

**MAINE TURNPIKE AUTHORITY**

**ADDENDUM NO. 1**

**CONTRACT 2016.05**

**BRIDGE REPAIRS**

**RAMP J OVER RTE. 1 SB & RAMP M (MM 1.59)**

**TWO ROD ROAD UNDERPASS (MM 42.0)**

**CUMMINGS ROAD UNDERPASS (MM 44.6)**

**EXIT 45 UNDERPASS (MM 44.9)**

**FOREST AVENUE OVERPASS NB & SB (MM 50.0)**

**SNOW FENCE INSTALLATIONS**

**The bid opening date is Thursday 4/21/16 at 11am.**

The following changes are made to the Proposal, Specifications and the Plans.

**GENERAL**

Addendum No. 2 is scheduled to be issued on April 19, 2016 (Tuesday) all questions regarding Contract 2016.05 should be submitted by the end of the day April 15, 2016 (Friday) to be answered in an addendum. Questions received after that time may not be answered.

**PROPOSAL**

Proposal Sheet P-2 through P-7 are deleted and replaced with P-2 (Revised 4/14/2016) through P-7 (Revised 4/14/2016) attached hereto. The revision to these proposal sheets P-2 is a decrease in quantity of Item 518.39 Granite Curb Joint Mortar and Bedding Mortar Repair, the addition of Item 518.392 Granite Curb Repair and the removal of Item 656.62 Temporary Slope Drains.

**SPECIFICATIONS**

- Special Provision Section 107.1 Contract Time and Contract Completion Date.  
Remove and replace the entire section with the following:

This Subsection is amended by the addition of the following:

All work shall be completed on or before December 16, 2016. The deck repair and associated work at Cummings Road shall be substantially complete by July 15, 2016. The work at all other locations except bearing installation Forest Avenue shall be substantially complete by November 15, 2016

- Special Provision Section 107.1.1 Substantial Completion has been added and attached hereto.

- Special Provision 107.4.6 Prosecution of Work.  
Remove and replace the entire section with the following:

The following activities shall not begin until the date specified:

All work requiring bridge closure at Cummings Road shall be done in the permitted bridge closure windows as outlined in Special Provision 652.

- Special Provision Section 518 STRUCTURAL CONCRETE REPAIR (Partial Depth Concrete Deck Repairs)(Full Depth Concrete Deck Repairs), Section 518.02 Repair Materials.  
Add the following paragraph after the third paragraph:

Materials for the partial depth concrete deck repairs and the full depth concrete deck repairs to be completed at Cummings Road shall use a fast setting material from the MaineDOT's Qualified Product's List (QPL) of concrete patching materials to allow bridge to reopen to traffic at the end of each work period. All requirements of the manufacturer's published recommendations shall be met. All materials used for repair of concrete or reinforcing steel shall meet the applicable requirements of Division 700 as specified in Standard Specification Sections 502 and 503, respectively.

- Special Provision Section 518 STRUCTURAL CONCRETE REPAIR (Granite Curb Joint Mortar and Bedding Mortar Repair) has been removed and replaced with STRUCTURAL CONCRETE REPAIR (Granite Curb Joint Mortar and Bedding Mortar Repair) (Granite Curb Repair) and attached hereto.
- Special Provision Section 652 MAINTENANCE OF TRAFFIC (Specific Project Maintenance of Traffic Requirements) has been removed and replaced and attached hereto.

## **PLANS**

Sheet ES-01 sheet 2 of 35 is revised as follows: Item 518.39 Granite Curb Joint Mortar and Bedding Mortar Repair quantity has been updated, Item 518.392 Granite Curb Repair has been added and Item 656.62 Temporary Slope Drains has been removed.

Sheet MOT-07 sheet 9 of 35 is revised as follows: Detour Plan and associated traffic control has been added.

Sheet MOT-13 sheet 15 of 35 is revised as follows: Sign associated with the Cummings Road detour have been added.

Sheet S-01 sheet 16 of 35 is revised as follows: Granite curb repair detail and note has been added. Concrete header note 6 has been updated.

Sheet S-02 sheet 17 of 35 is revised as follows: Fabric Trough Support Detail has been modified.

Sheet S-05 sheet 20 of 35 is revised as follows: Maintenance of traffic and work hour note 3 has been updated.

## **QUESTIONS**

**The following are questions asked at the pre-bid meeting held on January 12, 2016 or submitted to the Maine Turnpike Authority in writing. Answers to the questions are noted. Bidders shall utilize this information in preparing their bid.**

Question 1: Why are the maintenance of traffic (MOT) items not individually listed? Previous projects broke out the MOT which was easier for contractor bidding.

Answer: The lump sum MOT will remain for this project due to the high number of project locations.

Question 2: Is the bridge closed on Cummings Road for 2 weeks straight?

Answer: Cummings Road MOT has been changed as part of this addendum to nightly bridge closures with a detour under this contract. See the attached plan sheets and specifications for updated project requirements.

Question 3: Why is Pay Item 656.62 Temporary Slope Drain required? Item shows up in the Forest Avenue repair plans.

Answer: Temporary slope drains were included to control water from scuppers until the new drainage trough were in place. This is considered contractor mean and methods and Pay Item 656.62 Temporary Slope Drain has been removed from the contract.

Question 4: Can Ramp J be closed to weld bent plates for fabric trough?

Answer: Trough can be installed with the use of lane closures, no closure is required.

Question 5: How are the weld plates accessed from the top side? Is the bent plate installed with the fabric already on it? If the fabric is installed afterwards, how do you tighten the bolts with limited access?

Answer: Welding from the top side is accomplished through the finger joints, removal of the joint armor will not be permitted. The fabric is installed after the weld plates are installed. The fabric trough support geometry has been modified to allow for more access to the connections. See attached Sheet S-02.

Question 6: Is there any additional hardware required to be ordered by the Contractor for the sign clips?

Answer: No, the Maine Turnpike Authority will provide all clips and associated hardware.

Question 7: Does new pavement need to cover the backwall repair area on Ramp J?

Answer: Yes, pavement patch over repaired area should be laid to match existing pavement. The pavement patch material shall be PERMA-PATCH “Permanent Pavement Repair” or approved equal.

Question 8: What is the intent of the granite curb repair at this location?

Answer: The intent of the granite curb repair on Ramp J is to remove and reset the damaged portion of curb at the locations noted on the plans. An additional detail has been provided on Sheet S-01 to clarify the detail.

Question 9: Is substantial completion date an option? The lead time for bearings is up to 3 months, the contractor does not want to leave the bridge jacked while bearings are manufactured.

Answer: The Contract end date has been modified and substantial completion dates have been added to accommodate Forest Avenue bearing installation and bridge repair work at Cummings Road. See attached Special Provision Section 107.1 Contract Time and Contract Completion Date and Special Provision Section 107.1.1 Substantial Completion.

## **ATTACHMENTS**

- Proposal Sheets P-2 to P-7 (6 pages)
- Special Section 107.1.1 (1 page)
- Special Provision 518 (2 pages)
- Special Provision 652 (8 pages)
- Plan Sheets ES-01, M0T-07, MOT-13, S-01, S-02 & S-05 (6 pages)
- Pre-Bid Agenda (4 pages)
- Pre-Bid Sign-In Sheet (1 page)

**Notes:** The above items shall be considered as part of the bid submittal.

The total number of pages included with this addendum is Fifteen (33).

All bidders are requested to acknowledge the receipt of the Addendum No. 1 by signing below and faxing this sheet to Nathaniel Carll, Purchasing Department, Maine Turnpike Authority at 207-871-7739. Bidders are also required to acknowledge receipt of this Addendum No. 1 on Page P-8 of the bid package.

\_\_\_\_\_  
Business Name

\_\_\_\_\_  
Print Name and Title

\_\_\_\_\_  
Signature

\_\_\_\_\_  
Date

April 14, 2016

Very truly yours,

MAINE TURNPIKE AUTHORITY

\_\_\_\_\_  
Nathaniel Carll  
Purchasing Department  
Maine Turnpike Authority

**SCHEDULE OF BID PRICES  
CONTRACT NO. 2016.05  
Bridge Repairs**

Item No	Item Description	Units	Approx. Quantities	Unit Prices in Numbers		Bid Amount in Numbers	
				Dollars	Cents	Dollars	Cents
202.191	Removing Existing Drain Troughs	Lump Sum	1				
202.2021	Removing Pavement Surface - Bridge Deck	Square Yard	34				
403.210	Hot Mix Asphalt, 9.5 mm Nominal Maximum Size	Ton	2				
409.15	Bituminous Tack Coat, Applied	Gallon	2				
419.30	Sawing Bituminous Pavement	Linear Foot	230				
502.219	Structural Concrete, Abutments and Retaining Walls (1 CY)	Lump Sum	1				
507.095	Aluminum Bridge Railing - Splice Modification	Each	92				
514.06	Curing Box for Concrete Cylinders	Each	2				
515.201	Pigmented Protective Coating for Concrete Surfaces	Square Yard	920				
515.202	Clear Protective Coating for Concrete Surfaces	Square Yard	330				
518.39	Granite Curb Joint Mortar and Bedding Mortar Repair	Linear Foot	51				

**CARRIED FORWARD:**

Item No	Item Description	Units	Approx. Quantities	Unit Prices in Numbers		Bid Amount in Numbers	
				Dollars	Cents	Dollars	Cents
<b>BROUGHT FORWARD:</b>							
518.392	Granite Curb Repair	Linear Foot	7				
518.40	Epoxy Injection Crack Repair	Linear Foot	123				
518.43	Parapet Joint Repair	Linear Foot	290				
518.51	Repair of Upward Facing Surfaces - below Reinforcing Steel < 8 inches	Square Foot	147				
518.511	Partial Depth Concrete Deck Repairs	Square Foot	100				
518.512	Full Depth Concrete Deck Repairs	Square Foot	90				
518.52	Repair of Upward Facing Surfaces > 8 inches	Cubic Yard	1				
518.60	Repair of Vertical Surfaces < 8 Inches	Square Foot	359				
518.611	Parapet Replacement	Lump Sum	1				
518.70	Repair of Overhead Surfaces < 8 inches	Square Foot	34				
520.211	Expansion Device Pourable Seal	Lump Sum	1				
520.221	Expansion Device Modification	Each	1				

**CARRIED FORWARD:**

Item No	Item Description	Units	Approx. Quantities	Unit Prices in Numbers		Bid Amount in Numbers	
				Dollars	Cents	Dollars	Cents
<b>BROUGHT FORWARD:</b>							
521.32	Fabric Trough for Finger Joint	Each	1				
523.52	Bearing Installation	Each	24				
523.5402	Laminated Elastomeric Bearings, Expansion	Each	24				
603.91	Pressure Treated Wood Drain Trough	Linear Foot	200				
607.431	Snow Fence - Ramp J over Route 1 and Ramp M Bridge (2-Bar)	Linear Foot	208				
607.431	Snow Fence - Saco Interchange Bridge (2-Bar)	Linear Foot	544				
607.431	Snow Fence - Scarborough Interchange Bridges (2-Bar)	Linear Foot	324				
607.431	Snow Fence - Gorham Road Bridge (2-Bar)	Linear Foot	348				
607.431	Snow Fence - Exit 44 Bridge over I-95 (2-Bar)	Linear Foot	308				
607.431	Snow Fence - Cummings Road Bridge (2-Bar)	Linear Foot	331				
615.07	Loam	Cubic Yard	5				
618.14	Seeding Method Number 2	Unit	0.5				

**CARRIED FORWARD:**

Item No	Item Description	Units	Approx. Quantities	Unit Prices in Numbers		Bid Amount in Numbers	
				Dollars	Cents	Dollars	Cents
<b>BROUGHT FORWARD:</b>							
619.1201	Mulch	Unit	0.5				
619.1202	Temporary Mulch	Lump Sum	1				
619.1401	Erosion Control Mix	Cubic Yard	40				
629.05	Hand Labor, Straight Time	Hour	100				
631.10	Air Compressor (including operator)	Hour	50				
631.11	Air Tool (including operator)	Hour	100				
631.36	Foreman	Hour	50				
631.40	Welder (including operator)	Hour	10				
645.14	Special Work No.: Route 236 Underpass	Lump Sum	1				
645.14	Special Work No.: Ramp H Underpass	Lump Sum	1				
645.14	Special Work No.: Gorham Road Underpass	Lump Sum	1				
645.14	Special Work No.: Cummings Road Underpass	Lump Sum	1				

**CARRIED FORWARD:**

Item No	Item Description	Units	Approx. Quantities	Unit Prices in Numbers		Bid Amount in Numbers	
				Dollars	Cents	Dollars	Cents
<b>BROUGHT FORWARD:</b>							
652.361	Traffic Control Devices and Maintenance of Traffic Control Devices - Ramp J and Ramp M	Lump Sum	1				
652.361	Traffic Control Devices and Maintenance of Traffic Control Devices - Two Rod Road	Lump Sum	1				
652.361	Traffic Control Devices and Maintenance of Traffic Control Devices - Cummings Road	Lump Sum	1				
652.361	Traffic Control Devices and Maintenance of Traffic Control Devices - Exit 45	Lump Sum	1				
652.361	Traffic Control Devices and Maintenance of Traffic Control Devices - Forest Avenue	Lump Sum	1				
652.361	Traffic Control Devices and Maintenance of Traffic Control Devices - Exit 36	Lump Sum	1				
652.361	Traffic Control Devices and Maintenance of Traffic Control Devices - Exit 42	Lump Sum	1				
652.361	Traffic Control Devices and Maintenance of Traffic Control Devices - Gorham Road	Lump Sum	1				
652.361	Traffic Control Devices and Maintenance of Traffic Control Devices - Exit 44	Lump Sum	1				
652.38	Flaggers	Hour	500				
652.41	Portable-Changeable Message Sign	Each	4				
652.45	Truck Mounted Attenuator	Calendar Day	63	150	00	9450	00

**CARRIED FORWARD:**

Item No	Item Description	Units	Approx. Quantities	Unit Prices in Numbers		Bid Amount in Numbers	
				Dollars	Cents	Dollars	Cents
<b>BROUGHT FORWARD:</b>							
656.50	Baled Hay, in place	Each	20				
656.632	30 inch Temporary Silt Fence	Linear Foot	260				
659.10	Mobilization	Lump Sum	1				
<b>TOTAL:</b>							

### 107.1.1 Substantial Completion

This Subsection is amended by the addition of the following:

Substantially complete shall be defined by the Authority as the following:

- Cummings Road: All bridge deck work, snow fence, sign clip installations, and surface pavement over repair areas complete and roadway(s) opened to normal traffic.
- All Other Locations: All work except bearing installation at Forest Avenue.

