

MAINE TURNPIKE AUTHORITY

ADDENDUM NO. 1

CONTRACT 2014.14

BRIDGE REPAIRS

FOREST AVENUE BRIDGES (MILE 50.0)

RIVERSIDE STREET BRIDGES (MILE 51.2)

The following changes are made to the Proposal, Plans, and Specifications.

PROPOSAL

Proposal Sheet P-3 is deleted and replaced with Proposal Sheet P-3 (Revised 04/10/14) attached hereto. The revision modifies the item number for High Performance Membrane from 503.14 to 508.14.

Proposal Sheet P-7 is deleted and replaced with Proposal Sheet P-7 (Revised 04/10/14) attached hereto. The revision permits the (bid) Total to be moved to Proposal Sheet P-7.1.

Proposal Sheet P-7.1 (Added 04/10/14) is added and attached hereto. The revision permits the addition of two pay items and moves the (bid) Total to this page.

PLANS

Sheet 2 of 40, Item 503.14 shall be corrected to read 508.14 in the item list.

Sheet 2 of 40 shall be corrected to add the following two Pay Items to the item list:

Item No.	Description	Unit	Totals
503.14	Epoxy- Coated Reinforcing Steel, Fabricated and Delivered	Pound	4,500
503.15	Epoxy- Coated Reinforcing Steel, Placing	Pound	4,500

Reinforcing steel locations can be determined from the Reinforcing Schedule.

Sheet 26 of 40, Section A-A shall be deleted and replaced with Section A-A included in this addendum.

Sheet 28 of 40, Section F-F shall be deleted and replaced with Section F-F included in this addendum.

Sheet 29 of 40, Additional Reinforcing Steel around Bridge Drains detail shall be deleted and replaced with Additional Reinforcing Steel around Bridge Drains detail included in this addendum.

SPECIFICATIONS

Special Provision 107.4.7 Limitations of Operations

The following second sentence of the first paragraph of the Bridge Repair Work Section is deleted and replaced with the following:

If the permanent repairs cannot be completed in time to open the roadway the Contractor shall temporarily “patch” the concrete repair and open the roadway up to two lanes of mainline traffic. Temporary repairs greater than 4 inches in depth shall be patched with rapid set concrete; and temporary repairs less the 4 inches in depth shall be patched with either rapid set concrete or bituminous pavement.

Special Provision 502 Structural Concrete, Section 502.19 Basis of Payment, first paragraph, second sentence; the words “bridge drains” is deleted.

Special Provision 502 Structural Concrete (Structural Concrete, Abutments and Retaining Walls), Section 502.18 Method of Measurement is revised to read – “All Portland Cement Concrete to secure the new neoprene strip seal joints

Special Provision 503 Reinforcing Steel (Measurement and Payment) is deleted and not replaced.

Special Provision 652, Maintenance of Traffic, Temporary mainline lane closures, the fourth paragraph (SP-152) will be deleted and replaced with the following:

The lane closure setup may not begin until the beginning time specified. Lane closures that are setup early or that remain in place outside of the approved period shall be subject to a lane rental fee of \$500 per five minutes for every five minutes outside of the approved time. The installation of the construction signs will be considered setting up the lane closure. Removal of the last construction sign will be considered the removal of a lane closure. Construction signs shall be installed immediately prior to the start of the lane closure and shall be promptly removed when no longer required. The installation and removal of a lane closure including signs, channelizing devices and arrow boards shall be a continuous operation. The Authority reserves the right to order removal of an approved lane closure.

QUESTIONS

The following are questions asked at the pre-bid meeting held on April 01, 2014 or submitted to the Maine Turnpike Authority in writing. Answers to the questions are noted. Bidders shall utilize this information in preparing their bid.

Question 1: Will the addendum address maintenance of the emergency repair.

Answer: Yes, this addendum addresses maintenance of the emergency repair. See the Special Provision Maintenance and Removal of Temporary Roadway Plating, included in this addendum, for the work required due to the installation of the temporary steel plates at Forest Avenue southbound.

A copy of the Emergency Repair Solicitation, and a sketch showing the plate demolition as the work progresses, are attached for the bidders' information.

Question 2: The contract completion date is difficult due with night paving and temperatures. Will any adjustments be made?

Answer: No adjustments will be made to Appendix A.

Question 3: Will density requirements be waived for late season paving?

Answer: No, density requirements will not be waived for late season paving.

Question 4: Riverside Street calls for pre-phase 1 paving repairs, what does that entail and how is it paid.

Answer: The existing shoulders need to be inspected for pavement failure prior to shifting traffic onto them. The work entails sounding of shoulder areas for delaminated pavement and membrane; and removing and patching suspect and actual delaminated pavement and membrane areas. Payment for providing lane closures shall be incidental to the Maintenance of Traffic Control pay item. The sounding of the shoulder pavement, and removal and patching of suspect and actual delaminated pavement and membrane areas will be paid time and material.

Question 5: Will milling the bridge decks be allowed?

Answer: No, milling will not be allowed on the bridge decks.

