/2016	In Charge of	NLB	1/2016

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MAINE TURNPIKE

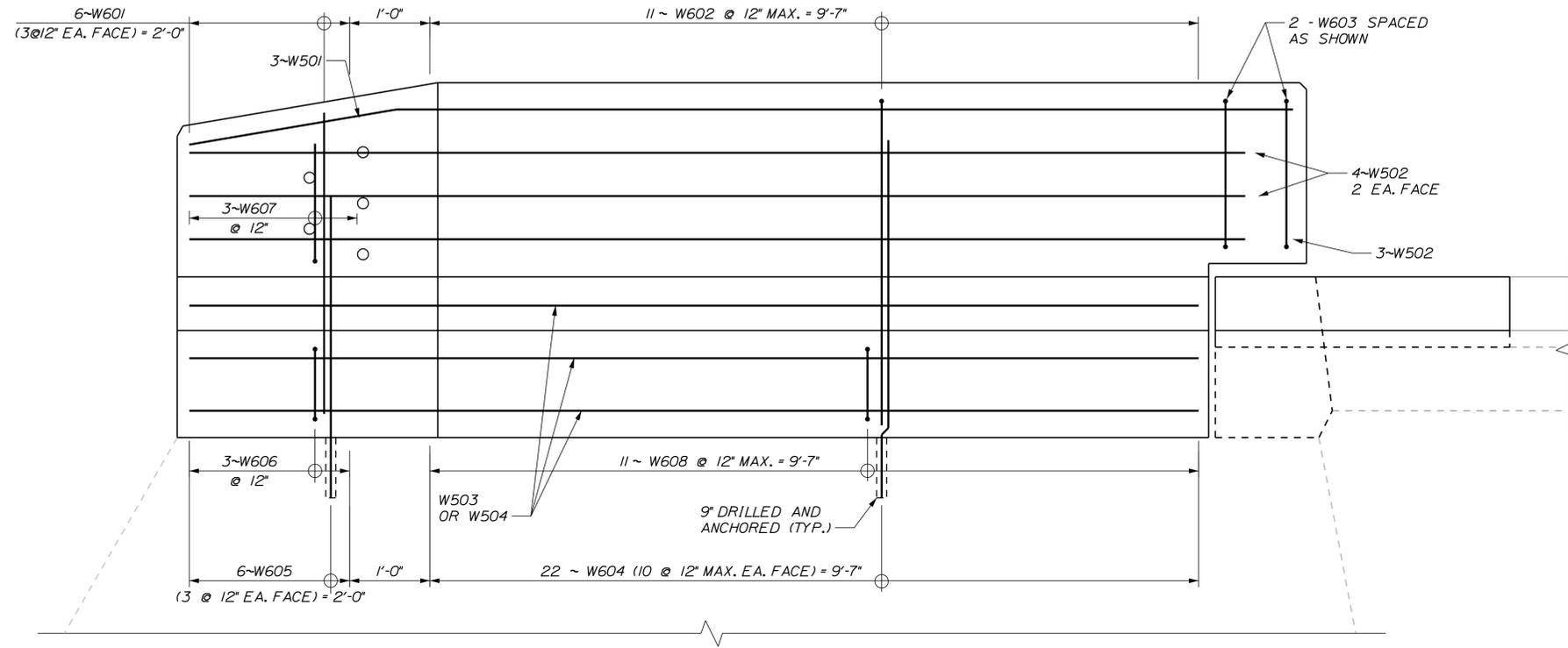
THE GOLD STAR MEMORIAL HIGHWAY

MTA PROJECT MANAGER: R. NORWOOD

BRIDGE REPAIRS
2 LOCATIONS (MILE 60.8 & MILE 61.6)
EAGLES NEST OVERPASS (MILE 60.8)
END POST DETAILS - 2 OF 3

SHEET NUMBER: EN-10
CONTRACT: 2016.01
46 OF 77

Date: 1/26/2016



END POST ELEVATION
1" = 1'-0"

Filename: ... \047_60.8_EndPost_Details_3.dgn

Scale:			
AS NOTED			
No.	Revision	By	Date

Designed by:					
TY-LIN INTERNATIONAL					
CONSULTANT PROJECT MANAGER: Norman L. Baker					
	By	Date		By	Date
Designed	DSM	1/2016	Checked	NLB	1/2016
Drawn	PJB	1/2016	In Charge of	NLB	1/2016

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THE GOLD STAR
MEMORIAL HIGHWAY

MTA PROJECT MANAGER: R. NORWOOD

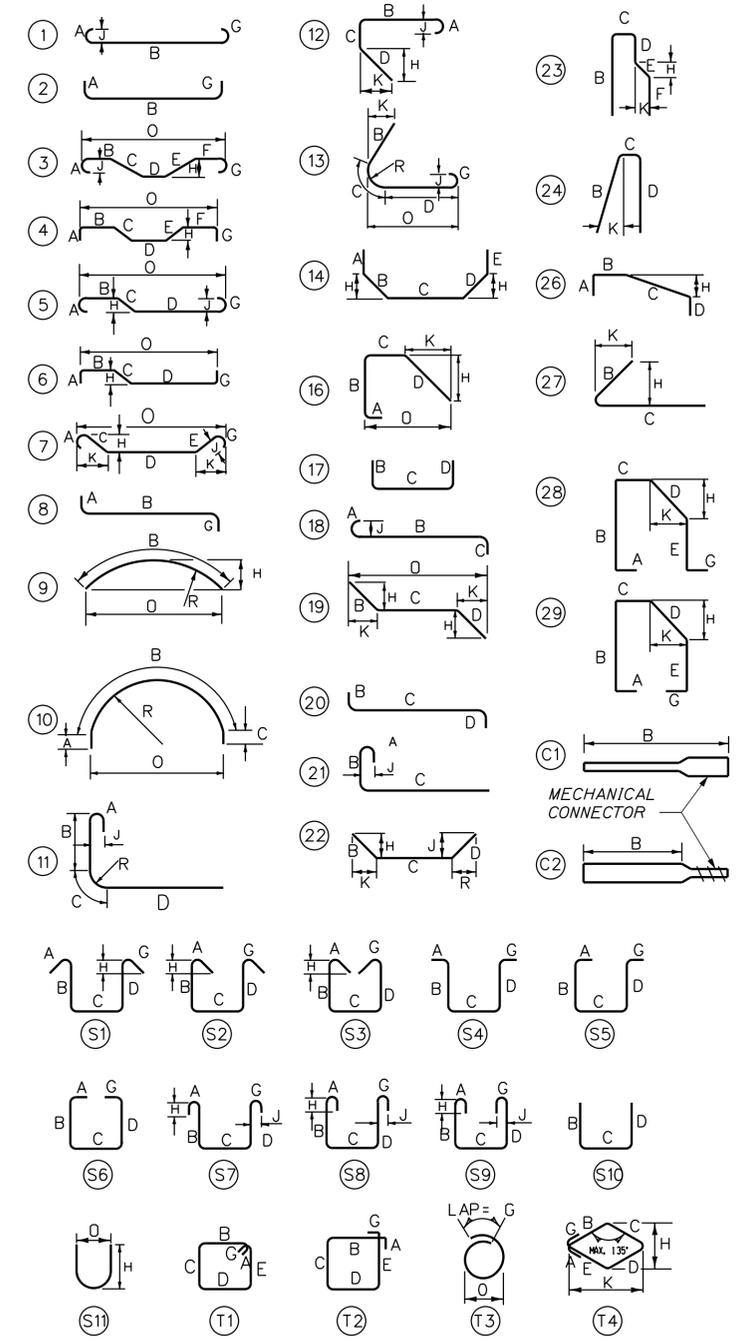
BRIDGE REPAIRS
 2 LOCATIONS (MILE 60.8 & MILE 61.6)
 EAGLES NEST OVERPASS (MILE 60.8)
 END POST DETAILS - 3 OF 3

CONTRACT: 2016.01
SHEET NUMBER: EN-11
47 OF 77

Date: 1/26/2016

STANDARD BAR BEND TYPES

DECK																
MARK	SIZE	NO.	LENGTH	TYPE	A	B	C	D	E	F	G	H	K	O	J	REMARKS
D501	5	24	4'-4"	17		1'-10"	1'-7"	11"								TOP LONGITUDINAL BARS
D502	5	16	2'-5"	8	0'-11"	0'-5"					1'-1"					BOTTOM LONGITUDINAL BARS
D503	5	16	2'-0"	STR												MIDDLE LONGITUDINAL BARS
D504	5	48	2'-0"	STR												SIDEWAYS BARS
D505	5	24	6'-3"	T1	0'-5"	1'-1"	1'-7"	1'-1"	1'-7"		0'-5"					PRIMARY SHEAR REINFORCEMENT
END POST																
MARK	SIZE	NO.	LENGTH	TYPE	A	B	C	D	E	F	G	H	K	O	J	REMARKS
W501	5	24	13'-9"	19		2'-7"	11'-2"	0'-0"				0'-5"	2'-7"	13'-9"		TOP LONGITUDINAL BARS IN END POST
W502	5	56	13'-9"	STR												NEXT TO TOP LONGITUDINAL BARS IN END POST
W503	5	24	12'-7"	STR												OUTER BOTOM LONGITUDINAL BARS IN END POST
W504	5	40	12'-8"	19		2'-8"	10'-0"	0'-0"				0'-8"	2'-7"	12'-7"		INNER BOTOM LONGITUDINAL BARS IN END POST
W601	6	48	3'-8"	STR												SLOPED SECTION REINFORCEMENT
W602	6	88	9'-10"	17		4'-1"	1'-8"	4'-1"								PRIMARY SHEAR REINFORCEMENT
W603	6	16	8'-6"	T1	0'-8"	1'-8"	1'-11"	1'-8"	1'-11"		0'-8"					CANTILEVER SHEAR REINFORCEMENT
W604	6	176	4'-6"	STR												STRAIGHT DOWELS FOR PRIMARY SHEAR REINFORCEMENT
W605	6	48	4'-11"	STR												STRAIGHT DOWELS FOR PRIMARY SHEAR REINFORCEMENT
W606	6	24	3'-9"	17		1'-6"	0'-9"	1'-6"								SIDEWAYS U BARS IN SLOPED SECTION
W607	6	24	3'-2"	S10		1'-8"	1'-6"	0'-0"								L BARS IN SLOPED SECTION
W608	6	88	5'-1"	17		2'-2"	0'-9"	2'-2"								SIDEWAYS U BARS IN PRIMARY SECTION

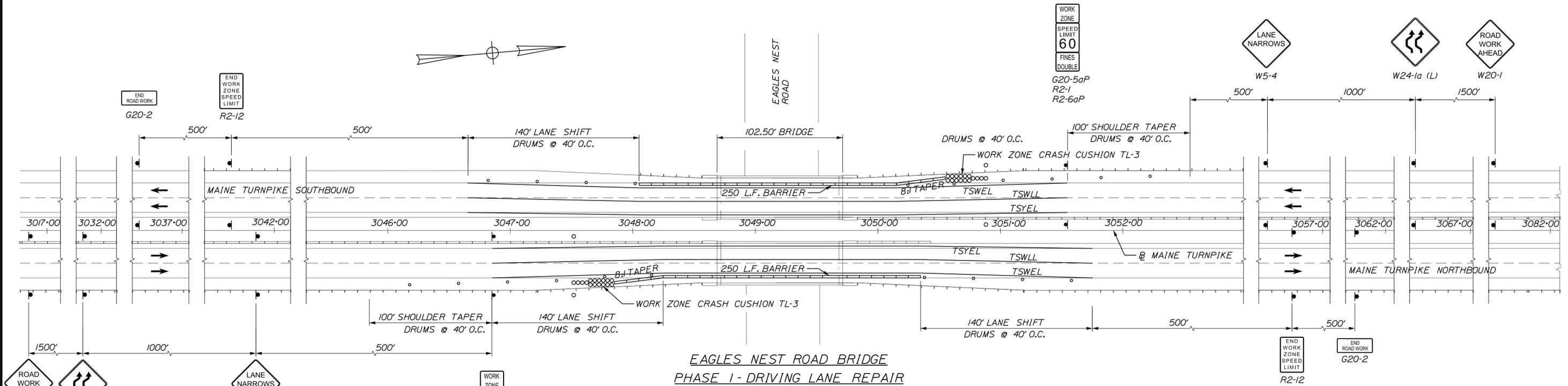


- REINFORCING GENERAL NOTES:**
- ALL DIMENSIONS ARE OUT-TO-OUT OF BAR.
 - BENDING DETAILS AND HOOKS SHALL CONFORM TO THE RECOMMENDATIONS OF THE CURRENT REVISION OF ACI STANDARD 315 AND ACI STANDARD 318.
 - THE FIRST DIGIT FOLLOWING THE LETTER(S) OF THE MARK INDICATES THE SIZE OF THE BAR.
 - ALL REINFORCEMENT SHALL BE EPOXY COATED

Filename: ... \048_60.8_Reinf_Schedule.dgn

<p>Scale: NOT TO SCALE</p> <table border="1" style="width: 100%; border-collapse: collapse;"> <thead> <tr> <th>No.</th> <th>Revision</th> <th>By</th> <th>Date</th> </tr> </thead> <tbody> <tr> <td> </td> <td> </td> <td> </td> <td> </td> </tr> </tbody> </table>	No.	Revision	By	Date					<p>Designed by:</p> <p style="text-align: center;">TYLIN INTERNATIONAL</p> <p>CONSULTANT PROJECT MANAGER: Norman L. Baker</p> <table border="1" style="width: 100%; border-collapse: collapse;"> <thead> <tr> <th>By</th> <th>Date</th> <th>By</th> <th>Date</th> </tr> </thead> <tbody> <tr> <td>Designed TVN</td> <td>1/2016</td> <td>Checked DSM</td> <td>1/2016</td> </tr> <tr> <td>Drawn DSM</td> <td>1/2016</td> <td>In Charge of NLB</td> <td>1/2016</td> </tr> </tbody> </table>	By	Date	By	Date	Designed TVN	1/2016	Checked DSM	1/2016	Drawn DSM	1/2016	In Charge of NLB	1/2016	<p>T.Y. Lin International</p> <p>12 Northbrook Drive Building A, Suite One Falmouth, Maine 04105 TEL: (207) 781-4721 FAX: (207) 781-4753</p>	<p style="font-size: 1.2em; font-weight: bold;">THE GOLD STAR MEMORIAL HIGHWAY</p>	<p>BRIDGE REPAIRS</p> <p>2 LOCATIONS (MILE 60.8 & MILE 61.6)</p> <p>EAGLES NEST OVERPASS (MILE 60.8)</p> <p>REINFORCEMENT SCHEDULE</p>
No.	Revision	By	Date																					
By	Date	By	Date																					
Designed TVN	1/2016	Checked DSM	1/2016																					
Drawn DSM	1/2016	In Charge of NLB	1/2016																					
			MTA PROJECT MANAGER: R. NORWOOD	CONTRACT: 2016.01																				
				SHEET NUMBER: EN-12 48 OF 77																				

Date: 1/26/2016



**EAGLES NEST ROAD BRIDGE
PHASE I - DRIVING LANE REPAIR**

ITEM # 526.306 - TEMPORARY CONCRETE BARRIER - TYPE 1 - SUPPLIED BY AUTHORITY

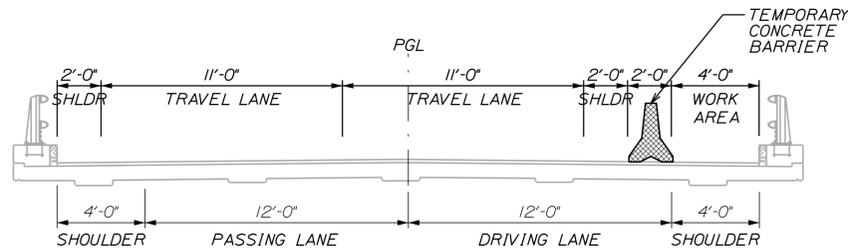
PHASE	STATION	OFFSET	TO	STATION	OFFSET
1	3047+85	RT		3050+35	RT
1	3048+05	LT		3050+55	LT
2	3047+05	RT		3050+35	RT
2	3048+05	LT		3051+35	LT

ITEM # 527.341 - WORK ZONE CRASH CUSHIONS - TL-3

PHASE	STATION	OFFSET
1	3047+85	RT
1	3050+55	LT

PAVEMENT STRIPING TABLE

MARKING TYPE	PHASE I - DRIVING LANE REPAIR						PHASE II - PASSING LANE REPAIR					
	NORTHBOUND BARREL			SOUTHBOUND BARREL			NORTHBOUND BARREL			SOUTHBOUND BARREL		
	START STATION	END STATION	LENGTH (FT.)	START STATION	END STATION	LENGTH (FT.)	START STATION	END STATION	LENGTH (FT.)	START STATION	END STATION	LENGTH (FT.)
TSWEL	3046+85	3051+75	490.0	3046+65	3051+55	490.0	3046+85	3051+75	490.0	3046+65	3051+55	490.0
TSWLL	3046+85	3051+75	490.0	3046+65	3051+55	490.0	3046+85	3051+75	490.0	3046+65	3051+55	490.0
TSYCL	3046+85	3051+75	490.0	3046+65	3051+55	490.0	3046+85	3051+75	490.0	3046+65	3051+55	490.0
BWLL - REMOVE	3046+85	3051+75	61.25	3046+65	3051+55	122.5	-	-	-	-	-	-
SYCL - REMOVE	3046+85	3051+75	245.0	3046+65	3051+55	490.0	-	-	-	-	-	-
SWEL - REMOVE	-	-	-	-	-	-	3046+85	3051+75	245.0	3046+65	3051+55	490.0



**EAGLES NEST ROAD BRIDGE DECK CROSS SECTIONS
PHASE I - DRIVING LANE REPAIR**

NOT TO SCALE
(LOOKING IN DIRECTION OF TRAFFIC)
(PHASE II SIMILAR/OPPOSITE)

LEGEND

- TSWEL - TEMPORARY SOLID WHITE EDGE LINE
- TSWLL - TEMPORARY SOLID WHITE LANE LINE
- TSYEL - TEMPORARY SOLID YELLOW EDGE LINE
- TSYCL - TEMPORARY SOLID YELLOW CENTER LINE
- SWEL - SOLID WHITE EDGE LINE
- BWLL - BROKEN WHITE LANE LINE
- SYEL - SOLID YELLOW EDGE LINE

NOTES:

- PHASE II - PASSING LANE REPAIR SHALL BE SIMILAR/OPPOSITE TO PHASE I - DRIVING LANE REPAIR.
- FOR PHASE II, LEADING END OF TEMPORARY CONCRETE BARRIER SHALL PASS THROUGH AND OVERLAP WITH THE EXISTING MEDIAN GUARDRAIL AS SHOWN ON THE CONCRETE BARRIER/GUARDRAIL OVERLAP DETAIL.
- INSTALL REFLECTORIZED BEAM GUARDRAIL DELINEATORS ON EXISTING GUARDRAIL IN AREAS WHERE TRAFFIC IS SHIFTED OVER THE EXISTING WHITE OR YELLOW EDGE LINE, AS DIRECTED IN SPECIAL PROVISION 606.
- TEMPORARY RAISED PAVEMENT MARKERS SHALL BE INSTALLED ON THE TEMPORARY CONCRETE BARRIER (SEE BARRIER SPECIFICATION). TEMPORARY RAISED PAVEMENT MARKERS SHALL ALSO BE INSTALLED ALONG THE NON-BARRIER SIDE OF ALL LANE SHIFTS AND ACROSS THE BRIDGE PARAPET. MARKERS SHALL BE SPACED AT 30' ON CENTER.
- PRIOR TO INSTALLING CONSTRUCTION SIGNING, THE CONTRACTOR SHALL NOTIFY THE RESIDENT OF ANTICIPATED CONSTRUCTION SEQUENCING FOR THE PROJECT. THE MAINTENANCE OF TRAFFIC LAYOUT, INCLUDING SIGNING, STRIPING, AND LOCATION OF CONTROL DEVICES, SHALL BE MODIFIED BY THE CONTRACTOR AS NECESSARY TO ACCOMMODATE THEIR ANTICIPATED CONSTRUCTION SEQUENCE.
- 60 LF OF TEMPORARY CONCRETE BARRIER IS PRESENT IN THE NORTHBOUND MEDIAN ON THE NORTH END OF THE BRIDGE. THIS BARRIER MAY BE USED DURING PHASE 2. RELOCATION AND STACKING OF EXISTING TEMPORARY CONCRETE BARRIER SHALL BE PAID UNDER ITEM 526.306.

Scale: 80 0 40 80

No.	Revision	By	Date

Designed by:

TYLIN INTERNATIONAL

CONSULTANT PROJECT MANAGER: Norman L. Baker

By	Date	By	Date
Designed	JRH 10/2015	Checked	KSD 11/2015
Drawn	JJB 10/2015	In Charge of	NLB 11/2015

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MAINE TURNPIKE

THE GOLD STAR MEMORIAL HIGHWAY

MTA PROJECT MANAGER: R. NORWOOD

BRIDGE REPAIRS
2 LOCATIONS (MILE 60.8 & MILE 61.6)
MILE 60.8 OVERPASS
MAINTENANCE OF TRAFFIC

SHEET NUMBER: EN-13
CONTRACT: 2016.01
49 OF 77

Date: 1/26/2016

Filename: ... \MSTA\050_60.8_HDPlan_01.dgn

ITEM # 606.1723 - BRIDGE TRANSITION - TYPE III

STATION	OFFSET	TO	STATION	OFFSET
3048-37	± 42' ± LT		3048-58	± 41' ± LT
3048-37	± 9' ± RT		3048-58	± 9' ± RT
3048-37	± 42' ± RT		3048-58	± 41' ± RT
3049-82	± 41' ± LT		3050-03	± 42' ± LT
3049-82	± 9' ± LT		3050-03	± 9' ± LT
3049-82	± 41' ± RT		3050-03	± 42' ± RT

ITEM # 606.1724 - BRIDGE TRANSITION - TYPE III MODIFIED

STATION	OFFSET	TO	STATION	OFFSET
3049-82	± 9' ± RT		3050-09	± 9' ± RT

ITEM # 606.2652 - TERMINAL END - REMOVE AND DISPOSE

STATION	OFFSET
3048-12	± 43' ± RT

ITEM # 606.278 - TERMINAL END - ANCHORED END

STATION	OFFSET
3048-12	± 43' ± RT

ITEM # 606.279 - TERMINAL END - ANCHORED END - THRIE BEAM

STATION	OFFSET
3050-34	± 9' ± RT

ITEM # 606.3606 - GUARDRAIL - REMOVE, MODIFY, AND RESET - DOUBLE RAIL

STATION	OFFSET	TO	STATION	OFFSET	LENGTH
3047-55	± 9' ± RT		3048-37	± 9' ± RT	82.0'
3050-03	± 9' ± LT		3050-85	± 9' ± LT	82.0'

ITEM # 606.3621 - GUARDRAIL ADJUST, SINGLE RAIL

STATION	OFFSET	TO	STATION	OFFSET	LENGTH
3048-12	± 43' ± RT		3048-37	± 42' ± RT	25.0'

ITEM # 606.3631 - GUARDRAIL - REMOVE AND DISPOSE

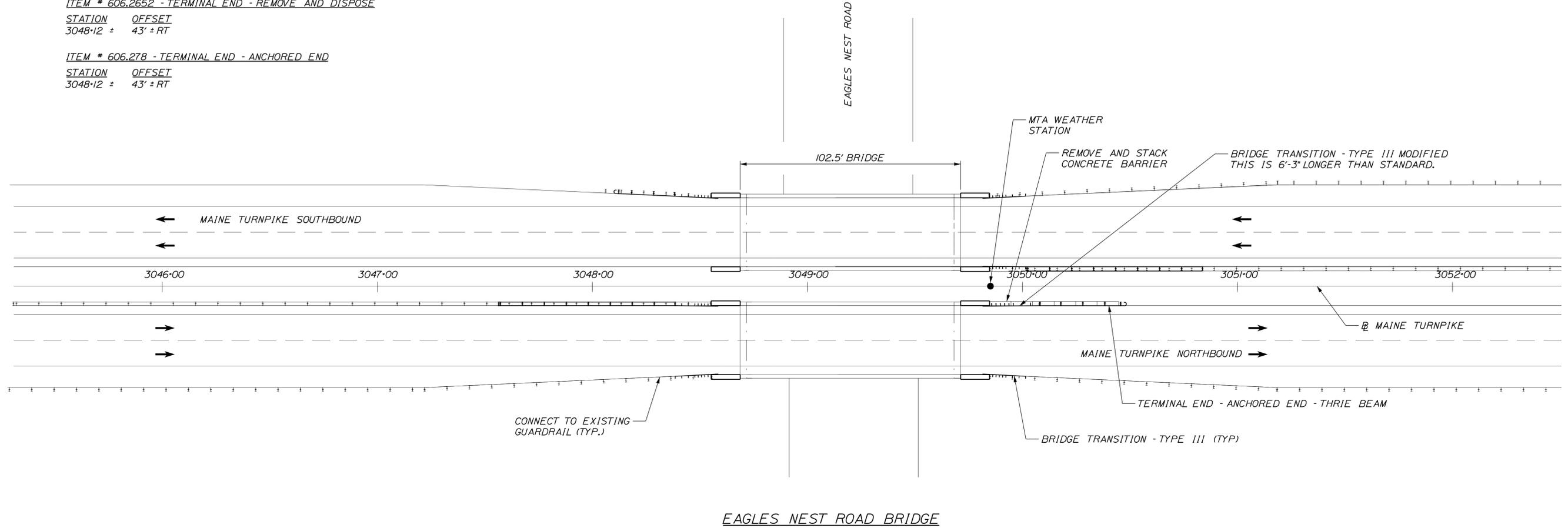
STATION	OFFSET	TO	STATION	OFFSET
3048-37	± 42' ± LT		3048-58	± 41' ± LT
3048-37	± 9' ± RT		3048-58	± 9' ± RT
3048-37	± 42' ± RT		3048-58	± 41' ± RT
3049-82	± 41' ± LT		3050-03	± 42' ± LT
3049-82	± 9' ± LT		3050-03	± 9' ± LT
3049-82	± 41' ± RT		3050-03	± 42' ± RT

ITEM # 606.65 - GUARDRAIL THRIE BEAM - SINGLE RAIL

STATION	OFFSET	TO	STATION	OFFSET	LENGTH
3050-09	± 9' ± RT		3050-34	± 9' ± RT	25.0'

ITEM # 606.191 - CONCRETE CURB TYPE 2

STATION	OFFSET	TO	STATION	OFFSET	LENGTH
3048-43	± 42' ± LT		3048-55	± 41' ± LT	11.9'
3048-43	± 9' ± RT		3048-55	± 9' ± RT	11.9'
3048-43	± 42' ± RT		3048-55	± 41' ± RT	11.9'
3049-85	± 41' ± LT		3049-97	± 42' ± LT	11.9'
3049-85	± 9' ± LT		3049-97	± 9' ± LT	11.9'
3049-85	± 41' ± RT		3049-97	± 42' ± RT	11.9'



NOTES:

1. ALL STATIONING IS APPROXIMATE.
2. FOR REMOVE AND STACK OF EXISTING TEMPORARY CONCRETE BARRIER SEE NOTE 6 ON MILE 60.8 OVERPASS MAINTENANCE OF TRAFFIC SHEET.



No.	Revision	By	Date

Designed by:

TYLIN INTERNATIONAL

CONSULTANT PROJECT MANAGER: Norman L. Baker

	By	Date	By	Date
Designed	JRH	10/2015	Checked	KSD 11/2015
Drawn	JJB	10/2015	In Charge of	NLB 11/2015

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THE GOLD STAR MEMORIAL HIGHWAY

MTA PROJECT MANAGER: R. NORWOOD

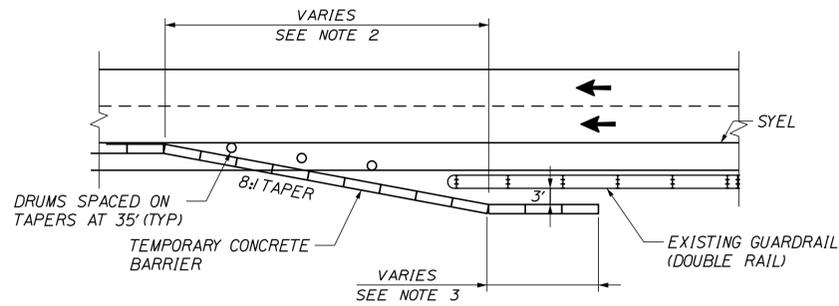
BRIDGE REPAIRS
 2 LOCATIONS (MILE 60.8 & MILE 61.6)
 MILE 60.8 OVERPASS
 GENERAL PLAN

CONTRACT: 2016.01

SHEET NUMBER: EN-14

50 OF 77

Date: 1/26/2016



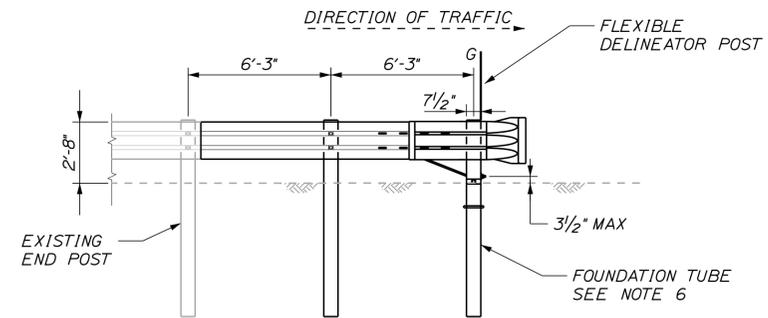
EXISTING GUARDRAIL ON NEAR SIDE OF MEDIAN
CONCRETE BARRIER/GUARDRAIL OVERLAP DETAIL
 NTS

CONCRETE BARRIER/GUARDRAIL OVERLAP NOTES:

1. BARRIER ENDS SHALL BE PROTECTED BY A WORK ZONE CRASH CUSHION (TL-3) OR LAPPED BEHIND GUARDRAIL. SEE DETAILS THIS SHEET.
2. 8:1 MINIMUM TAPERED BARRIER LENGTH DEPENDENT ON LOCATION OF BARRIER RELATIVE TO MAINE TURNPIKE SHOULDERS OR LANES.
3. IF THE 50' MIN. LENGTH OF OVERLAP CANNOT BE MET, THEN THE EXISTING GUARDRAIL END MUST BE ANCHORED IN ACCORDANCE WITH DRAWING SEW02a IN THE AASHTO-AGC-ARBTA JOINT COMMITTEE TASK FORCE 13 REPORT, DRAFTED MAY 1995. PAYMENT SHALL BE INCIDENTAL TO GUARDRAIL ITEMS.

GUARDRAIL INSTALLATION AND MODIFICATION NOTES

1. THE CONTRACTOR SHALL FOLLOW ALL MANUFACTURER'S INSTALLATION INSTRUCTIONS FOR THE INSTALLATION AND/OR MODIFICATION OF ANY AND ALL GUARDRAIL COMPONENTS TO BE INSTALLED UNDER THIS CONTRACT. THE CONTRACTOR SHALL NOTIFY THE RESIDENT ENGINEER IF THE CONTRACTOR DISCOVERS CONFLICTS OR IRREGULARITIES OF ANY SORT BETWEEN THE MANUFACTURER'S INSTALLATION INSTRUCTIONS AND THESE CONTRACT DOCUMENTS.
2. THE CONTRACTOR IS REQUIRED TO HAVE AN APPROVED CRASH END TREATMENT ON ALL GUARDRAIL ENDS IN ALL WORK AREAS AT THE END OF EACH DAY. THE CONTRACTOR SHALL ENSURE THAT ENOUGH TIME EXISTS IN THE WORK DAY TO COMPLETE ALL MODIFICATIONS AND/OR INSTALLATIONS TO END TERMINALS. THE CONTRACTOR SHALL ENSURE THAT ALL PARTS AND MANPOWER ARE ON-SITE PRIOR TO UNDERTAKING ANY GUARDRAIL MODIFICATIONS.
3. THE CONTRACTOR SHALL REMOVE AND DISPOSE OF ALL CUT BOLTS, DISCARDED PARTS, TRASH, PAVEMENT DEBRIS, ETC., AFTER COMPLETING GUARDRAIL MODIFICATIONS AND/OR INSTALLATIONS.
4. CONNECTIONS FOR PROPOSED RAIL TO EXISTING RAIL SHALL BE INCIDENTAL TO THE INSTALLATION OF PROPOSED GUARDRAIL.
5. ALL SALVAGEABLE GUARDRAIL BEAM AND POSTS FROM REMOVED GUARDRAIL SHALL BE STACKED AT THE GRAY MAINTENANCE YARD AT MM 46 S.B. THE CONTRACTOR SHALL DISPOSE OF ALL UNSALVAGABLE COMPONENTS INCLUDING ALL OFFSET BLOCKS, BOLTS, AND OTHER HARDWARE.
6. FOUNDATION TUBES INSTALLED AS PART OF CAT, FLEAT OR TRAILING TERMINAL ENDS SHALL NOT PROTRUDE FROM THE GROUND MORE THAN 3 1/2". ADDITIONAL SITE GRADING MAY BE REQUIRED TO ADJUST FOUNDATION TUBE REVEAL AND IF REQUIRED SHALL NOT DIRECT STORMWATER SURFACE DRAINAGE TOWARDS THE SHOULDER OR TRAVEL WAY. ADDITIONAL SITE GRADING SHALL BE INCIDENTAL TO THE END TERMINAL INSTALLATION.