Supplemental Liquidated damages on a calendar day basis in accordance with Subsection 107.7.2 shall be assessed for each calendar day that substantial completion is not achieved. Supplemental Liquidated damages for substantial completion will end when substantial completion is accepted by the Resident. If the work remains incomplete at the Contract Completion Date, liquidated damages on a calendar day basis in accordance with Subsection 107.7.2 shall be assessed for each calendar day that Contract completion is not achieved. If substantial completion is not completed by the Contract Completion date both supplemental liquidated damages and liquidated damages will be incurred.

SPECIAL PROVISION

SECTION 518

STRUCTURAL CONCRETE REPAIR

(Granite Curb Joint Mortar and Bedding Mortar Repair)  
(Granite Curb Repair)

518.01 Description

The following sentence is added:

This work shall consist of the removal and replacement of existing deteriorated granite curb joint mortar and granite curb bedding mortar as shown on the plans and as approved by the Resident.

Areas of existing deteriorated granite curb bedding mortar less than ½ inch in depth, will not be repaired under this pay item but shall be repaired as requested by the Resident using hand tools and equipment unit prices. Materials required shall be receipted costs plus 15%. Small tools, defined as those costing less than \$600, shall be incidental to the hand labor item.

518.02 Repair Materials

The following sentence is added:

Mortar shall be an approved epoxy resin mortar or an approved polymer modified cementitious repair mortar.

The following Subsection is added:

518.032 Construction Requirements

The Resident will designate areas where the existing granite curb joint mortar and the existing granite curb bedding mortar is to be repaired.

In areas designated for Granite Curb Joint Mortar Repair, the existing granite curb joint mortar shall be removed between curb sections to a minimum depth of 1 in. from the face of curb. Any loose mortar shall also be removed. The repair area shall be repointed with new mortar and tooled concave at the face of curb. The mortar shall be proportioned, mixed, and applied in accordance with the Manufacturer's recommendations.

In areas designated for Granite Curb Bedding Mortar Repair, the existing granite bedding mortar shall be removed under the curb to a minimum depth of 1 in. from the face of curb. Any loose or deteriorated mortar shall also be removed. The mortar shall be replaced with new mortar and finished with a 45° bevel at the face of curb. The mortar shall be proportioned, mixed, and applied in accordance with the Manufacturer's recommendations.

In areas designated for Granite Curb Repair located at Ramp J, existing granite curb shall be removed and reset. Any unsound concrete behind the granite curb shall be chipped and patched. The removal limits of the unsound concrete shall be in accordance with Standard Specification 518 and the limits shown on the Plans. Patch material shall be in accordance with Special Provision 518, Concrete Patching Materials. The adjacent pavement shall be removed and replaced to the limits shown on the plans. Any loose mortar shall be removed. The joint mortar and bedding mortar shall be replaced. The new bedding mortar shall be finished with a 45° bevel at the face of the curb. The mortar shall be proportioned, mixed, and applied in accordance with the Manufacturer’s recommendations. Granite anchor bars shall be replaced or grouted if necessary and reset into the patched concrete areas. Joints formed by fractures in the existing granite curbs shall be grouted, grout material shall be in accordance with Standard Specification 518.

518.10 Method of Measurement

The following sentence is added:

Granite Curb Joint Mortar and Bedding Mortar Repair will be measured for payment by the linear foot along the face of the curb, horizontally and vertically, complete and accepted.

Granite Curb Repair will be measured for payment by the linear foot along the face of the curb horizontally complete and accepted.

518.11 Basis of Payment

The following sentence is added:

Granite Curb Joint Mortar and Bedding Mortar Repair will be paid for at the contract unit price per linear foot, which includes all materials, labor, equipment, and incidentals necessary to complete the work including removal of existing mortar.

Granite Curb Repair will be paid for at the contract unit price per linear foot, which includes all materials, labor, equipment, and incidentals necessary to complete the work including removal and replacement of existing granite curb, pavement, waterproofing membrane, chipping and patching of concrete adjacent to curb location.

Payment will be made under:

<u>Pay Item</u>		<u>Pay Unit</u>
518.39	Granite Curb Joint Mortar and Bedding Mortar Repair	Linear Foot
518.392	Granite Curb Repair	Linear Foot

SPECIAL PROVISION

SECTION 652

MAINTENANCE OF TRAFFIC

(Specific Project Maintenance of Traffic Requirements)

This Specification describes the specific project maintenance of traffic requirements for this Project.

The following minimum traffic requirements shall be maintained. These requirements may be adjusted based on the traffic volume when authorized by the Authority.

Ramp J Traffic Control Requirements

One-lane traffic shall be maintained on Ramp J at all times in accordance with the details shown on the Plans. The minimum open travel way lane width shall be 15'-0" with the exception of when the existing non-locking compression seal is being removed and replaced in the center portion of the deck, or when the new drainage trough is being installed under the north abutment and access to the work is required from the deck the center portion of the deck.

Any work which does not permit a minimum open travel way lane width of 15'-0" shall be undertaken during permitted work hours when over limit loads are not routinely permitted to move. Over limit moves are not routinely permitted 1/2 hour after sunset to 1/2 hour before sunrise, legal holidays and weekends in July and August. See Appendix A for permitted Ramp J work hours.

Ramp M and Route 1 Southbound Traffic Control Requirements

Two lanes of traffic shall be maintained on Ramp M and Route 1 Southbound at all times except when a Lane 2 closure is required to install the new drainage trough under the north abutment in accordance with the details shown on the Plans or when a Lane 1 or Lane 2 closure is required to install snow fence. The Ramp M Lane 2 closure shall be in accordance with the details shown on the Plans. The minimum open travel way lane width shall be 15'-0".

Two Rod Road Traffic Control Requirements

One lane of traffic in each direction shall be maintained on Two Rod Road at all times except when a lane closure is required to undertake project work; and one lane of bi-directional traffic, controlled by flaggers, shall be maintained on Two Road at all times. One lane of bi-directional traffic shall be in accordance with the Local Road Single Lane Closure details shown on the Plans.

Maine Turnpike Under Two Rod Road Traffic Control Requirements

Three lanes of traffic in each direction shall be maintained on the Maine Turnpike under Two Rod Road except when lane closure(s) are required to undertake project work. Single lane closures and double lane closures shall be in accordance with the Single Lane Closure and Double Lane Closure details shown on the Plans.

See Appendix A for permitted Maine Turnpike Single Lane Closure and Double Lane Closure hours on the Turnpike at Two Rod Road.

#### Cummings Road Traffic Control Requirements

One lane of traffic in each direction shall be maintained on Cummings Road at all times except when a road closure is required to undertake project work. The road closure and detour setup shall be in accordance with the Cummings Road Closure and Detour details shown in the Plans. The detour traffic control items shall remain covered in place until the completion of the adjacent roadway project by the City of South Portland/Town of Scarborough. The adjacent roadway project is likely to be complete by November 1, 2016.

The Cummings Road Closure and Detour is permitted Sunday through Thursday evenings from 8:00 p.m. until 6:00 a.m. the following morning.

#### Maine Turnpike Under Cummings Road Traffic Control Requirements

Two lanes of traffic in each direction shall be maintained on the Maine Turnpike under Cummings Road except when lane closure(s) are required to undertake project work. Single lane closures shall be in accordance with the Single Lane Closure details shown on the Plans, and shall incorporate advance signing and maintenance of traffic devices for Interchange ramp access and egress.

See for Appendix A permitted Maine Turnpike Single Lane Closure hours on the Turnpike at Cummings Road.

#### Forest Avenue Traffic Control Requirements

One lane of traffic in each direction shall be maintained on Forest Avenue at all times with bi-directional lane shifts. The Lane Shift shall be in accordance with the details shown on the Plans. The minimum open travel way lane width shall be 12'-0". Sidewalk closures are also required adjacent to the contractor's staging area and will require temporary closure with signage.

Maine Turnpike Over Forest Avenue Traffic Control Requirements

Two lanes of traffic in each direction shall be maintained on the Maine Turnpike over Forest Avenue at all times. No Maine Turnpike traffic control, northbound or southbound, is required under this contract.

Exit 36 - Saco Interchange Ramps Traffic Control Requirements

One lane of traffic in each direction shall be maintained on the Saco Interchange Bridges at all times in the existing location except when a lane shift is required to undertake project work. The one lane of traffic shall be in accordance with the traffic control details shown on the Plans.

The detailed lane shift is permitted when Maine Turnpike Single Lane Closures and Double Lane Closures are permitted on the Maine Turnpike under Exit 36.

Maine Turnpike Under Exit 36 - Saco Interchange Ramps Traffic Control Requirements

Three lanes of traffic in each direction shall be maintained on the Maine Turnpike under Exit 36 except when lane closure(s) are required to undertake project work. Single lane closures and double lane closures shall be in accordance with the Single Lane Closure and Double Lane Closure details shown on the Plans.

See Appendix A for permitted Maine Turnpike Single Lane Closure and Double Lane Closure hours on the Turnpike at Exit 36.

Exit 42 - Scarborough Interchange Ramps Traffic Control Requirements

One lane of traffic in each direction shall be maintained on the Scarborough Interchange ramps at all times. A right shoulder closure, lane shift and narrow lane are permitted when required to undertake project work. The one lane of traffic shall be in accordance with the traffic control details shown on the Plans.

The detailed lane shift is permitted when Maine Turnpike Single Lane Closures and Double Lane Closures are permitted on the Maine Turnpike under Exit 42, and when over limit loads are not permitted to move on the Turnpike.

Maine Turnpike Under Exit 42 - Scarborough Interchange Ramps Traffic Control Requirements

Three lanes of traffic in each direction shall be maintained on the Maine Turnpike under the Scarborough Interchange ramps except when lane closure(s) are required to undertake project work. Single lane closures and double lane closures shall be in accordance with the Single Lane Closure and Double Lane Closure details shown on the Plans.

See Appendix A for permitted Maine Turnpike Single Lane Closure and Double Lane Closure hours on the Turnpike at Exit 42.

Gorham Road Traffic Control Requirements

One lane of traffic in each direction shall be maintained on the Gorham Road bridges at all times in their existing location except when a narrow lane with minor lane shift is required to undertake project work. The narrow lane with minor lane shift shall be in accordance with the traffic control details shown on the Plans.

The detailed narrow lane with minor lane shift is permitted when Maine Turnpike Single Lane Closures and Double Lane Closures are permitted on the Maine Turnpike under Gorham Road or I-295 SB under Gorham Road.

Maine Turnpike Under Gorham Road Traffic Control Requirements

Three lanes of traffic northbound and two lanes of traffic southbound shall be maintained on the Maine Turnpike under Gorham Road at all times except when lane closure(s) are required to undertake project work. Single lane closures and double lane closures shall be in accordance with the Single Lane Closure and Double Lane Closure details shown on the Plans.

See Appendix A for permitted Maine Turnpike Single Lane Closure and Double Lane Closure hours on the Turnpike at Gorham Road.

Exit 44 - I-295 Southbound On Ramp Under Gorham Road Traffic Control Requirements

Two lanes of traffic in the southbound direction shall be maintained on the I-295 Southbound On ramp under Gorham Road at all times except when lane closure(s) are required to undertake project work. Single lane closures shall be in accordance with the Single Lane Closure details shown on the Plans.

See Appendix A for permitted Maine Turnpike Single Lane Closure hours on the I-295 Southbound On ramp.

Exit 44 - I-295 Southbound Interchange Ramp Over I-95 Traffic Control Requirements

Two lanes of traffic in the southbound direction shall be maintained on I-295 Southbound Interchange Ramp at all times except when a lane closure, shoulder closure and lane shift is required to undertake project work. The lane and shoulder closures with minor lane shift shall be in accordance with the traffic control details shown on the Plans.

The detailed lane and shoulder closures with the minor lane shift is permitted when Maine Turnpike Single Lane Closures and Double Lane Closures are permitted on the Maine Turnpike under the I-295 Southbound Interchange ramps.

Maine Turnpike Under Exit 44 - I-295 Southbound Interchange Ramps Traffic Control Requirements

Two lanes of traffic in each direction shall be maintained on the Maine Turnpike under the I-295 Southbound Interchange ramps at all times except when lane closure(s) are required to

undertake project work. Single lane closures shall be in accordance with the Single Lane Closure details shown on the Plans.

See Appendix A for permitted Maine Turnpike Single Lane Closure and Double Lane Closure hours at on the Turnpike under the Exit 44 - I-295 Southbound Interchange ramps.

#### Exit 45 - Interchange Ramp Traffic Control Requirements

One lane of traffic in each direction shall be maintained on the Exit 45 Interchange Bridge except when Appendix A permits the Turnpike ramp to be closed.

#### 652.7 Method of Measurement

The first paragraph is revised to read as follows:

Signs and panel markers will not be measured separately but shall be incidental to the various Traffic Control Devices and Maintenance of Traffic Control Devices lump sum pay items. Flashing arrow boards, barricades, battery operated flashing and steady burn lights, drums and cones will not be measured separately but shall be incidental to the various Traffic Control Devices and Maintenance of Traffic Control Devices lump sum pay items. Such payment shall be full compensation for all incidentals necessary to install and maintain the respective devices. Portable change-able message signs will be measured by each unit authorized and installed on the project. No additional payment will be made for devices that require replacement due to poor condition or inadequate retro-reflectivity.

#### 652.8 Basis of Payment

The first paragraph is revised to read as follows:

The accepted quantity of signs and panel markers will not be measured separately but shall be incidental to the various Traffic Control Devices and Maintenance of Traffic Control Devices lump sum pay items. Such payment shall be full compensation for furnishing and installing all signs, sign supports, and all incidentals necessary to complete the installation of the signs.