Question 6: The contract calls for 2 new bridge drains. How large a hole can be created and are we allowed to cut existing rebar to get the drain in place?

Answer: In the upper half of the slab the Contractor may oversize the hole for the drain up to 8 inches beyond the drain body. In the lower half of the slab the Contractor may oversize the hole for the drain up to 4 inches beyond the drain body. Top and bottom mat transverse reinforcing steel is at seven inches on-center, parallel to the skew. Top and bottom mat longitudinal reinforcing steel spacing varies. Two top mat and two bottom mat transverse bars may be cut; a similar number of longitudinal bars may be cut. The eight flare bars are replaced with four #5 U bars.

Question 7: Why is reinforcing steel incidental instead of a schedule provided and paid for per pound?

Answer: The Authority has provided a reinforcing steel schedule, and reinforcing steel pay item under this Addendum.

Question 8: There are no rebar call outs on section F-F on sheet 28 of 40.

Answer: See revised section F-F included in this addendum.

Question 9: Is elastomeric concrete required or transit concrete mix at Riverside Street?

Answer: For header concrete repairs undertaken behind concrete barrier the Contractor, at his option, may replace the existing header concrete with transit mix concrete, a repair mix specified in Special Provision 518, or elastomeric concrete.

For header concrete repairs not undertaken behind concrete barrier that needs to be opened to traffic at the end of the work shift the Contractor shall replace the existing header concrete with elastomeric concrete.

Question 10: The new hot rubber detail for the bead on the final lift of pavement (Detail A sheet 30) is that what you want?

Answer: The call out on Detail A sheet 30 that reads “Seal with hot rubber after paving and prior to seal coating shoulder” shall be deleted and not replaced.

Question 11: It item number 503.14 for High performance membrane correct?

Answer: No, the correct item number is 508.14. See all corrections noted in this addendum.

Notes: The above items shall be considered as part of the bid submittal.

The total number of pages included with this addendum is Seventeen (17).

All bidders are requested to acknowledge the receipt of the Addendum No. 1 by signing below and faxing this sheet to Nathaniel Carll, Purchasing Department, Maine Turnpike Authority at 207-871-7739. Bidders are also required to acknowledge receipt of this Addendum No. 1 on Page P-8 of the bid package.

Business Name

Print Name and Title

Signature

Date

April 10, 2014

Very truly yours,

MAINE TURNPIKE
AUTHORITY

Nathaniel Carll
Purchasing Department
Maine Turnpike Authority

SPECIAL PROVISION

MAINTENANCE AND REMOVAL OF TEMPORARY ROADWAY PLATING

Description

The work involves maintaining six advanced notice signs; furnishing, installing and maintaining a PCMS on the approach side of the repair; maintaining the 9.5 mm HMA pavement ramps, on and off of the steel plates; and removing the steel plates in phases, as the Forest Avenue Southbound south joint work progresses.

The Contractor shall monitor, maintain, cover, uncover and remove, as necessary, the six advance warning signs (two Uneven Lanes – 16 SF/ ea; two 50 MPH – 20 SF/ ea; two Bump with and Steel Plate Ahead placard – 18 SF/ ea).

Payment for monitoring, maintaining, covering and uncovering the six advance warning signs shall be incidental to the Maintenance of Traffic Control pay item. Removing and returning the signs and posts/ stubs to the Crosby Maintenance Facility will be paid time and material.

Should the advanced notice signs be damaged beyond repair, replacement of the signs will be provided by the Authority. Replacement of the advanced notice signs, if required, is not included in this Special Provision.

The Contractor shall monitor and patch the 9.5 mm HMA pavement ramps as necessary to provide a smooth transition onto and off of the roadway plates. Patch materials shall be bagged or plant mixed QPR.

Payment for monitoring the 9.5 mm HMA pavement ramps shall be incidental to the Maintenance of Traffic Control pay item. Patching and removal of the 9.5 mm HMA pavement ramps will be paid time and material. Lane closures needed shall be incidental to the Maintenance of Traffic Control pay item. Final temporary ramp removal will be paid time and material.

Should the temporary ramps deteriorate to a condition beyond patching, removal and reinstallation of the temporary HMA pavement ramps will be required. Removal and reinstallation of the temporary HMA pavement ramps, if required, is not included in this Special Provision.

The Contractor shall monitor the position and fastening of the steel plates; and shall remove the excess plate(s) as the work progresses. Sections of plate in excess of 10 square feet shall be returned to the Authority's Crosby Maintenance Facility. Sections shall be removed and returned in the largest pieces possible.

Payment for monitoring the position and fastening of the steel plates shall be incidental to the Maintenance of Traffic Control pay item. Securing or re-securing the steel plates, if required, will be paid time and material. Lane closures needed shall be incidental to the Maintenance of Traffic Control pay item. Removal of excess plate(s) as the work progresses, and returning pieces to the Crosby Maintenance Facility shall be incidental to Item 202.122 Removing Existing Concrete.