TERMINAL END - ANCHORED END - THRIE BEAM
(ITEM 606.279)
 1/4" = 1'-0"

Filename: ... \MSTA\051_GR-Details.dgn

Scale:			
NOT TO SCALE			
No.	Revision	By	Date

Designed by:					
TYLIN INTERNATIONAL					
CONSULTANT PROJECT MANAGER: Norman L. Baker					
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Designed	JRH	11/2015	Checked	JRH	11/2015
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**THE GOLD STAR
MEMORIAL HIGHWAY**

MTA PROJECT MANAGER: R. NORWOOD

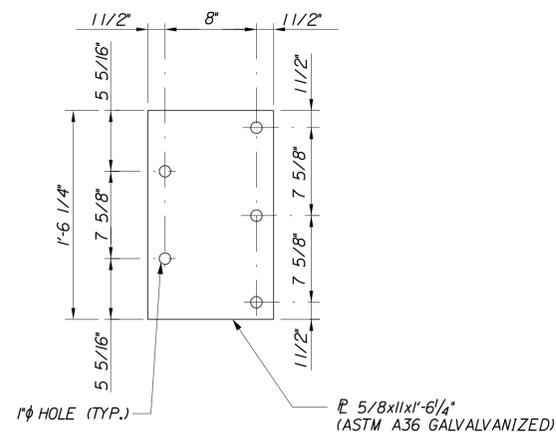
BRIDGE REPAIRS
 2 LOCATIONS (MILE 60.8 & MILE 61.6)

GUARDRAIL DETAILS

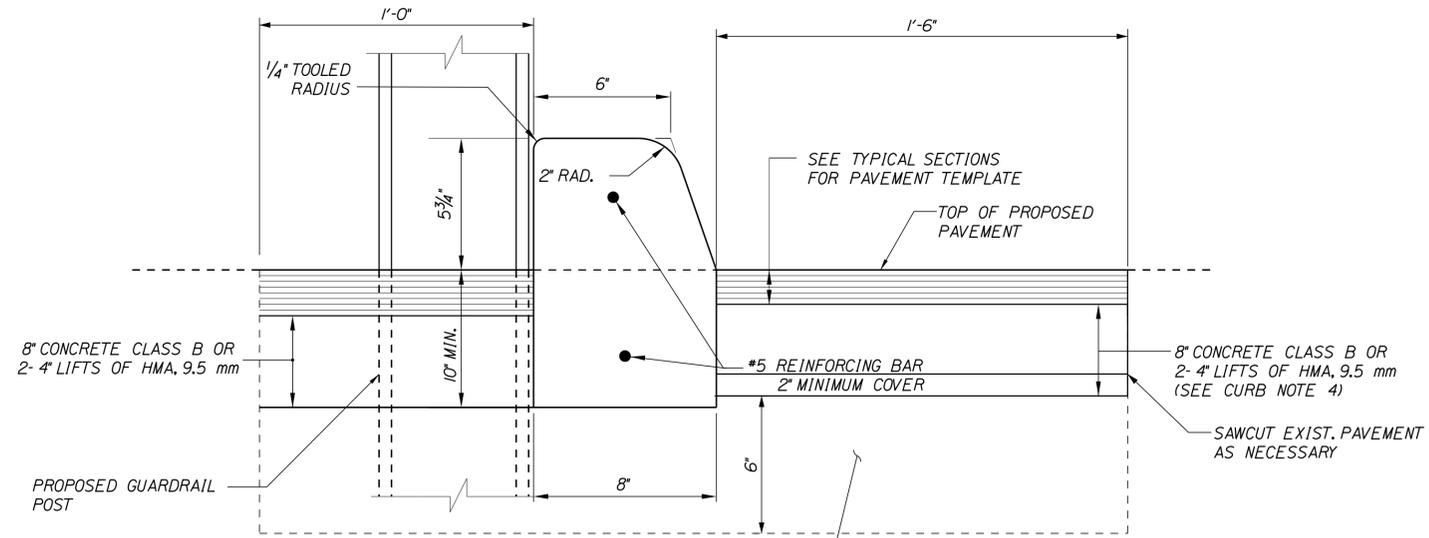
SHEET NUMBER: EN-15
 CONTRACT: 2016.01
 51 OF 77

Date: 1/26/2016

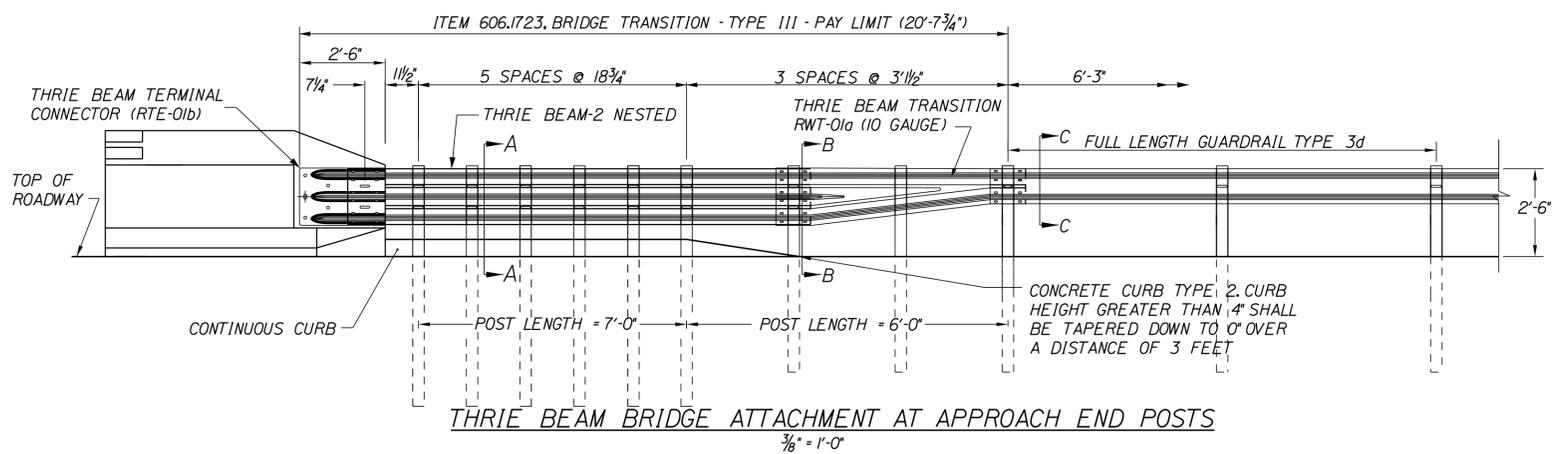
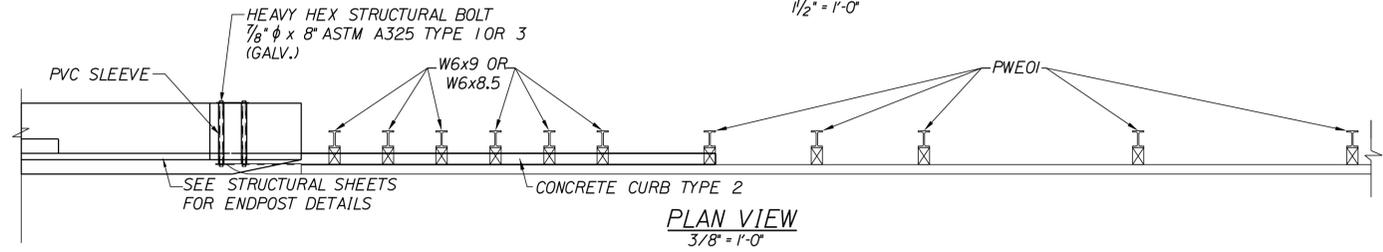
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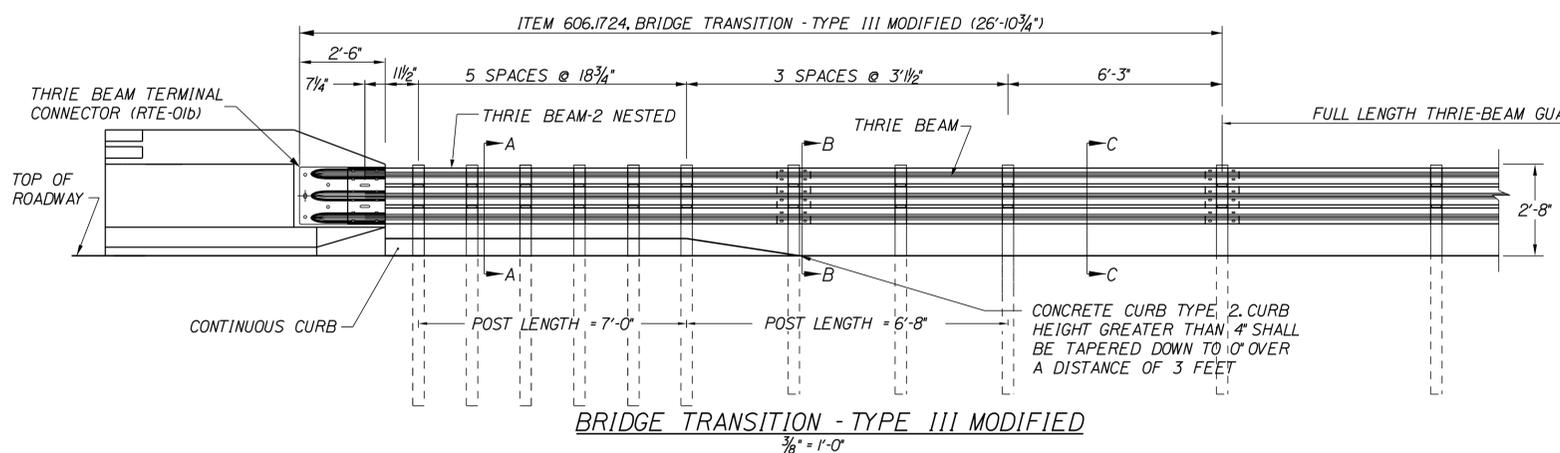
ANCHOR PLATE
1/2" = 1'-0"



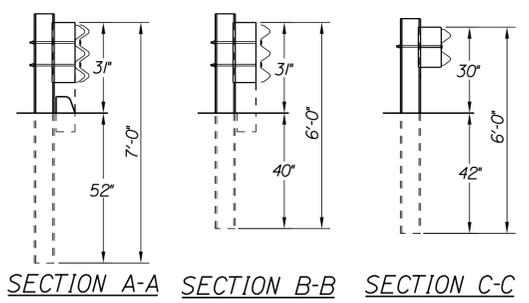
CONCRETE CURB
TYPE 2
3" = 1'-0"



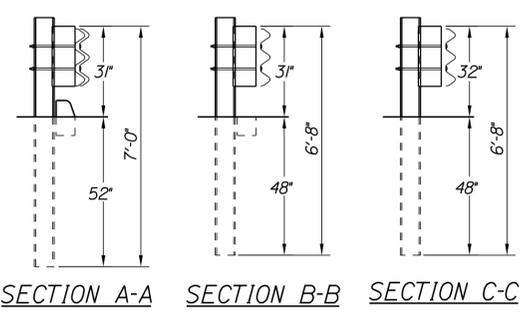
THRIE BEAM BRIDGE ATTACHMENT AT APPROACH END POSTS
3/8" = 1'-0"



BRIDGE TRANSITION - TYPE III MODIFIED
3/8" = 1'-0"



SECTION A-A SECTION B-B SECTION C-C



SECTION A-A SECTION B-B SECTION C-C

- CURB NOTES:**
- CURBING SHALL BE EITHER PRECAST CONCRETE, CAST-IN-PLACE CONCRETE OR GRANITE TO MEET DIMENSIONS SHOWN ON THE PLANS. CURBING SHALL BE PAID UNDER ITEM 609.191 CONCRETE CURB TYPE 2.
 - CONCRETE CURBS USED IN CONJUNCTION WITH THRIE-BEAM BRIDGE ATTACHMENT SHALL BE TYPE 2, SEE DETAILS THIS SHEET. CONCRETE CURBS SHALL BE CONTINUOUS WITHOUT DRAINAGE CUTS.
 - CURB TRANSITION SHALL BE INCLUDED FOR PAYMENT UNDER ITEM 609.191 CONCRETE CURB TYPE 2.
 - SAWCUTTING EXISTING PAVEMENT, CONCRETE FILL AND PAVEMENT TO LIMITS SHOWN SHALL BE INCIDENTAL TO ITEM 609.191

- GENERAL NOTES:**
- ADDITIONAL HOLES MAY BE MADE IN THE THRIE-BEAM PANELS BY DRILLING, PUNCHING, OR OTHER MEANS THAT PRODUCE A NEAT, CLEAN HOLE. BURNING HOLES WILL NOT BE ALLOWED.
 - THRIE BEAM SHALL BE PLACED WITH THE COMPOSITE BLOCKOUT FACE IN FRONT OF OR DIRECTLY ABOVE THE CURB FACE.
 - RAIL ELEMENT SHALL MEET ALL REQUIREMENTS OF AASHTO M-180 EXCEPT AS MODIFIED ON THE PLANS. THE THRIE BEAM TRANSITION TO W-BEAM SHALL BE OF THE SAME MATERIAL, BUT SHALL NOT BE LESS THAN 10 GAUGE.
 - AFTER INSTALLATION IS COMPLETE, UPSET THE THREAD ON THE ANCHOR BOLTS IN THREE PLACES AROUND EACH BOLT, AT THE JUNCTION OF THE NUT AND THE EXPOSED THREAD, WITH A CENTER PUNCH OR SIMILAR TOOL.
 - STANDARD BARRIER HARDWARE HAS BEEN USED TO DEVELOP THESE GUARDRAIL ATTACHMENTS. DESIGNATIONS PROVIDED IN PARENTHESIS RELATE TO STANDARD ELEMENTS DETAILED IN "A GUIDE TO STANDARDIZED BARRIER RAIL HARDWARE," 1979, AASHTO-AGC-ARTBA JOINT COOPERATE COMMITTEE.
 - 1" HOLE IN CONCRETE SHALL BE FORMED BY A METHOD APPROVED BY THE ENGINEER

Scale: NOT TO SCALE

No.	Revision	By	Date

Designed by: **TYLIN INTERNATIONAL**

CONSULTANT PROJECT MANAGER: Norman L. Baker

	By	Date	By	Date	
Designed	JRH	1/2016	Checked	JRH	1/2016
Drawn	JJB	1/2016	In Charge of	NLB	1/2016

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MAINE TURNPIKE

THE GOLD STAR MEMORIAL HIGHWAY

MTA PROJECT MANAGER: R. NORWOOD

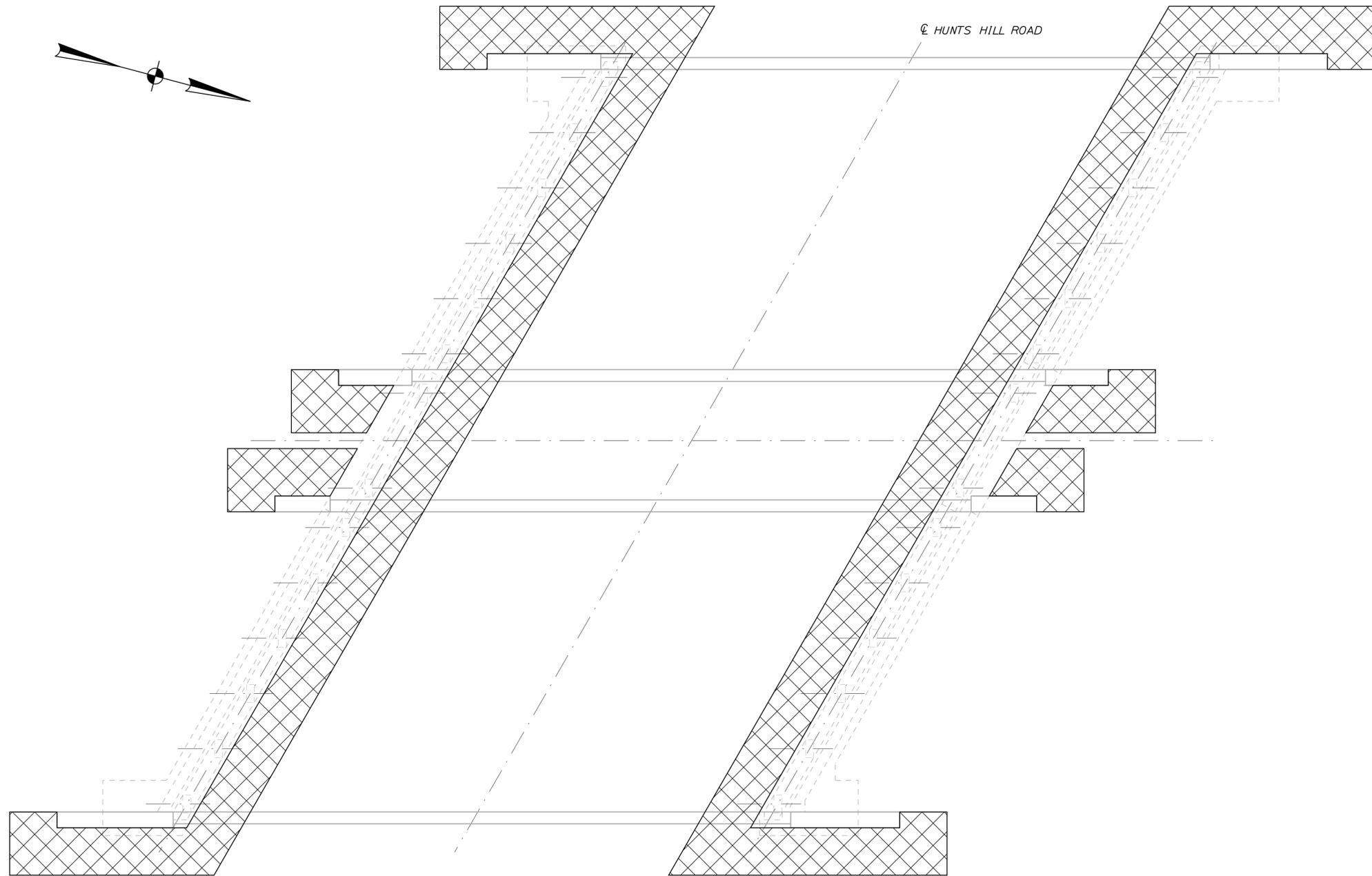
BRIDGE REPAIRS
2 LOCATIONS (MILE 60.8 & MILE 61.6)

BRIDGE TRANSITION DETAILS

SHEET NUMBER: EN-16
CONTRACT: 2016.01
52 OF 77

Date: 1/26/2016

Filename: ... \053_616_Limits_of_Disturbance.dgn



LIMIT OF DISTURBANCE PLAN - HUNTS HILL ROAD
NOT TO SCALE

LEGEND:

-  CONTRACT LIMIT OF DISTURBANCE
-  CONTRACTOR'S LIMIT OF DISTURBANCE AREA FOR ACCESS AND STORAGE

AREAS:

LOCATION	CONTRACT LIMIT OF DISTURBANCE	CONTRACTOR'S LIMIT OF DISTURBANCE AREA FOR ACCESS AND STORAGE
HUNTS HILL ROAD OVERPASSES (MILE 61.6)	0.00 ACRES	0.10 ACRES

NOTES:

1. EROSION AND SEDIMENT CONTROL DEVICES REQUIRED FOR THE CONTRACTOR'S ACCESS LOCATIONS AND STORAGE AREAS WILL NOT BE MEASURED FOR PAYMENT.
2. THE CONTRACTOR WILL BE ALLOWED TO STORE MATERIAL AND EQUIPMENT ON GRAVEL AND PAVEMENT WITHIN THE CLOSED PORTIONS OF THE MAINLINE TURNPIKE AND HUNTS HILL ROAD. THESE AREAS ARE EITHER INCLUDED IN THE LIMITS OF DISTURBANCE OR HAVE BEEN PREVIOUSLY SURFACED AND DO NOT ADD TO THE LIMITS OF DISTURBANCE.
3. CONTRACTOR SHALL TAKE PRECAUTIONS TO AVOID IMPACTING OR DISTURBING ROADWAY DITCHES.

Scale: NOT TO SCALE

No.	Revision	By	Date

Designed by:

TY-LIN INTERNATIONAL

CONSULTANT PROJECT MANAGER: Norman L. Baker

	By	Date	By	Date	
Designed	DSM	1/2016	Checked	NLB	1/2016
Drawn	PJB	1/2016	In Charge of	NLB	1/2016

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 **THE GOLD STAR MEMORIAL HIGHWAY**

MTA PROJECT MANAGER: R. NORWOOD

BRIDGE REPAIRS
2 LOCATIONS (MILE 60.8 & MILE 61.6)
HUNTS HILL OVERPASS (MILE 61.6)
LIMIT OF DISTURBANCE

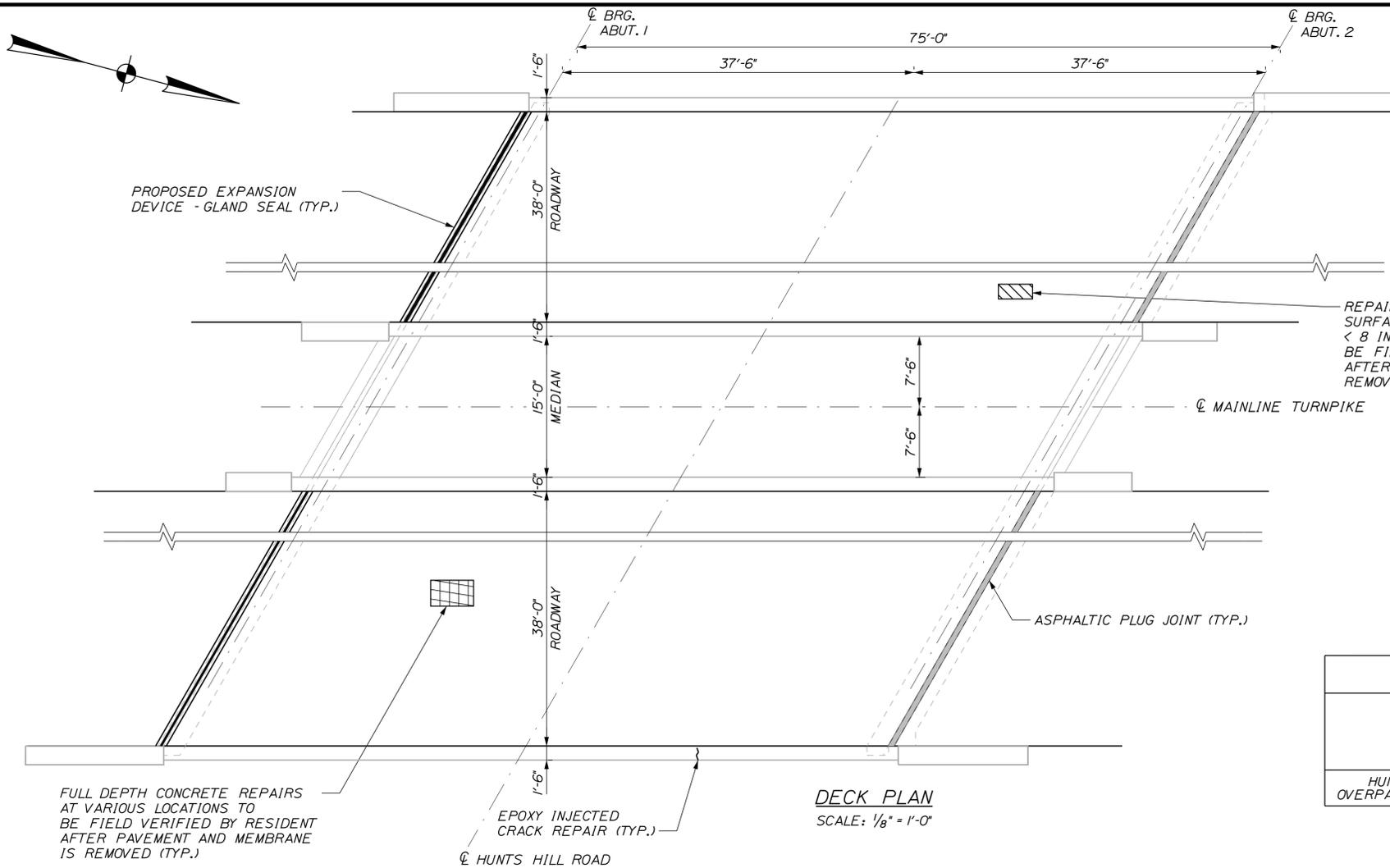
CONTRACT: 2016.01

SHEET NUMBER: HH-01

53 OF 77

Date: 1/26/2016

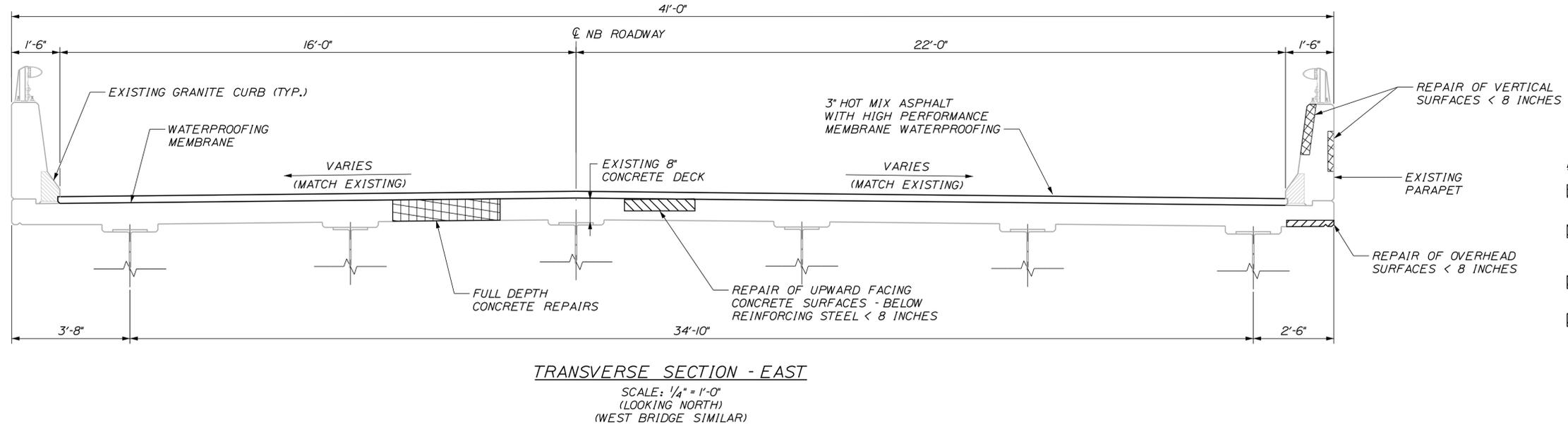
Filename: ... \MSTA\054_01.6_Plan_Typ.dgn



NOTES:

- CONTRACTOR SHALL REPAIR AREAS OF CONCRETE DECK DETERIORATION AS IDENTIFIED BY THE RESIDENT. RESIDENT SHALL IDENTIFY AND MARK AREAS FOR DECK REPAIR AFTER THE WEARING SURFACE AND MEMBRANE ARE REMOVED. CONTRACTOR SHALL ALSO REPAIR DETERIORATED AREAS OF FASCIA, FASCIA OVERHANGS, AND PARAPET JOINTS. COORDINATE WORK WITH THE RESIDENT. PAYMENT FOR REPAIR WORK SHALL BE UNDER THE 518 PAY ITEMS.
- CONTRACTOR SHALL REPAIR AREAS OF DETERIORATED GRANITE CURB BEDDING AND JOINT MORTAR ON THE DECK AS REQUIRED. LOCATIONS OF GRANITE CURB BEDDING AND JOINT MORTAR SHALL BE FIELD DETERMINED BY THE RESIDENT. PAYMENT SHALL BE UNDER THE 518 PAY ITEMS.
- SEAL SHOULDER AREAS WITH TWO APPLICATIONS OF EMULSIFIED BITUMINOUS SEALER PER MAINE DOT STANDARD SPECIFICATION 403.03 AFTER COMPLETING ALL DECK PAVING WORK.
- ALL PVC DECK WEEPS WHICH DRAIN ONTO PIERS, ABUTMENTS OR STRUCTURAL STEEL SHALL BE EXTENDED AND DIVERTED AWAY FROM BRIDGE COMPONENTS BELOW DECK, AND SHALL EXTEND 1 FT BELOW BOTTOM FLANGE, AS DIRECTED BY THE RESIDENT. PAYMENT SHALL BE BY THE VARIOUS LABOR AND EQUIPMENT ITEMS.