The second paragraph is revised to read as follows:

The accepted quantity of flashing arrow boards, barricades, battery operated flashing and steady burn lights, drums and cones will not be paid separately but shall be incidental to the various Traffic Control Devices and Maintenance of Traffic Control Devices lump sum pay items. Such payment shall be full compensation for all incidentals necessary to install and maintain the respective devices.

The accepted quantity of portable change-able message signs will be paid for at the contract unit price each of the actual number of portable change-able message signs authorized furnished and installed. Such payment shall be full compensation for all incidentals necessary to install and maintain the respective portable change-able message signs. See Special Provision Section 652 Maintenance of Traffic (General) for additional information.

The accepted quantity of Flaggers will be paid for at the contract unit price per hour for the actual number of Flaggers authorized and furnished. See General Provision 652, and Special Provisions 652.7 Method of Measurement under Section 652 Maintenance of Traffic (General) for additional information.

The accepted quantity of Truck Mounted Attenuator will be paid for at the contract unit price per calendar day for the actual number of Truck Mounted Attenuator days authorized and furnished. See Special Provision Section 652 Maintenance of Traffic (Truck Mounted Attenuator) for additional information.

#### 652.8.1.2 Payment by Lump Sum

The following paragraphs are added to the end of the section:

Traffic Control Devices and Maintenance of Traffic Control Devices - Ramp J and Ramp M will be paid at the contract lump sum price. Such payment shall be full compensation for furnishing and installing all signs, drums, cones, barricades, and flashing arrow panels as shown on the Plans, or necessary for effective traffic control on Ramp J and Ramp M, and for all days the Contractor maintains traffic as specified herein, and for moving devices as many times as necessary; for replacing devices damaged, lost or stolen; and for cleaning, maintaining and removing all devices used for traffic control. Ramp Route 1 Southbound will not be paid separately but shall be incidental to this pay item.

Traffic Control Devices and Maintenance of Traffic Control Devices - Two Rod Road will be paid at the contract lump sum price. Such payment shall be full compensation for furnishing and installing all signs, drums, cones, barricades, and flashing arrow panels as shown on the Plans, or necessary for effective traffic control on Two Rod Road and the Maine Turnpike under Two Rod Road, and for all days the Contractor maintains traffic as specified herein, and for moving devices as many times as necessary; for replacing devices damaged, lost or stolen; and for cleaning, maintaining and removing all devices used for traffic control.

Traffic Control Devices and Maintenance of Traffic Control Devices - Cummings Road will be paid at the contract lump sum price. Such payment shall be full compensation for furnishing and installing all signs, drums, cones, barricades, and flashing arrow panels as shown on the Plans, or necessary for effective traffic control on Cummings Road and the Maine Turnpike under Cummings Road,, and for all days the Contractor maintains traffic as specified herein, and for moving devices as many times as necessary; for replacing devices damaged, lost or stolen; and for cleaning, maintaining and removing all devices used for traffic control.

Traffic Control Devices and Maintenance of Traffic Control Devices - Exit 45 will be paid at the contract lump sum price. Such payment shall be full compensation for furnishing and installing all signs, drums, cones, barricades, and flashing arrow panels as shown on the Plans, or necessary for effective traffic control on the Exit 45 – Interchange Ramp bridge and for all days the Contractor maintains traffic as specified herein, and for moving devices as many times as necessary; for replacing devices damaged, lost or stolen; and for cleaning, maintaining and removing all devices used for traffic control.

Traffic Control Devices and Maintenance of Traffic Control Devices - Forest Avenue will be paid at the contract lump sum price. Such payment shall be full compensation for furnishing and installing all signs, drums, cones, barricades, and flashing arrow panels as shown on the Plans, or necessary for effective traffic control on Forest Avenue, and for all days the Contractor maintains traffic as specified herein, and for moving devices as many times as necessary; for replacing devices damaged, lost or stolen; and for cleaning, maintaining and removing all devices used for traffic control.

Traffic Control Devices and Maintenance of Traffic Control Devices - Exit 36 will be paid at the contract lump sum price. Such payment shall be full compensation for furnishing and installing all signs, drums, cones, barricades, and flashing arrow panels as shown on the Plans, or necessary for effective traffic control on the Exit 36 bridges and the Maine Turnpike the Exit 36 bridges, and for all days the Contractor maintains traffic as specified herein, and for moving devices as many times as necessary; for replacing devices damaged, lost or stolen; and for cleaning, maintaining and removing all devices used for traffic control.

Traffic Control Devices and Maintenance of Traffic Control Devices - Exit 42 will be paid at the contract lump sum price. Such payment shall be full compensation for furnishing and installing all signs, drums, cones, barricades, and flashing arrow panels as shown on the Plans, or necessary for effective traffic control on the Exit 42 bridge and the Maine Turnpike under the Exit 42 bridge, and for all days the Contractor maintains traffic as specified herein, and for moving devices as many times as necessary; for replacing devices damaged, lost or stolen; and for cleaning, maintaining and removing all devices used for traffic control.

Traffic Control Devices and Maintenance of Traffic Control Devices - Gorham Road will be paid at the contract lump sum price. Such payment shall be full compensation for furnishing and installing all signs, drums, cones, barricades, and flashing arrow panels as shown on the Plans, or necessary for effective traffic control on Gorham Road and the Maine Turnpike and I-295 Southbound On ramp under Gorham Road, and for all days the Contractor maintains traffic as specified herein, and for moving devices as many times as necessary; for replacing devices damaged, lost or stolen; and for cleaning, maintaining and removing all devices used for traffic control.

Traffic Control Devices and Maintenance of Traffic Control Devices - Exit 44 will be paid at the contract lump sum price. Such payment shall be full compensation for furnishing and installing all signs, drums, cones, barricades, and flashing arrow panels as shown on the Plans, or necessary for effective traffic control on the Exit 44 - I 295 Southbound On bridge and the Maine Turnpike under the Exit 44 - I 295 Southbound On bridge, and for all days the Contractor maintains traffic as specified herein, and for moving devices as many times as necessary; for replacing devices damaged, lost or stolen; and for cleaning, maintaining and removing all devices used for traffic control.

652.8.2 Other Items

The following pay items are added:

652.361	Traffic Control Devices and Maintenance of Traffic Control Devices - Ramp J and Ramp M	Lump Sum
652.361	Traffic Control Devices and Maintenance of Traffic Control Devices - Two Rod Road	Lump Sum
652.361	Traffic Control Devices and Maintenance of Traffic Control Devices - Cummings Road	Lump Sum
652.361	Traffic Control Devices and Maintenance of Traffic Control Devices - Exit 45	Lump Sum
652.361	Traffic Control Devices and Maintenance of Traffic Control Devices - Forest Avenue	Lump Sum
652.361	Traffic Control Devices and Maintenance of Traffic Control Devices - Exit 36	Lump Sum
652.361	Traffic Control Devices and Maintenance of Traffic Control Devices - Exit 42	Lump Sum
652.361	Traffic Control Devices and Maintenance of Traffic Control Devices - Gorham Road	Lump Sum
652.361	Traffic Control Devices and Maintenance of Traffic Control Devices - Exit 44	Lump Sum

Date: 4/14/2016

ITEM NO.	ITEM DESCRIPTION	UNIT	RAMP J	TWO ROD ROAD	CUMMINGS ROAD	EXIT 45	FOREST AVENUE	SNOW FENCE	TOTALS
202.191	Removing Existing Drain Troughs	LS					1		1
202.2021	Removing Pavement Surface - Bridge Deck	SY			34				34
403.210	Hof Mix Asphalt, 9.5 mm Nominal Maximum Size	TON			2				2
409.15	Bituminous Tack Coat, Applied	GAL			2				2
419.30	Sawing Bituminous Pavement	LF			230				230
502.219	Structural Concrete, Abutments and Retaining Walls (1 CY)	LS					1		1
507.095	Aluminum Bridge Railing - Splice Modification	EA						92	92
514.06	Curing Box for Concrete Cylinders	EA		1			1		2
515.201	Pigmented Protective Coating for Concrete Surfaces	SY					920		920
515.202	Clear Protective Coating for Concrete Surfaces	SY		330					330
518.39	Granite Curb Joint Mortar and Bedding Mortar Repair	LF		51					51
518.392	Granite Curb Repair	LF	7						7
518.40	Epoxy Injection Crack Repair	LF		75			48		123
518.43	Parapet Joint Repair	LF		290					290
518.51	Repair of Upward Facing Surfaces - below Reinforcing Steel < 8 inches	SF					147		147
518.511	Partial Depth Concrete Deck Repairs	SF			100				100
518.512	Full Depth Concrete Deck Repairs	SF			90				90
518.52	Repair of Upward Facing Surfaces > 8 inches	CY	1						1
518.60	Repair of Vertical Surface < 8 inches	SF		20			339		359
518.611	Parapet Replacement	LS		1					1
518.70	Repair of Overhead Surfaces < 8 inches	SF					34		34
520.211	Expansion Device Pourable Seal	LS				1			1
520.221	Expansion Device Modification	EA	1						1
521.32	Fabric Trough for Finger Joint	EA	1						1
523.52	Bearing Installation	EA					24		24
523.5402	Laminated Elastomeric Bearings, Expansion	EA					24		24
603.91	Pressure Treated Wood Drain Trough	LF					200		200
607.431	Snow Fence - Ramp J over Route 1 SB and Ramp M (2-Bar)	LF	208						208
607.431	Snow Fence - Saco Interchange Bridge (2-Bar)	LF						544	544
607.431	Snow Fence - Scarborough Interchange Bridge (2-Bar)	LF						324	324
607.431	Snow Fence - Gorham Road Bridge (2-Bar)	LF						348	348
607.431	Snow Fence - Exit 44 Bridge over I-95 (2-Bar)	LF						308	308
607.431	Snow Fence - Cummings Road Bridge (2-Bar)	LF			331				331
615.07	Loam	CY					5		5
618.14	Seeding Method Number 2	UNIT					0.5		0.5
619.1201	Mulch	UNIT					0.5		0.5
619.1202	Temporary Mulch	LS	0.1	0.1	0.1	0.1	0.6		1
619.1401	Erosion Control Mix	CY					40		40
629.05	Hand Labor, Straight Time	HR	20	20	20	20	20		100
631.10	Air Compressor (including operator)	HR	10	10	10	10	10		50
631.11	Air Tool (including operator)	HR	20	20	20	20	20		100
631.36	Foreman	HR	10	10	10	10	10		50
631.40	Welder (including operator)	HR	10						10
645.14	Special Work No.: Route 236 Underpass	LS						1	1
645.14	Special Work No.: Ramp H Underpass	LS						1	1
645.14	Special Work No.: Gorham Road Underpass	LS						1	1
645.14	Special Work No.: Cummings Road Underpass	LS			1				1
652.361	Traffic Control Devices and Maintenance of Traffic Control Devices - Ramp J and Ramp M	LS	1						1
652.361	Traffic Control Devices and Maintenance of Traffic Control Devices - Two Rod Road	LS		1					1
652.361	Traffic Control Devices and Maintenance of Traffic Control Devices - Cummings Road	LS			1				1
652.361	Traffic Control Devices and Maintenance of Traffic Control Devices - Exit 45	LS				1			1
652.361	Traffic Control Devices and Maintenance of Traffic Control Devices - Forest Avenue	LS					1		1
652.361	Traffic Control Devices and Maintenance of Traffic Control Devices - Exit 36	LS						1	1
652.361	Traffic Control Devices and Maintenance of Traffic Control Devices - Exit 42	LS						1	1
652.361	Traffic Control Devices and Maintenance of Traffic Control Devices - Gorham Road	LS						1	1
652.361	Traffic Control Devices and Maintenance of Traffic Control Devices - Exit 44	LS						1	1
652.38	Flaggers	HR		500					500
652.41	Portable-Changable Message Sign	EA			4				4
652.45	Truck Mounted Attenuator	CD	12	10	9			32	63
656.50	Baled Hay, in place	EA	5	5	5		5		20
656.632	30 inch Temporary Silt Fence	LF					260		260
659.10	Mobilization	LS	0.1	0.1	0.2	0.1	0.3	0.2	1

**NOTE**

THIS PROJECT INCLUDES WORK AT THE FOLLOWING LOCATIONS:

BRIDGE REPAIRS: RAMP J OVER ROUTE 1 SOUTHBOUND AND RAMP M (MM 1.59), TWO ROD ROAD UNDERPASS (MM 42.0), CUMMINGS ROAD UNDERPASS (MM 44.6), EXIT 45 UNDERPASS (MM 44.9) AND FOREST AVENUE OVERPASS (MM 50.0)

SNOW FENCE INSTALLATIONS: RAMP J OVER ROUTE 1 SOUTHBOUND AND RAMP M (MM 1.59), SACO INTERCHANGE EXIT 36 (MM 35.7), SCARBOROUGH INTERCHANGE EXIT 42 (MM 42.5), I-295 SOUTHBOUND UNDERPASS EXIT 44 (MM 44.3), GORHAM ROAD UNDERPASS (MM 44.0), GORHAM ROAD OVER I-295 (MM 44.0) AND CUMMINGS ROAD UNDERPASS (MM 44.6).

BRIDGE MOUNTED SIGN CLIP INSTALLATION: ROUTE 236 UNDERPASS (MM 1.25), RAMP H UNDERPASS (MM 1.60), GORHAM ROAD UNDERPASS (MM 44.0) AND CUMMINGS ROAD UNDERPASS (MM 44.6).

**GENERAL NOTES**

1. THE CONTRACTOR IS REQUIRED TO CALL DIG SAFE AT 1-888-344-7233 AT LEAST 72 HOURS PRIOR TO START OF WORK.

2. THE CONTRACTOR SHALL NOTIFY THE RESIDENT 10 DAYS PRIOR TO CONSTRUCTION SO THE RESIDENT CAN ARRANGE FOR MAINE TURNPIKE UNDERGROUND UTILITY LOCATION. ALL PROPOSED SIGN LOCATIONS AND EXCAVATION LOCATIONS SHALL BE MARKED AT THE NOTIFICATION TIME. EXCAVATION WILL NOT BE PERMITTED UNTIL THE AUTHORITY HAS LOCATED AND MARKED ITS UNDERGROUND UTILITIES, OR NOTIFIED THE RESIDENT THERE ARE NO UNDERGROUND UTILITIES IN THE MARKED AREAS.