Item No	Item Description	Units	Approx. Quantities	Unit Prices in Numbers		Bid Amount in Numbers	
				Dollars	Cents	Dollars	Cents
BROUGHT FORWARD:							
409.15	Bituminous Tack Coat - Applied	Gallon	420				
409.151	Bituminous Tack Coat - Applied, Bridge	Gallon	235				
419.30	Sawing Bituminous Pavement	Linear Foot	2,220				
502.21	Structural Concrete, Abutments and Retaining Walls	Cubic Yard	25				
502.70	Bridge Drains	Each	2				
502.701	Bridge Drain Grate Modification	Each	12				
502.81	Installation of Plastic Deck Drains	Each	30				
507.095	Aluminum Bridge Railing – Splice Modification	Each	34				
508.14	High Performance Membrane	Lump Sum	1				
511.091	Temporary Earth Support Systems	Lump Sum	1				
514.06	Curing Box for Concrete Cylinders	Each	2				
518.39	Granite Curb Joint Mortar and Bedding Mortar Repair	Linear Foot	220				

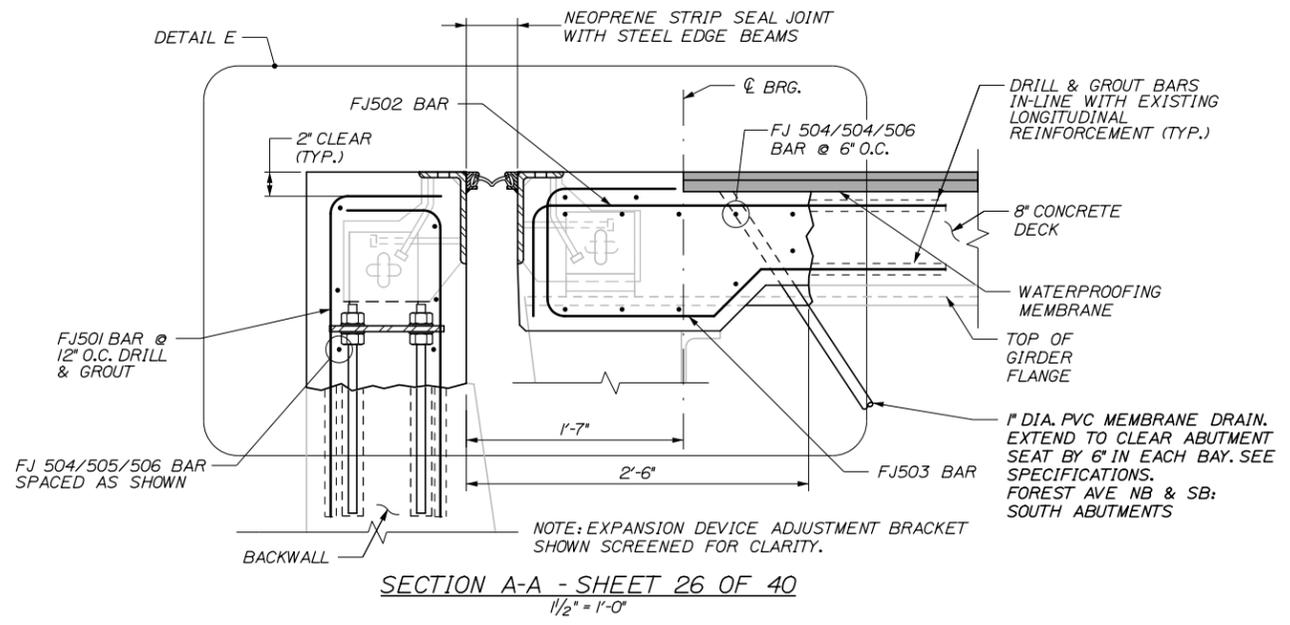
CARRIED FORWARD:

Item No	Item Description	Units	Approx. Quantities	Unit Prices in Numbers		Bid Amount in Numbers	
				Dollars	Cents	Dollars	Cents
BROUGHT FORWARD:							
652.312	Type III Barricades	Each	8				
652.33	Drum	Each	365				
652.35	Construction Signs	Square Foot	3,500				
652.361	Maintenance of Traffic Control Devices	Lump Sum	1				
652.38	Flaggers	Hour	50				
652.41	Portable - Changeable Message Sign	Each	2				
652.451	Truck Mounted Attenuator	Calendar Day	85				
656.50	Baled Hay, in place	Each	15				
656.60	Temporary Berms	Linear Foot	1,400				
656.62	Temporary Slope Drains	Linear Foot	50				
656.632	30 inch Temporary Silt Fence	Linear Foot	2,150				
659.10	Mobilization	Lump Sum	1				