ESTIMATED REPAIR QUANTITIES (BOTH EAST & WEST DECKS)					
	EPOXY INJECTED CRACK REPAIRS (L.F.)	REPAIR OF UPWARD FACING CONCRETE SURFACES - BELOW REINFORCING STEEL < 8 INCHES (S.F.)	FULL DEPTH CONCRETE REPAIRS (S.F.)	REPAIR OF VERTICAL SURFACES < 8 INCHES (S.F.)	REPAIR OF OVERHEAD SURFACES < 8 INCHES (S.F.)
HUNTS HILL OVERPASS (MILE 61.6)	40	320	60	150	90



LEGEND:

- LIMITS OF FULL DEPTH CONCRETE REPAIR (REPAIR AREAS TO BE FIELD VERIFIED)
- LIMITS OF REPAIR OF UPWARD FACING CONCRETE SURFACES - BELOW REINFORCING STEEL < 8 INCHES (REPAIR AREAS TO BE FIELD VERIFIED)
- LIMITS OF REPAIR OF VERTICAL SURFACES < 8 INCHES (REPAIR AREAS TO BE FIELD VERIFIED)
- REPAIR OF OVERHEAD SURFACES < 8 INCHES (REPAIR AREAS TO BE FIELD VERIFIED)

Scale: AS NOTED

No.	Revision	By	Date

Designed by:

TYLIN INTERNATIONAL

CONSULTANT PROJECT MANAGER: Norman L. Baker

By	Date	By	Date
Designed DSM	1/2016	Checked NLB	1/2016
Drawn PJB	1/2016	In Charge of NLB	1/2016

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MAINE TURNPIKE

THE GOLD STAR MEMORIAL HIGHWAY

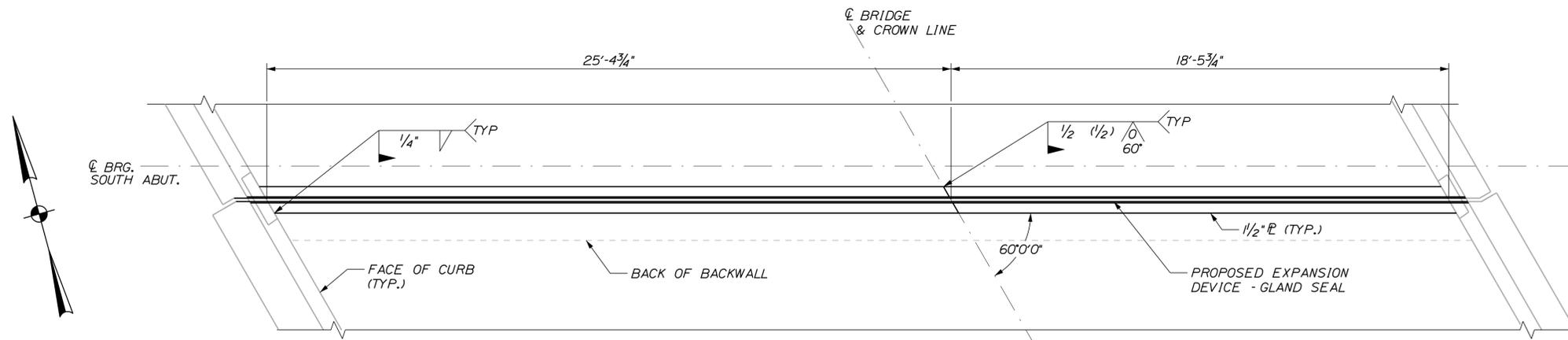
MTA PROJECT MANAGER: R. NORWOOD

BRIDGE REPAIRS
2 LOCATIONS (MILE 60.8 & MILE 61.6)
HUNTS HILL OVERPASS (MILE 61.6)
DECK PLAN & TYPICAL SECTION

SHEET NUMBER: HH-02
CONTRACT: 2016.01
54 OF 77

Date: 1/26/2016

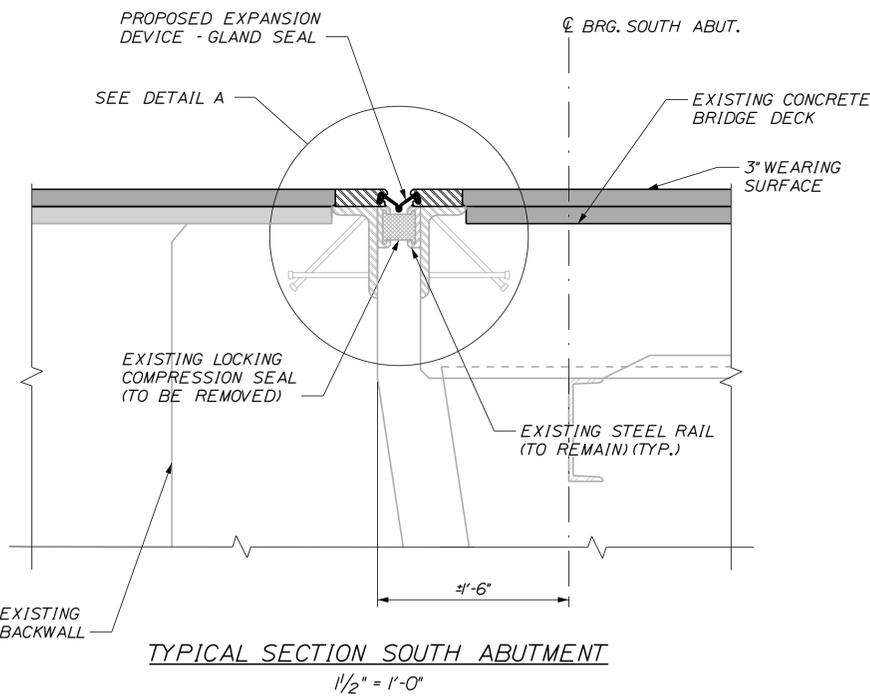
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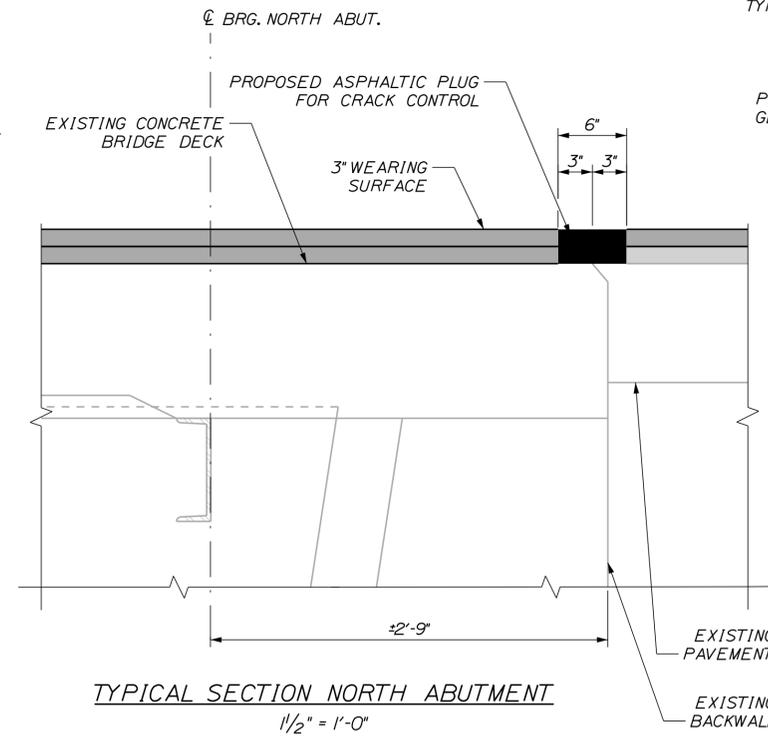
SOUTH ABUTMENT PLAN - WEST BRIDGE
 $\frac{3}{8}'' = 1'-0''$
 (EAST BRIDGE SIMILAR)

EXPANSION JOINT NOTES:

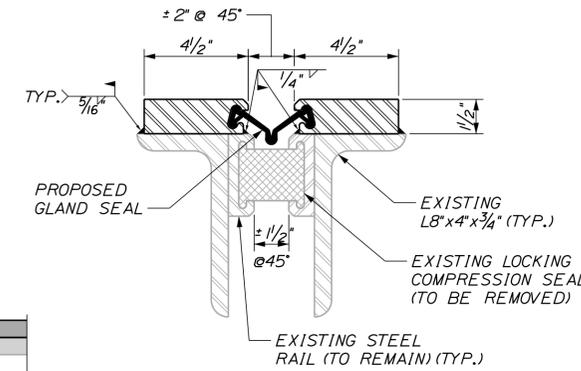
1. ALL STEEL COMPONENTS SHALL BE AASHTO M270 GRADE 36, UNLESS OTHERWISE NOTED.
2. CONTRACTOR SHALL FIELD MEASURE EXISTING CURB PLATE AND JOINT DIMENSIONS PRIOR TO DEVELOPMENT OF SHOP DRAWINGS.
3. EXPANSION JOINT MODIFICATIONS AT EACH ABUTMENT SHALL BE PAID UNDER ITEM 520.2211, EXPANSION DEVICE MODIFICATIONS.
4. TEMPORARY HOT MIX ASPHALT BITUMINOUS RAMPS SHALL BE PLACED IF TRAFFIC IS TO BE RUN OVER JOINT AFTER INSTALLATION OF STEEL PLATE BUT BEFORE PROPOSED PAVEMENT IS COMPLETED. THE TEMPORARY RAMP SHALL EXTEND 35 FEET MINIMUM FROM FACE OF JOINT PLATE FOR EACH INCH OF HEIGHT. HOT MIX ASPHALT FOR TEMPORARY RAMPS AND REMOVAL OF THE TEMPORARY RAMPS SHALL NOT BE MEASURED FOR PAVEMENT, BUT SHALL BE INCIDENTAL TO ITEM 520.2211.



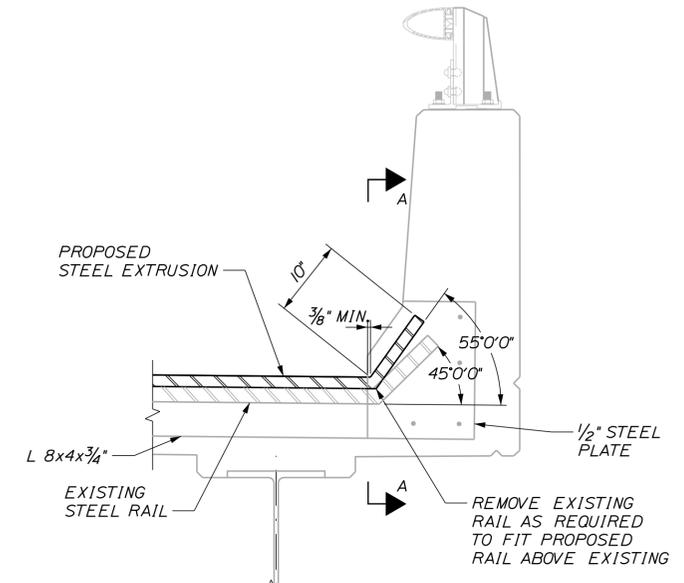
TYPICAL SECTION SOUTH ABUTMENT
 $\frac{1}{2}'' = 1'-0''$



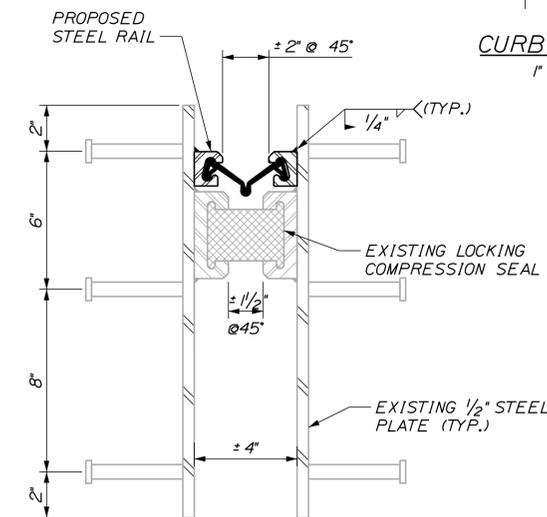
TYPICAL SECTION NORTH ABUTMENT
 $\frac{1}{2}'' = 1'-0''$



DETAIL A
 $\frac{3}{8}'' = 1'-0''$



CURB DETAIL
 $1'' = 1'-0''$



SECTION A-A
 $\frac{3}{8}'' = 1'-0''$

Scale: AS NOTED			
No.	Revision	By	Date

Designed by: TYLIN INTERNATIONAL					
CONSULTANT PROJECT MANAGER: Norman L. Baker					
	By	Date		By	Date
Designed	DSM	1/2016	Checked	NLB	1/2016
Drawn	PJB	1/2016	In Charge of	NLB	1/2016

T.Y. Lin International
 12 Northbrook Drive
 Building A, Suite One
 Falmouth, Maine 04105
 TEL: (207) 781-4721
 FAX: (207) 781-4753



THE GOLD STAR MEMORIAL HIGHWAY

MTA PROJECT MANAGER: R. NORWOOD

BRIDGE REPAIRS
 2 LOCATIONS (MILE 60.8 & MILE 61.6)
 HUNTS HILL OVERPASS (MILE 61.6)
 JOINT REPAIRS

CONTRACT: 2016.01

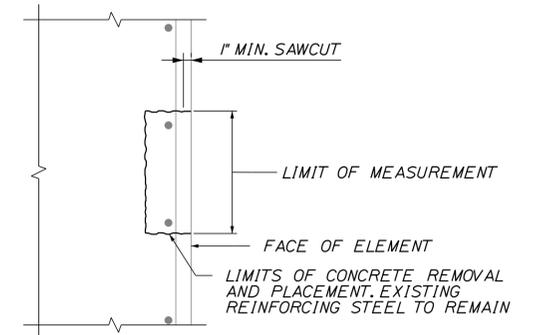
SHEET NUMBER: HH-03

55 OF 77

Date: 1/26/2016

Filename: ... \MSTA\056_61.6_Sub_Repair_1.dgn

HUNTS HILL ROAD MILE (61.6 NB) SOUTH ABUTMENT ESTIMATED QUANTITY OF REPAIRS			
	NB ABUT.	SB ABUT.	TOTAL
UPWARD FACING CONCRETE SURFACES	0 S.F.	0 S.F.	0 S.F.
VERTICAL SURFACES < 8 INCHES	5 S.F.	10 S.F.	15 S.F.
OVERHEAD SURFACES < 8 INCHES	0 S.F.	0 S.F.	0 S.F.
EPOXY INJECTED CRACK REPAIRS	60 L.F.	40 L.F.	100 L.F.



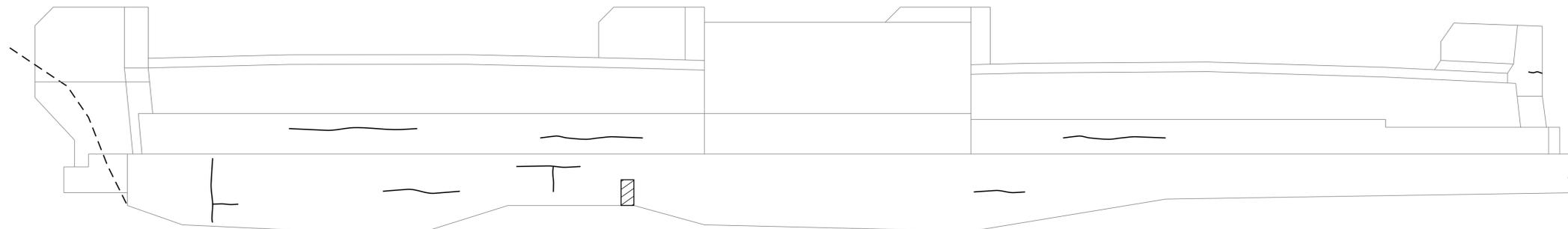
CONCRETE REPAIR DETAIL

NOTES:

- FOR PROCEDURAL NOTES SEE SHEET "GENERAL NOTES"

LEGEND:

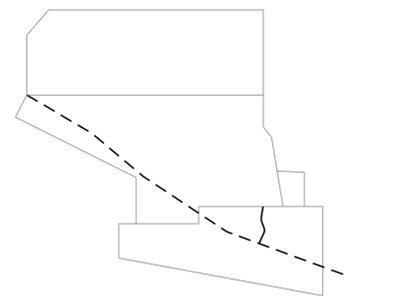
- LIMIT OF CONCRETE SURFACE REPAIRS
- LIMIT OF EPOXY INJECTED CRACK REPAIR
- EXISTING CONCRETE
- EXISTING AND PROPOSED GROUND



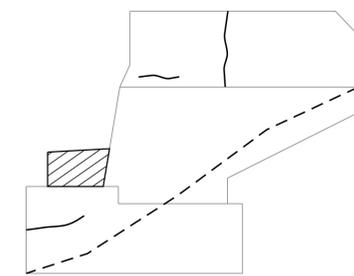
NORTHBOUND ABUTMENT

SOUTH ABUTMENT ELEVATION

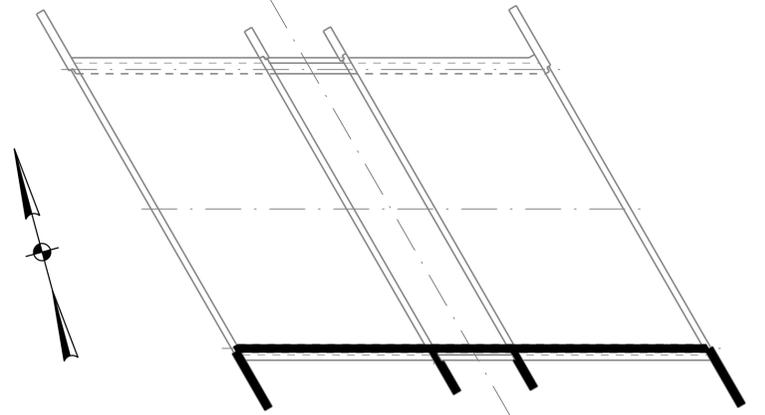
SOUTHBOUND ABUTMENT



SOUTHWEST WINGWALL ELEVATION



SOUTHEAST WINGWALL ELEVATION



LOCATION PLAN

Scale: NOT TO SCALE

No.	Revision	By	Date

Designed by:

TYLIN INTERNATIONAL

CONSULTANT PROJECT MANAGER: Norman L. Baker

	By	Date	By	Date	
Designed	DSM	1/2016	Checked	NLB	1/2016
Drawn	PJB	1/2016	In Charge of	NLB	1/2016

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THE GOLD STAR MEMORIAL HIGHWAY

MTA PROJECT MANAGER: R. NORWOOD

BRIDGE REPAIRS
 2 LOCATIONS (MILE 60.8 & MILE 61.6)
 HUNTS HILL OVERPASS (MILE 61.6)
 SUBSTRUCTURE REPAIR - 1 OF 2

SHEET NUMBER: HH-04
 CONTRACT: 2016.01
 56 OF 77

Date: 1/26/2016

Filename: ... \MSTA\057_61.6_Sub_Repair_2.dgn

HUNTS HILL ROAD MILE (61.6 SB) BRIDGE ABUTMENTS ESTIMATED QUANTITY OF REPAIRS			
	NB ABUT.	SB ABUT.	TOTAL
UPWARD FACING CONCRETE SURFACES	0 S.F.	0 S.F.	0 S.F.
VERTICAL SURFACES < 8 INCHES	0 S.F.	0 S.F.	0 S.F.
OVERHEAD SURFACES < 8 INCHES	0 S.F.	0 S.F.	0 S.F.
EPOXY INJECTED CRACK REPAIRS	30 L.F.	25 L.F.	55 L.F.

NOTES:

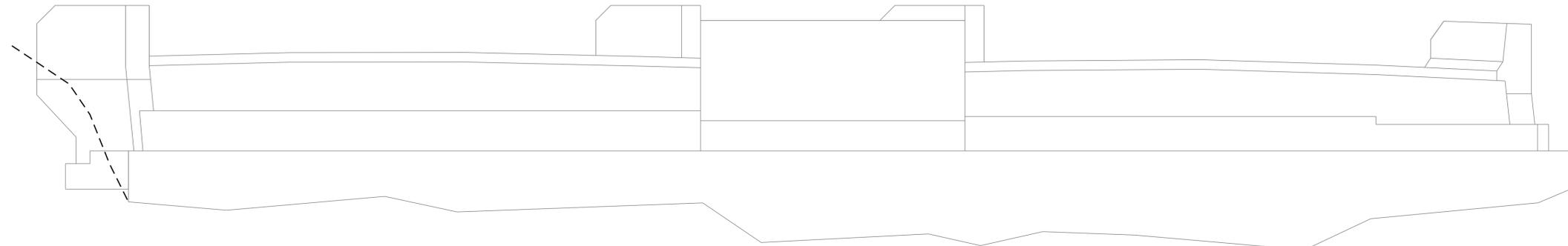
- FOR CONCRETE REPAIR DETAIL SEE SHEET HH-04
- FOR PROCEDURAL NOTES SEE SHEET "GENERAL NOTES"

LEGEND:

-  LIMIT OF CONCRETE SURFACE REPAIRS
-  LIMIT OF EPOXY INJECTED CRACK REPAIR
-  EXISTING CONCRETE
-  EXISTING AND PROPOSED GROUND



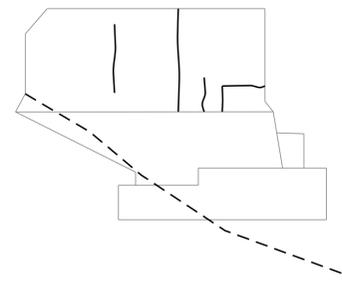
BRIDGE SEAT PLAN



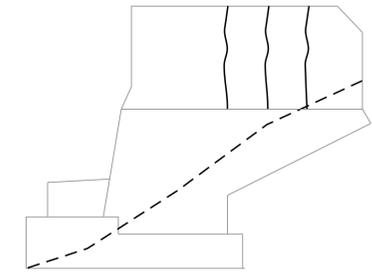
NORTH ABUTMENT ELEVATION

SOUTHBOUND ABUTMENT

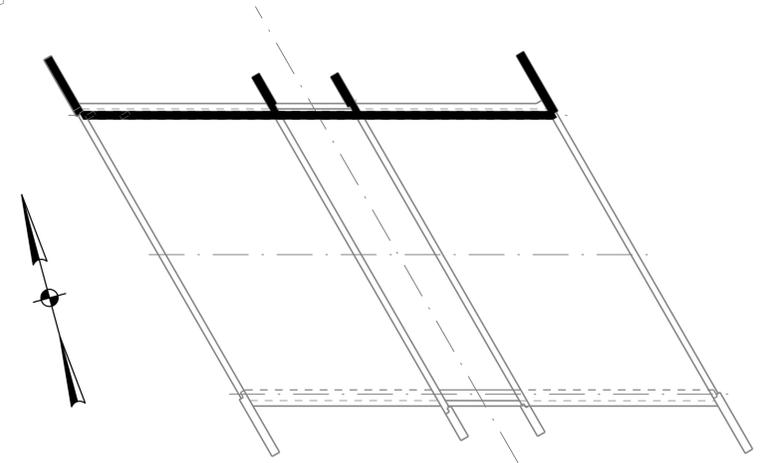
NORTHBOUND ABUTMENT



NORTHWEST WINGWALL ELEVATION



NORTHEAST WINGWALL ELEVATION



LOCATION PLAN

Scale: NOT TO SCALE

No.	Revision	By	Date

Designed by:

TYLIN INTERNATIONAL

CONSULTANT PROJECT MANAGER: Norman L. Baker

	By	Date	Checked	By	Date
Designed	DSM	1/2016		NLB	1/2016
Drawn	PJB	1/2016	In Charge of	NLB	1/2016

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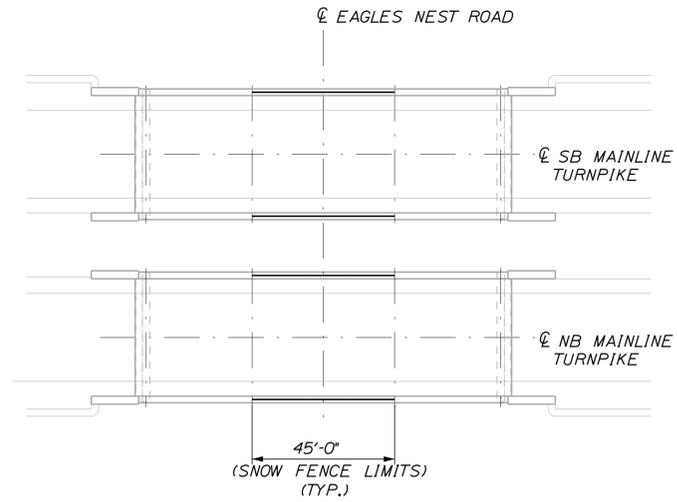
**THE GOLD STAR
MEMORIAL HIGHWAY**

MTA PROJECT MANAGER: R. NORWOOD

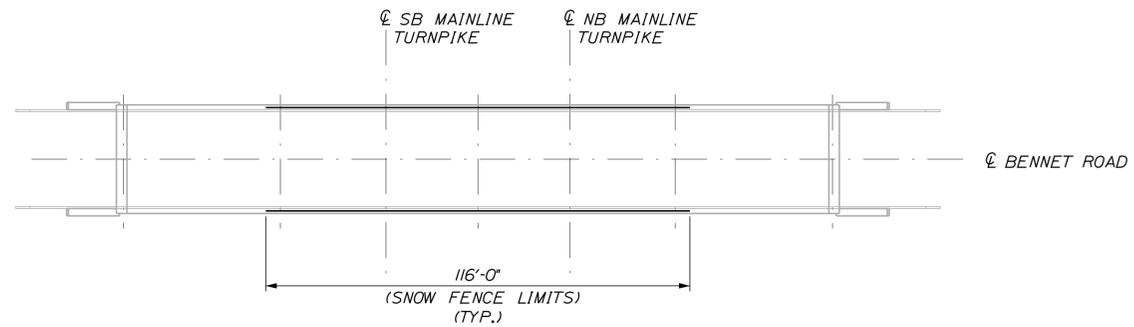
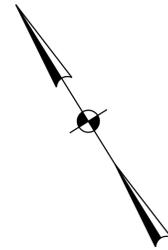
BRIDGE REPAIRS
 2 LOCATIONS (MILE 60.8 & MILE 61.6)
 HUNTS HILL OVERPASS (MILE 61.6)
 SUBSTRUCTURE REPAIR - 2 OF 2

SHEET NUMBER: HH-05
 CONTRACT: 2016.01
 57 OF 77

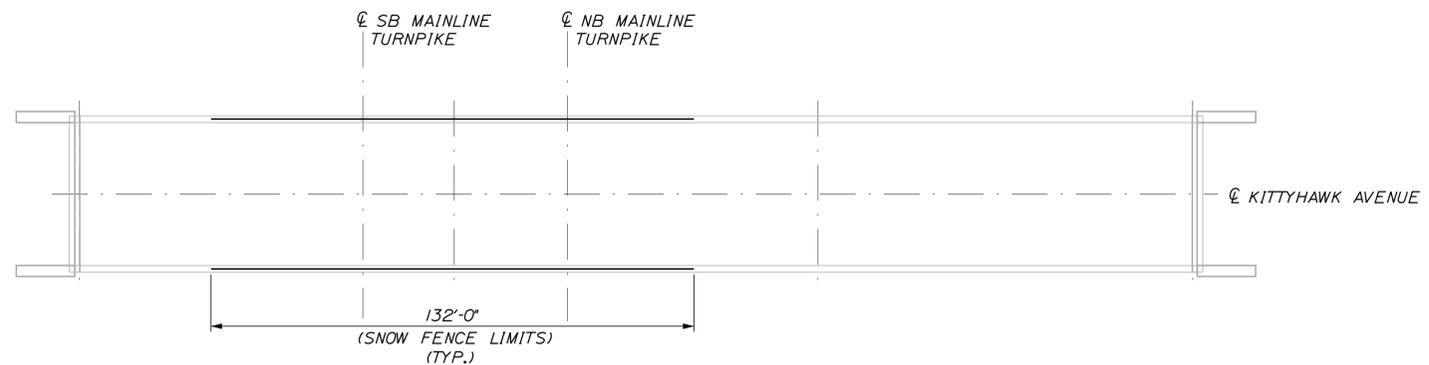
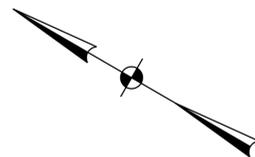
Date: 1/26/2016
Date: 1/26/2016



EAGLE NEST ROAD OVERPASS (MILE 60.8)
1" = 25'



BENNETT ROAD UNDERPASS (MILE 68.6)
1" = 25'



KITTYHAWK AVENUE UNDERPASS (MILE 75.0)
1" = 25'

NOTES:

1. SEE SHEET SF-2 AND SF-3 FOR SNOW FENCE DETAILS.
2. EXACT SNOW FENCE EXTENTS SHALL BE DETERMINED BY THE RESIDENT.

Filename: ... \058_DeckPlan-SnowFence.dgn

Scale:			
AS NOTED			
No.	Revision	By	Date

Designed by:					
TY-LIN INTERNATIONAL					
CONSULTANT PROJECT MANAGER: Norman L. Baker					
	By	Date		By	Date
Designed	DSM	1/2016	Checked	NLB	1/2016
Drawn	PJB	1/2016	In Charge of	NLB	1/2016

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Falmouth, Maine 04105
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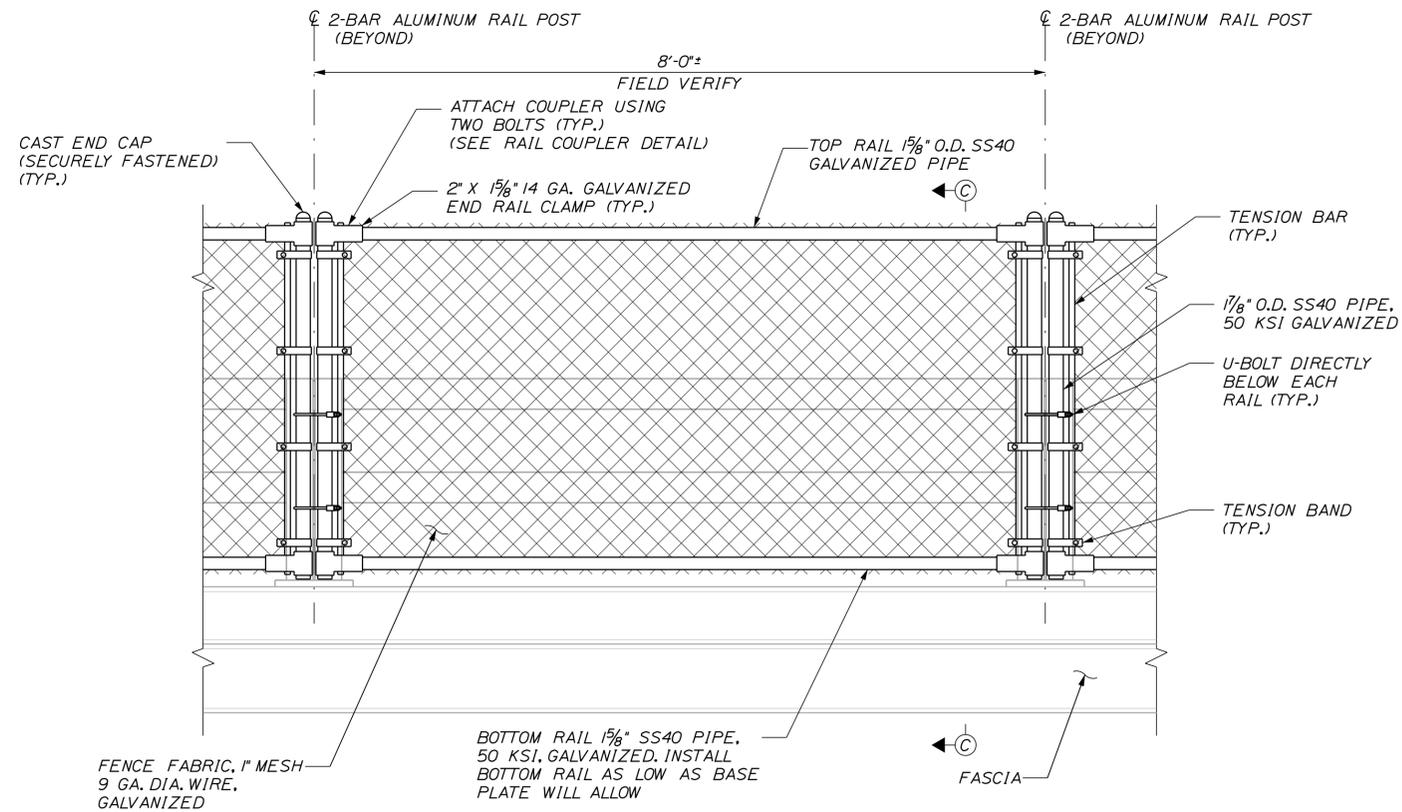
THE GOLD STAR
MEMORIAL HIGHWAY

MTA PROJECT MANAGER: R. NORWOOD

BRIDGE REPAIRS
2 LOCATIONS (MILE 60.8 & MILE 61.6)
SNOW FENCE DETAILS 1

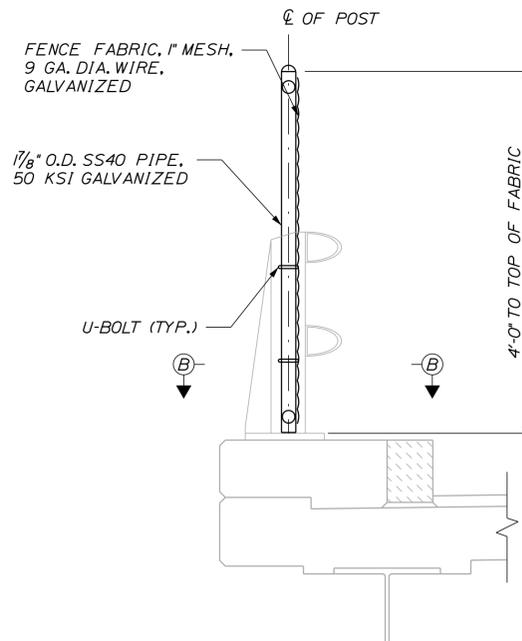
SHEET NUMBER: SF-01
CONTRACT: 2016.01
58 OF 77

Date: 1/26/2016



**SNOW FENCE POST ATTACHMENT
TO 2-BAR ALUMINUM RAIL**
1" = 1'-0"

NOTE:
FENCE SHOWN FROM BACK
OF BRIDGE RAIL.



SECTION C-C
1" = 1'-0"

NOTES:
1. SEE SHEET SF-03 FOR NOTES AND SECTION B-B.