THE AUTHORITY HAS PROGRAMMED TWO FIELD VISITS FOR MAINE TURNPIKE UTILITY COORDINATION ON THIS PROJECT. SHOULD THE CONTRACTOR NEED ADDITIONAL SIGN LOCATIONS AND/OR ADDITIONAL EXCAVATION LOCATIONS MARKED, OR SHOULD THE CONTRACTOR FAIL TO MAINTAIN THE AUTHORITY'S PREVIOUSLY ESTABLISHED DIG SAFE MARKS, THE AUTHORITY SHALL DEDUCT THE ADDED MARKING COSTS FROM THE CONTRACTOR'S PAYMENTS.

3. EXCAVATIONS ACCOMPLISHED AS PART OF THIS PROJECT SHALL BE CONSTRUCTED IN ACCORDANCE WITH OSHA SUBPART P OF 29 CFR PART 1926.650-652 (CONSTRUCTION STANDARDS FOR EXCAVATIONS).

4. ALL DETAILS SHALL BE IN CONFORMANCE WITH MAINE DEPARTMENT OF TRANSPORTATION (MAINEDOT) STANDARD DETAILS HIGHWAY AND BRIDGES 2014 LATEST REVISIONS AND MDT BEST MANAGEMENT PRACTICES FOR EROSION AND SEDIMENT CONTROL LATEST REVISION UNLESS OTHERWISE INCLUDED IN THESE PLANS.

5. REINFORCING STEEL SHALL HAVE A CLEAR COVER OF 2" UNLESS OTHERWISE NOTED.

6. CHAMFER ALL EXPOSED CONCRETE EDGES 3/4" UNLESS OTHERWISE NOTED.

7. WHERE DRILLING AND ANCHORING OF REINFORCING STEEL IS SPECIFIED THE CONTRACTOR SHALL USE A MATERIAL LISTED ON THE MAINEDOT PREQUALIFIED LIST OF CONCRETE ADHESIVE ANCHORING MATERIALS. THE DEPTH OF EMBEDMENT SHALL BE SUFFICIENT TO DEVELOP 125% OF THE YIELD STRENGTH OF THE BAR, BUT SHALL BE NO LESS THAN THE MINIMUM DEPTH OF EMBEDMENT WHEN SPECIFIED. WHERE MINIMUM EMBEDMENT DEPTHS HAVE NOT BEEN SPECIFIED BAR LENGTHS HAVE BEEN DEVELOPED BASED ON AN ASSUMED EMBEDMENT DEPTH OF 9" FOR #5 BARS AND 11" FOR #6 BARS. THE CONTRACTOR SHALL VERIFY THE REQUIRED DEPTH OF EMBEDMENT AND ADJUST THE REQUIRED BAR LENGTHS AS REQUIRED.

8. THERE ARE NO PERMANENT OR TEMPORARY EASEMENTS ASSOCIATED WITH THIS PROJECT. ALL WORK SHALL BE COMPLETED WITHIN THE EXISTING RIGHT OF WAY.

9. THE CONTRACTOR SHALL SUBMIT THE PROPOSED STAGING AREA(S) TO THE RESIDENT PRIOR TO STARTING WORK.

10. COPIES OF AS-BUILT PLANS ARE POSTED ON THE MAINE TURNPIKE AUTHORITY WEBSITE AT WWW.MAINE TURNPIKE.COM/PROJECT-AND-PLANNING/CONSTRUCTION CONTRACTS. THE COMPLETENESS AND ACCURACY OF THESE PLANS IS NOT GUARANTEED.

**DRAINAGE NOTES**

1. NO EXISTING DRAINAGE SHALL BE ABANDONED, REMOVED OR PLUGGED WITHOUT APPROVAL OF THE RESIDENT.

**EROSION CONTROL**

1. ADDITIONAL MEASURES MAY BE PROPOSED BY THE CONTRACTOR DUE TO SITE OR WEATHER CONDITIONS. THE RESIDENT MAY DIRECT THE CONTRACTOR TO IMPLEMENT ADDITIONAL MEASURES. ANY ADDITIONAL MEASURES APPROVED BY THE RESIDENT WILL BE MEASURED FOR PAYMENT.

2. ALL TEMPORARY EROSION CONTROL DEVICES SHALL BE INSTALLED IN ACCORDANCE WITH THE MAINE DEPARTMENT OF TRANSPORTATION BEST MANAGEMENT PRACTICES.

**GUARDRAIL NOTES**

1. AT THE END OF THE WORK DAY, EVERYDAY, THE CONTRACTOR IS REQUIRED TO HAVE AN APPROVED CRASHWORTHY END TREATMENT ON ALL GUARDRAIL WITHIN ALL WORK AREAS THAT ARE ACCESSIBLE TO TRAFFIC.

2. CONNECTIONS FOR PROPOSED GUARDRAIL TO EXISTING GUARDRAIL SHALL BE INCIDENTAL TO THE PROPOSED GUARDRAIL ITEMS.

3. FOR ALL NEW GUARDRAIL TYPE 3d W-BEAM, OFFSET BLOCKS SHALL BE NON-WOOD CONFORMING TO NCHRP 350 TEST LEVEL.

4. ALL PROPOSED GUARDRAIL AND RESET GUARDRAIL SHALL BE INSTALLED IN A MANNER TO AVOID DRAINAGE STRUCTURES AND UTILITIES.

**MATERIALS**

**CONCRETE**

SUBSTRUCTURE CONCRETE REPAIRS SHALL BE PER SPECIAL PROVISION 518. ALL OTHER CONCRETE SHALL BE CLASS AAA.

**REINFORCING STEEL**

AASHTO M31, GRADE 60. ALL REINFORCING SHALL BE EPOXY-COATED.

**ANCHOR RODS**

SHALL MEET THE REQUIREMENTS OF ASTM F1554, GRADE 55 AND SHALL BE SWEDGED OR THREADED ON THE EMBEDDED PORTION OF THE ROD.

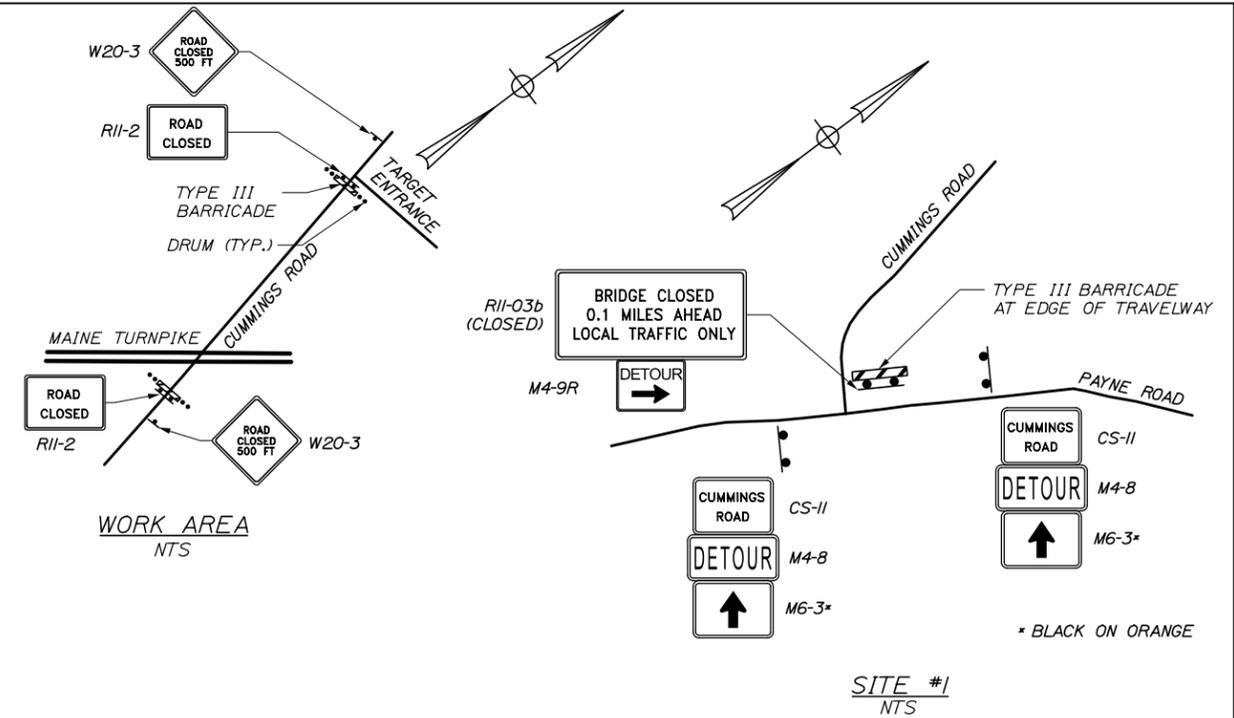
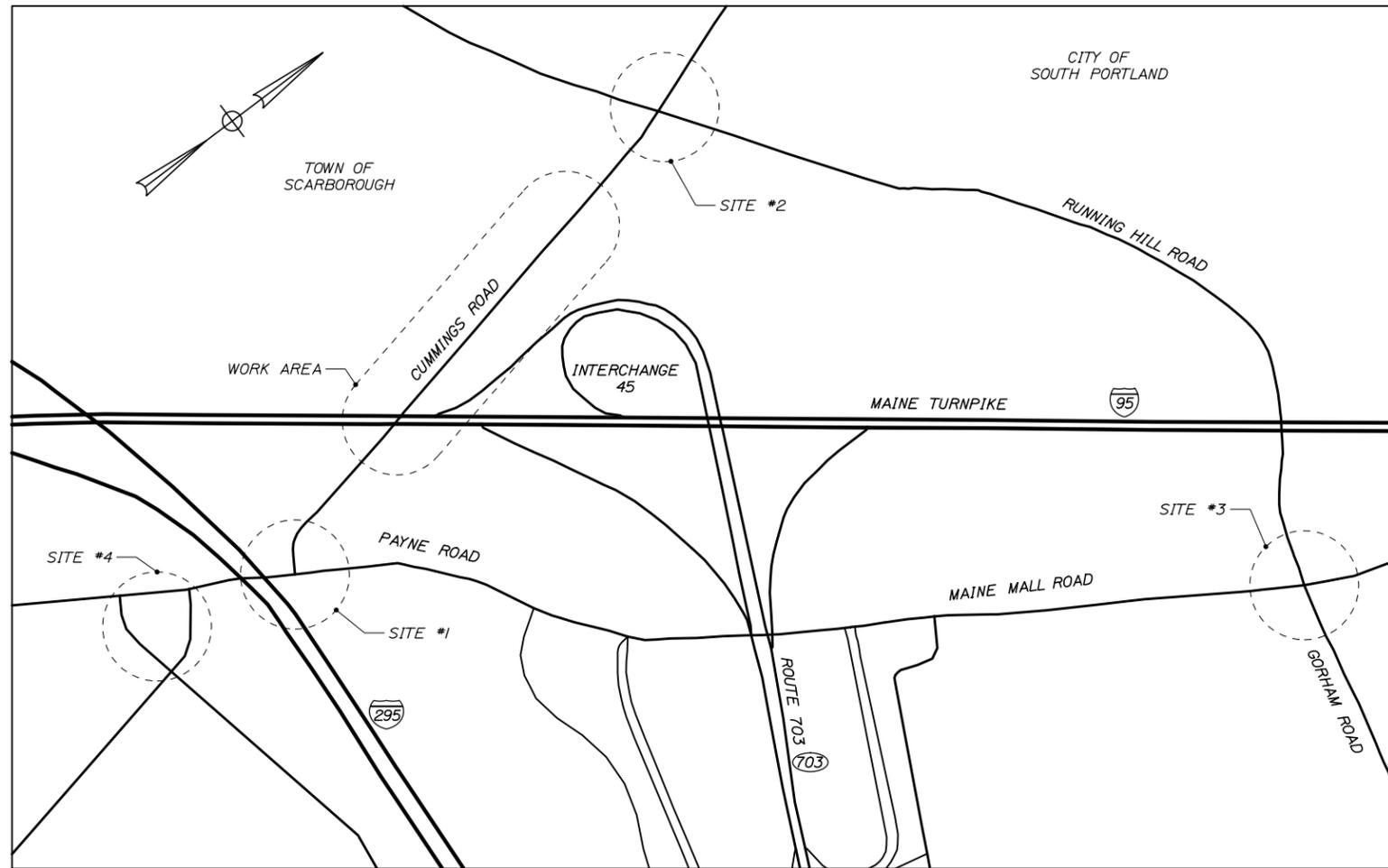
**LIST OF ABBREVIATIONS**

- ABUT. - ABUTMENT
- ADDL. - ADDITIONAL
- ALT. - ALTERNATE
- APPROX. - APPROXIMATELY
- BOT. - BOTTOM
- BRG. - BEARING
- CL. - CLEAR
- CL. - CENTERLINE
- CONC. - CONCRETE
- CONSTR. - CONSTRUCTION
- DEMO. - DEMOLITION
- DIA. - DIAMETER
- EA. - EACH
- EB - EASTBOUND
- E.F. - EACH FACE
- EL. - ELEVATION
- EQ. - EQUAL
- EXIST. - EXISTING
- EXP. - EXPANSION
- F.F. - FAR FACE
- JT. - JOINT
- MAX. - MAXIMUM
- MAINEDOT - MAINE DEPARTMENT OF TRANSPORTATION
- MIN. - MINIMUM
- MTA - MAINE TURNPIKE AUTHORITY
- NB - NORTHBOUND
- N.F. - NEAR FACE
- N.T.S. - NOT TO SCALE
- PED. - PEDESTAL
- PGL - PROFILE GRADE LINE
- PL. - PLATE
- PROP. - PROPOSED
- P.S.I. - POUNDS per SQUARE INCH
- RDWY. - ROADWAY
- SHLDR. - SHOULDER
- SB - SOUTHBOUND
- SP. - SPACES
- STA. - STATION
- T.&B. - TOP & BOTTOM
- TPKE. - TURNPIKE
- TYP. - TYPICAL
- U.O.N. - UNLESS OTHERWISE NOTED
- VERT. - VERTICAL
- WB - WESTBOUND
- W.P. - WORKING POINT