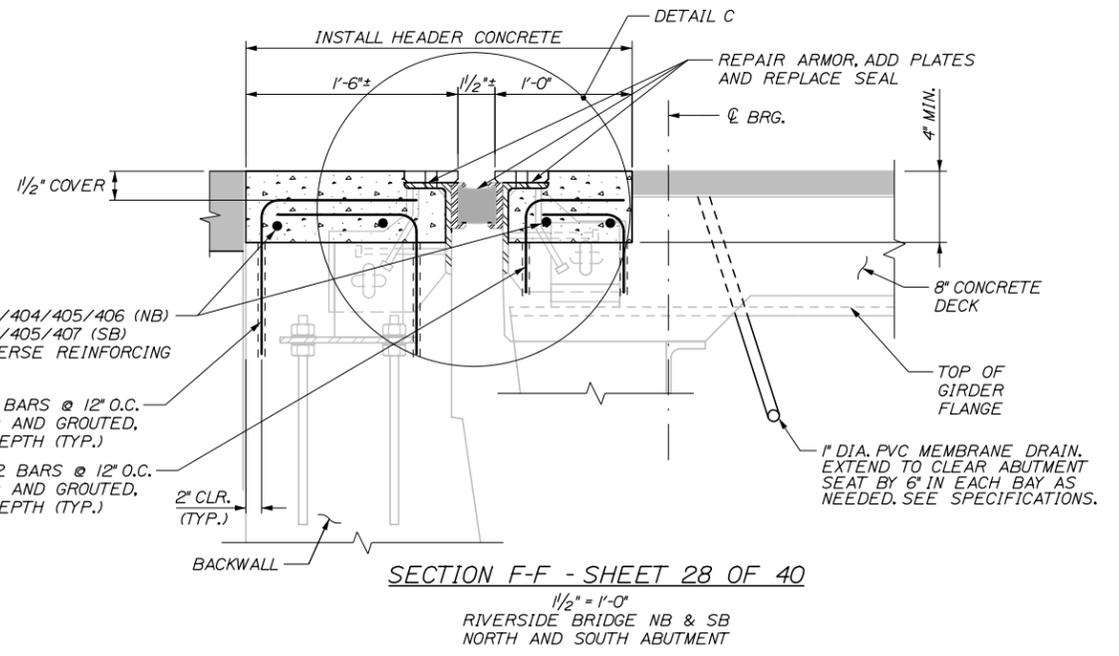
CARRIED FORWARD:

Item No	Item Description	Units	Approx. Quantities	Unit Prices in Numbers		Bid Amount in Numbers	
				Dollars	Cents	Dollars	Cents
BROUGHT FORWARD:							
503.14	Epoxy-Coated Reinforcing Steel, Fabricated and Delivered	Pound	4,500				
503.15	Epoxy-Coated Reinforcing Steel, Placing	Pound	4,500				
TOTAL:							

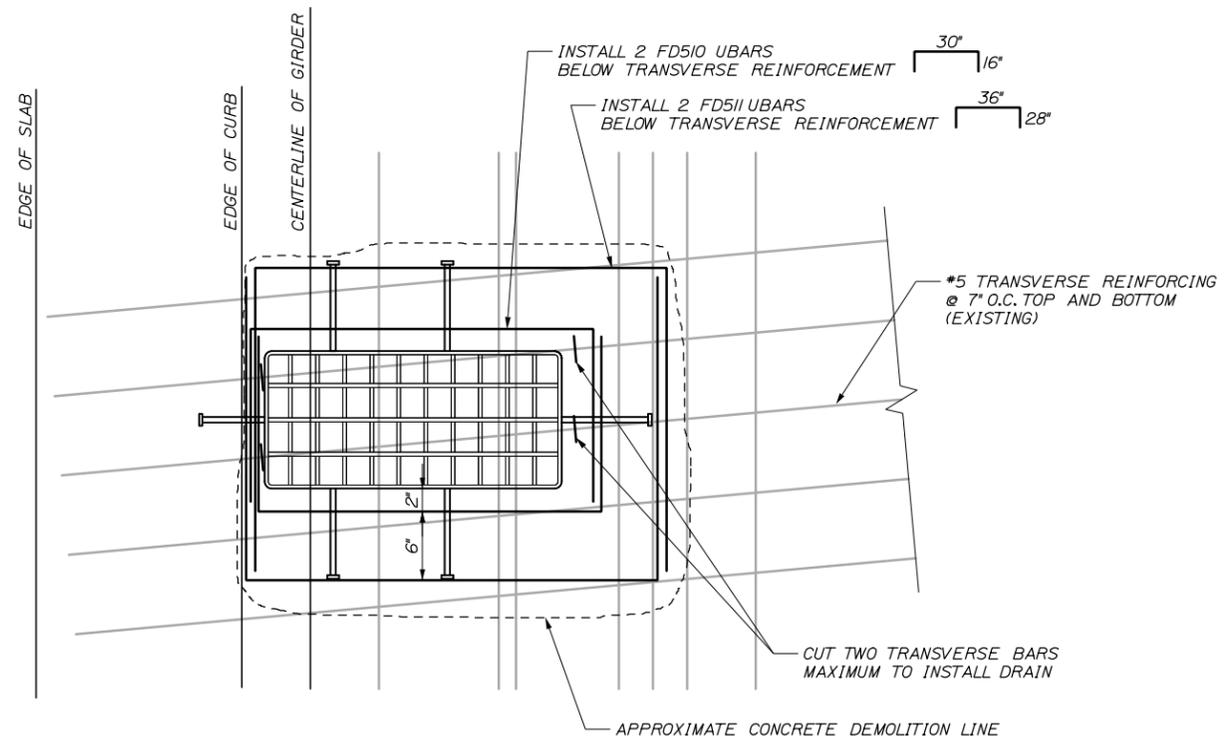
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SECTION A-A - SHEET 26 OF 40
1/2" = 1'-0"