Filename: ... \MSTA\059_SnowFence_Details_1.dgn

Scale:			
AS NOTED			
No.	Revision	By	Date

Designed by:					
TY-LIN INTERNATIONAL					
CONSULTANT PROJECT MANAGER: Norman L. Baker					
	By	Date		By	Date
Designed	DSM	1/2016	Checked	NLB	1/2016
Drawn	PJB	1/2016	In Charge of	NLB	1/2016

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Falmouth, Maine 04105
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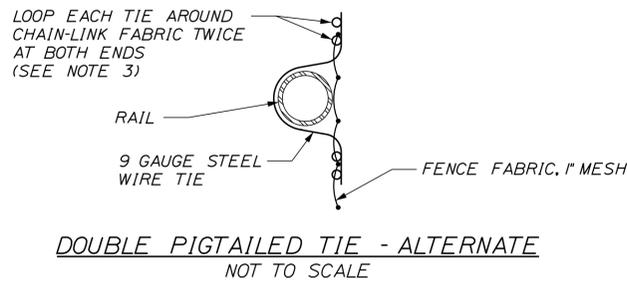
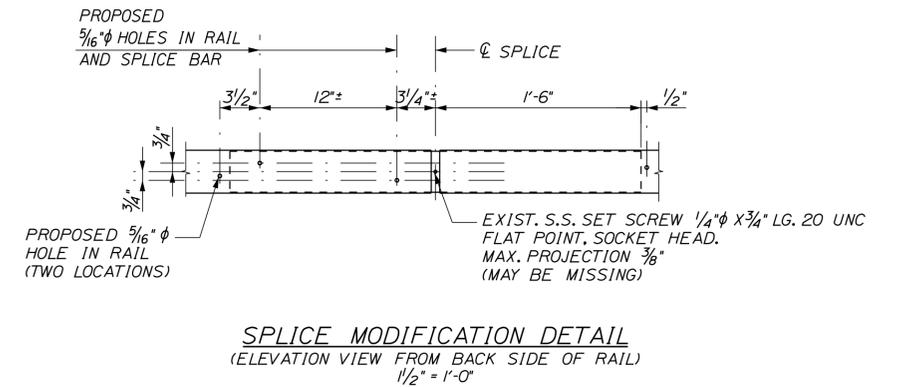
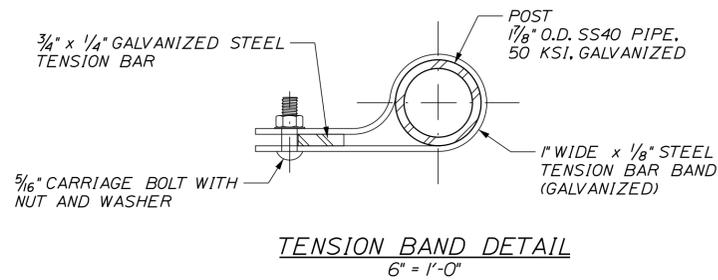
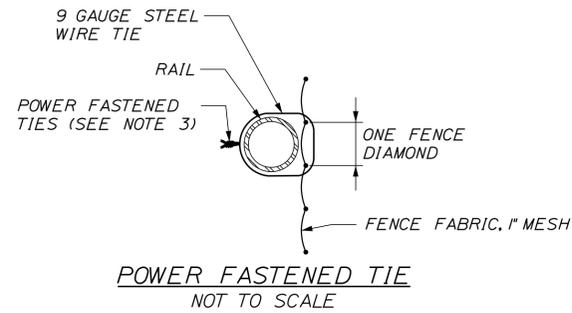
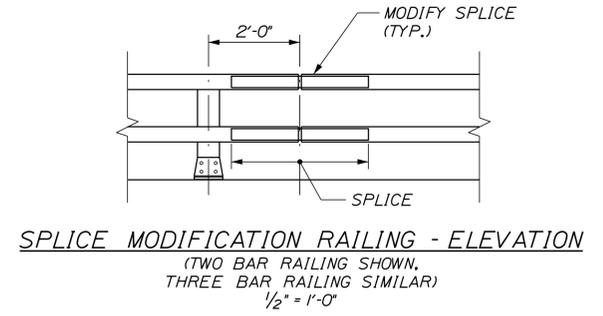
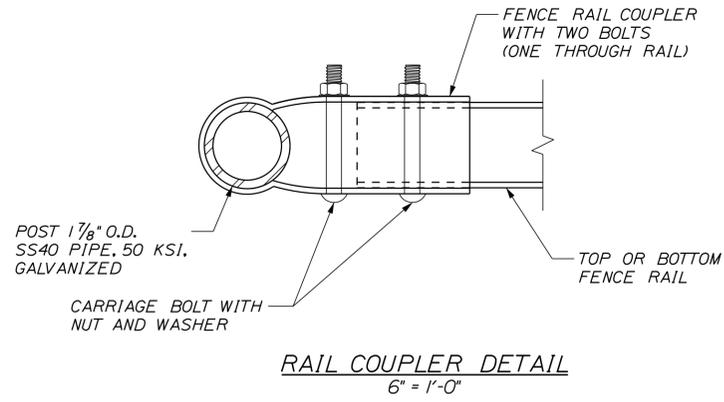
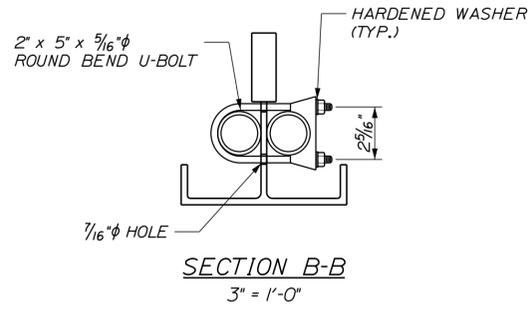


**THE GOLD STAR
MEMORIAL HIGHWAY**

MTA PROJECT MANAGER: R. NORWOOD

BRIDGE REPAIRS
2 LOCATIONS (MILE 60.8 & MILE 61.6)
SNOW FENCE DETAILS II

Date: 1/26/2016



NOTES:

1. INSTALL FENCE FABRIC BETWEEN ALUMINUM BRIDGE RAILS AND SNOW FENCE FRAME.
2. INSTALL CHAIN LINK FENCE FABRIC WITH TWISTED BARBS AT THE BOTTOM RAIL.
3. ROUND WIRE TIES SHALL BE 9 GAUGE ZINC-COATED STEEL PREFORMED TO THE RADIUS OF THE POST AND POWER-FASTENED TO WRAP 360 DEGREES AROUND THE POST AND ONE COMPLETE DIAMOND OF THE CHAIN-LINK FENCE. THE TWO ENDS SHALL BE TWISTED TOGETHER IN A CLOSE HELIX OF 1/2 MACHINE TURNS (3 FULL TWISTS) TIGHTLY AROUND THE POST AND CHAIN-LINK FABRIC. SPACE TIES @ 6" O.C. TO BOTTOM RAIL AND @ 12" O.C. TO ALL POSTS AND OTHER RAILS. TWISTED ENDS SHALL BE BENT DOWN UPON COMPLETION. ALTERNATIVELY, WIRE TIES MAY BE STANDARD ROUND 9 GAUGE ZINC-COATED STEEL. ALL TIES SHALL BE WRAPPED AROUND CHAIN-LINK FABRIC TWICE (DOUBLE PIGTAILED) AT BOTH ENDS. SPACE TIES @ 6" O.C. TO BOTTOM RAIL AND @ 12" O.C. TO ALL POSTS AND OTHER RAILS.
4. ALL BOLTS AND NUTS SHALL BE STEEL CONFORMING TO ASTM A307 AND ASTM A563 GRADE A RESPECTIVELY. WASHERS SHALL BE HARDENED STEEL COMMERCIAL TYPE A PLAIN AND SHALL MEET THE DIMENSIONAL REQUIREMENTS OF ANSI B18.22. ALL HARDWARE SHALL BE HOT-DIP GALVANIZED IN ACCORDANCE WITH AASHTO M111 (ASTM A 123) OR AASHTO M232 (ASTM A153) AS APPLICABLE.
5. SEE SHEET SF-1 FOR SNOW FENCE LIMITS.
6. THE EXISTING RAILING SPLICES ON BENNETT ROAD UNDERPASS AND KITTYHAWK AVENUE UNDERPASS SHALL BE REPAIRED BASED ON THE SPLICE MODIFICATION DETAIL PRESENTED. THIS WORK SHALL BE PAID FOR UNDER ITEM 507.095, "ALUMINUM BRIDGE RAILING - SPLICE MODIFICATION."

Filename: ... \MSTA\060_SnowFence_Details_2.dgn

Scale:			
AS NOTED			
No.	Revision	By	Date

Designed by:					
TYLIN INTERNATIONAL					
CONSULTANT PROJECT MANAGER: Norman L. Baker					
	By	Date		By	Date
Designed	DSM	1/2016	Checked	NLB	1/2016
Drawn	PJB	1/2016	In Charge of	NLB	1/2016

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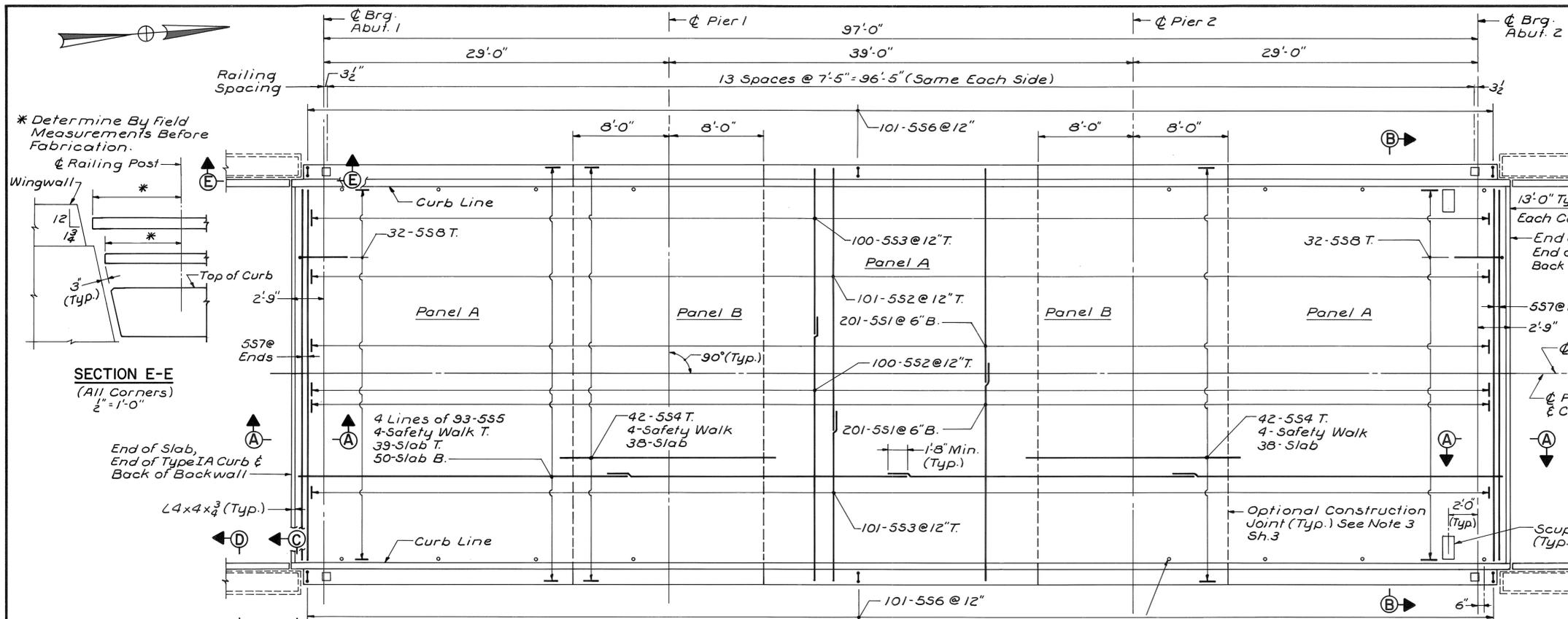


THE GOLD STAR MEMORIAL HIGHWAY

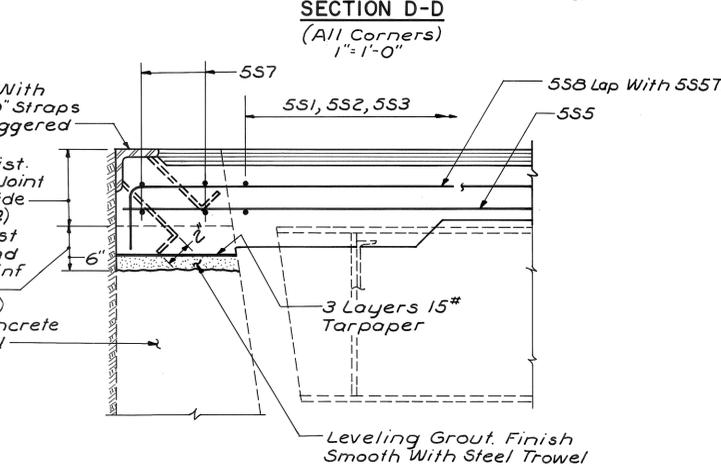
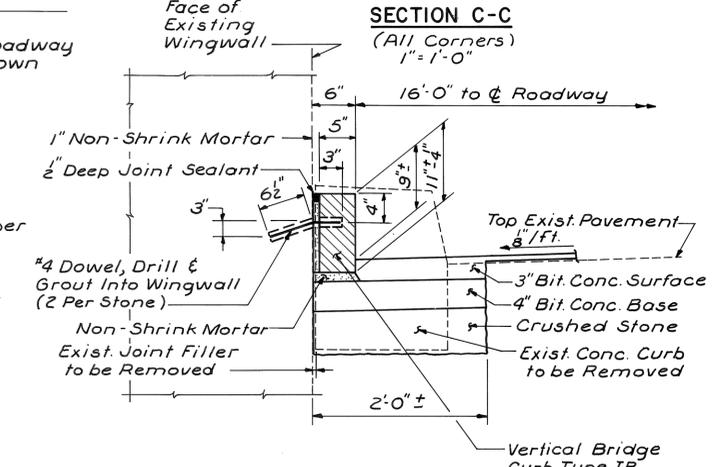
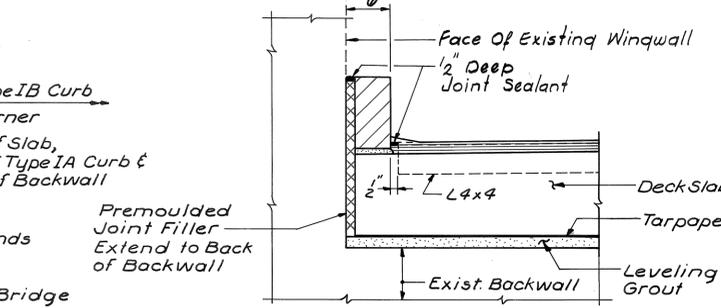
MTA PROJECT MANAGER: R. NORWOOD

BRIDGE REPAIRS
 2 LOCATIONS (MILE 60.8 & MILE 61.6)
 SNOW FENCE DETAILS III

SHEET NUMBER: SF-03
 CONTRACT: 2016.01
 60 OF 77



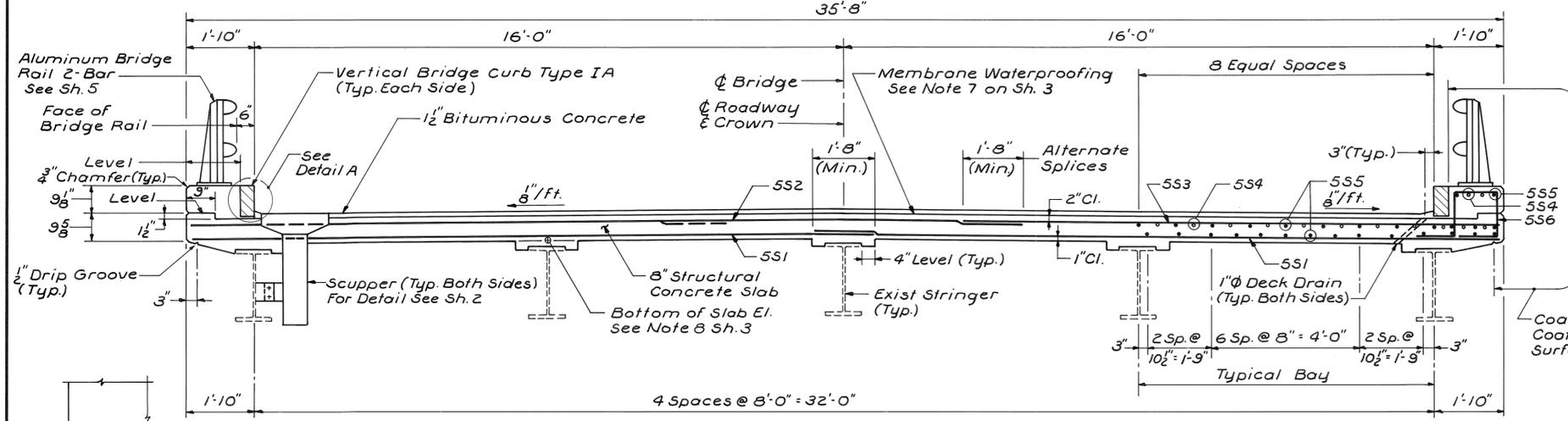
- NOTES**
1. For General Notes & Quantities, See Sh. 2.
 2. For Steel Reinforcing Schedule, See Sh. 6.
 3. All Steel Reinforcing in Deck Slabs & Safety Curbs Shall be Epoxy Coated.
 4. Installation of Terminal Connector And Removal of Concrete Block Shall Be Done On Approach Wingwalls Only Of Both Northbound And Southbound Bridges.



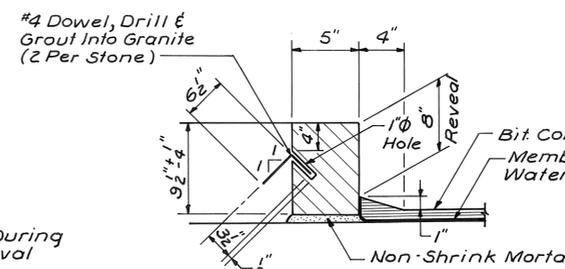
NOTE
Southbound Bridge Shown
Northbound Bridge Identical.

DECK PLAN
3/8" = 1'-0"

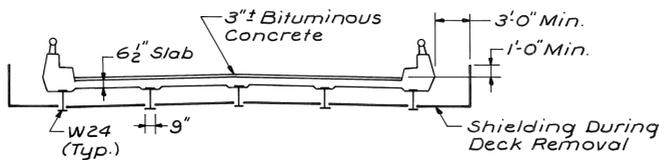
1" ϕ Plastic Deck Drains @ 9'-0" O.C.
As Shown. See Note 6 on Sh. 3



SECTION B-B
1/2" = 1'-0"



DETAIL A
1/2" = 1'-0"



EXISTING DECK REMOVAL
No Scale

V-GROOVE DETAIL
No Scale

NO.	REVISION	BY	DATE	IN CHARGE
		DESIGNED: I.S.	1-87	
		DRAWN: R.D.F.	1-87	
		CHECKED: J.R.B.	1-87	
		BY	DATE	OF J.P.W.

AS-BUILT
SHEET NUMBER: AB-01
61 OF 77

MAINE TURNPIKE AUTHORITY
MAINE TURNPIKE

EAGLE NEST ROAD
DECK REINFORCING-N.B. & S.B. BRIDGES

HOWARD, NEEDLES, TAMMEN & BERGENDOFF ARCHITECTS ENGINEERS PLANNERS
BOSTON

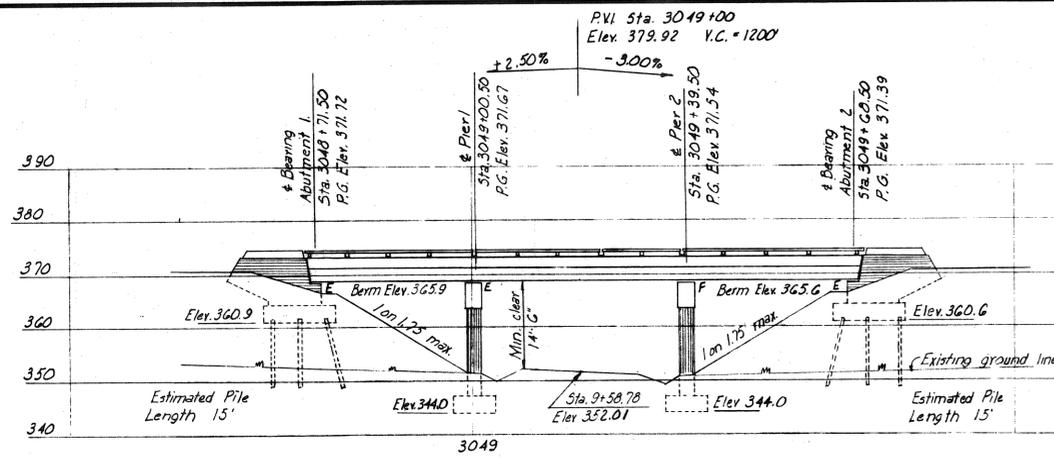
SCALE: AS NOTED
SHEET NO. 4 OF 13

GENERAL NOTES

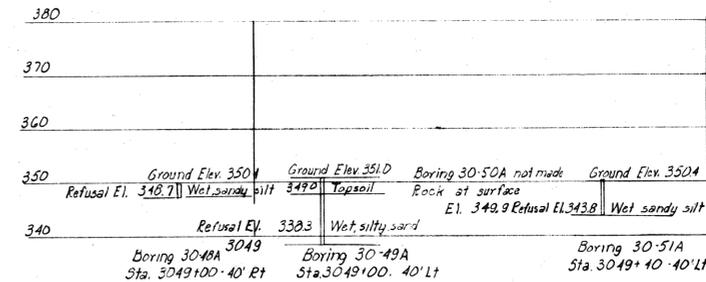
Design Specifications: A.A.S.H.O. (1953) with minor modifications
 Design Live Load: H 20 S 16
 Maximum Pile Load: Abutments - 35.4 Ton/pile
 Maximum Base Pressure: Piers 2.9 Ton/sq.ft.

REFERENCES

Drawing Number	Title	Superstructure				
		Substructure	Steel Deck	Steel I-beam	Steel Floor	Concrete
SD-1A	Standard Abut Details	✓	✓	✓	✓	✓
SD-2	Standard Pier Details	✓	✓	✓	✓	✓
SD-3	Abutment Drainage Details	✓	✓	✓	✓	✓
SD-4	Standard Pile Details	✓	✓	✓	✓	✓
SD-5	Standard Handrail, Bearing Devices and Misc. Details	✓	✓	✓	✓	✓
SD-6	Standard Diaphragm Details	✓	✓	✓	✓	✓
SD-11A	Type "X" and "Y" Expansion Joints	✓	✓	✓	✓	✓
SD-17	Standard Bridge Floor Cross Section, 28'-0" & 30'-0" Roadways	✓	✓	✓	✓	✓



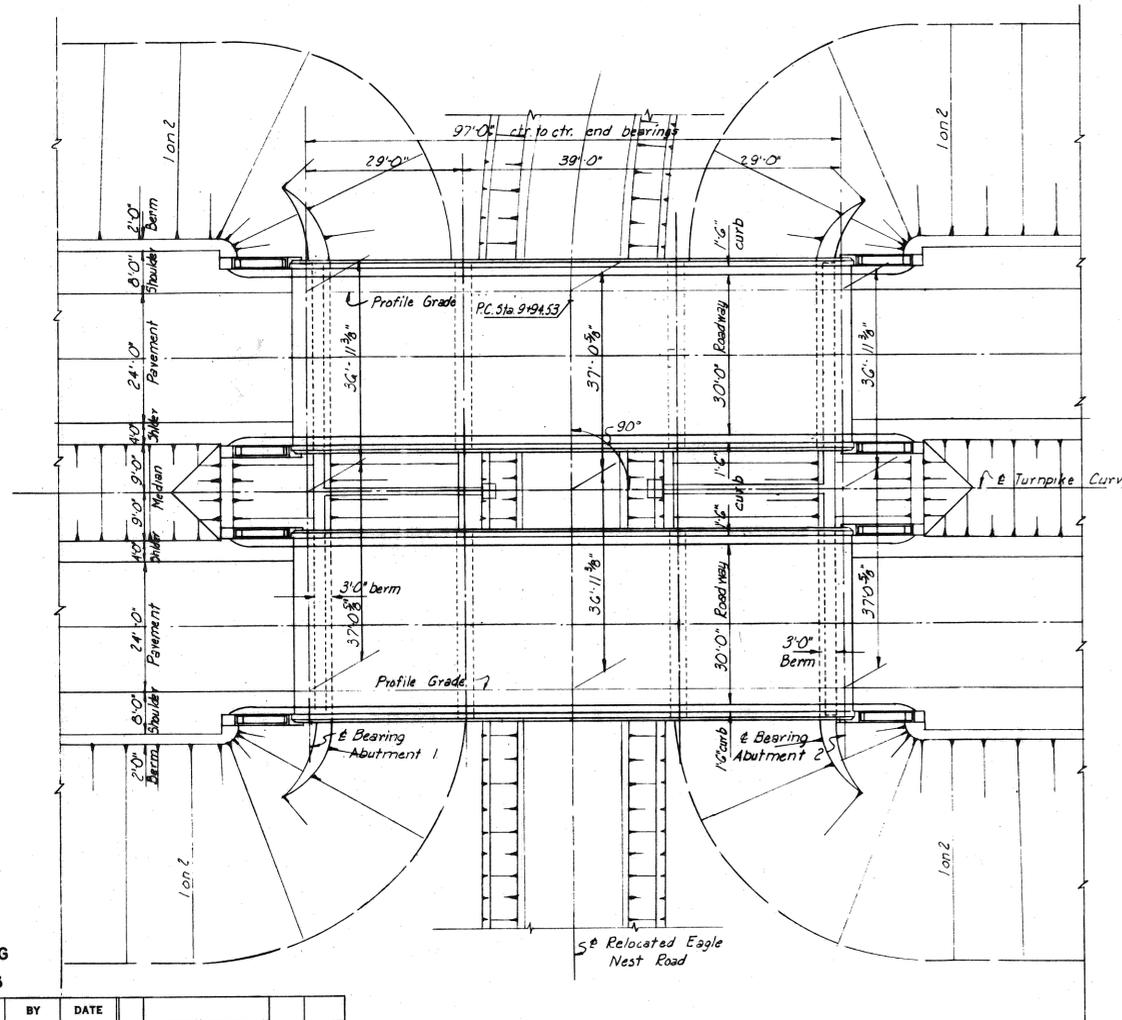
ELEVATION



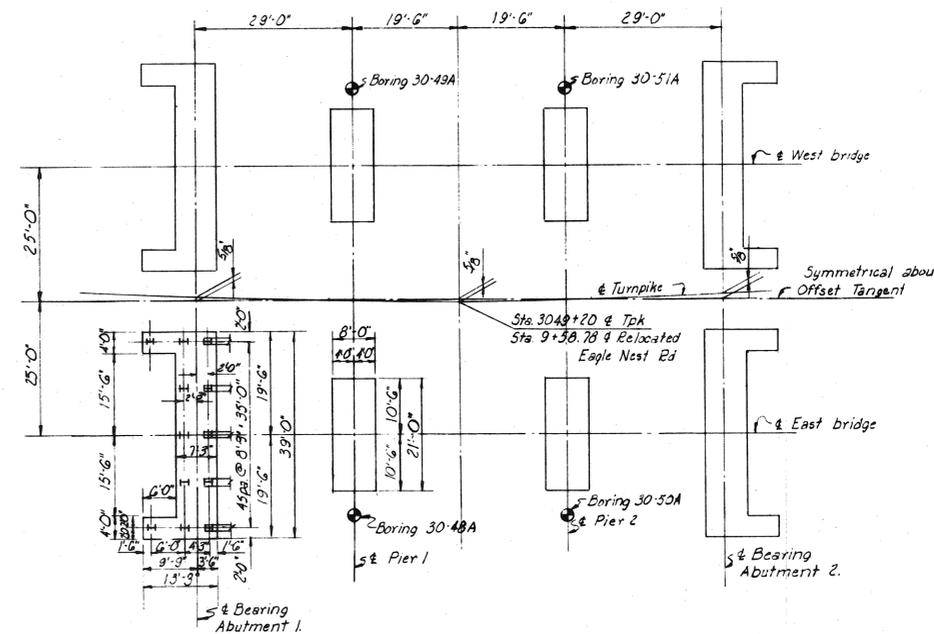
BORING DATA

Turnpike Curve Data
 P.I. Sta. 3067+44.07
 $\Delta = 20^{\circ}00' Lt$
 $D = 0^{\circ}30'$
 $R = 11459.16$
 $T = 2020.56$
 $L = 4000'$
 $E = 176.78$

Relocated Eagle Nest Road Curve Data
 P.I. Sta. 11+28.44
 $\Delta = 31^{\circ}20'00" Rt$
 $D = 12^{\circ}00'$
 $R = 477.47'$
 $T = 133.91'$
 $L = 261.11$
 $E = 18.42$

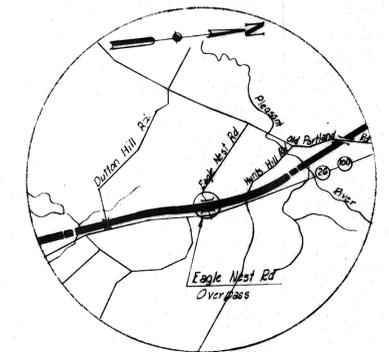


PLAN



FOOTING PLAN

Note: Piles shown battered shall be battered 3/1ft.
 All piles shall be 10BP42
 All pier footings are identical
 Abutment 2 footings are the same as Abutment 1 footings by rotation.