Filename: 002\_Quantities.dgn

Scale:	Designed by:		HNTB CORPORATION 340 County Road, Suite 6-C Westbrook, ME 04092 TEL (207) 774-5155 FAX (207) 228-0909		<b>THE GOLD STAR MEMORIAL HIGHWAY</b>	BRIDGE REPAIRS  ESTIMATED QUANTITIES AND GENERAL NOTES	Contract 2016.05 Addendum No. 1 Page 23 of 33																		
<table border="1" style="width: 100%; border-collapse: collapse;"> <thead> <tr> <th>No.</th> <th>Revision</th> <th>By</th> <th>Date</th> </tr> </thead> <tbody> <tr> <td>△</td> <td>Addendum #1 - Updated Item List</td> <td>AES</td> <td>04/16</td> </tr> </tbody> </table>		No.	Revision	By	Date	△	Addendum #1 - Updated Item List	AES	04/16	CONSULTANT PROJECT MANAGER: Ashley E. Stephens, P.E.															
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				MTA PROJECT MANAGER: Ralph C. Norwood, IV, P.E., P.T.O.E.		CONTRACT: 2016.05																			
						SHEET NUMBER: ES-01																			
						2 OF 35																			

Date: 4/14/2016



**CUMMINGS ROAD TRAFFIC CONTROL**

MAINLINE TRAFFIC CONTROL SHALL CONSIST OF SINGLE LANE CLOSURES USING THE STANDARD TURNPIKE MAINLINE TRAFFIC CONTROL DETAILS. NO MAINLINE TRAFFIC STOPPAGES ARE ANTICIPATED. THE APPROACH TO THE EXIT 45 NB OFF RAMP SHALL BE SIGNED FOR EXIT OPEN, AND THE EXIT 45 SB ON RAMP SHALL BE SIGNED FOR ROAD WORK AHEAD, LEFT/RIGHT LANE CLOSED AND SPEED LIMIT 50 AHEAD.

**SIGNS REQUIRED:**

- ALL STANDARD TURNPIKE SINGLE AND DOUBLE MAINLINE LANE CLOSURE SIGNS
- W20-1(AHEAD) - ROAD WORK AHEAD
- W20-5L (AHEAD) - LEFT LANE CLOSED AHEAD
- W20-5R (AHEAD) - RIGHT LANE CLOSED AHEAD
- W24-1L - LANE SHIFT LEFT ICON
- W24-1R - LANE SHIFT RIGHT ICON
- E5-1 - EXIT/ ARROW ICON
- E5-2 - EXIT OPEN

**M-O-T DEVICES REQUIRED:**

- ARROW BOARDS
- DRUMS
- TRUCK MOUNTED ATTENUATOR
- PORTABLE CHANGEABLE MESSAGE SIGNS

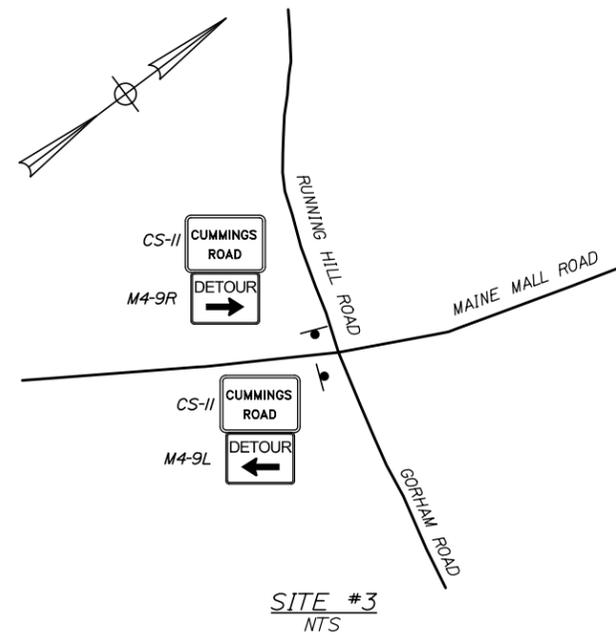
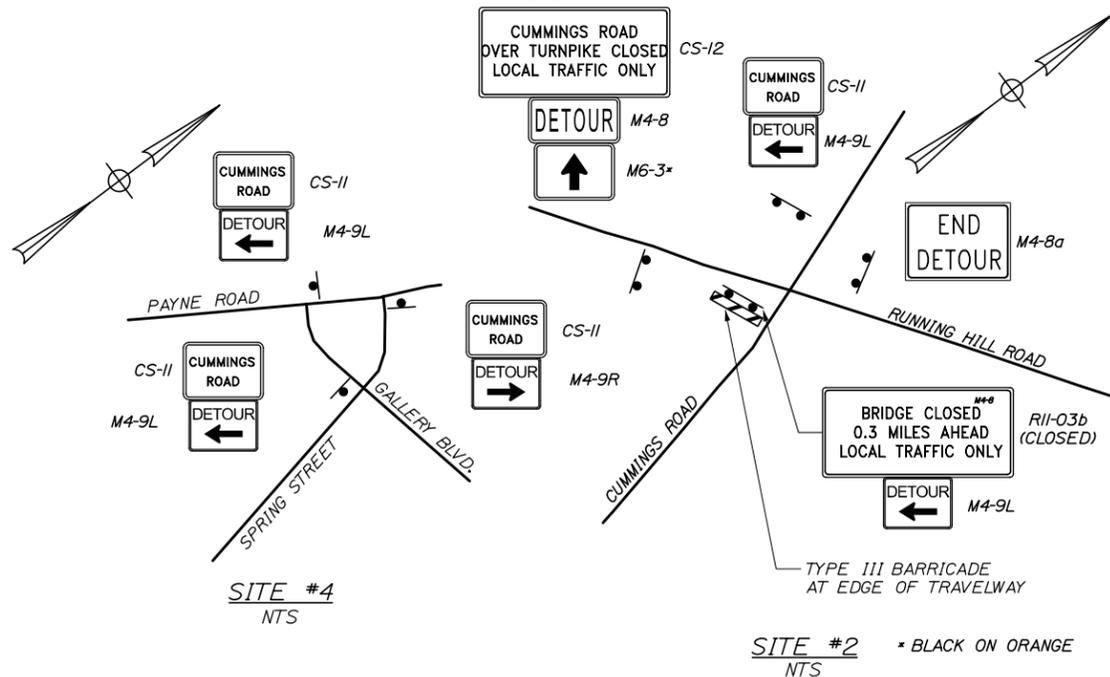
CUMMINGS ROAD TRAFFIC CONTROL SHALL CONSIST OF A DETOUR ROUTING THE TRAFFIC OVER THE BRIDGE TO THE EAST ACROSS PAYNE ROAD, MAINE MALL ROAD AND RUNNING HILL ROAD. TEMPORARY LOCAL ROAD TRAFFIC STOPPAGES ARE NOT ANTICIPATED.

**SIGNS REQUIRED:**

- R11-03B (BRIDGE) - BRIDGE CLOSED/ X.X MILES AHEAD/ LOCAL TRAFFIC ONLY
- W20-3 (500 FT) - ROAD CLOSED 500 FT
- R11-2 - ROAD CLOSED
- M4-8 - DETOUR
- M4-8a - END DETOUR
- M4-9L - DETOUR LEFT
- M4-9R - DETOUR RIGHT
- M6-3 - ARROW UP ICON (BLACK ON ORANGE)
- CS-II - CUMMINGS ROAD
- CS-12 - CUMMINGS ROAD OVER TURNPIKE CLOSED

**M-O-T DEVICES REQUIRED:**

- DRUMS
- TYPE III BARRICADES
- PORTABLE CHANGEABLE MESSAGE SIGNS
- LIGHT TOWERS (IF NIGHT WORK ANTICIPATED OR REQUIRED)



Scale:

No.	Revision	By	Date
1	Addendum #1 - Detour Added	AES	04/16

Designed by:

**HNTB**

CONSULTANT PROJECT MANAGER: Ashley E. Stephens, P.E.

	By	Date	By	Date	
Designed	CAH	03/16	Checked	AES	03/16
Drawn	SLR	03/16	In Charge of	RAL	03/16

HNTB CORPORATION  
 340 County Road, Suite 6-C  
 Westbrook, ME 04092  
 TEL (207) 774-5155  
 FAX (207) 228-0909

**MAINE TURNPIKE**

**THE GOLD STAR MEMORIAL HIGHWAY**

MTA PROJECT MANAGER: Ralph C. Norwood IV, P.E., P.T.O.E.

BRIDGE REPAIRS

MAINTENANCE OF TRAFFIC CUMMINGS ROAD

SHEET NUMBER: MOT-07

9 OF 35

CONTRACT: 2016.05

Contract 2016.05  
 Addendum No. 1  
 Page 24 of 33

Filename: 009\_Cummings Road Detour.dgn

Date: 4/14/2016

IDENTIFICATION NUMBER	SIZE OF SIGN		TEXT	TEXT DIMENSIONS (INCHES)			NUMBER OF SIGNS REQUIRED	COLOR		BORDER RADIUS	AREA IN SQUARE FEET
	WIDTH	HEIGHT		LETTER HEIGHT	VERTICAL SPACING	ARROW RTE. MKR.		BACK-GROUND	LEGEND BORDER		
W20-5a(L) (1/2 MILE)	48"	48"		TEXT DIMENSIONS SHALL CONFORM TO "2004 EDITION - STANDARD HIGHWAY SIGNS - 2012 SUPPLEMENT"			TBD BY CONTRACTOR	COLORS SHALL CONFORM TO "2004 EDITION - STANDARD HIGHWAY SIGNS - 2012 SUPPLEMENT"			16.00 (XXX)
W20-5L (1/2 MILE) (AHEAD)	48"	48"									16.00 (XXX)
W20-5a(R) (1/2 MILE)	48"	48"									16.00 (XXX)
W20-5R (1/2 MILE) (AHEAD)	48"	48"									16.00 (XXX)
W20-7a	48"	48"									16.00 (XXX)
W21-5aR	48"	48"									16.00 (XXX)
W21-5aL	48"	48"									16.00 (XXX)
W24-1L	48"	48"									16.00 (XXX)
W24-1R	48"	48"									16.00 (XXX)
W11-1	30"	30"									6.25 (XXX)
W16-1	24"	30"									5.00 (XXX)

IDENTIFICATION NUMBER	SIZE OF SIGN		TEXT	TEXT DIMENSIONS (INCHES)			NUMBER OF SIGNS REQUIRED	COLOR		BORDER RADIUS	AREA IN SQUARE FEET
	WIDTH	HEIGHT		LETTER HEIGHT	VERTICAL SPACING	ARROW RTE. MKR.		BACK-GROUND	LEGEND BORDER		
W21-5bR (1000 FT)	48"	48"		TEXT DIMENSIONS SHALL CONFORM TO "2004 EDITION - STANDARD HIGHWAY SIGNS - 2012 SUPPLEMENT"			TBD BY CONTRACTOR	COLORS SHALL CONFORM TO "2004 EDITION - STANDARD HIGHWAY SIGNS - 2012 SUPPLEMENT"			16.00 (XXX)
W21-5bL (1000 FT)	48"	48"									16.00 (XXX)
CS-3	48"	48"		6" 6" 6"	4" 4"			ORANGE	BLACK		16.00 (XXX)
CS-10	24"	24"		TEXT DIMENSIONS SHALL CONFORM TO "2004 EDITION - STANDARD HIGHWAY SIGNS - 2012 SUPPLEMENT"				COLORS SHALL CONFORM TO "2004 EDITION - STANDARD HIGHWAY SIGNS - 2012 SUPPLEMENT"			4.00 (XXX)
R9-9	30"	18"									3.75 (XXX)
R9-11 L R9-11 R	24"	12"		L-SHOWN, R-OPPOSITE							2.00 (XXX)
M4-9L M4-9R	30"	24"		R-SHOWN, L-OPPOSITE							5.00 (-)
R11-2	48"	30"									10.00 (-)
R11-3b (CLOSED)	60"	30"									12.50 (-)
W20-3	48"	48"									16.00 (XXX)
M4-8	30"	15"									3.10 (XXX)
M4-8a	24"	18"									3.00 (XXX)
M6-3	21"	15"						ORANGE	BLACK		2.20 (XXX)
CS-11	30"	24"						ORANGE	BLACK		5.00 (XXX)
CS-12	60"	30"						WHITE	BLACK		12.50 (XXX)

Filename: 015\_Sign Summary 2.dgn

Scale:

No.	Revision	By	Date
1	Addendum #1 - Added Detour Signs	AES	04/16

Designed by:

**HNTB**

CONSULTANT PROJECT MANAGER: Ashley E. Stephens, P.E.

	By	Date	By	Date	
Designed	CAH	03/16	Checked	AES	03/16
Drawn	SLR	03/16	In Charge of	RAL	03/16

HNTB CORPORATION  
 340 County Road, Suite 6-C  
 Westbrook, ME 04092  
 TEL (207) 774-5155  
 FAX (207) 228-0909

**MAINE TURNPIKE**

**THE GOLD STAR MEMORIAL HIGHWAY**

MTA PROJECT MANAGER: Ralph C. Norwood, IV, P.E., P.T.O.E.