SECTION F-F - SHEET 28 OF 40
1/2" = 1'-0"
RIVERSIDE BRIDGE NB & SB
NORTH AND SOUTH ABUTMENT



ADDITIONAL REINFORCING STEEL AROUND
BRIDGE DRAINS - SHEET 29 OF 40
1/2" = 1'-0"

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Addendum No. 1
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Filename: 001_Addendum1-01.dgn

Scale: AS NOTED			
No.	Revision	By	Date

Designed by:

HNTB

CONSULTANT PROJECT MANAGER: Robert J. Driscoll, P.E.			
By	Date	By	Date
Designed	TMH 04/14	Checked	CAH 04/14
Drawn	JRD 04/14	In Charge of	RAL 04/14

HNTB CORPORATION
340 County Road, Suite 6-C
Westbrook, ME 04092
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**THE GOLD STAR
MEMORIAL HIGHWAY**

MTA PROJECT MANAGER: Ralph C. Norwood, IV, P.E., P.T.O.E.

BRIDGE REPAIRS - FOREST AVENUE
AND RIVERSIDE STREET

DETAILS

SHEET NUMBER: 1

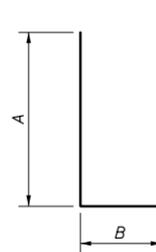
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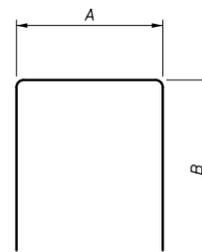
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Contract 2014.14 Bridge Repairs

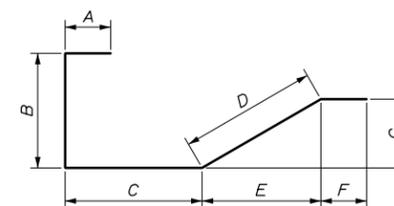
MARK	SIZE	NO.	LENGTH	TYPE	A	B	C	D	E	F	G	INCR.	REMARKS
Forest Avenue Joint Replacements													
* FJ501	5	184	2'-7 1/2"	118	2'-0"	0'-7 1/2"							Backwall L Bars - Drill & Grout 9" Min
* FJ502	5	86	3'-8 1/2"	118	3'-0"	0'-8 1/2"							Deck Top Mat L Bars - Drill & Grout 9" Min
* FJ503	5	140	4'-9"	131	0'-11"	0'-9 1/2"	1'-3"	0'-5 1/2"	0'-4"	1'-10"	0'-4"		Deck Bottom Mat Bars - Drill & Grout 9" Min
* FJ504	5	36	17'-2"	STR									Transverse Reinforcing - Phase I NB; Phase I SB
* FJ505	5	36	11'-9"	STR									Transverse Reinforcing - Phase II NB; Phase II SB; Drill & Grout 6" Min Into Phase I
* FJ506	5	36	17'-8"	STR									Transverse Reinforcing - Phase III NB; Phase III SB; Drill & Grout 6" Min Into Phase II
Riverside Street Header Concrete													
* RH401	4	416	1'-11"	118	1'-0"	0'-11"							Backwall L Bars - Drill & Grout 6" Min
* RH402	4	416	1'-3"	118	0'-10"	0'-5"							Deck L Bars - Drill & Grout 6" Min
* RH403	4	12	16'-2"	STR									Transverse Reinforcing - Phase I NB; Phase I SB; Phase II SB
* RH404	4	4	9'-2"	STR									Transverse Reinforcing - Phase II NB; Drill & Grout 6" Min Into Phase I
* RH405	4	8	11'-3"	STR									Transverse Reinforcing - Phase III NB; Phase III SB; Drill & Grout 6" Min Into Prev. Phase
* RH406	4	4	9'-8 1/2"	STR									Transverse Reinforcing - Phase IV NB; Drill & Grout 6" Min Into Prev. Phase
* RH407	4	4	14'-7"	STR									Transverse Reinforcing - Phase IV SB; Drill & Grout 6" Min Into Prev. Phases
Forest Avenue Bridge Drains													
* FD510	5	4	5'-2"	129	2'-6"	1'-4"							Drain Reinforcing
* FD511	5	4	7'-8"	129	3'-0"	2'-4"							Drain Reinforcing



TYPE 118



TYPE 129



TYPE 131

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	Drawn	JRD	In Charge of	RAL	04/14