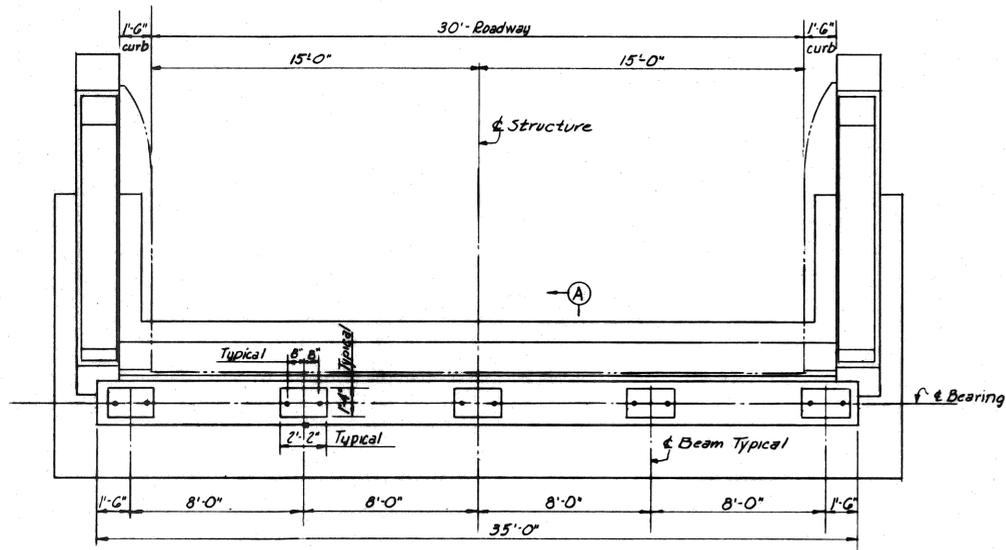
VICINITY MAP
 Scale: 1" = 1 Mile

DRAWING
 35.01.03

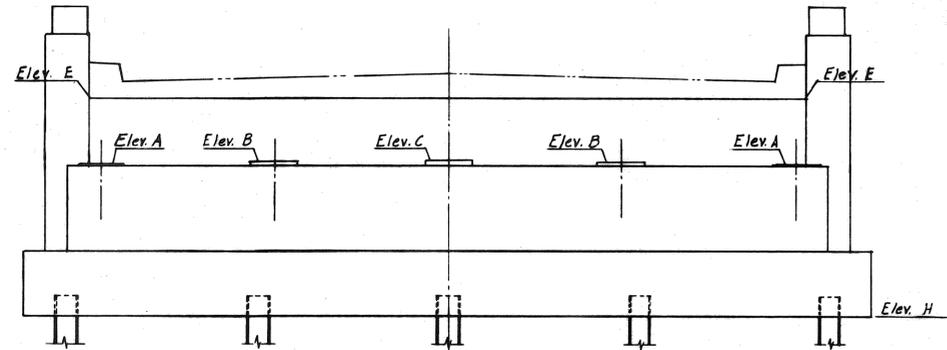
MADE	BY	DATE	NO.	REVISION	BY	DATE
TRACED	B.K.	4-7-54				
CHECKED	H.J.G.	4-8-54	1	As-Built	HBH	1/25/56
IN CHARGE OF	IDSK					

AS-BUILT
 SHEET NUMBER: AB-02
 62 OF 77

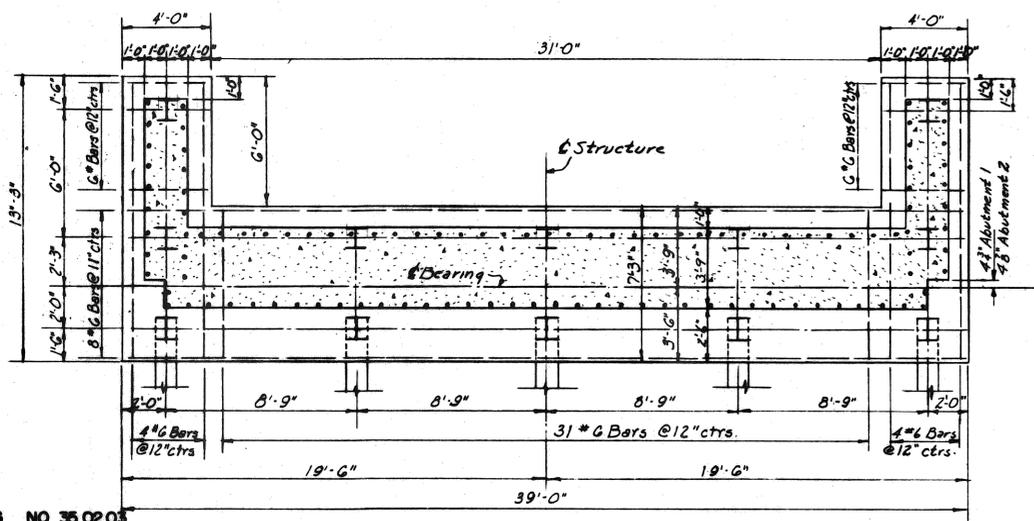
MAINE TURNPIKE AUTHORITY	
SECTION 2 — PORTLAND TO AUGUSTA	
STRUCTURE NO. 35	TURNPIKE OVER
EAGLE NEST ROAD	
STA. 3049 + 20	
GENERAL PLAN AND ELEVATION	
HOWARD, NEEDLES, TAMMEN & BERGENDOFF	SCALE: 1/16" = 1'-0" except as shown
CONSULTING ENGINEERS	CONTRACT NO.
NEW YORK KANSAS CITY	SHEET NO. 164 OF 382



PLAN

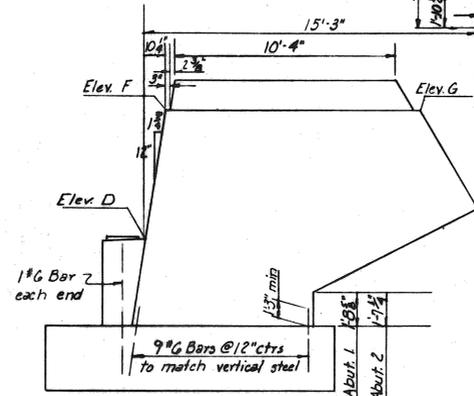


ELEVATION

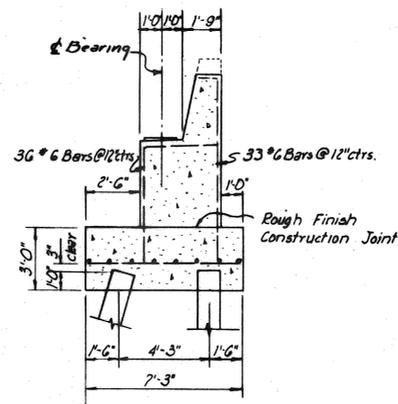


FOOTING PLAN
ABUTMENTS

ELEVATIONS	
Abut. 1	Abut. 2
A	368.10
B	368.15
C	368.23
D	368.02
E	370.94
F	373.86
G	373.87
H	360.90

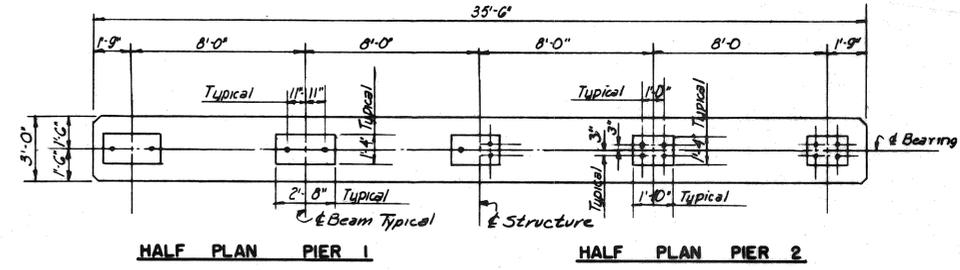


END ELEVATION



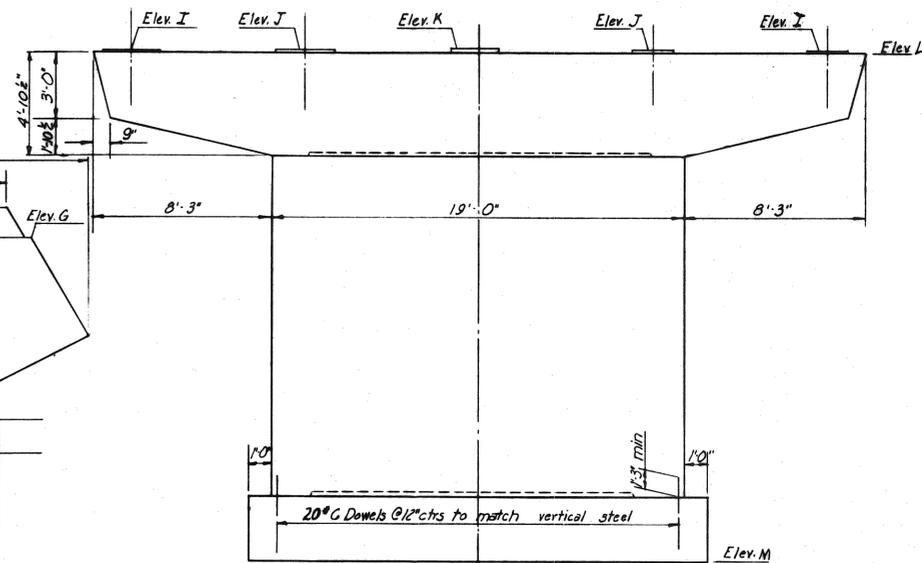
SECTION A-A

Note: Piles shall be 10BP42
Battered piles shall be
battered 3'/ft.

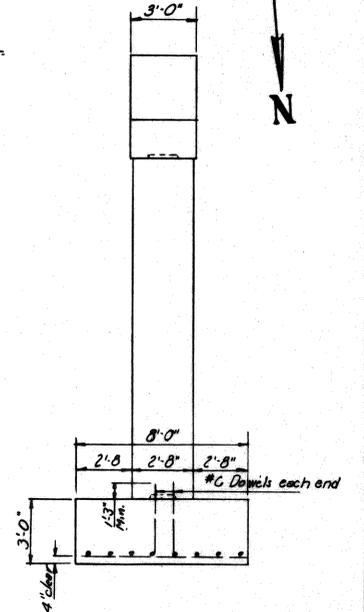


HALF PLAN PIER 1

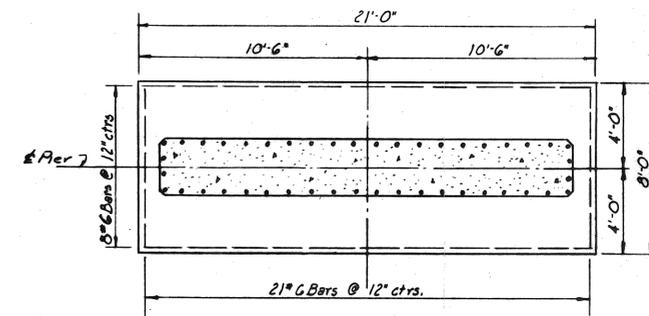
HALF PLAN PIER 2



ELEVATION



END ELEVATION



FOOTING PLAN

ELEVATION	
Pier 1	Pier 2
I	362.88
J	362.90
K	362.98
L	362.86
M	344.00

PIERS

AS-BUILT
SHEET NUMBER: AB-03
63 OF 77

DRAWING NO. 35.02.03

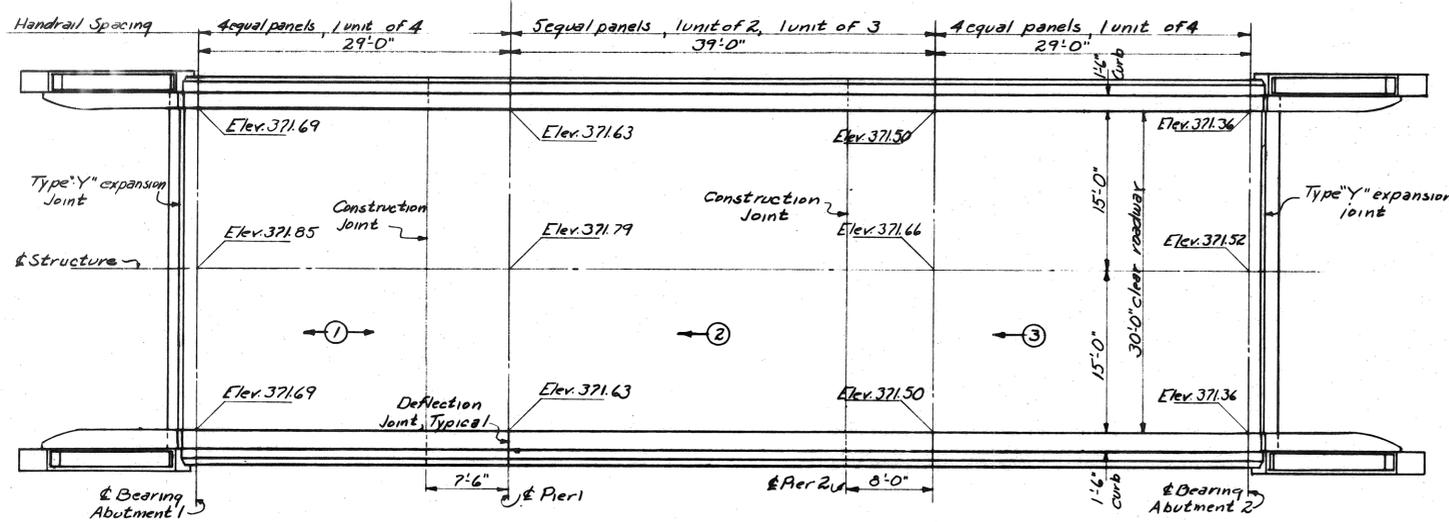
BY	DATE	REVISION	BY	DATE
MADE	B.K.	4-1-54		
TRACED				
CHECKED	R.S.G.	4-25-54	As-Built	NON/ARSL
IN CHARGE OF	I.D.S.K.			

MAINE TURNPIKE AUTHORITY
MAINE TURNPIKE
 SECTION 2 — PORTLAND TO AUGUSTA

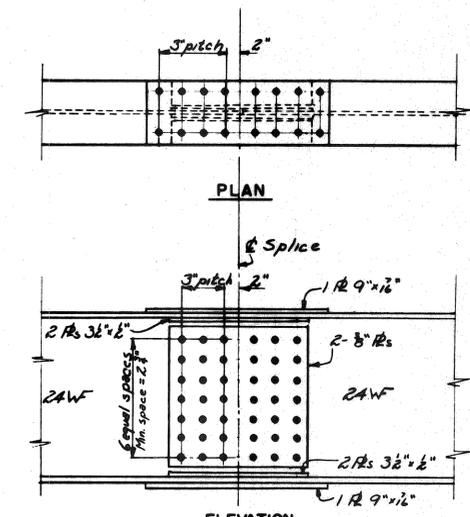
STRUCTURE NO. 35 TURNPIKE OVER
 EAGLE NEST ROAD
 STA. 3049 + 20
ABUTMENTS AND PIERS

HOWARD, NEEDLES, TAMMEN & BERGENDOFF CONSULTING ENGINEERS
 NEW YORK KANSAS CITY

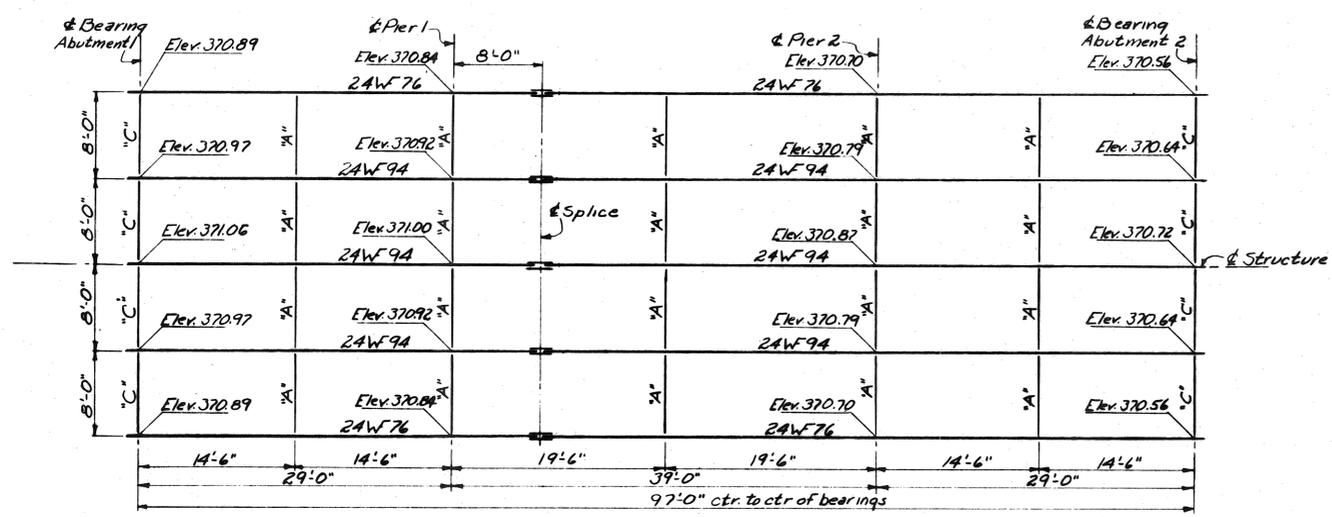
SCALE: 1/4" = 1'-0"
 CONTRACT NO. _____
 SHEET NO. 65 OF 322



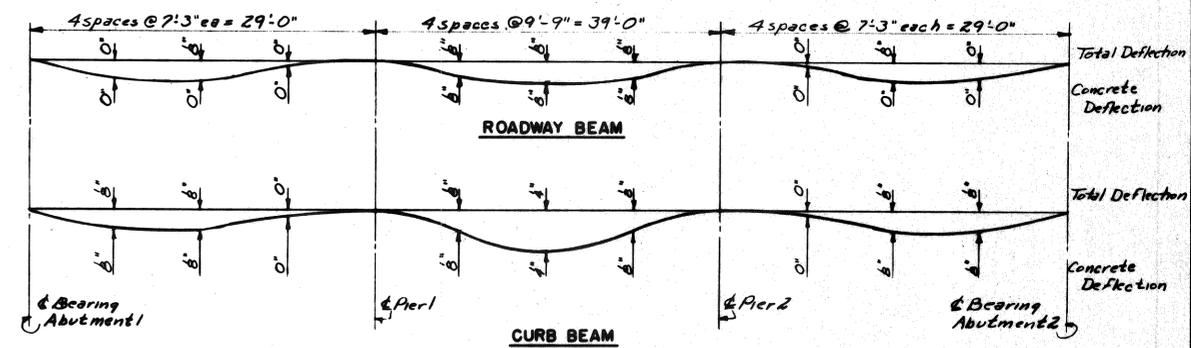
PLAN
Scale: 6"=1'-0"



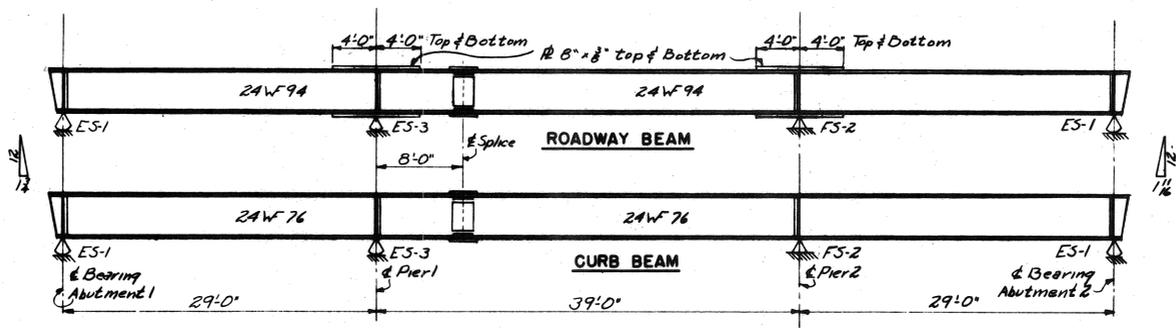
TYPICAL BEAM SPLICE
Scale: 1"=1'-0"



FRAMING PLAN
Scale: 6"=1'-0"



DEAD LOAD DEFLECTION DIAGRAMS
No Scale



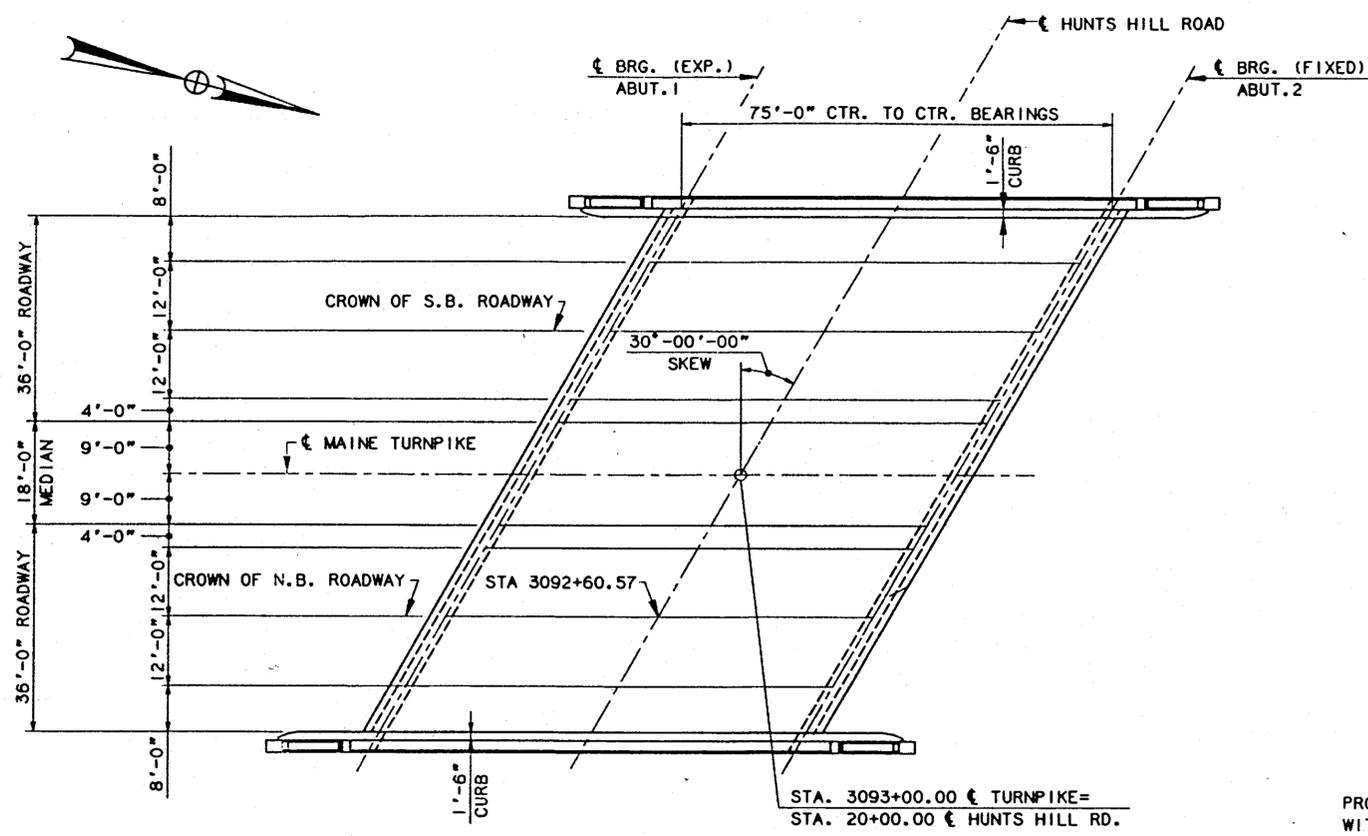
BEAM ELEVATIONS
No Scale

Notes:
Elevations shown on floor plan are to top of finished roadway.
Elevations shown on framing plan are to top of Beam Flanges.
Slab is 6" non-composite design with 2" bituminous wearing surface as shown on Standard Drawing No. 17.
Sequence and direction of placing concrete are noted thus ->.
Marks "A" and "X" on framing plan represent Standard Diaphragms as shown on Standard Drawing No. 6.
Bevels at beam ends are with respect to beam axis.
All bearing stiffeners are 7x4x8/16.

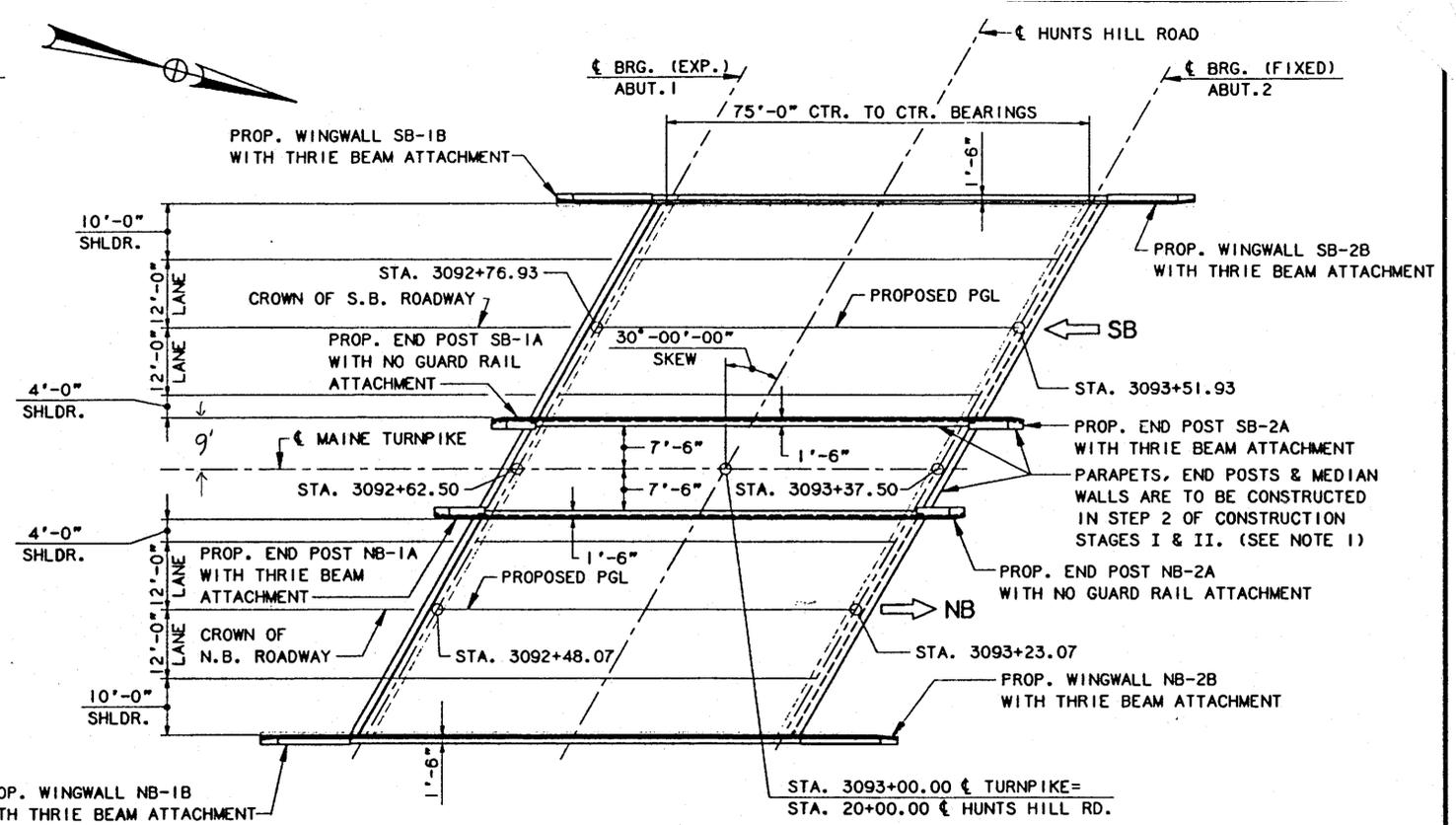
AS-BUILT
SHEET NUMBER: AB-04
64 OF 77

DRAWING NO. 35.03.03				
BY	DATE			
MADE	RSG	4-5-54		
TRACED				
CHECKED	H.J.G.	4-6-54	1	As-Built
IN CHARGE OF	IDSK	No.	REVISION	BY DATE

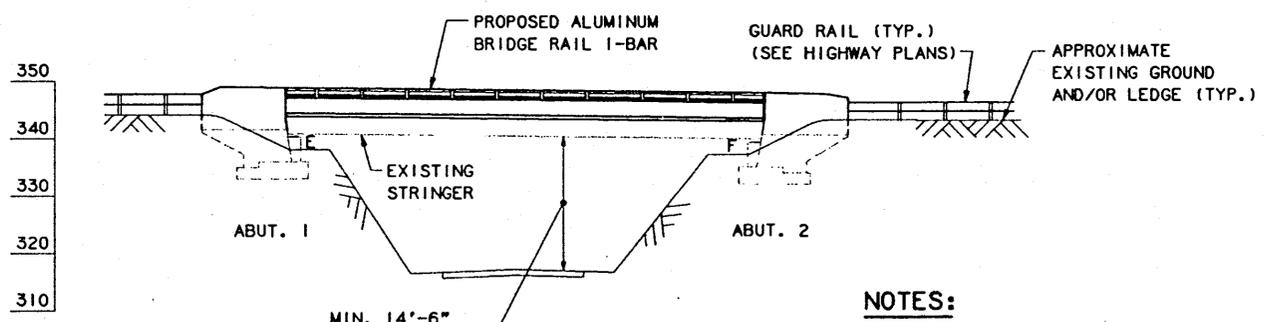
MAINE TURNPIKE AUTHORITY
MAINE TURNPIKE
 SECTION 2 — PORTLAND TO AUGUSTA
 STRUCTURE NO. 35 TURNPIKE OVER
EAGLE NEST ROAD
 STA. 3049+20.00
SUPERSTRUCTURE
 HOWARD, NEEDLES, TAMMEN & BERGENDOFF SCALE: As Shown
 CONSULTING ENGINEERS CONTRACT NO. _____
 NEW YORK KANSAS CITY SHEET NO. 166 OF 382



PLAN - EXISTING
1/16" = 1'-0"



PLAN - PROPOSED
1/16" = 1'-0"

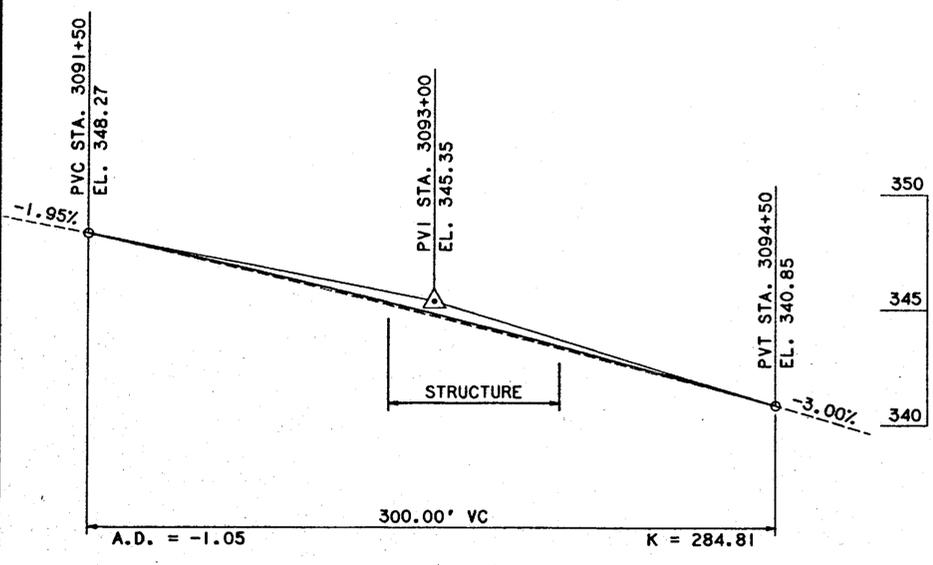


ELEVATION - PROPOSED
1/16" = 1'-0"

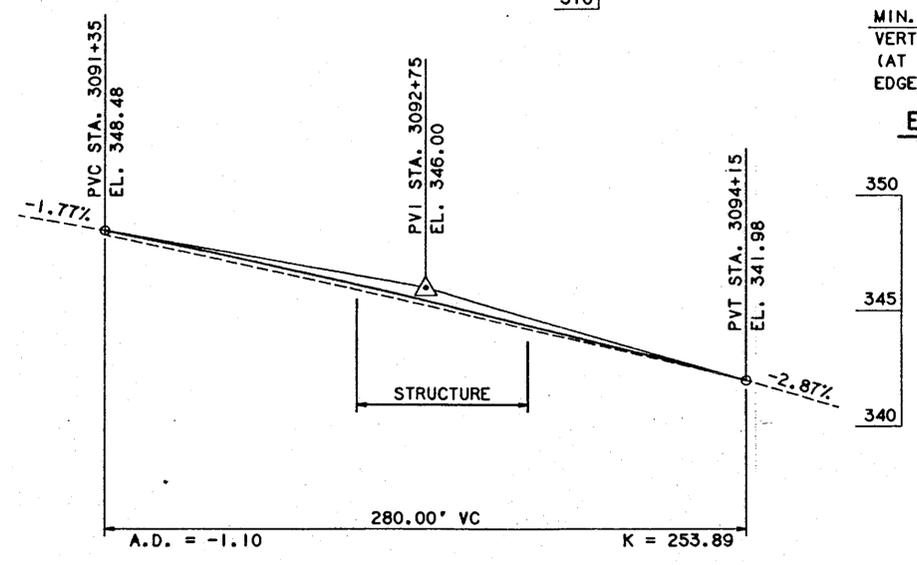
NOTES:

1. FOR EXISTING AND PROPOSED CROSS SECTIONS AND SEQUENCE OF CONSTRUCTION, SEE SHEET NOS. HH-3 & HH-4 RESPECTIVELY.
2. FOR MEDIAN WALL DETAILS, SEE SHEET NO. HH-7.
3. FOR END POST DETAILS, SEE SHEET NOS. HH-11 AND HH-12.
4. FOR ABUTMENT DETAILS, SEE SHEET NO. HH-7.
5. FOR DECK SECTIONS, SEE SHEET NOS. HH-15 AND HH-16.
6. FOR WINGWALL MODIFICATIONS, SEE SHEET NOS. HH-8, HH-9 AND HH-10.