BRIDGE REPAIRS

SIGN SUMMARY 2

SHEET NUMBER: MOT-13

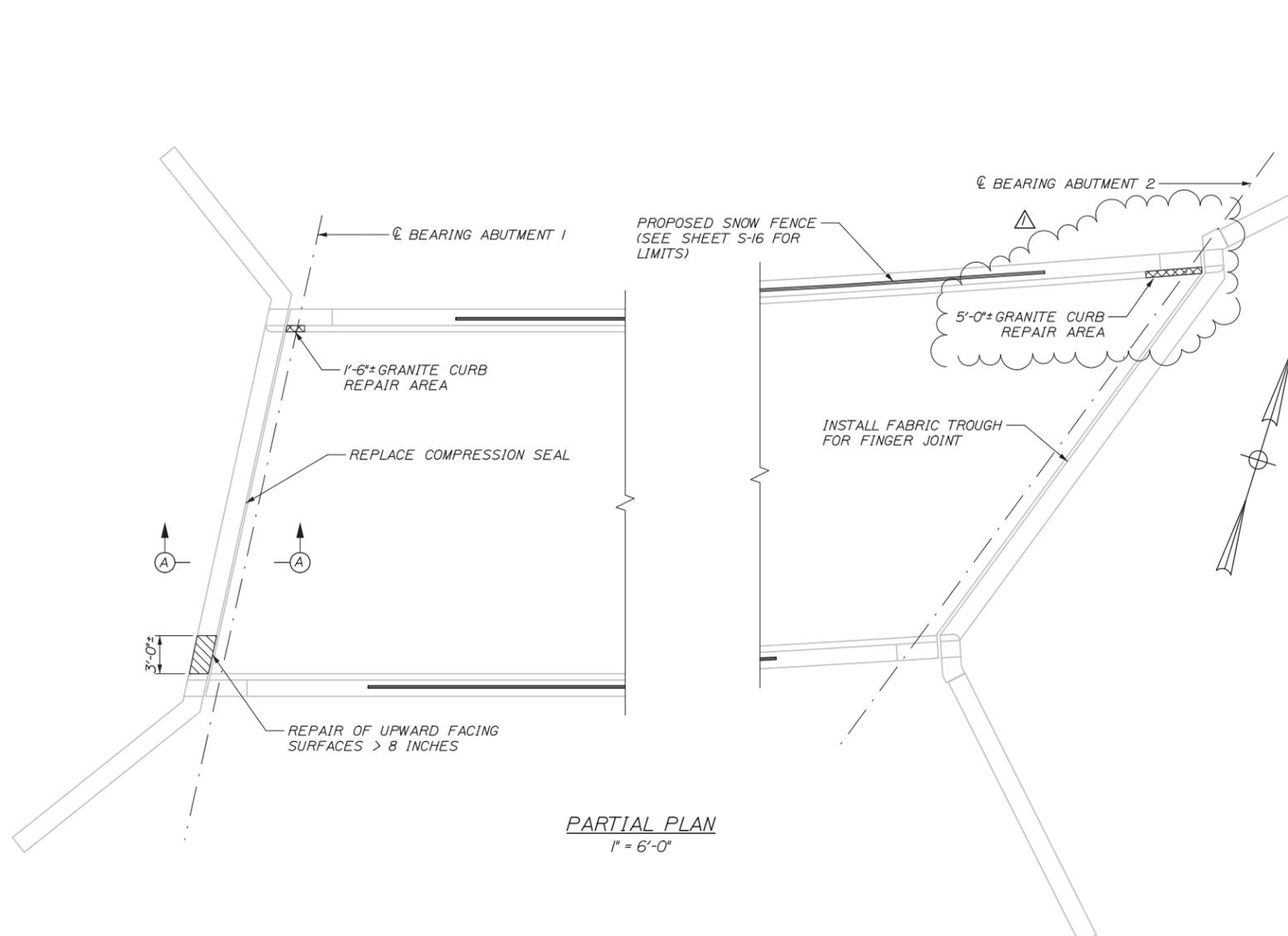
15 OF 35

CONTRACT: 2016.05

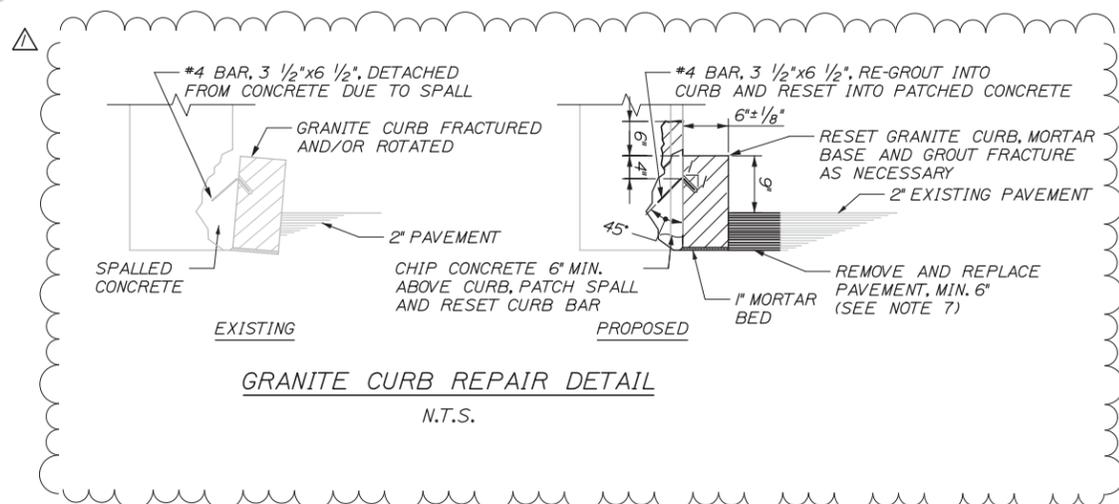
Contract 2016.05  
 Addendum No. 1  
 Page 25 of 33

Date: 4/15/2016

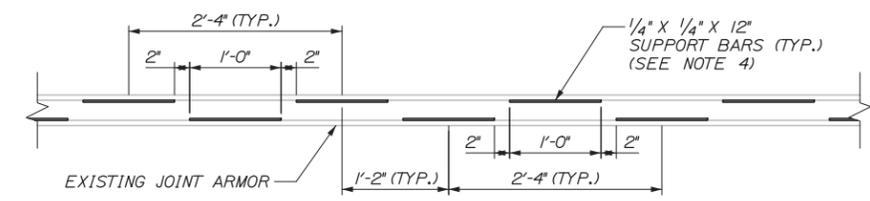
Filename: 016\_Ramp J Deck Plan & Comp Seal.dgn



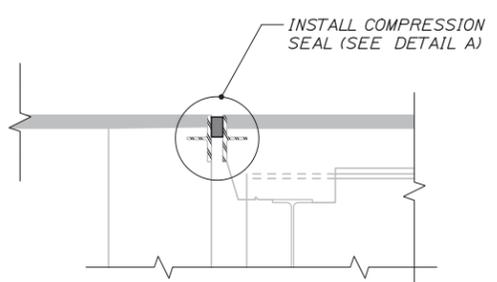
**PARTIAL PLAN**  
1" = 6'-0"



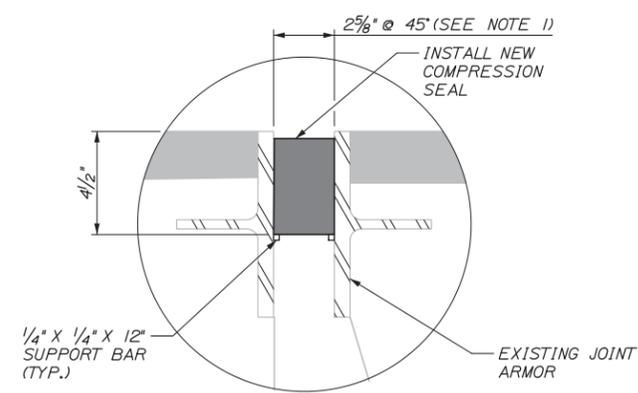
**GRANITE CURB REPAIR DETAIL**  
N.T.S.



**SUPPORT BAR LAYOUT DETAIL**  
**PLAN VIEW**  
1" = 1'-0"



**SECTION A-A**  
3/4" = 1'-0"



**DETAIL A**  
3" = 1'-0"

**NOTES:**

- CONTRACTOR SHALL FIELD VERIFY THE JOINT OPENING WIDTH PRIOR TO FABRICATION AND INSTALLATION OF THE NEW COMPRESSION SEAL, SEE DETAIL A.
- COMPRESSION SEAL SHALL BE A WATSON BOWMAN ACME ELASTOMERIC COMPRESSION SEAL, MODEL NUMBER WA-350 OR APPROVED EQUAL.
- COMPRESSION SEAL SHALL BE INSTALLED IN STRICT CONFORMANCE WITH THE MANUFACTURER'S SPECIFICATIONS.
- PRIOR TO INSTALLATION OF COMPRESSION SEAL, THE RESIDENT AND CONTRACTOR SHALL INSPECT THE CONDITION OF THE SUPPORT BARS. IN LOCATIONS WHERE THE SUPPORT BAR IS MISSING OR DAMAGED, THE CONTRACTOR SHALL INSTALL NEW SUPPORT BARS CONSISTENT WITH DETAIL A AND THE SUPPORT BAR LAYOUT DETAIL. THIS WORK SHALL BE INCIDENTAL TO ITEM 520.221, EXPANSION DEVICE MODIFICATION.
- FOR FABRIC TROUGH FOR FINGER JOINT DETAILS SEE SHEET S-02.
- CONCRETE HEADER REPAIR SHALL BE PAID FOR UNDER ITEM 518.52 REPAIR OF UPWARD FACING SURFACES > 8 INCHES IN ACCORDANCE WITH SPECIFICATIONS. SEE SHEET S-07 FOR REMOVAL PROCEDURE AND CONCRETE SURFACE REPAIR PROCEDURE NOTES. PAVEMENT OVER REPAIR AREA SHALL BE REPLACED WITH PERMA-PATCH PERMANENT PAVEMENT REPAIR MATERIAL OR APPROVED EQUAL. PAVEMENT MATERIAL SHALL BE INCIDENTAL TO THE REPAIR ITEM 518.52 REPAIR OF UPWARD FACING SURFACES > 8 INCHES.
- THE CONTRACTOR SHALL REMOVE THE PAVEMENT AND MEMBRANE ADJACENT TO THE GRANITE CURB REPAIR AREA TO EXPOSE THE MORTAR BEDDING. THE CURB SECTION SHALL BE REMOVED AND RESET IN ACCORDANCE WITH THE REPAIR DETAIL. SOPREMA ASLAN FLASHING RUBBERIZED SEALANT OR APPROVED EQUAL SHALL BE APPLIED TO DECK ADJACENT TO REPAIR AREA AND PERMA-PATCH PERMANENT PAVEMENT REPAIR MATERIAL WILL BE PLACED TO MATCH EXISTING PAVEMENT. GRANITE CURB REPAIR SHALL BE PAID FOR UNDER ITEM 518.392 GRANITE CURB REPAIR IN ACCORDANCE WITH SPECIAL PROVISION 518. RUBBERIZED SEALANT AND PAVEMENT PATCH MATERIAL SHALL BE INCIDENTAL TO REPAIR ITEM 518.392 GRANITE CURB REPAIR.

No.	Revision	By	Date
1	Addendum #1 - Added Repair Detail	AES	04/16

Designed by:					
<b>HNTB</b>					
CONSULTANT PROJECT MANAGER: Ashley E. Stephens, P.E.					
	By	Date	By	Date	
Designed	AES	03/16	Checked	BRG	03/16
Drawn	PEB	03/16	In Charge of	RAL	03/16

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**THE GOLD STAR  
MEMORIAL HIGHWAY**

MTA PROJECT MANAGER: Ralph C. Norwood, IV, P.E., P.T.O.E.

Contract 2016.05  
Addendum No. 1  
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**BRIDGE REPAIRS**

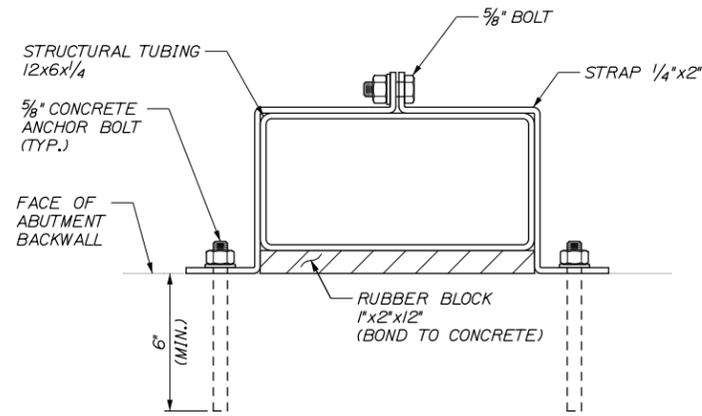
RAMP J OVER RTE. 1 SB & RAMP M  
DECK PLAN & COMPRESSION SEAL DETAIL

SHEET NUMBER: S-01

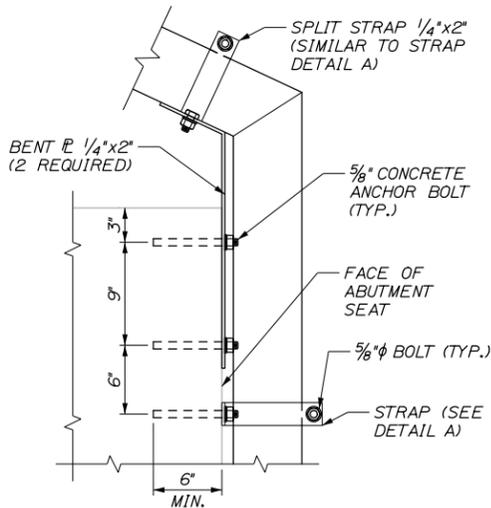
CONTRACT: 2016.05

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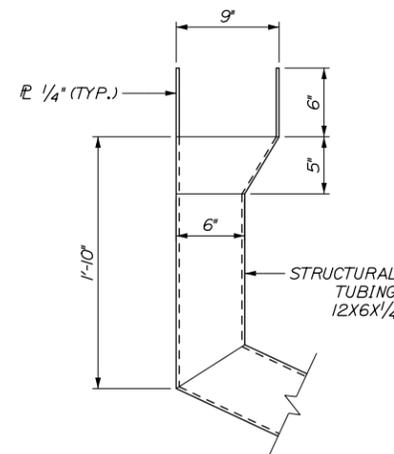
Date: 4/14/2016