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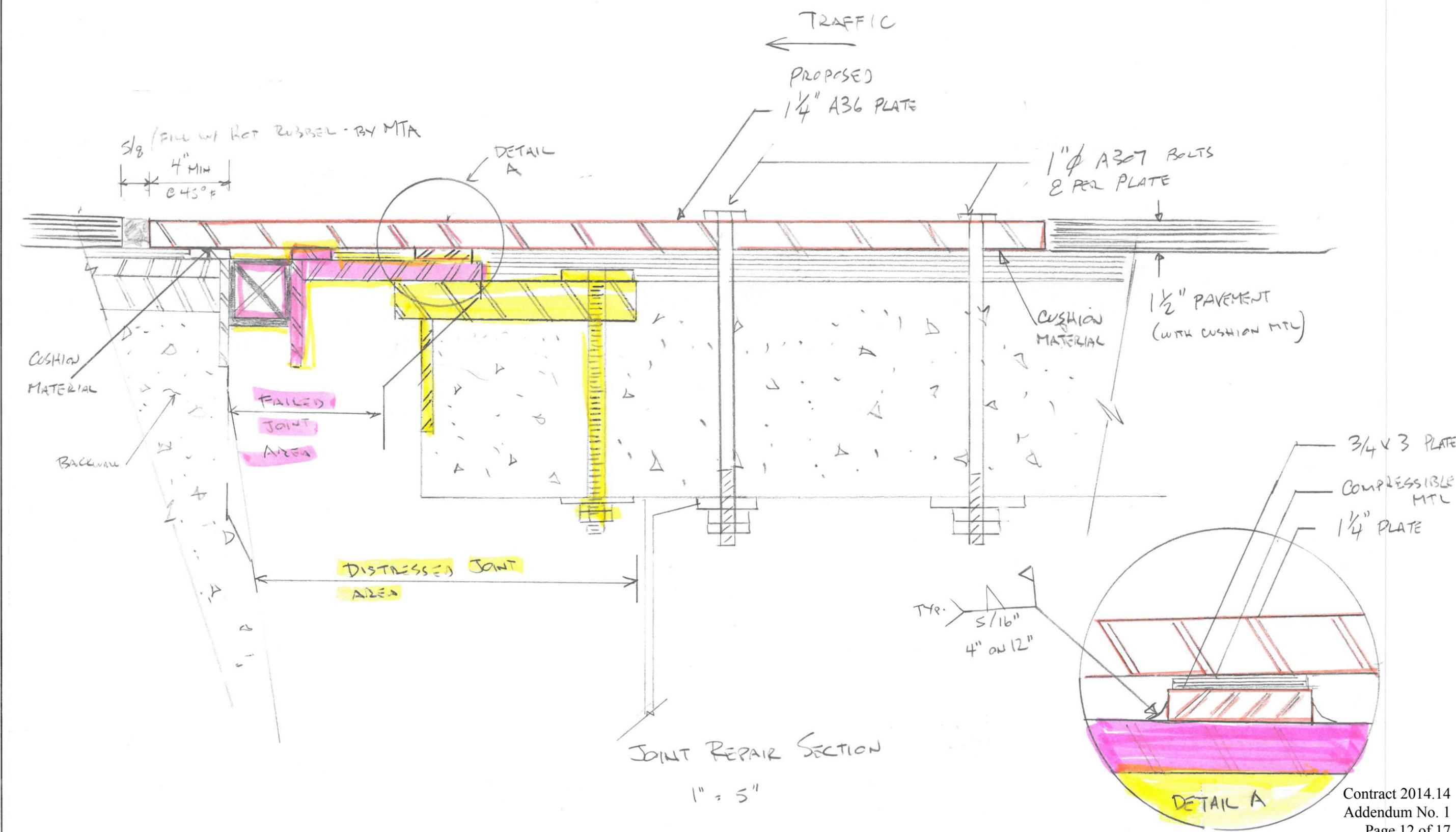
BRIDGE REPAIRS - FOREST AVENUE
AND RIVERSIDE STREET

REINFORCING STEEL

SHEET NUMBER: 2
CONTRACT: 2014.14
- OF -

Filename: 002_Addendum1-02.dgn

Date: 3/28/2014



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Page 12 of 17

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By	Date	Checked	By	Date
CHN	3/14	RJC	RJC	3/14
CHN	3/14	In Charge of	RAL	04/14