AS-BUILT
SHEET NUMBER: AB-05
65 OF 77



PROPOSED SOUTHBOUND PROFILE
HORIZ. 1" = 40'-0"
VERT. 1" = 4'-0"

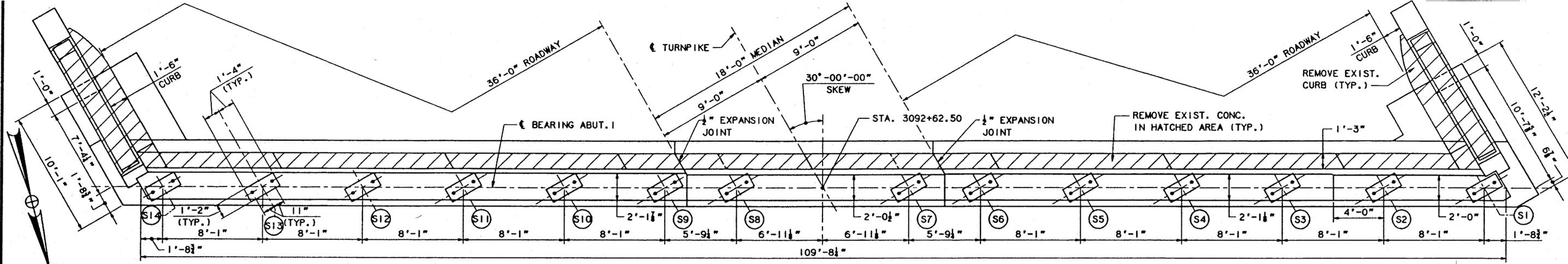


PROPOSED NORTHBOUND PROFILE
HORIZ. 1" = 40'-0"
VERT. 1" = 4'-0"

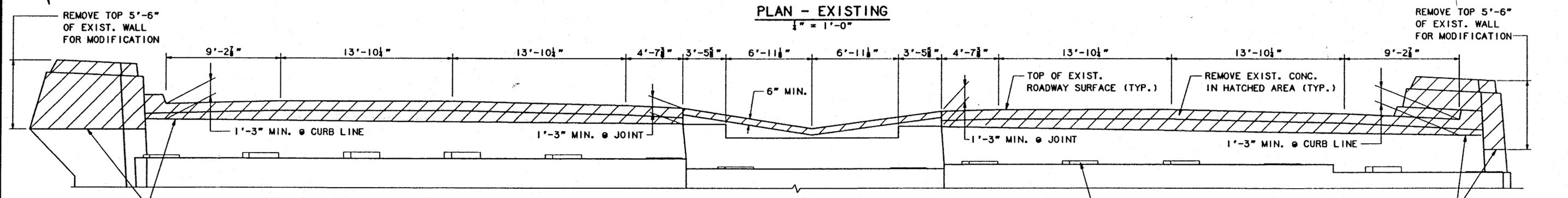
Maine Turnpike Authority
Maine Turnpike
HUNTS HILL ROAD OVERPASS
GENERAL PLAN AND ELEVATION

CINTB
ARCHITECTS ENGINEERS PLANNERS
Contract 99.1
Sheet No. HH-2
20 of 50

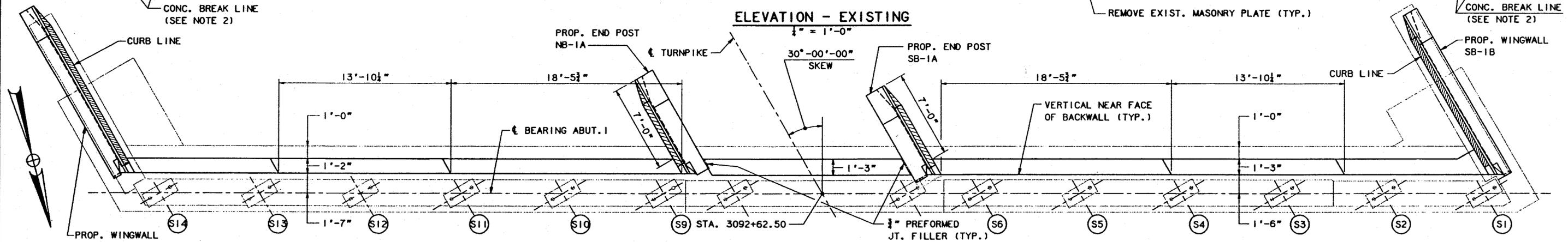
No.	Revision	By:	Date:	In Charge of:
		Designed	AD 12/98	
		Drawn	LS 12/98	
		Checked	PMK 12/98	
				RAL



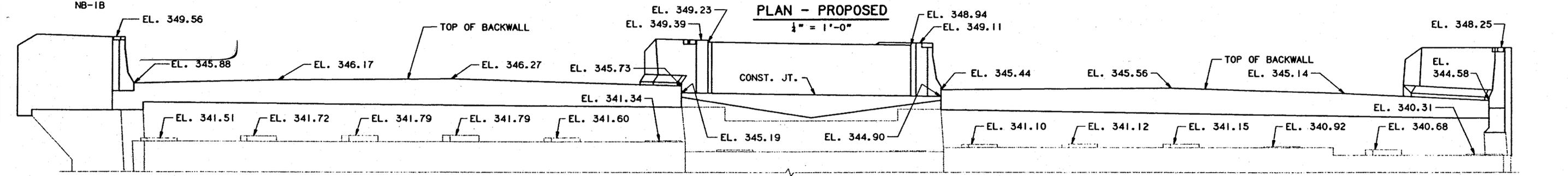
PLAN - EXISTING
1/4" = 1'-0"



ELEVATION - EXISTING
1/4" = 1'-0"



PLAN - PROPOSED
1/4" = 1'-0"



ELEVATION - PROPOSED
1/4" = 1'-0"

NOTES

1. TOP OF BACKWALL ELEVATIONS SHOWN ARE AT NEAR FACE OF BACKWALL.
2. SAW CUT 1" MIN. DEEP BEFORE REMOVING EXISTING CONCRETE.
3. THE COST FOR PREFORMED JOINT FILLER SHALL BE INCLUDED IN ITEM 502.21, STRUCTURAL CONCRETE, ABUTMENTS AND RETAINING WALLS.
4. REMOVAL OF THE EXISTING ARMOR JOINT AT THE TOP OF BACKWALL SHALL BE INCIDENTAL TO ITEM 202.122.
5. SEE SHEET NOS. HH-18, HH-19 AND HH-20 FOR ABUTMENT I EXPANSION JOINT DETAILS.

6. ALL EXISTING WEEP HOLES SHALL BE CLEANED OF DEBRIS AND TO THE SATISFACTION OF THE ENGINEER. PAYMENT SHALL BE INCIDENTAL TO ITEM 202.122.
7. ALL EXISTING CONCRETE BEARING PADS, EXCEPT AT S7 AND S8, SHALL BE DEMOLISHED AND REBUILT TO THE EXISTING SURVEY ELEVATIONS, AS SHOWN, AND TO THE SATISFACTION OF THE ENGINEER. PAYMENT SHALL BE INCIDENTAL TO ITEM 523.30. SEE DETAIL - CONCRETE BEARING PAD ON SHEET NO. HH-7.

AS-BUILT
SHEET NUMBER: AB-06
66 OF 77

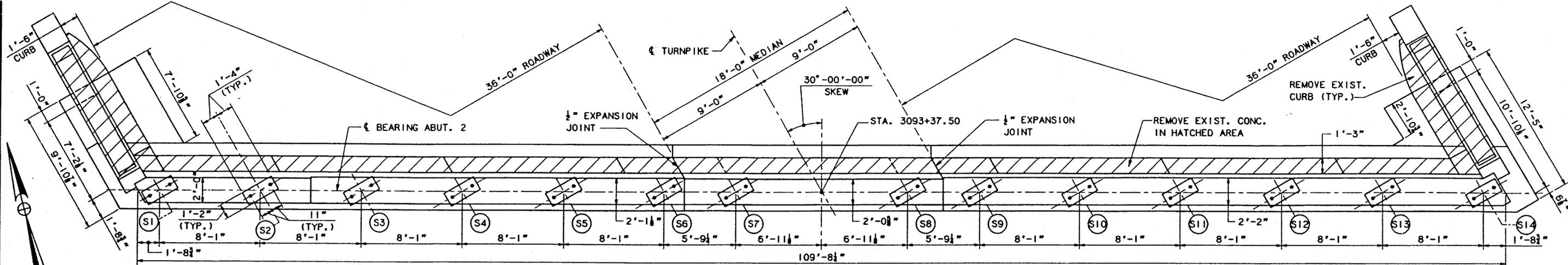
No.	Revision	By:	Date:	in charge of:
		Designed	AD 12/98	
		Drawn	LS 12/98	
		Checked	PMK 12/98	
				RAL

Maine Turnpike Authority
Maine Turnpike

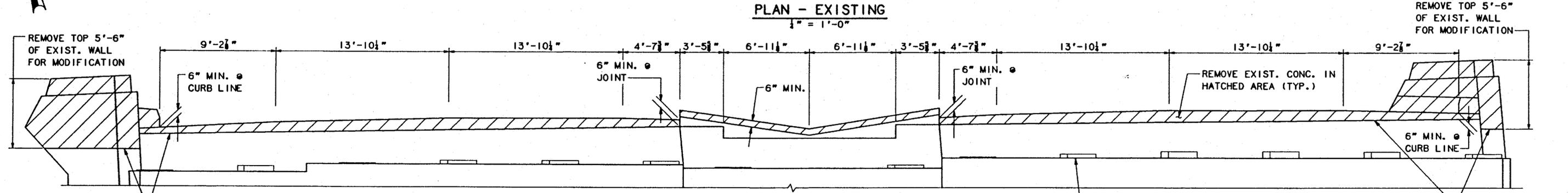
HUNTS HILL ROAD OVERPASS
ABUTMENT I
PLAN & ELEVATION

Transpass
ARCHITECTS ENGINEERS PLANNERS

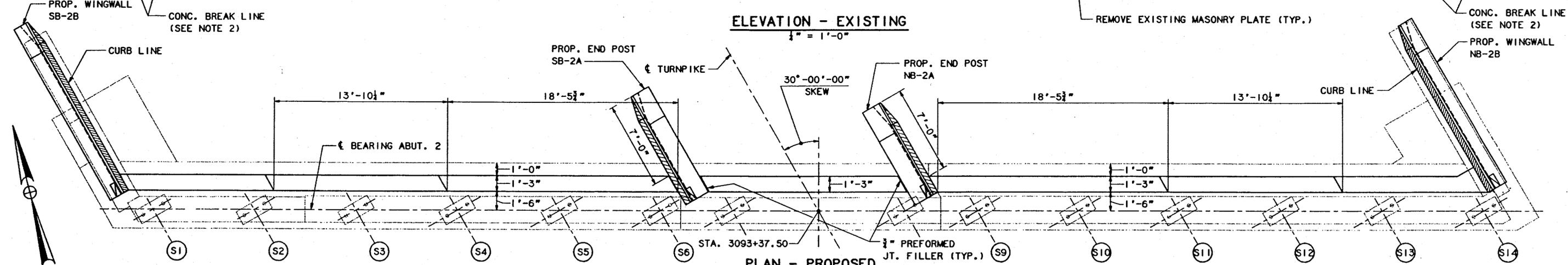
Contract 99.1
Sheet No. HH-5
23 of 50



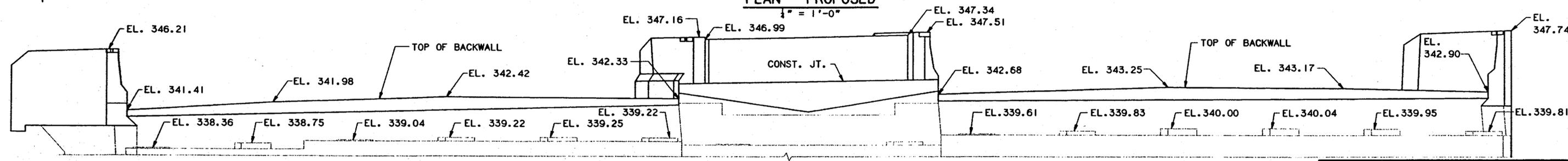
PLAN - EXISTING
1/4" = 1'-0"



ELEVATION - EXISTING
1/4" = 1'-0"



PLAN - PROPOSED
1/4" = 1'-0"



ELEVATION - PROPOSED
1/4" = 1'-0"

NOTES

1. TOP OF BACKWALL ELEVATIONS SHOWN ARE AT NEAR FACE OF BACKWALL.
2. SAW CUT 1" MIN. DEEP BEFORE REMOVING EXISTING CONCRETE.
3. THE COST FOR PREFORMED JOINT FILLER SHALL BE INCLUDED IN ITEM 502.21, STRUCTURAL CONCRETE, ABUTMENTS AND RETAINING WALLS.
4. ALL EXISTING WEEP HOLES SHALL BE CLEANED OF DEBRIS AND TO THE SATISFACTION OF ENGINEER. PAYMENT SHALL BE INCIDENTAL TO ITEM 202.122.

5. ALL EXISTING CONCRETE BEARING PADS, EXCEPT AT S7 AND S8, SHALL BE DEMOLISHED AND REBUILT TO THE EXISTING SURVEY ELEVATIONS, AS SHOWN, AND TO THE SATISFACTION OF THE ENGINEER. PAYMENT SHALL BE INCIDENTAL TO ITEM 523.30. SEE DETAIL - CONCRETE BEARING PAD ON SHEET NO. HH-7.

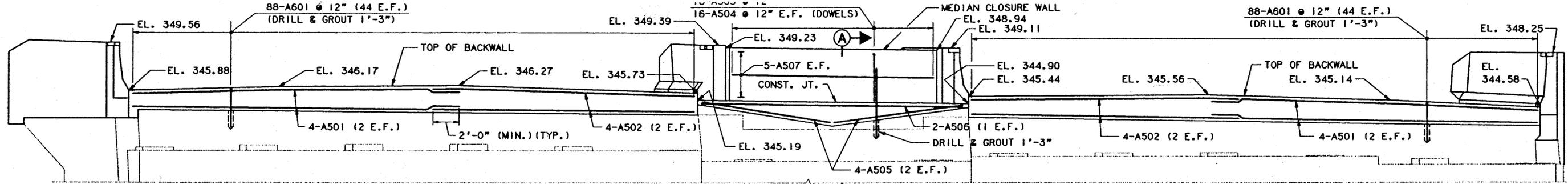
Maine Turnpike Authority
Maine Turnpike

HUNTS HILL ROAD OVERPASS
ABUTMENT 2
PLAN & ELEVATION

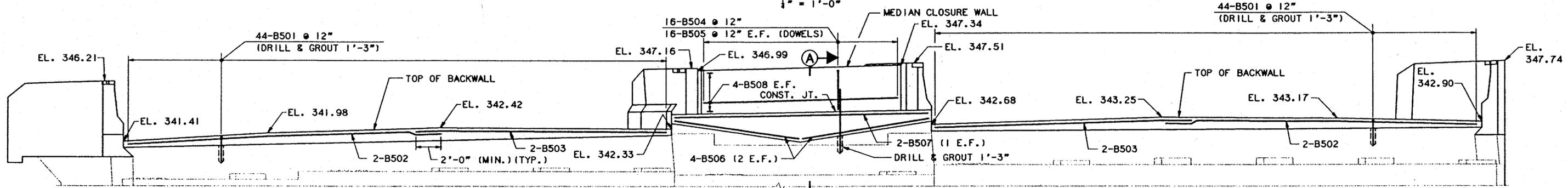
Transpass
ARCHITECTS ENGINEERS PLANNERS

Contract 99.1 Sheet No. HH-6
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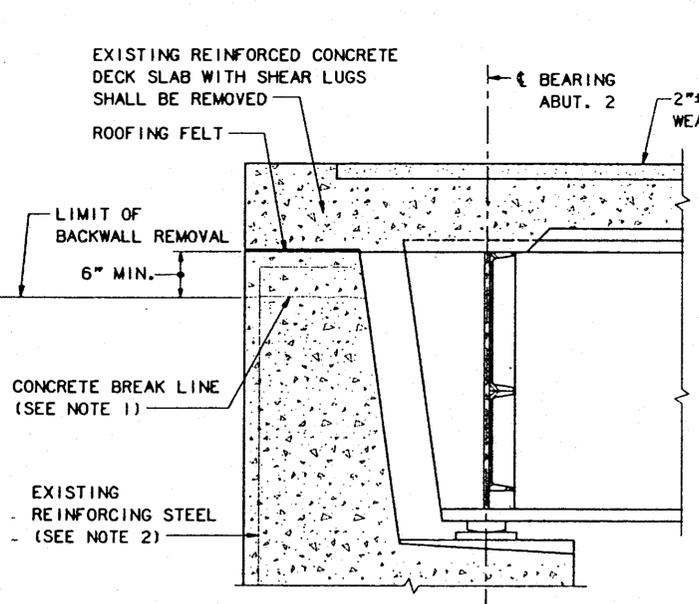
	By: Date:
	Designed AD 12/98
	Drawn LS 12/98
	Checked PMK 12/98
No. Revision By: Date: in charge of:	RAL



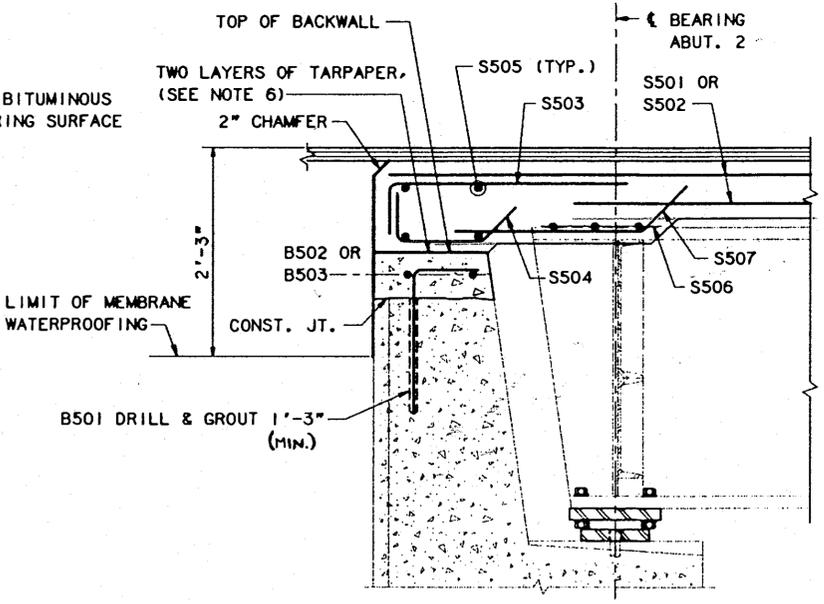
ELEVATION - PROPOSED ABUTMENT 1
1/4" = 1'-0"



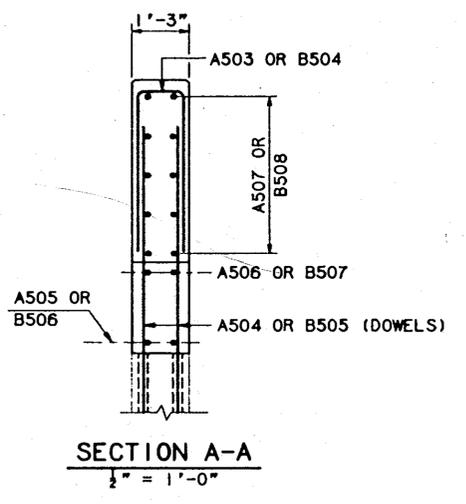
ELEVATION - PROPOSED ABUTMENT 2
1/4" = 1'-0"



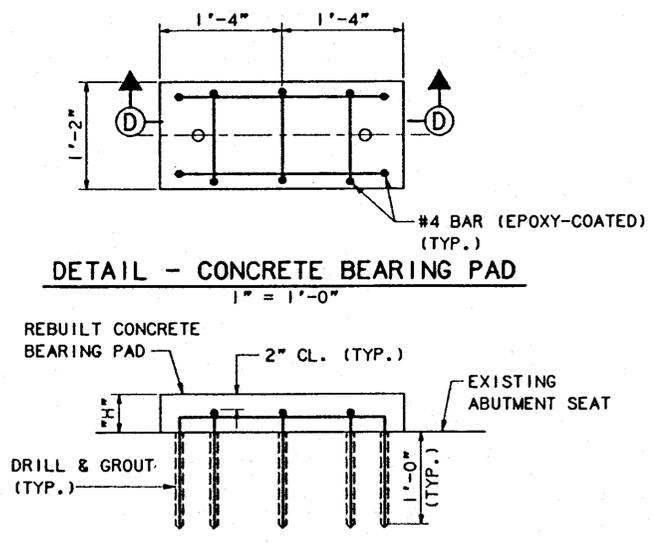
EXISTING SECTION AT ABUTMENT 2
1" = 1'-0"



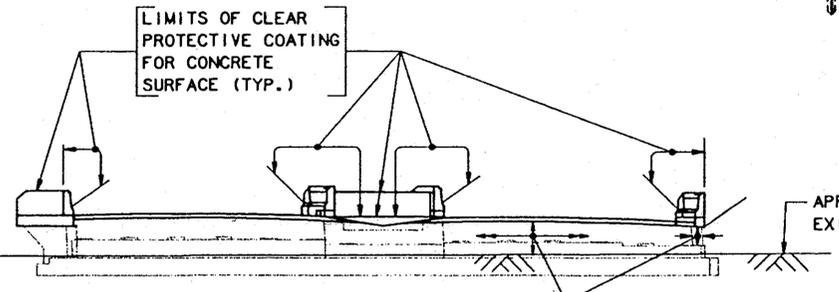
SECTION C-C
1" = 1'-0"



SECTION A-A
1/4" = 1'-0"



DETAIL - CONCRETE BEARING PAD
1" = 1'-0"



LIMITS OF CONCRETE PROTECTIVE COATING
AT ABUTMENTS, WINGWALLS AND END POSTS
1/16" = 1'-0"

NOTES:

- SAW CUT 1" MIN. BEFORE REMOVING EXISTING CONCRETE BACKWALL AND REINFORCING STEEL TO THE INDICATED LIMIT. RECONSTRUCT ABUTMENT BACKWALL TO ELEVATIONS INDICATED ON SHEETS HH-5 & HH-6 RESPECTIVELY, PRIOR TO FORMING NEW DECK SLAB ON TOP OF BACKWALL.
- ADDITIONAL EXISTING REINFORCING STEEL, NOT SHOWN, MAY EXIST IN THE ABUTMENT BACKWALL. THE SECTION PROVIDED IS ONLY FOR GENERAL PURPOSES.
- FOR LOCATION OF SECTION C-C, SEE SHEET NOS. HH-15 AND HH-16.
- PREPARE THE EXISTING BACKWALL SURFACE TO BE SOUND, CLEAN AND FREE OF BOND INHIBITING MATERIALS. ANY CONCRETE AREAS FOUND TO BE UNSOUND, SHALL BE RE-CHIPPED BY THE CONTRACTOR, UNTIL APPROVED BY THE ENGINEER. (PAYMENT SHALL BE INCIDENTAL TO ITEM NO. 502.21)
- APPLY A BONDING AGENT TO THE EXISTING BACKWALL SURFACE ONCE THE REMOVAL LIMIT IS APPROVED OF BY THE ENGINEER. THE MANUFACTURER'S INSTRUCTIONS, FOR THE BONDING AGENT SELECTED, MUST BE FOLLOWED AND APPROVED OF BY THE ENGINEER. (PAYMENT SHALL BE INCIDENTAL TO ITEM 502.21)
- THE TOP OF BACKWALL SHALL BE TROWELED TO AN EVEN SURFACE AND PARALLEL TO THE PROFILE GRADE PLUS CONFORMING TO THE ROADWAY CROSS SLOPE. TARPAPER SHALL BE IMPREGNATED FELTED PAPER CONFORMING TO THE REQUIREMENTS OF ASTM D227.

NOTE:
THIS DETAIL APPLIES ONLY
WHEN "H" IS 4" OR GREATER.

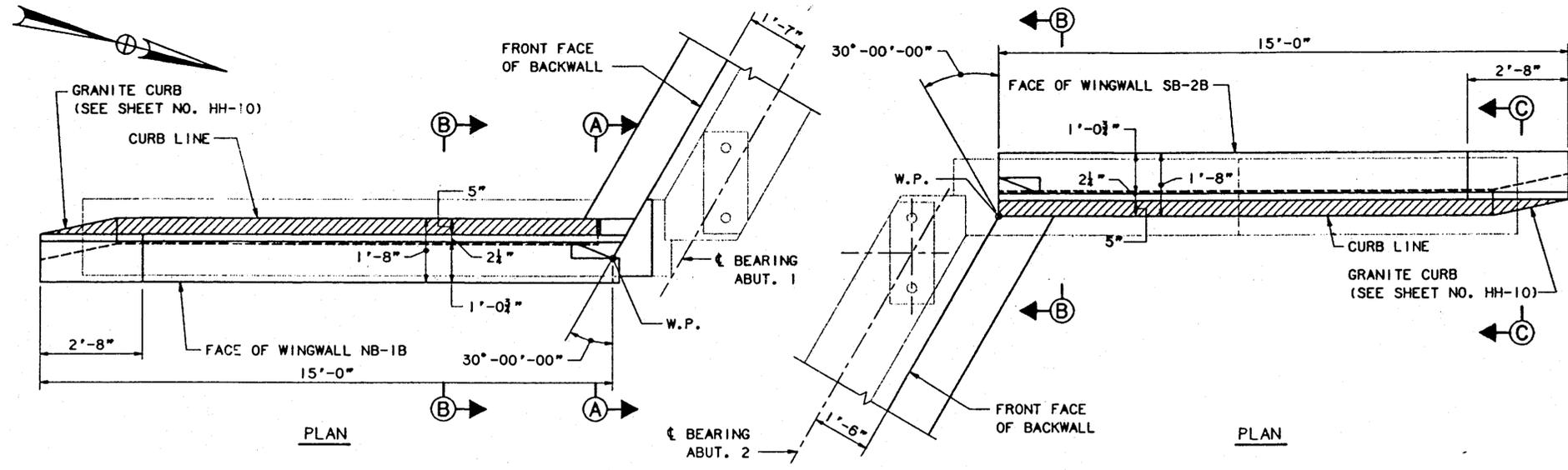
AS-BUILT
SHEET NUMBER: AB-08
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Maine Turnpike Authority
Maine Turnpike
HUNTS HILL ROAD OVERPASS
ABUTMENT DETAILS

Transpass
ARCHITECTS ENGINEERS PLANNERS

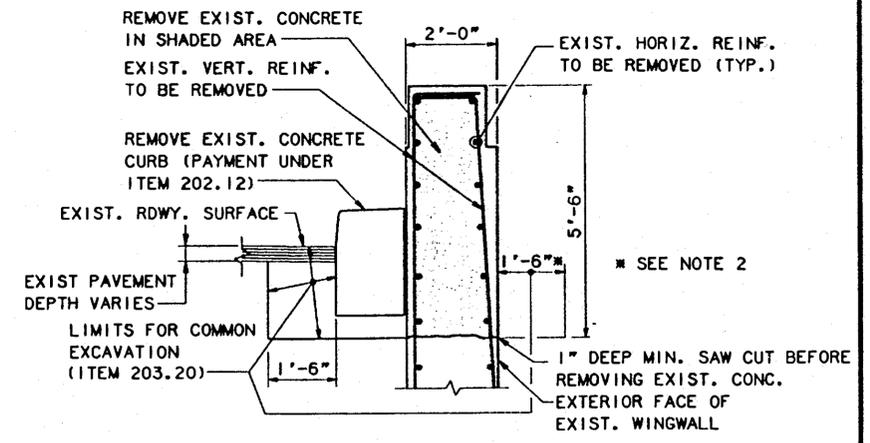
Contract 99.1
Sheet No. HH-7
25 of 50

By:	Date:
Designed	AD 12/98
Drawn	LS 12/98
Checked	PMK 12/98
In charge of:	RAL

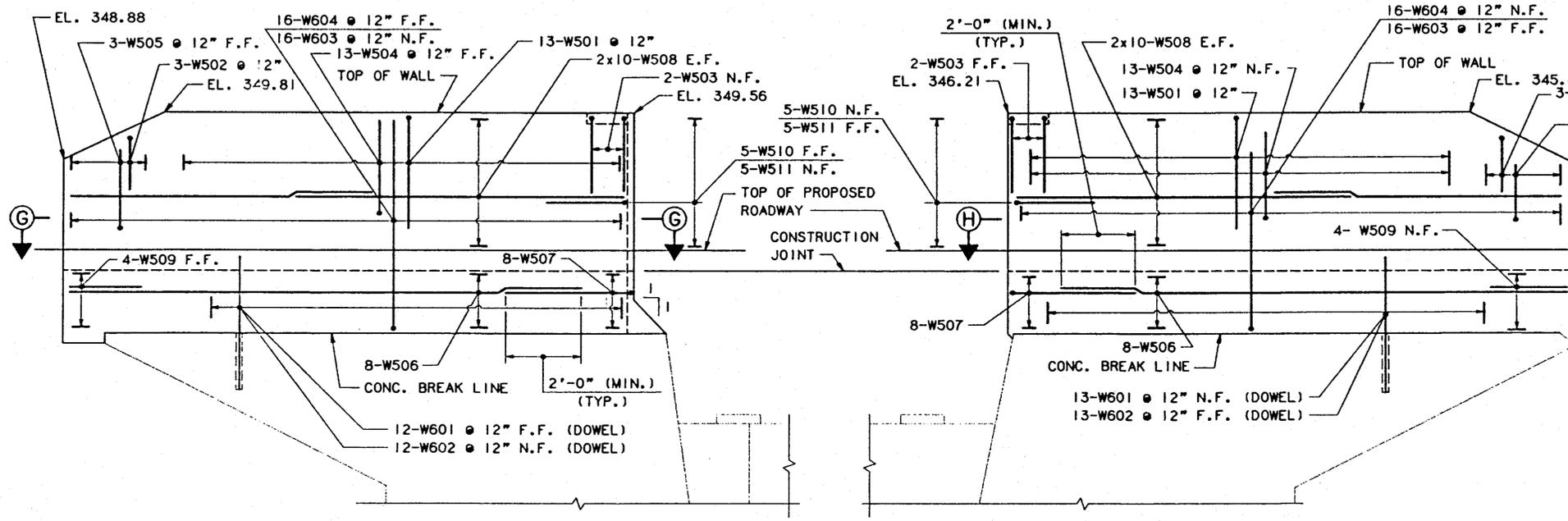


PLAN

PLAN

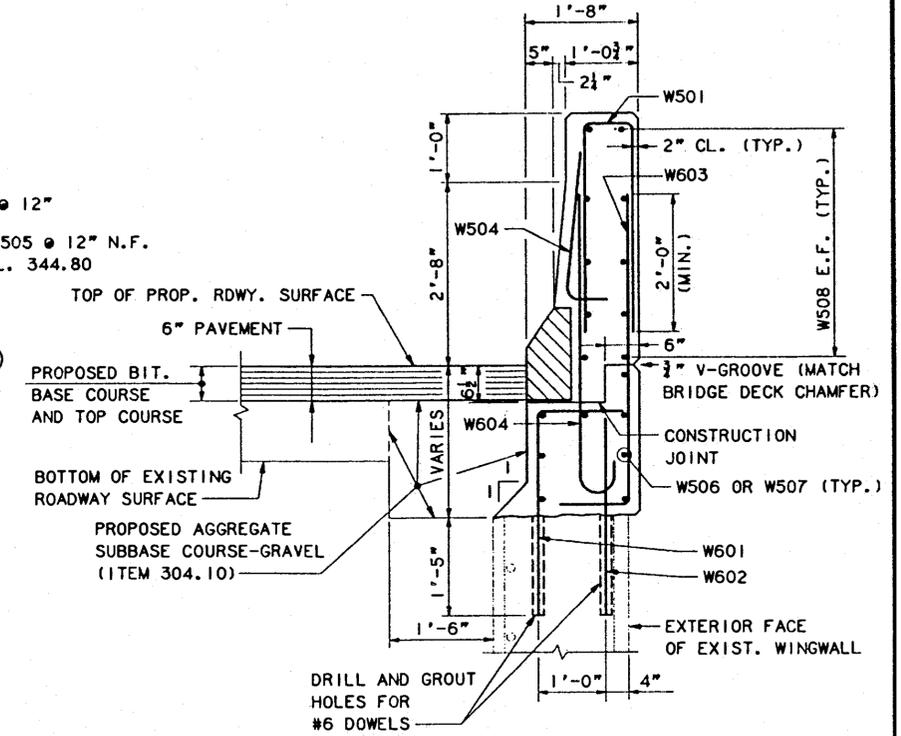


EXISTING SECTION B-B
1/2" = 1'-0"

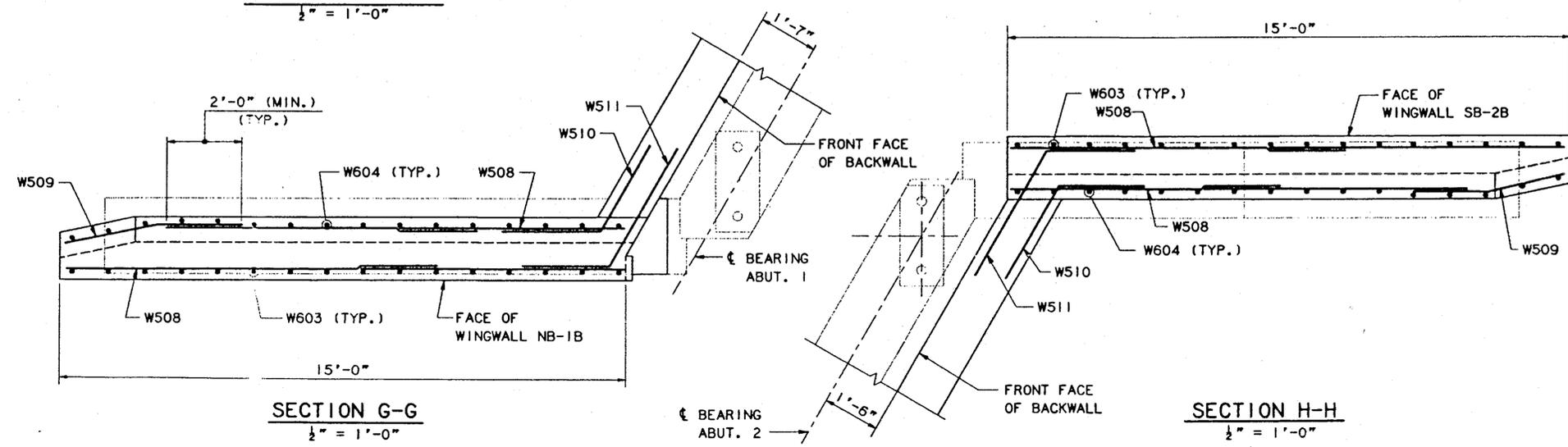


ELEVATION
WINGWALL NB-1B
1/2" = 1'-0"

ELEVATION
WINGWALL SB-2B
1/2" = 1'-0"



PROPOSED SECTION B-B
3/8" = 1'-0"



SECTION G-G
1/2" = 1'-0"

SECTION H-H
1/2" = 1'-0"

- NOTES**
- FOR SECTIONS A-A, C-C AND GRANITE CURB DETAILS, SEE SHEET NO. HH-10.
 - DEPTH SHALL BE FROM EXISTING GRADE.