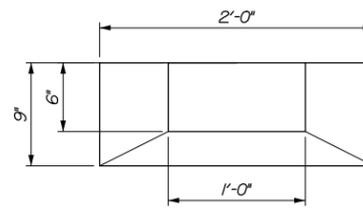
DETAIL A  
3" = 1'-0"



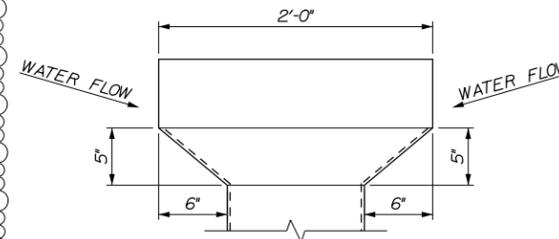
DETAIL C  
1/2" = 1'-0"



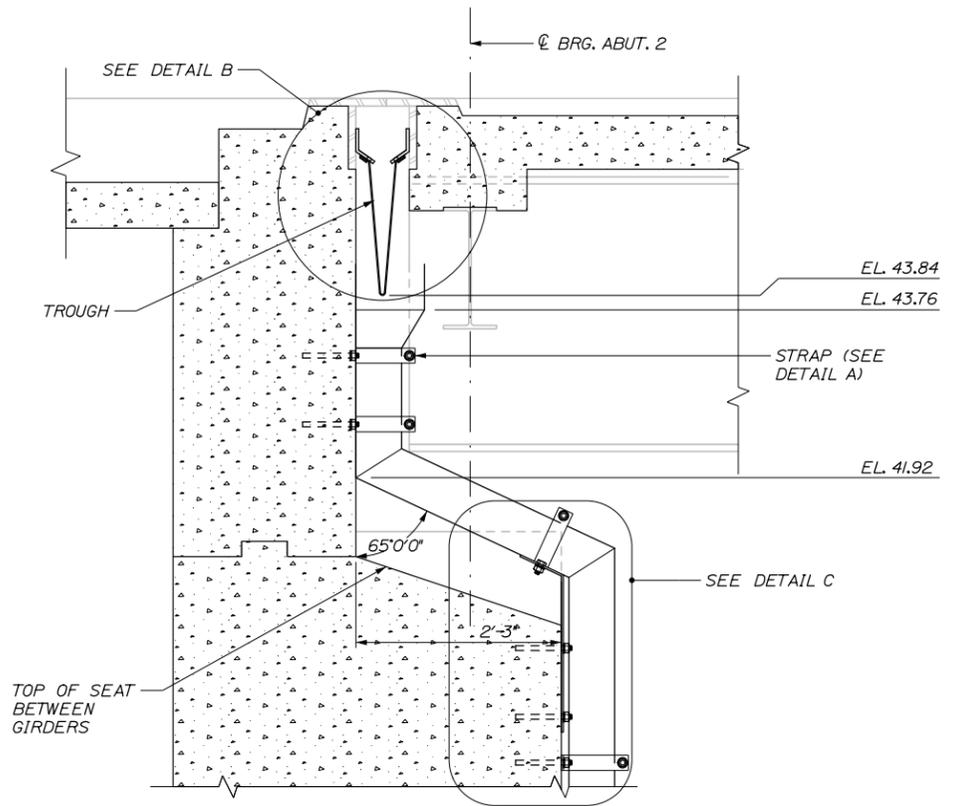
SIDE VIEW



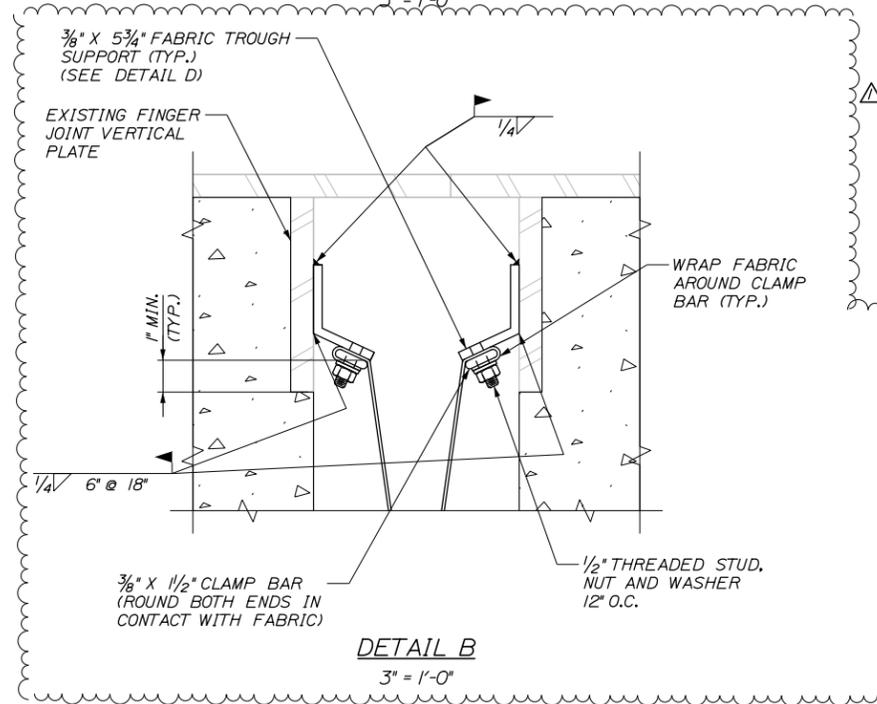
TOP VIEW



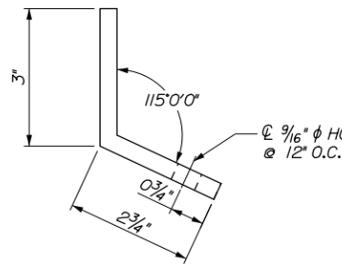
FRONT VIEW  
HOPPER DETAILS  
1/2" = 1'-0"



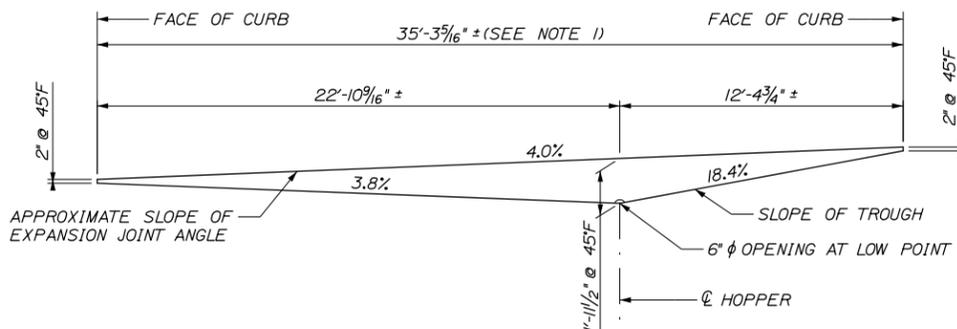
SECTION THROUGH HOPPER AT ABUTMENT  
N.T.S.



DETAIL B  
3" = 1'-0"



DETAIL D  
6" = 1'-0"



FABRIC TROUGH PROFILE AT ABUTMENT 2  
1/4" = 1'-0"

**TROUGH AND HOPPER NOTES:**

1. CONTRACTOR SHALL CONFIRM DIMENSIONS AND ELEVATIONS ON SITE PRIOR TO FABRICATION OF TROUGH AND HOPPER.
2. ALL PLATES SHALL BE 1/4" THICK AND SHALL CONFORM TO ASTM A36 UNLESS OTHERWISE NOTED.
3. ALL DRAIN OR HOPPER COMPONENTS SHALL BE BLAST CLEANED TO THE REQUIREMENT OF SSPC-CP6/NANCE 3 AND HOT-DIPPED GALVANIZED IN ACCORDANCE WITH ASTM A123 OR A153 AS APPLICABLE. ALL ASSOCIATED FASTENERS SHALL BE HOT-DIP GALVANIZED.
4. SHOP DRAWINGS FOR THE COMPLETE FABRIC TROUGH SYSTEM INCLUDING THE STEEL ATTACHMENT PLATES, FASTENERS, AND THE FABRIC TROUGHS SHALL BE SUBMITTED FOR APPROVAL PRIOR TO FABRICATION.
5. THE CONTRACTOR SHALL COAT THE EXPOSED SURFACES OF THE EXISTING FINGER JOINT VERTICAL PLATES WITH COLD-APPLIED GALVANIZING AFTER TROUGH INSTALLATION.
6. COAT BOLTS, THREADED STUDS AND NUTS WITH NICKEL BASED ANTI-SEIZE LUBRICANT DURING INSTALLATION.
7. ALL COSTS FOR THE COMPLETE FABRIC TROUGH SYSTEM, INCLUDING PREPARING AND COATING FINGER JOINT VERTICAL PLATES, SHALL BE PAID UNDER PAY ITEM 521.32 FABRIC TROUGH FOR FINGER JOINT.

Filename: 017\_Ramp J Trough Details.dgn

Scale:		Designed by:	
No.	Revision	By	Date
1	Addendum #1 - Updated Trough Support	AES	04/16

<b>HNTB</b>			
CONSULTANT PROJECT MANAGER: Ashley E. Stephens, P.E.			
Designed	AES	03/16	Checked BRG 03/16
Drawn	PEB	03/16	In Charge of RAL 03/16

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**THE GOLD STAR  
MEMORIAL HIGHWAY**

MTA PROJECT MANAGER: Ralph C. Norwood IV, P.E., P.T.O.E

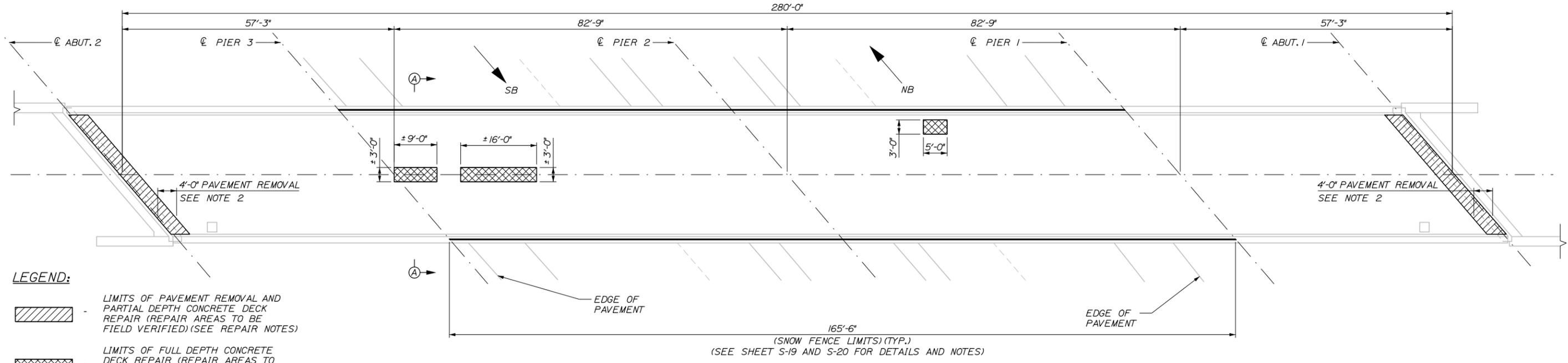
Contract 2016.05  
Addendum No. 1  
Page 27 of 33

**BRIDGE REPAIRS**

RAMP J OVER RTE. 1 SB & RAMP M  
FABRIC TROUGH DETAILS

SHEET NUMBER: S-02  
CONTRACT: 2016.05  
17 OF 35

Date: 4/14/2016

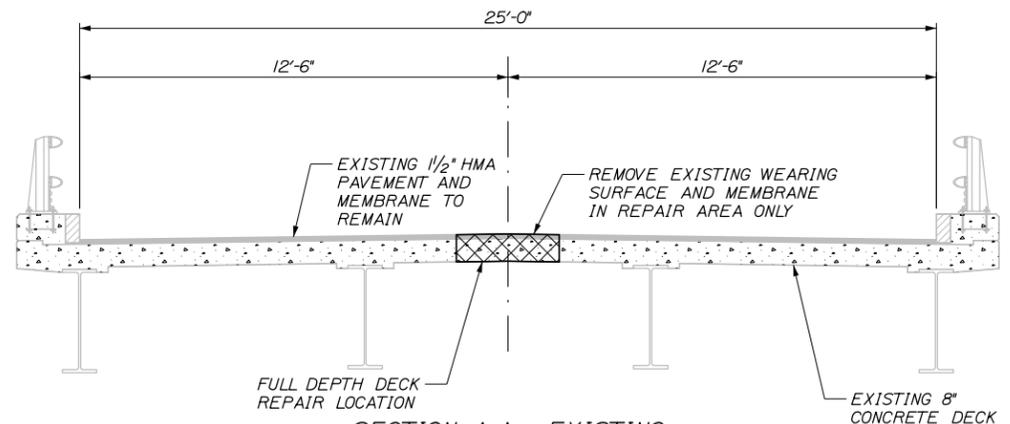


**LEGEND:**

- LIMITS OF PAVEMENT REMOVAL AND PARTIAL DEPTH CONCRETE DECK REPAIR (REPAIR AREAS TO BE FIELD VERIFIED) (SEE REPAIR NOTES)
- LIMITS OF FULL DEPTH CONCRETE DECK REPAIR (REPAIR AREAS TO BE FIELD VERIFIED) (SEE REPAIR NOTES)