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MAINE TURNPIKE

THE GOLD STAR MEMORIAL HIGHWAY

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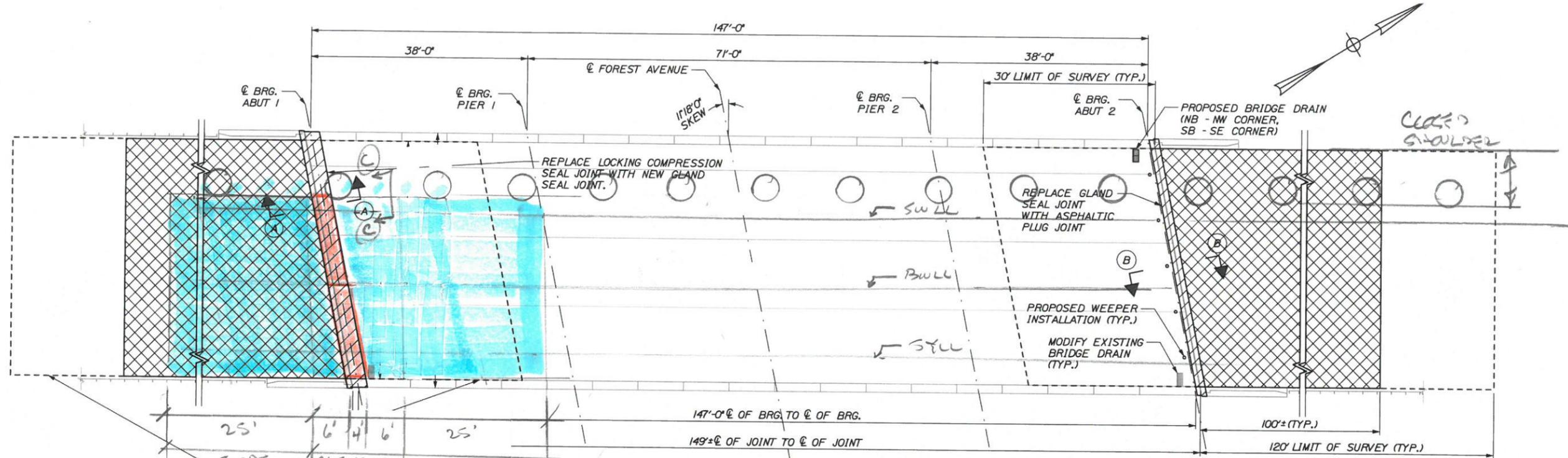
BRIDGE REPAIRS - FOREST AVENUE
SOUTH ABUTMENT - SOUTHBOUND
JOINT REPAIR

SHEET NUMBER: _____ OF _____

CONTRACT: 2014

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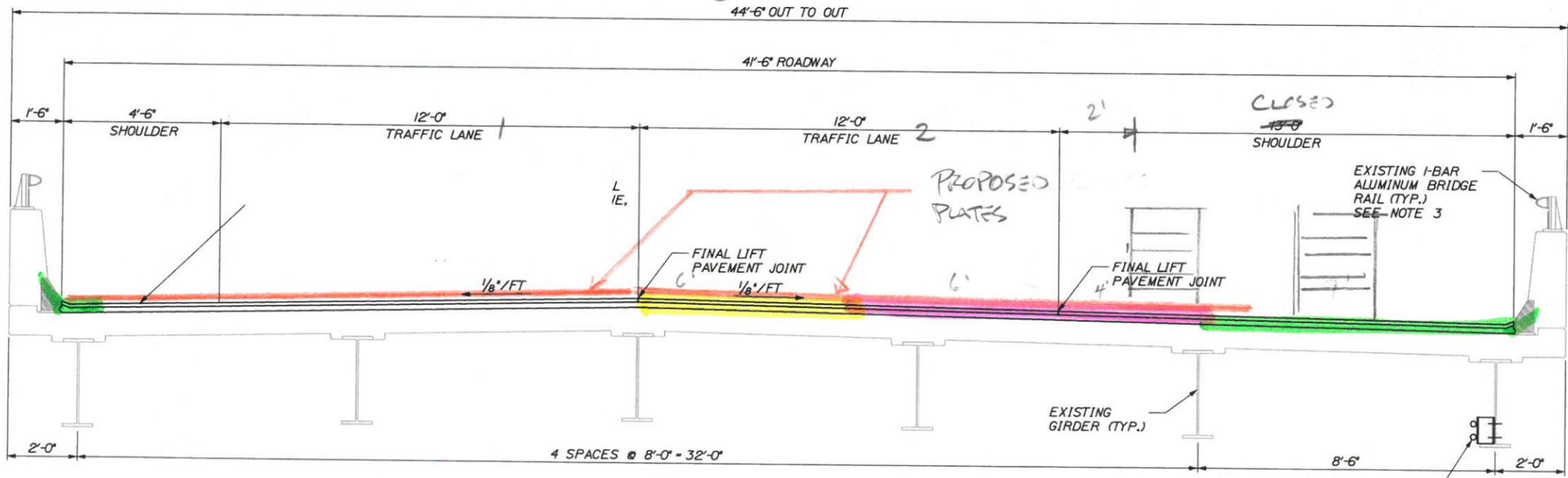
Date: 3/28/2014



TEMP STEEL
 TEMP PAVEMENT

DECK PLAN
 1" = 10'-0"
 NORTHBOUND ROADWAY
 (SOUTHBOUND SIMILAR)

FAILED AREA
 DISTRESSED AREA
 NEW JOINT - 2006



TRANSVERSE SECTION
 1/2" = 1'-0"
 (NB - LOOKING NORTH)
 (SB - LOOKING SOUTH)

WORK LIMITED TO PLATE INSTALLATION
 & TEMPORARY PAVEMENT

Contract 2014.14
 Addendum No. 1
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No.	Revision	By	Date