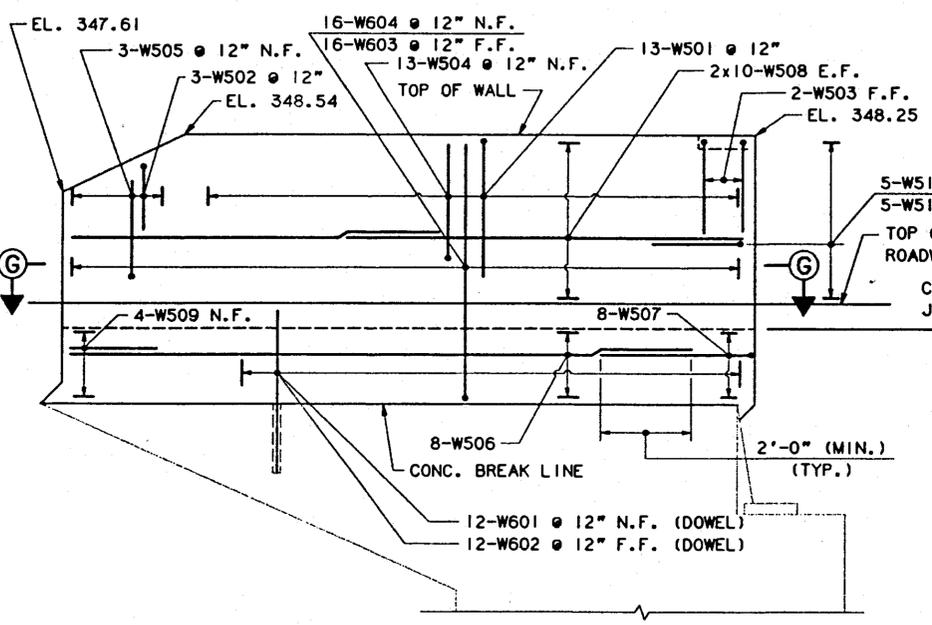
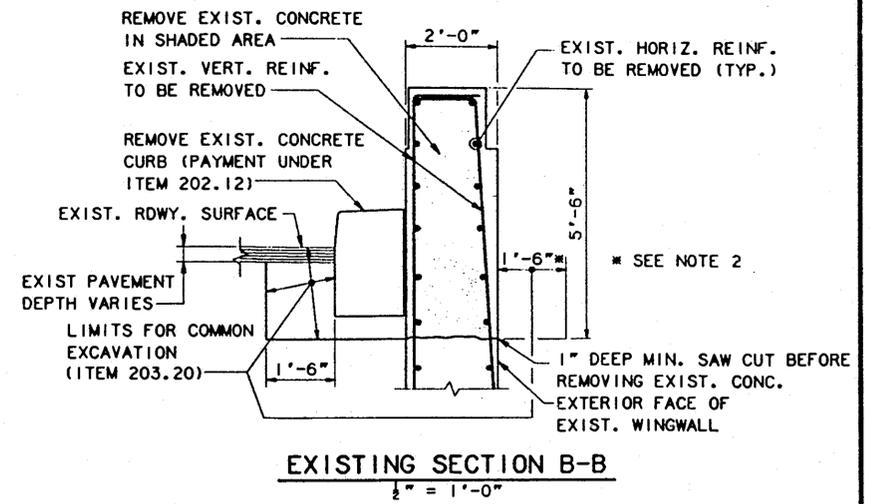
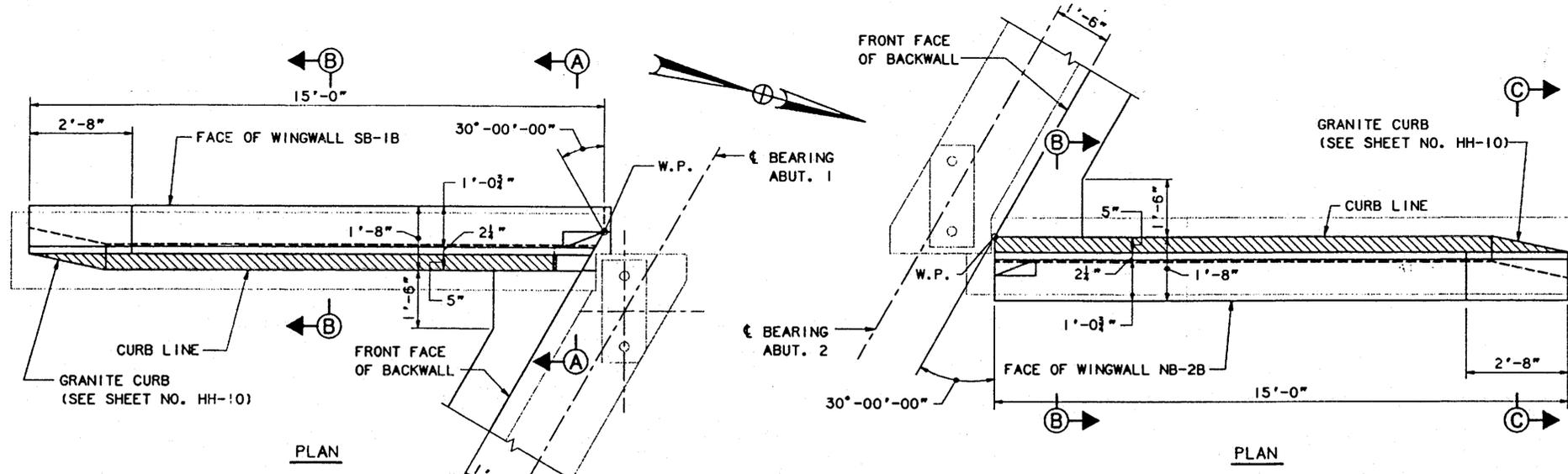
AS-BUILT
SHEET NUMBER: AB-09
69 OF 77

Maine Turnpike Authority
Maine Turnpike
HUNTS HILL ROAD OVERPASS
WINGWALL MODIFICATIONS I

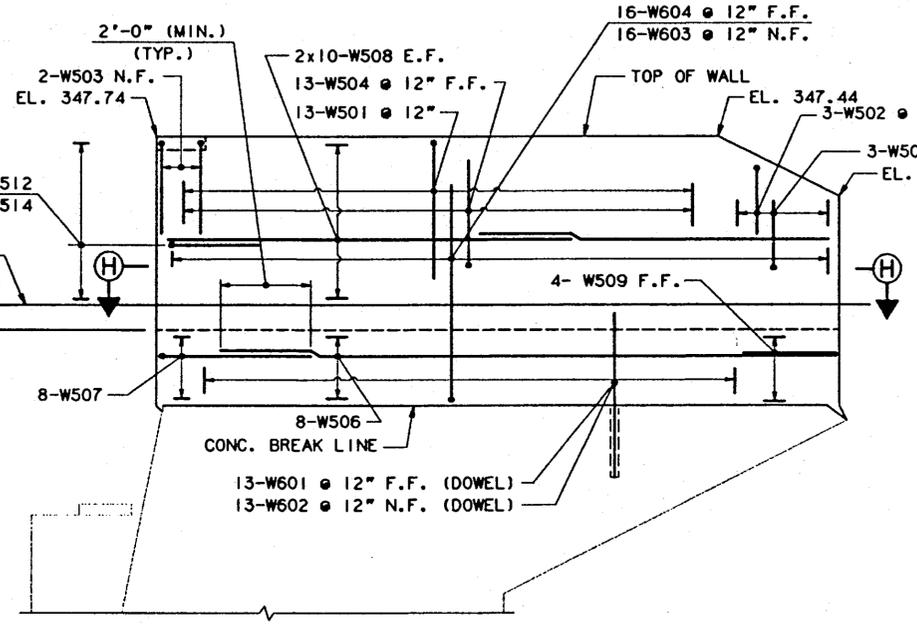
HNTB
ARCHITECTS ENGINEERS PLANNERS

Contract 99.1	Sheet No. HH-8 26 of 50
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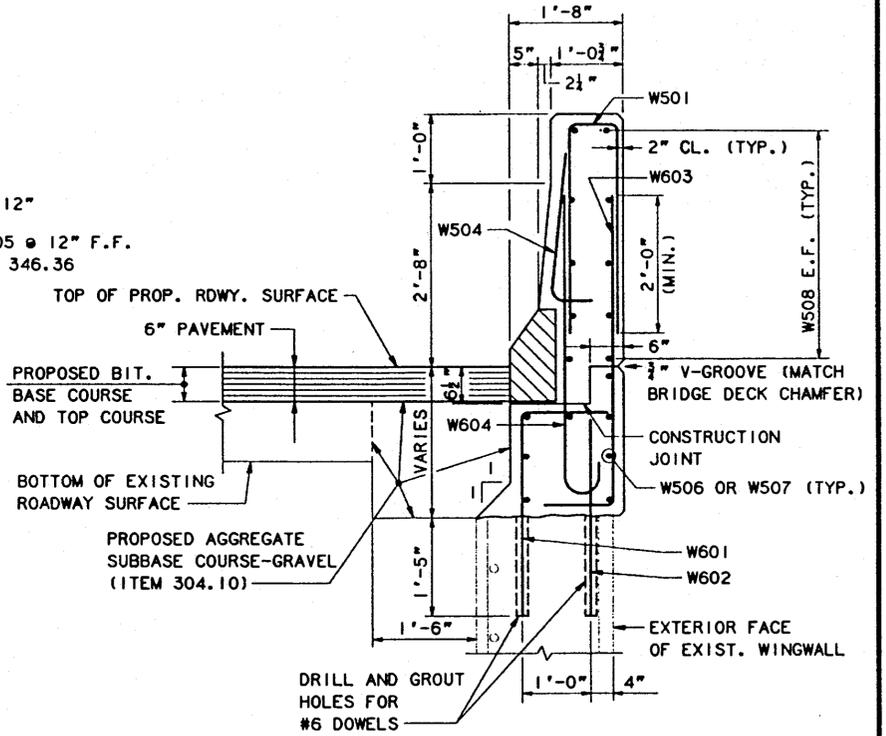
By: AD	Date: 12/98
Designed: AD	Date: 12/98
Drawn: LS	Date: 12/98
Checked: PMK	Date: 12/98
In charge of: RAL	



ELEVATION
WINGWALL SB-1B
1/2" = 1'-0"

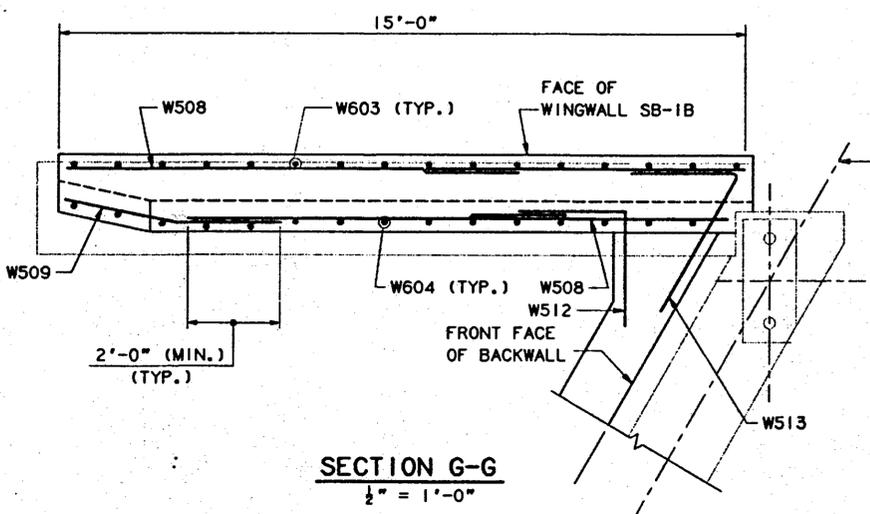


ELEVATION
WINGWALL NB-2B
1/2" = 1'-0"

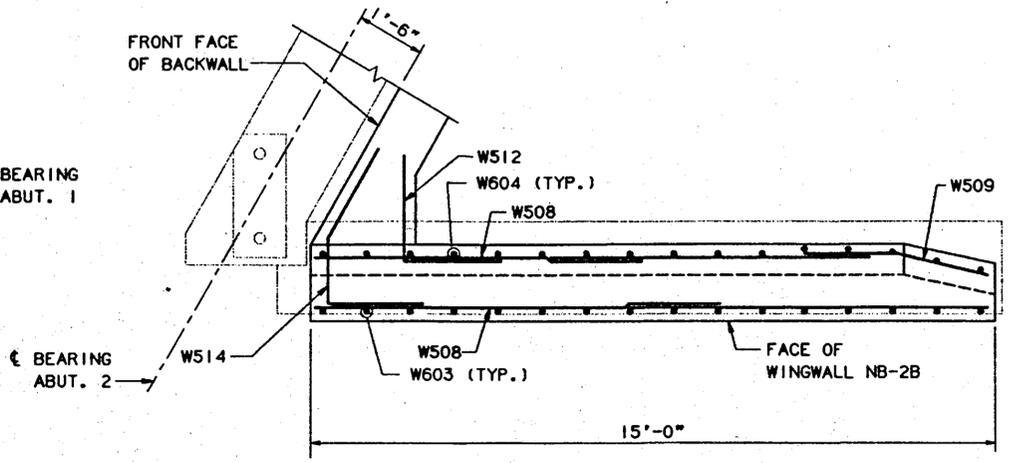


PROPOSED SECTION B-B
1/2" = 1'-0"

- NOTES**
- FOR SECTIONS A-A, C-C AND GRANITE CURB DETAILS, SEE SHEET NO. HH-10.
 - DEPTH SHALL BE FROM EXISTING GRADE.



SECTION G-G
1/2" = 1'-0"



SECTION H-H
1/2" = 1'-0"

AS-BUILT
SHEET NUMBER: AB-10
70 OF 77

Maine Turnpike Authority
Maine Turnpike
HUNTS HILL ROAD OVERPASS
WINGWALL
MODIFICATIONS II

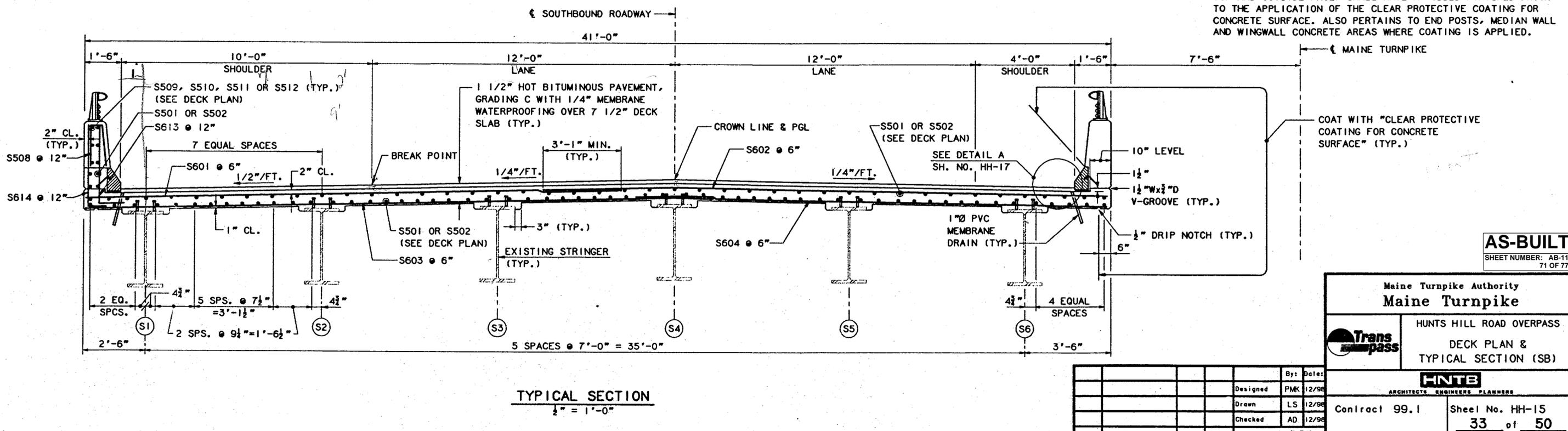
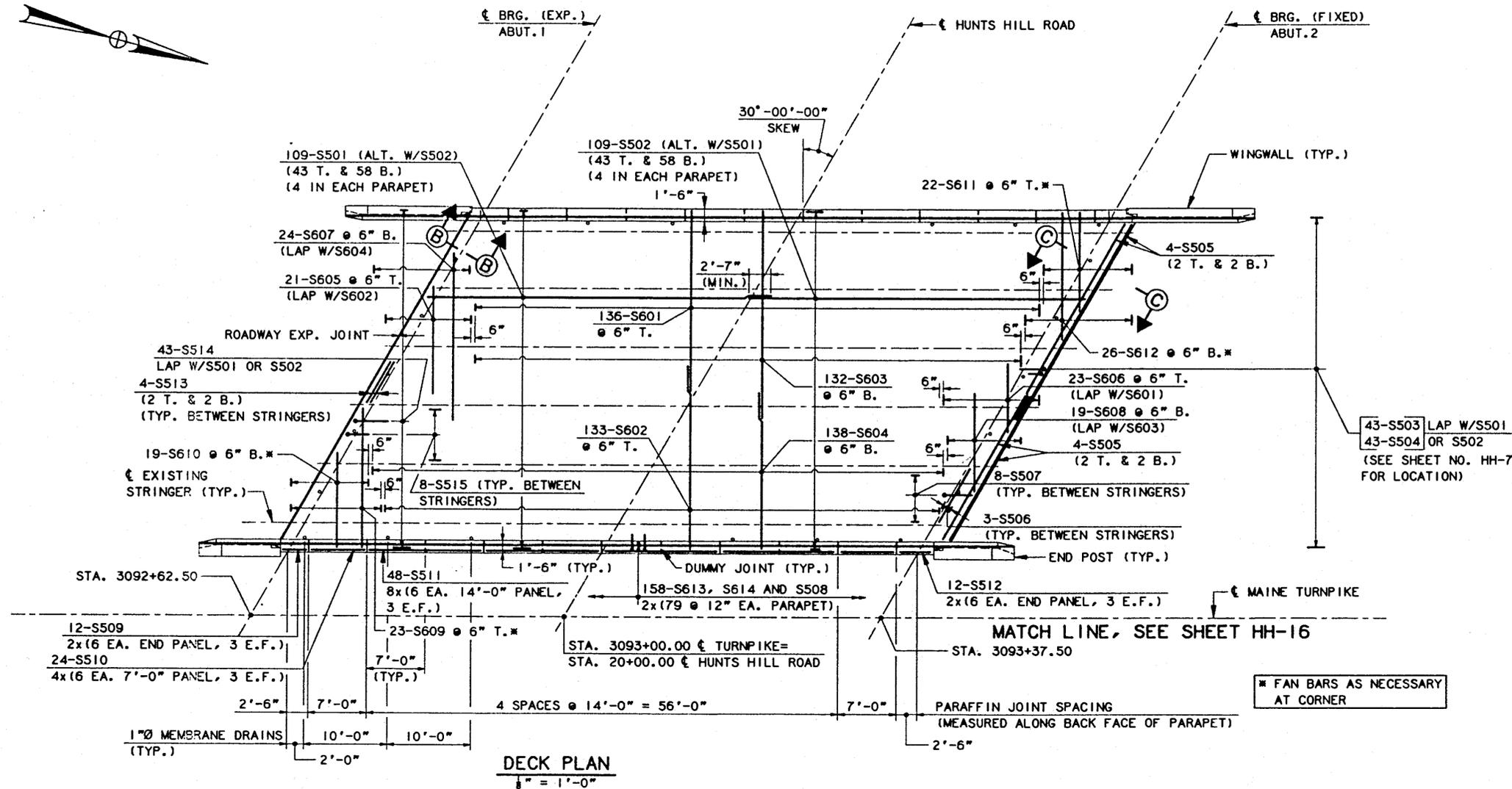
HNTB
ARCHITECTS ENGINEERS PLANNERS
Contract 99.1 Sheet No. HH-9
27 of 50

By: AD	Date: 2/99
Designed: AD	Date: 2/99
Drawn: LS	Date: 2/99
Checked: PMK	Date: 2/99
In charge of: RAL	



SUPERSTRUCTURE NOTES

1. CHAMFER ALL EXPOSED EDGES OF CONCRETE 3/4", UNLESS OTHERWISE NOTED.
2. FORM A 1 1/2" W. X 3/4" D. V-GROOVE ON THE FASCIAS AT THE HORIZONTAL JOINT BETWEEN THE PARAPET AND SLAB.
3. REINFORCING STEEL SHALL HAVE A MINIMUM COVER OF 2", UNLESS OTHERWISE NOTED.
4. THE FORMWORK AND ITS SUPPORTS, OVER THE FULL WIDTH OF THE STRUCTURAL SLAB, SHALL REMAIN IN PLACE UNTIL A MINIMUM OF 48 HOURS HAS ELAPSED AFTER PLACEMENT OF THE SLAB. AFTER WHICH, REMOVAL OF FORMWORK MEETING THE REQUIREMENTS FOR FORM REMOVAL OF SECTION 502, STRUCTURAL CONCRETE, OF THE STANDARD SPECIFICATIONS, MAY PROCEED.
5. ADJUST REINFORCING STEEL TO FIT AROUND THE DRAINS IN A MANNER APPROVED BY THE ENGINEER. DO NOT CUT TRANSVERSE REINFORCING BARS.
6. CLEAR PROTECTIVE COATING FOR CONCRETE SURFACE SHALL BE APPLIED TO THE FOLLOWING AREAS: ENTIRE PERIMETER OF THE PARAPETS, FASCIA DOWN TO DRIP NOTCH AND ALL EXPOSED CONCRETE SURFACES ON THE END POSTS PLUS MEDIAN WALL.
7. FOR REINFORCING STEEL SCHEDULE, SEE SHEET NO. HH-26.
8. FOR ALUMINUM BRIDGE RAIL DETAILS, SEE SHEET NO. HH-22.
9. DO NOT COVER DECK DRAINS WITH MEMBRANE WATERPROOFING. DEPRESS DRAINS 1/2" BELOW TOP OF SLAB AND PROVIDE 23 GAUGE GALVANIZED SCREENS (1/4" MESH) OVER DRAINS. PAYMENT SHALL BE INCIDENTAL TO ITEM 502.261.
10. THE CONCRETE DECK SURFACE SHALL BE GIVEN A SMOOTH BULL FLOAT OR WOOD FLOAT FINISH.
11. MORTAR FOR BEDDING AND FOR JOINTS IN THE GRANITE CURB SHALL CONTAIN A NON-SHRINK ADDITIVE.
12. SEAL MEMBRANE ALONG CURB AND ALL DRAINS. ALLOW 1/4" FOR THICKNESS.
13. FOR RAIL POST SPACING, SEE SHEET HH-17.
14. STAY IN PLACE FORMS SHALL NOT BE USED.
15. FOR DECK SLAB DETAILS, SEE SHEET NO. HH-17.
16. FOR ROADWAY EXPANSION JOINT DETAILS, SEE SHEET NOS. HH-18, HH-19 AND HH-20.
17. FOR SECTION B-B, SEE SHEET NO. HH-20 AND FOR SECTION C-C, SEE SHEET NO. HH-7.
18. CURB JOINTS SHALL LINE UP WITH PARAFFIN AND DUMMY JOINTS.
19. ALL BRIDGE PARAPET CONCRETE, INCLUDING INSIDE FACE, TOP AND OUTSIDE FACE, SHALL HAVE A RUBBED FINISHED PRIOR TO THE APPLICATION OF THE CLEAR PROTECTIVE COATING FOR CONCRETE SURFACE. ALSO PERTAINS TO END POSTS, MEDIAN WALL AND WINGWALL CONCRETE AREAS WHERE COATING IS APPLIED.



AS-BUILT
SHEET NUMBER: AB-11
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Maine Turnpike Authority
Maine Turnpike

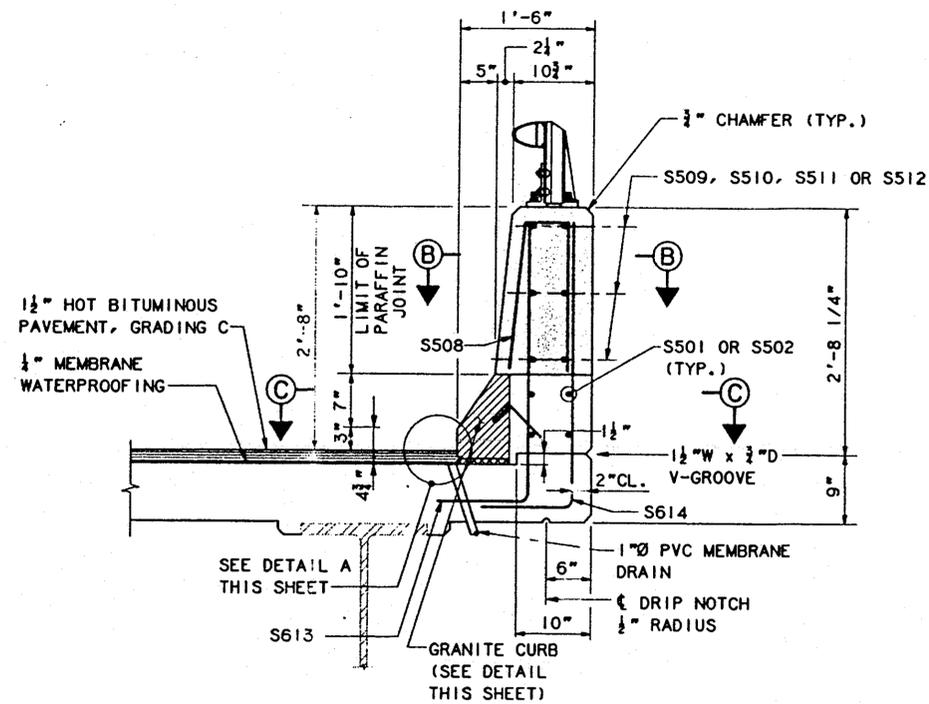
HUNTS HILL ROAD OVERPASS
DECK PLAN &
TYPICAL SECTION (SB)

Transpass

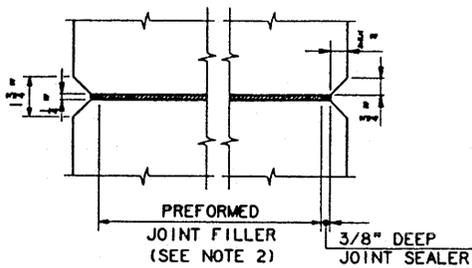
Contract 99.1 Sheet No. HH-15
33 of 50

HNTB
ARCHITECTS ENGINEERS PLANNERS

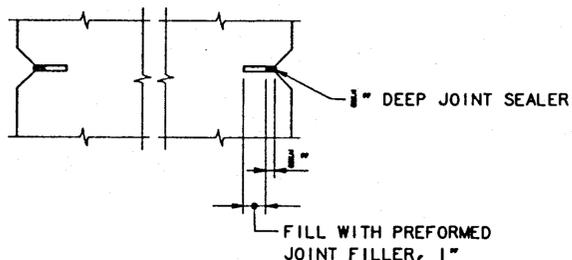
By: PMK	Date: 12/98
Designed: PMK	12/98
Drawn: LS	12/98
Checked: AD	12/98
In charge: RAL	



PARAPET DETAIL
 1" = 1'-0"



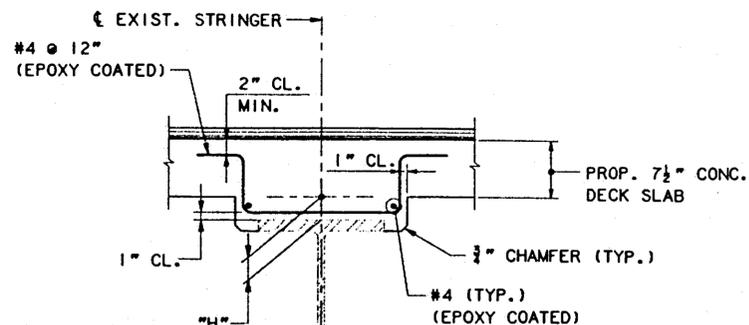
SECTION B-B
 3" = 1'-0"



SECTION C-C
 3" = 1'-0"

PARAFFIN AND DUMMY JOINT NOTES

1. CONCRETE SHALL BE PLACED SIMULTANEOUSLY ON BOTH SIDES OF JOINT.
2. PREFORMED JOINT FILLER SHALL CONFORM TO ASTM DESIGNATION D1751.
3. SECTION C-C ALSO APPLIES TO DUMMY JOINT LOCATIONS.
4. JOINT SEALER SHALL BE SIKAFLEX 1A.
5. PREFORMED JOINT FILLER AND JOINT SEALER SHALL BE INCIDENTAL TO ITEM 502.261, STRUCTURAL CONCRETE ROADWAY & PARAPETS ON STEEL BRIDGES.
6. CURB JOINTS SHALL BE ALIGNED WITH PARAFFIN AND DUMMY JOINTS.

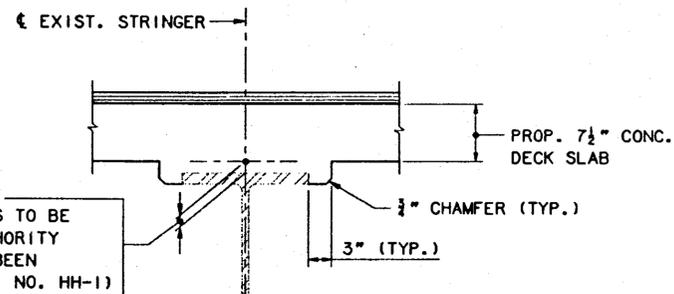


NOTES

1. THIS DETAIL APPLIES ONLY WHEN "H" IS GREATER THAN 3".
2. WHEN "H" IS 3" OR LESS, SEE BLOCKING POINT DETAIL BELOW.
3. ADDITIONAL CONCRETE REQUIRED FOR EXTRA DEPTH HAUNCHES SHALL BE INCIDENTAL TO ITEM 502.261.