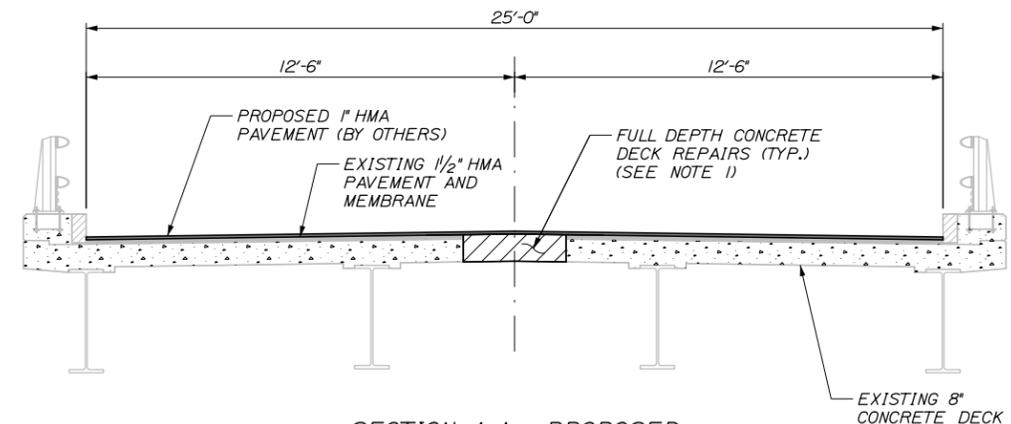
**PLAN**

$\frac{3}{32}'' = 1'-0''$



**SECTION A-A - EXISTING**

$\frac{3}{8}'' = 1'-0''$



**SECTION A-A - PROPOSED**

$\frac{3}{8}'' = 1'-0''$

**NOTES:**

1. FULL DEPTH DECK REPAIR SHALL OCCUR AT LOCATIONS IDENTIFIED IN PLAN VIEW ON THIS SHEET. FULL DEPTH REMOVAL INCLUDES REMOVAL OF EXISTING WEARING SURFACE ABOVE REPAIR AREA. CONCRETE REPLACEMENT DEPTH WILL BE FROM BOTTOM OF EXISTING DECK TO THE TOP OF EXISTING WEARING SURFACE. THIS WORK SHALL BE MEASURED FOR PAYMENT UNDER PAY ITEM 518.512, FULL DEPTH CONCRETE DECK REPAIRS.
2. THE EXISTING WEARING SURFACE AND MEMBRANE SHALL BE REMOVED IN LOCATIONS WHERE REPAIRS ARE IDENTIFIED FOR FULL DEPTH REPAIRS AND ADJACENT TO THE EXISTING BRIDGE JOINTS AS SHOWN IN THE PLAN VIEW FOR PARTIAL DEPTH REPAIRS. IN LOCATIONS OF PARTIAL DEPTH REPAIRS ADJACENT TO THE BRIDGE JOINTS, THE RESIDENT SHALL IDENTIFY AREAS OF DECK REPAIRS BASED ON REQUIREMENTS IN SPECIFICATION 518. THE ACTUAL REPAIR AREAS FOR THIS ITEM ARE UNDEFINED, FOR QUANTITIES IT WAS ASSUMED THAT 50% OF THE PAVEMENT REMOVAL AREA ADJACENT TO THE JOINT WILL REQUIRE PARTIAL DEPTH CONCRETE DECK REPAIRS. THIS WORK SHALL BE MEASURED FOR PAYMENT UNDER PAY ITEM 518.511, PARTIAL DEPTH CONCRETE DECK REPAIRS.
3. ALL WORK SHALL BE ACCOMPLISHED USING THE CONSTRUCTION SEQUENCE PRESENTED IN THE MAINTENANCE OF TRAFFIC DETAILS THE BRIDGE SHALL BE OPEN TO TRAFFIC AT THE END OF EVERY WORK DAY. SEE SPECIFICATION FOR ALLOWABLE WORK HOURS.
4. SHIELDING SHALL BE PROVIDED FOR ALL REPAIR LOCATIONS OVER MAINLINE TRAFFIC. SEE SPECIAL PROVISION 524 FOR SHIELDING LIMITS. SHIELDING SHALL BE INCIDENTAL TO THE REPAIR ITEM.
5. INSTALLATION OF WEARING SURFACE OVER THE PARTIAL DEPTH REPAIRS SHALL BE HAND PLACED.

**REMOVAL PROCEDURES (FULL DEPTH REPAIR):**

1. THE RESIDENT SHALL DELINEATE AREAS OF THE REPAIR SIMILAR TO WHAT IS SHOWN IN THE PLANS. AREAS DELINEATED REPRESENT OBVIOUS LOCATIONS OF REPAIR (I.E., EXPOSED REINFORCING OR SPALLS). THIS REPAIR PROJECT DOES NOT INTEND TO REPAIR SMALL AREAS OF DETERIORATION. RESIDENT TO FOCUS ON AREAS DELINEATED ON PLANS, UNLESS EXTENSIVE DETERIORATION OR DELAMINATIONS ARE FOUND IN OTHER LOCATIONS.
2. AFTER THE RESIDENT HAS DELINEATED THE REPAIR AREAS, THE CONTRACTOR SHALL AGREE ON THE LIMITS. SHOULD THE REMOVAL AREA LIMITS APPEAR TO CHANGE DURING THE REPAIR PROCESS, THE CONTRACTOR SHALL NOTIFY THE RESIDENT. THE RESIDENT AND CONTRACTOR SHALL AGREE ON THE REVISED PAY LIMITS PRIOR TO THE CONTRACTOR CONTINUING THE REMOVALS. WORK SHALL INCIDENTAL TO THE SPECIFIED REPAIR ITEM IN SPECIFICATION 518.
3. PERFORM 1 INCH DEEP SAWCUTS ALONG LIMITS OF REMOVAL.
4. CHIP CONCRETE TO DEPTH SHOWN AND DESCRIBED IN SPECIFICATION 518.

**REPAIR PROCEDURES (FULL DEPTH REPAIR):**

1. PREPARE AND PATCH REPAIR AREAS WITH FAST SETTING CONCRETE. SEE SPECIFICATIONS FOR MATERIAL, MATERIAL PREPARATION, PLACEMENT, AND CURING REQUIREMENTS.

Filename: 020\_Spring\_Street\_Deck\_Plan\_and\_Typical\_Section.dgn

Scale:		Designed by:	
No.	Revision	By	Date
1	Addendum #1 - Updated Note	AES	04/16

<b>HNTB</b>			
CONSULTANT PROJECT MANAGER: Ashley E. Stephens, P.E.			
Designed	AES	03/16	Checked
Drawn	PEB	03/16	In Charge of
			By
			Date
			By
			Date

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**THE GOLD STAR  
MEMORIAL HIGHWAY**

MTA PROJECT MANAGER: Ralph C. Norwood, IV, P.E., P.T.O.E.

Contract 2016.05  
 Addendum No. 1  
 Page 28 of 33

**BRIDGE REPAIRS**

CUMMINGS ROAD (SPRING STREET)  
 DECK PLAN AND TYPICAL SECTION

SHEET NUMBER: S-05  
 20 OF 35

**MAINE TURNPIKE AUTHORITY**

Pre-Bid Conference

CONTRACT 2016.05

BRIDGE REPAIRS

RAMP J OVER RTE. 1 SB & RAMP M (MM 1.59)

TWO ROD ROAD UNDERPASS (MM 42.0)

CUMMINGS ROAD UNDERPASS (MM 44.6)

EXIT 45 UNDERPASS (MM 44.9)

FOREST AVENUE OVERPASS NB & SB (MM 50.0)

SNOW FENCE INSTALLATIONS

April 11, 2016, 11:00 AM

1) Location:

The general limits of work are from Mile 1.25 to Mile 50.0.

The major components of work are listed below:

- Bridge Repairs at Ramp J – Joint seal installation and Fabric Trough Installation
- Bridge Repairs at Two Rod Road – Parapet repair, parapet joint repair, granite curb joint repair
- Bridge Repairs at Cummings Road – Deck repairs
- Bridge Repairs at Exit 45 – Pourable joint seal installation
- Bridge Repairs at Forest Avenue – Substructure repairs and abutment bearing replacement
- Snow Fence Installation at Ramp J (MM 1.59); Saco Interchange (MM 35.7); Scarborough Interchange (MM 42.5); I-295 Southbound Exit 44 (MM 44.3); Gorham Road Underpass (MM 44.0); Gorham Road over Ramp (MM 44.01) and Cummings Road (MM 44.6)
- Bridge Mounted Sign Clips Installation at Route 236 (MM 1.25); Ramp H (MM 1.60); Gorham Road (MM 44.0); and Cummings Road (MM 44.6).

2) General Description:

The work consists of bridge repairs at Ramp J, Two Rod Road, Cummings Road, Exit 45, and Forest Avenue; snow fence installations, and bridge mounted sign clip installations. The work also involves maintenance of traffic and all other work incidental thereto in accordance with the Plans and Specifications.

3) Bid:

- a) Opening: April 21, 2016 at 11:00 A.M. at MTA Headquarters 2360 Congress Street, Portland
- b) All bid and contractual questions shall be directed to Mr. Nathaniel Carll. Phone No. (207) 482-8115.
- c) All questions on plans and specifications shall be in writing and shall be directed to Nate Carll, Purchasing Manager, of the Maine Turnpike Authority. Fax No. (207) 871-7739.

- 4) Notification:
  - a) Contractor shall notify and obtain approval from the Authority prior to visiting the Project sites for field inspection. The contact person is Mr. Steve Tartre at (207) 871-7771, ext. 144.
- 5) Contract Specifications
  - a) The Specifications are divided into two parts: Part I, Supplemental Specifications and Part II, Special Provisions.
  - b) The Maine Turnpike Supplemental Specifications are additions and alternations to the 2014 Maine Department of Transportation Standard Specifications. See Subsection 100.1.
- 6) Construction Schedule/Prosecution of Work:
  - a) April 28, 2016 – estimated Contract Start Date
  - b) November 15, 2016 – Contract Completion Date
  - c) Proposed work at Cummings Road Bridge shall occur when the adjacent roadway contract by the City of Portland/Town of Scarborough has a two-week 24 hour per day closure in place. This closure is anticipated in September or October 2016. The roadway contractor will provide three weeks' notice to the start of the project. No work is permitted outside of this two week work window.
- 7) Maine Department of Labor – Fair Hourly Wages (Special Provision 104.3.8)
  - a) Contract includes Heavy and Bridge wage rates
- 8) Response to Damage Claims (General Provision 104.3.11):
  - a) Contractor responsible for responding to all damage claims, in writing, within 30 days.
- 9) Utility Coordination (Special Provision 104.4.6)
  - a) There is no utility relocation work scheduled or anticipated by the utility owners in this contract.
- 10) Cooperation With Other Contractors (Special Provision 104.4.7):

Numerous Maine Turnpike construction projects are scheduled between Mile 32 and Mile 46 in 2016. They include, but are not necessarily limited to:

  - a. Town of Scarborough and City of Portland paving contracts;
  - b. Contract 2015.03 Bridge Repairs – Exit 45 Bridge;
  - c. Contract 2015.12 Toll System Upgrades at Interchanges 32, 36 and 46 Northbound.
- 11) Lead Paint
  - a) Contractor to acknowledge the potential presence of lead paint and associated precautions thereto.

12) Permit Requirements

- a) Maine Pollutant Discharge Elimination System (MPDES) General Permit for Stormwater Discharge from Construction Activity.
- b) Limit of Disturbance Plan shall be submitted prior to any disturbance.

13) General Safety Requirements

- a) All jobsite personnel shall wear a safety vest labeled as ANSI 107-2004 standard performance for Class 3 risk exposures. This requirement also applies to truck drivers and equipment operators when out of an enclosed cab.
- b) U-Turns at toll plazas and median openings are not allowed (General Provision 105.5.1)
- c) Contractor access to and from the mainline shall not negatively impact mainline traffic flow. The Contractor may be required to establish lane closures to provide for safe access. Refer to Special Provision 652, Specific Project Maintenance of Traffic Requirements, for lane closure requirements and restrictions.
- d) All vehicles used on the Project, including company, personal, and delivery vehicles shall be equipped with amber flashing beacons in accordance with the General Provision 652.6.1.

14) Traffic Control (Special Provision Section 652):

- a) The Maintenance of Traffic Control pay items are identified per location and include traffic control devices and the maintenance of traffic control devices. See Special Provision 652 for descriptions and Appendix A for permissible lane closure times.
- b) Cummings Road Traffic control will be coordinated with adjacent roadway project by the City of South Portland/Town of Scarborough has its two-week 24 hour per day complete roadway closure in place. The closure will be the responsibility of the roadway project contractor, however securing/protecting the bridge deck during the completion of the deck repairs will be the responsibility of the contractor.
- c) Temporary (5 minute) night work lane stoppages will be permitted for joint installation at Ramp J.
- d) All temporary lane closures shall be made utilizing drums. The Contractor will not be permitted to store drums on the traffic side of the guardrail (face of guardrail) during non-work hours or when drums are not required for a lane closure. All drums shall be placed behind guardrail, concrete barrier, or removed from the roadway.
- e) Portable light towers will be required to illuminate the night construction work area.

15) Specific Contract Items

- a) Section 518 – Parapet Replacement
  - i) The work shall include the concrete removal and placement of a section of parapet located on Two Rod Road. This work also includes the installation of temporary thrie rail over the repair area prior to opening lane to traffic.

b) Section 518 – Full Depth Concrete Deck Repair

- i) This specific repair item occurs at the Cummings Road Underpass. The item includes the full depth removal and replacement of concrete in locations identified in the plans. The concrete replacement will extend from the bottom of existing deck to top of adjacent wearing surface.

c) Section 607 – Snow Fence

- i) There are seven bridges which are to receive snow fence as shown on the plans:

- (1) Ramp J over Rte. 1SB and Ramp M (MM 1.59)
- (2) Saco Interchange Bridges (MM 35.7)
- (3) Scarborough Interchange Bridge (MM 42.5)
- (4) I-295 Southbound Underpass Bridge (MM 44.3)
- (5) Gorham Road Underpass (MM 44.0)
- (6) Gorham Road over Ramp (MM 44.01)
- (7) Cummings Road Underpass (MM 44.6)

d) Section 645 – Highway Signing

- i) There are four bridge which are to receive bridge sign clips as shown on the plans:

- (1) Route 236 Underpass (MM 1.25)
- (2) Ramp H Underpass (MM 1.60)
- (3) Gorham Road Underpass (MM 44.0)
- (4) Cummings Road Underpass (MM 44.6)

16) Questions