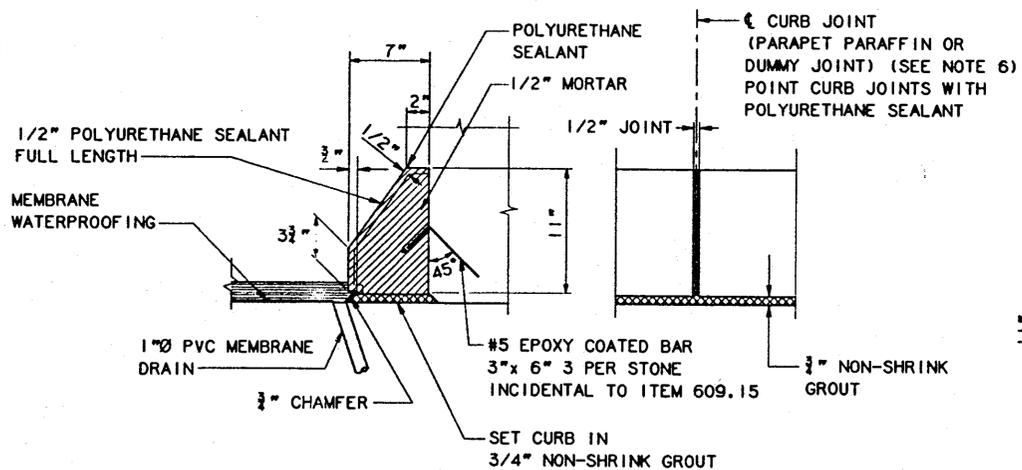
EXTRA DEPTH HAUNCH DETAILS

1" = 1'-0"

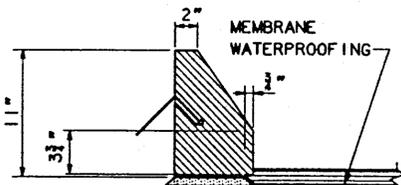


BLOCKING POINT DETAIL

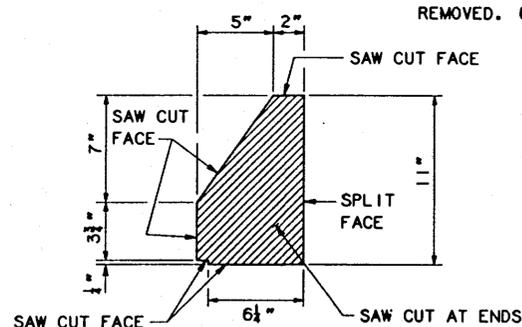
1" = 1'-0"



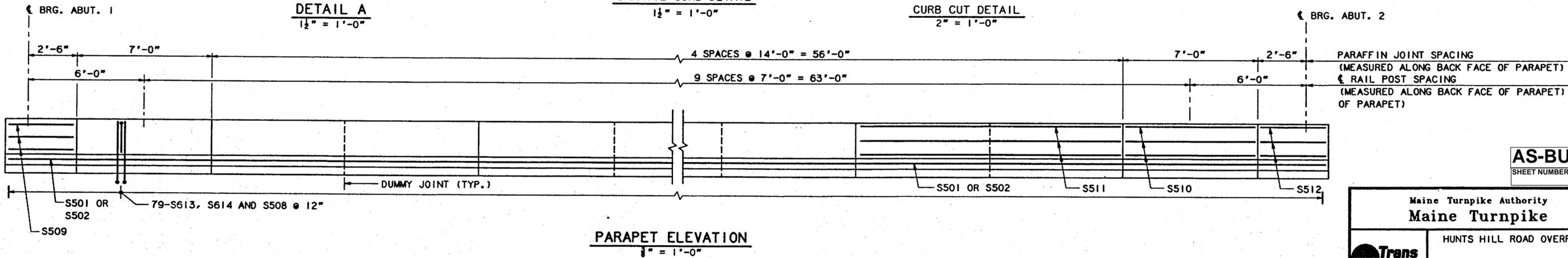
DETAIL A
 1 1/2" = 1'-0"



GRANITE CURB DETAIL
 1 1/2" = 1'-0"



CURB CUT DETAIL
 2" = 1'-0"



PARAPET ELEVATION
 1/4" = 1'-0"

AS-BUILT
 SHEET NUMBER: AB-12
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Maine Turnpike Authority
Maine Turnpike

HUNTS HILL ROAD OVERPASS
 DECK SLAB DETAILS

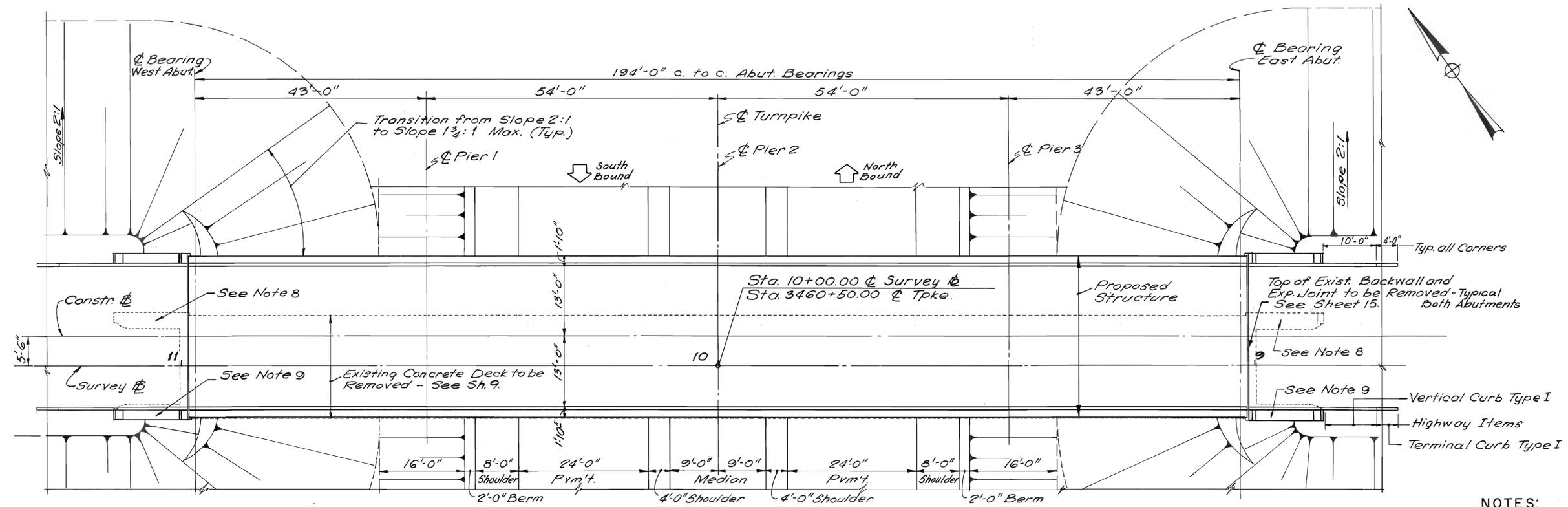


Contract 99.1

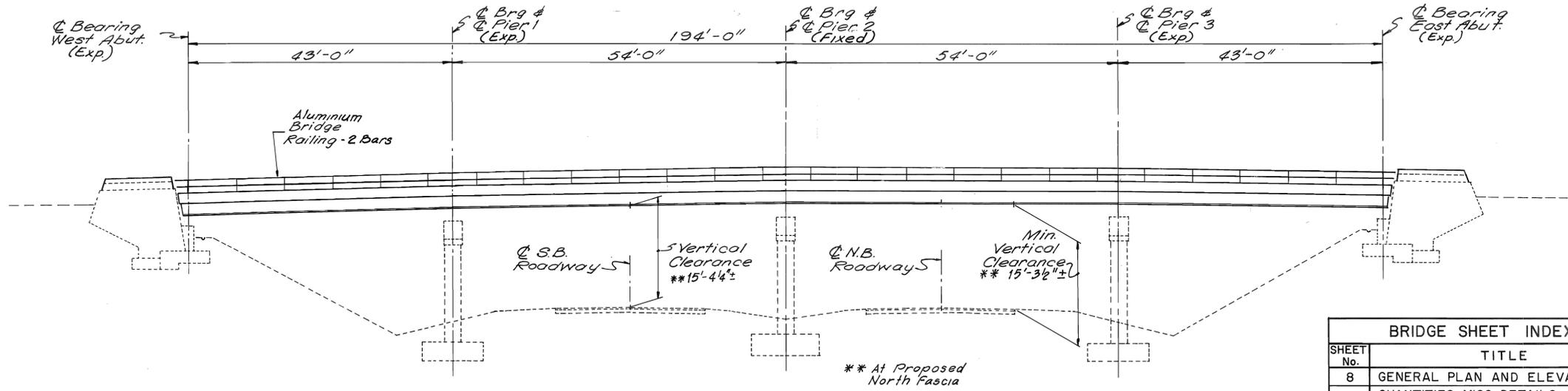
Sheet No. HH-17

35 of 50

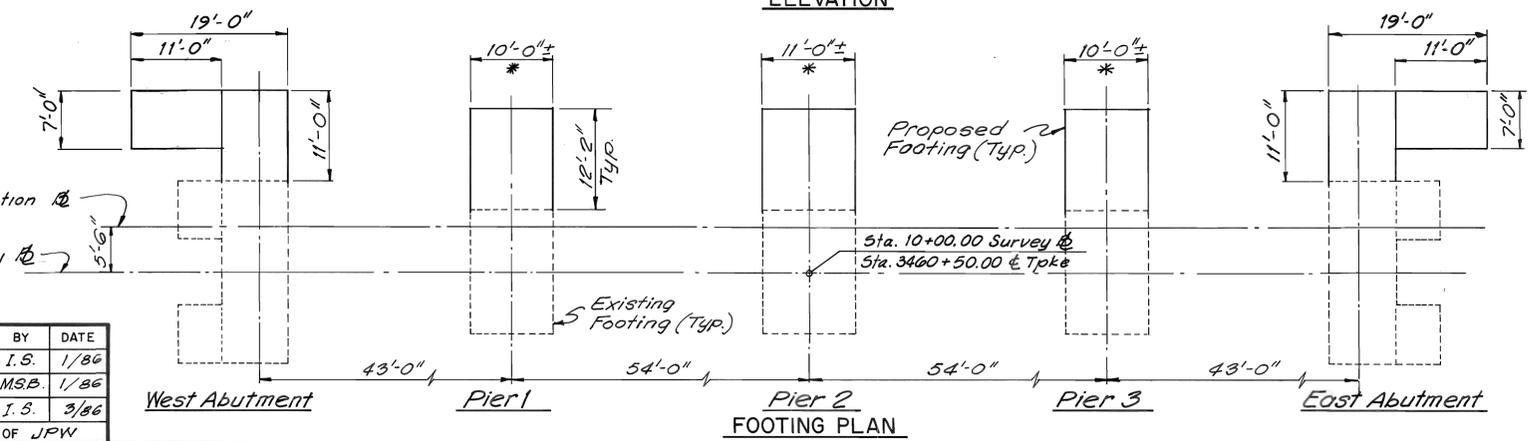
By: AD	Date: 12/98
Designed: AD	Date: 12/98
Drawn: LS	Date: 12/98
Checked: PMK	Date: 12/98
In charge of: RAL	



PLAN



ELEVATION



FOOTING PLAN

DESIGN
AASHTO Standard Specifications For Highway Bridges 1983 with All Interims.

CONTRACT
State of Maine, Department of Transportation, Standard Specifications, Highways & Bridges, Revision of January 1984.

LIVE LOAD
H15

MATERIALS
CONCRETE
All Concrete Shall be Class A
 $f_c = 1,200$ P.S.I., $n=9$

REINFORCING STEEL
ASTM A615 Grade 60
 $f_b = 24,000$ P.S.I.

STRUCTURAL STEEL
ASTM A36 $f_b = 20,000$ P.S.I.

HIGH STRENGTH BOLTS
ASTM A325

- NOTES:**
1. Reinforcing Steel to Have a Clear Cover as Shown on the Plans.
 2. Chamfer all Exposed Edges 1".
 3. Plans of Existing Bridges are Available at the Authority's Office at 430 Riverside St., Portland, Maine.
 4. Do not Break the Bond Between the Concrete Surfaces of Vertical Construction Joints in Superstructure Slab. Form a 1" V-Groove on the Outside Faces of Slab at Each Construction Joint. See Note "A" Sh.14.
 5. Provide 23 Gauge Galvanized Screen Over 1" Deck Drains, 18" Mesh.
 6. The Authority's Personnel Will Profile the Tops of All Stringers Before the Form Work is Started and Supply the Contractor With Final Bottom of Slab Elevation.
 7. Shielding Required During Concrete Removal Shall not Project Below the Bottom Flanges of Stringers. The Estimated Quantity of Shielding is The Minimum Required and is Based on the Following Limits:
 - a. Normal to Bridge: as Shown on Plans
 - b. Parallel to Bridge: Pier 1 to Pier 3
 8. Northwest & Northeast Existing Wingwalls to be Partially Removed - See Sheet 10.
 9. Southwest & Southeast Existing Wingwalls to be Modified - See Sheet 10.
 10. Place the Concrete in Panels "A" and "B" Before the Expansion Device is Installed. Place the Concrete in Panels A Before Placing in Panels B. Place the Concrete in Panels C Last.

BRIDGE SHEET INDEX	
SHEET No.	TITLE
8	GENERAL PLAN AND ELEVATION
9	QUANTITIES, MISC. DETAILS AND CONSTRUCTION SEQUENCE.
10	ABUTMENTS AND WINGWALLS.
11	PIER AND ABUTMENT DETAILS.
12	FRAMING PLAN.
13	STEEL DETAILS.
14	DECK REINFORCEMENT.
15	EXPANSION DEVICE DETAILS.
16	ALUMINUM BRIDGE RAILING.
17	REINFORCING SCHEDULE.

AS-BUILT
SHEET NUMBER: AB-14
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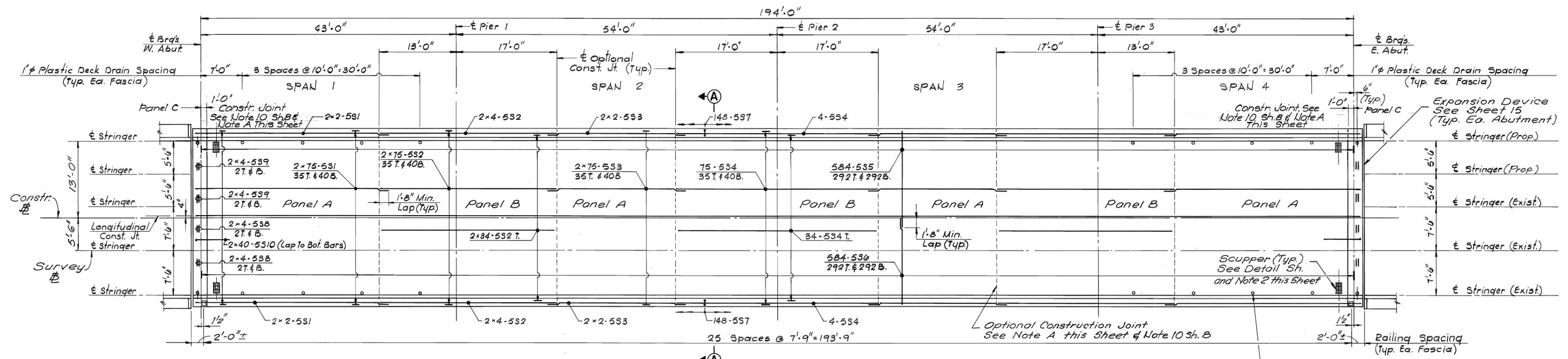
MAINE TURNPIKE AUTHORITY
MAINE TURNPIKE

BENNETT ROAD BRIDGE REPLACEMENT.
GENERAL PLAN AND ELEVATION.

HOWARD, NEEDLES, TAMMEN & BERGENDOFF ARCHITECTS ENGINEERS PLANNERS BOSTON

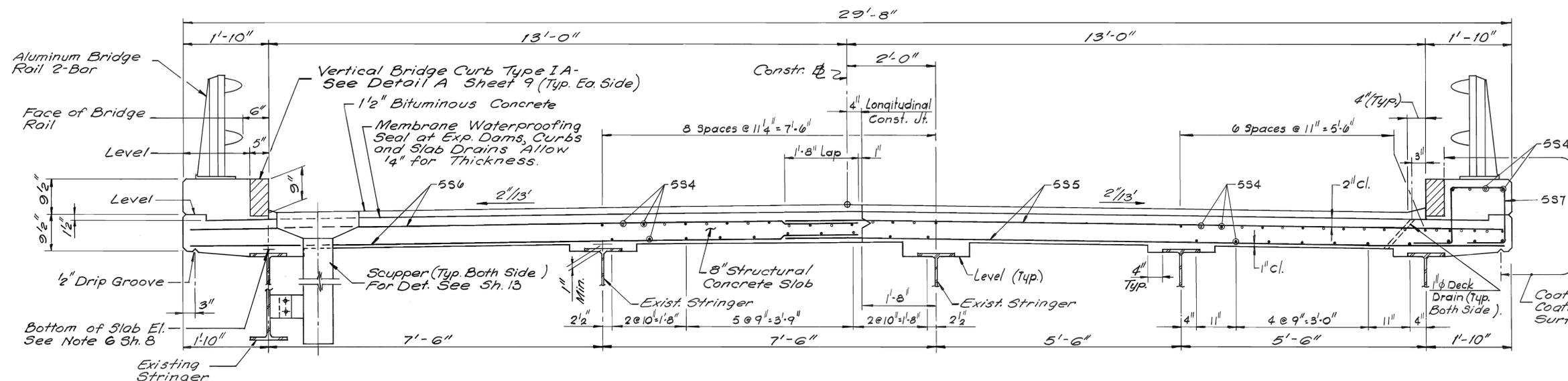
SCALE: 3/32"=1'-0"
SHEET NO. 8 OF 23

NO.	REVISION	BY	DATE	IN CHARGE OF
		DESIGNED: I.S.	1/86	
		DRAWN: MSB.	1/86	
		CHECKED: I.S.	3/86	
				JPW



DECK PLAN
1/8" = 1'-0"

1" Plastic Deck Drains Spaced as Shown, do not Cover with Membrane, Depress 1/2" Below Top of Slab. (Typ) See Note 5 Sheet B



SECTION A-A
3/4" = 1'-0"

NOTE A
As An Alternate And At The Contractors Option, The Deck Concrete May Be Placed In One Continuous Operation From Panel C To Panel A Thus Eliminating Slab Construction Joints At The Piers. Continuous Placing Of Concrete In The Deck Shall Be Made With The Use Of An Acceptable Set-Retarder.

- NOTES:**
1. For General Notes and Quantities, See Sheet 9.
 2. Locate Scupper in Field to Discharge into Existing Trough. Cut Steel in Field at Scuppers as Required.
 3. Length of Last Section of Curb to be Field Checked.
 4. Place Longitudinal Steel as Shown in Section A-A.
 5. For 1" V-Groove Details, See Sheet 9.
 6. For Railing Details, See Sheet 9 and 10.

AS-BUILT
SHEET NUMBER: AB-15
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MAINE TURNPIKE AUTHORITY
MAINE TURNPIKE

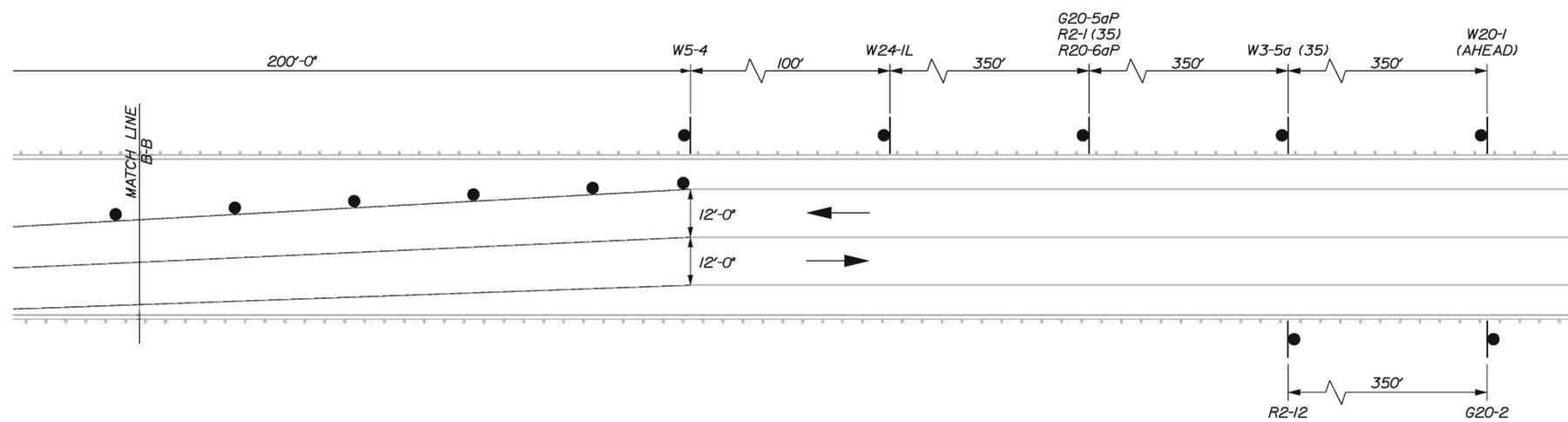
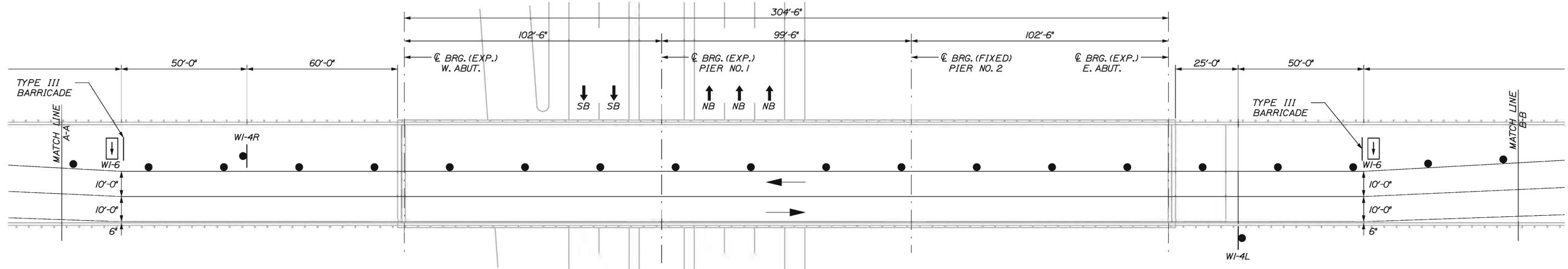
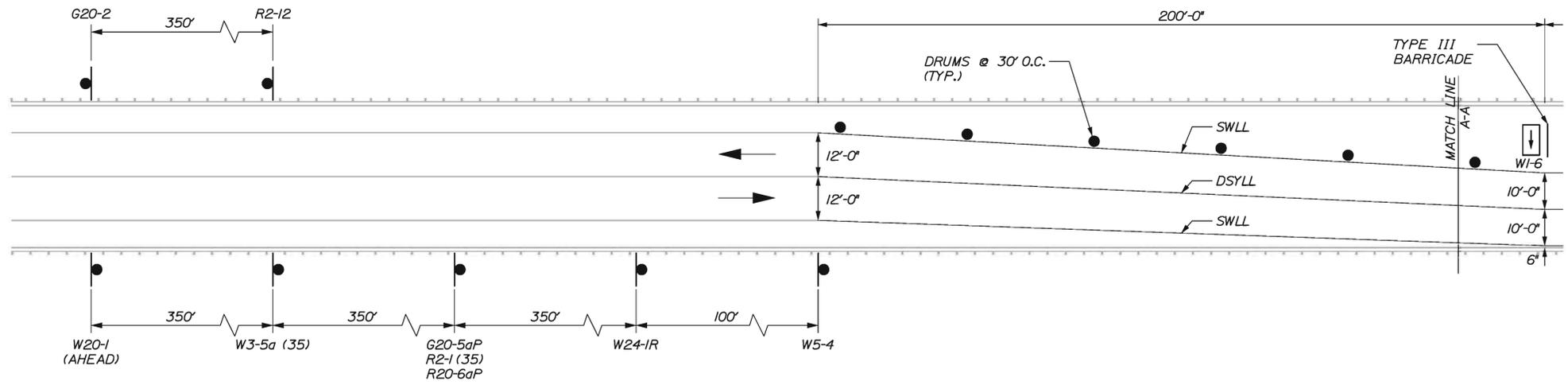
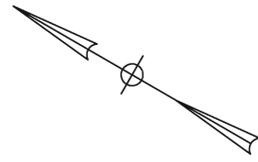
BENNETT ROAD BRIDGE REPLACEMENT.
DECK REINFORCEMENT

HOWARD, NEEDLES, TAMMEN & BERGENDOFF ARCHITECTS ENGINEERS PLANNERS BOSTON

SCALE: AS NOTED
SHEET NO. 14 OF 23

NO.	REVISION	BY	DATE	IN CHARGE OF
		DESIGNED:	I.S. 1/86	
		DRAWN:	M.S.B. 1/86	
		CHECKED:	S.F.L. 2/86	
		BY	DATE	
				J.P.W.

Date: 6/29/2011



AS-BUILT
SHEET NUMBER: AB-16
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NOTE:
PHASE II SIMILAR BUT OPPOSITE HAND.

Scale:

No.	Revision	By	Date

Designed by:

HNTB

	By	Date	By	Date
Designed	CAH	06/11	Checked	TRC 06/11
Drawn	MPC	06/11	In Charge of	RAL 06/11

HNTB CORPORATION
340 County Road, Suite 6-C
Westbrook, ME 04092
TEL (207) 774-5155
FAX (207) 228-0909

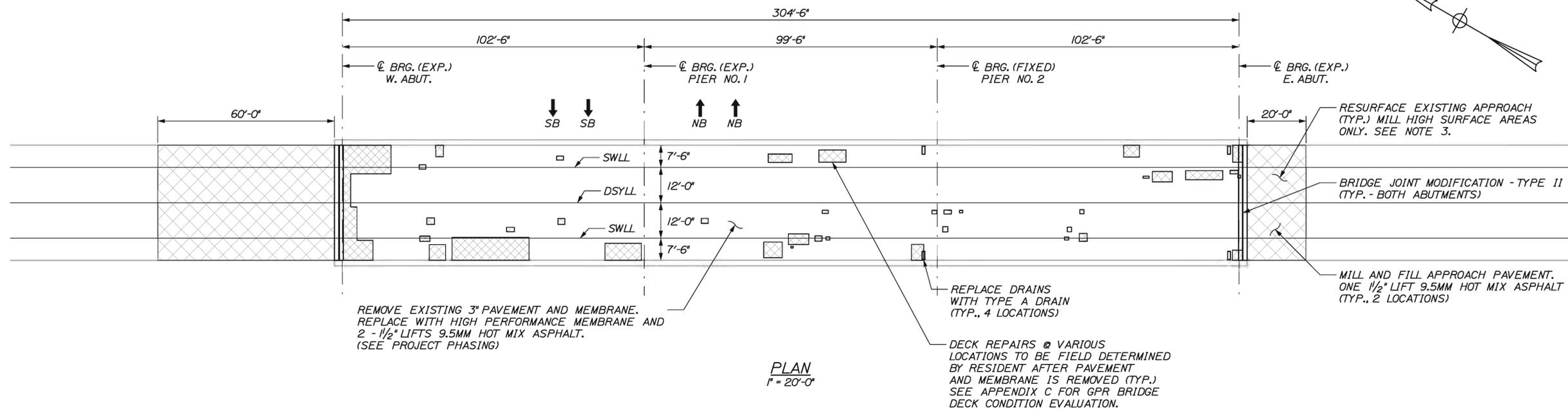
**THE GOLD STAR
MEMORIAL HIGHWAY**

ST. LAWRENCE & ATLANTIC RR OVERPASS
KITTYHAWK AVENUE UNDERPASS BRIDGE REPAIRS
KITTYHAWK AVENUE — PHASE I & II
TRAFFIC CONTROL PLANS

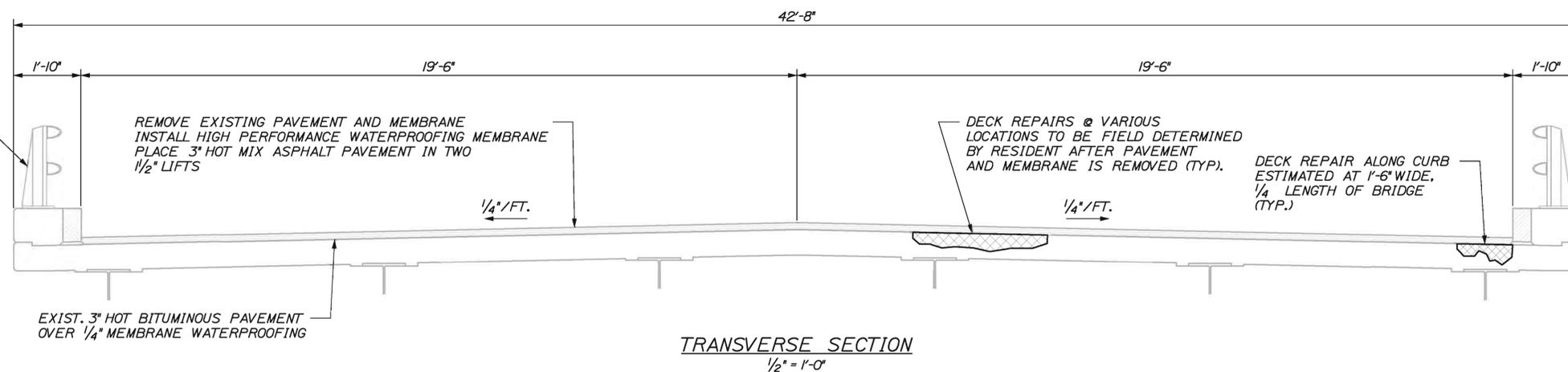
SHEET NUMBER: MOT-3
CONTRACT: 2011.07
8 OF 32

Filename: 008_KH-mot-1.dgn

Date: 6/30/2011



LEGEND
 EXAMPLE LIMITS OF DECK REPAIR
 ACTUAL REPAIR AREAS TO BE FIELD DETERMINED.



- NOTES:**
- CONTRACTOR SHALL REPAIR AREAS OF CONCRETE DECK DETERIORATION AS IDENTIFIED BY THE RESIDENT. LOCATIONS OF DECK REPAIRS ARE UNDETERMINED FOR THIS BRIDGE. RESIDENT SHALL IDENTIFY AND MARK AREAS FOR DECK REPAIR AFTER REMOVING THE WEARING SURFACE. COORDINATE WORK WITH RESIDENT. PAYMENT FOR DECK REPAIR WORK SHALL BE UNDER THE 518 PAY ITEMS.
 - CONTRACTOR SHALL REPAIR AREAS OF DETERIORATED GRANITE CURB JOINTS AND BEDDING MORTAR ON THE DECK AS REQUIRED. LOCATIONS OF DETERIORATED GRANITE CURB JOINTS AND BEDDING MORTAR ARE UNDETERMINED. (TYP.)
 - APPROACH AREAS REQUIRING SHIM PAVEMENT TO REACH FINAL PAVEMENT GRADE SHALL NOT BE MILLED.
 - RETROFIT ALL EXISTING ALUMINUM BRIDGE RAILING SPLICES NOT IN CONFORMANCE WITH THE ALUMINUM BRIDGE RAIL RAILING - SPLICE MODIFICATION DETAIL. PAYMENT TO BE MADE UNDER 507.095.

AS-BUILT
 SHEET NUMBER: AB-17
 77 OF 77

Scale:

No.	Revision	By	Date

Designed by:

HNTB

	By	Date		By	Date
Designed	CAH	06/11	Checked	TRC	06/11
Drawn	MPC	06/11	In Charge of	RAL	06/11

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THE GOLD STAR MEMORIAL HIGHWAY

**ST. LAWRENCE & ATLANTIC RR OVERPASS
 KITTYHAWK AVENUE UNDERPASS BRIDGE REPAIRS**
 KITTYHAWK DECK PLAN AND TYPICAL SECTION
 SHEET NUMBER: KH-1
 CONTRACT: 2011.07 26 OF 32

Filename: 026_PlanTyp.dgn