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By	Date	By	Date
Designed	3/14	Checked	3/14
Drawn	3/14	In Charge of	3/14

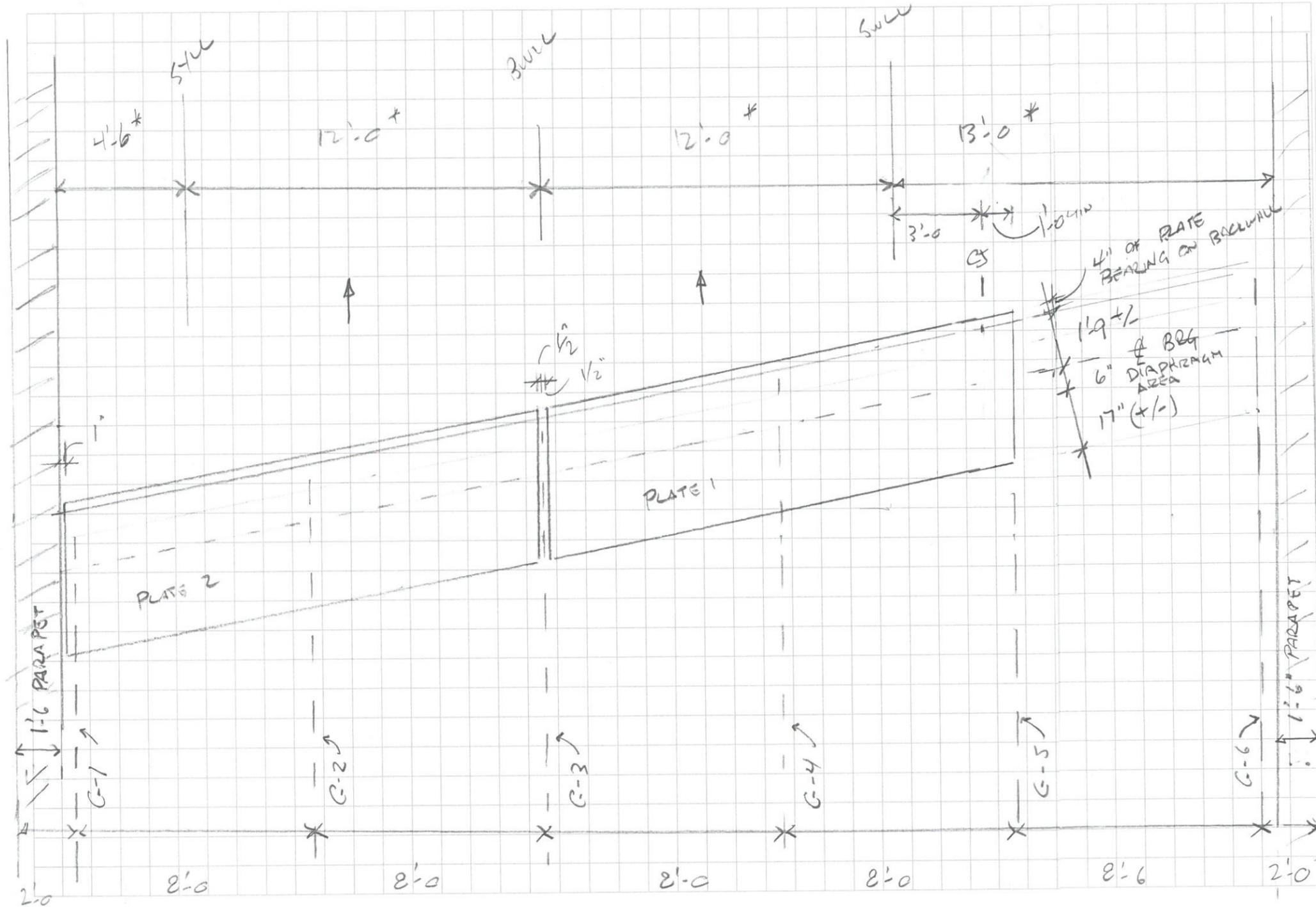
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THE GOLD STAR MEMORIAL HIGHWAY
 MTA PROJECT MANAGER:

BRIDGE REPAIRS - FOREST AVENUE
 SOUTH ABUTMENT - SOUTHBOUND
 JOINT REPAIR
 SHEET NUMBER: _____ OF _____
 CONTRACT: 2014

Filename: working-mpc.dgn

Date: 3/28/2014



FOREST AVE SB SOUTH ABUTMENT
PLATE LAYOUT

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By	Date	By	Date

Designed *CD* 3/14 Checked *RJ* 3/14
 Drawn *CD* 3/14 In Charge of *RAL* 04/14
 3/14

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THE GOLD STAR
MEMORIAL HIGHWAY

MTA PROJECT MANAGER:

BRIDGE REPAIRS - FOREST AVENUE
SOUTH ABUTMENT - SOUTHBOUND
JOINT REPAIR

CONTRACT: 2014

SHEET NUMBER: OF

Date: 3/28/2014

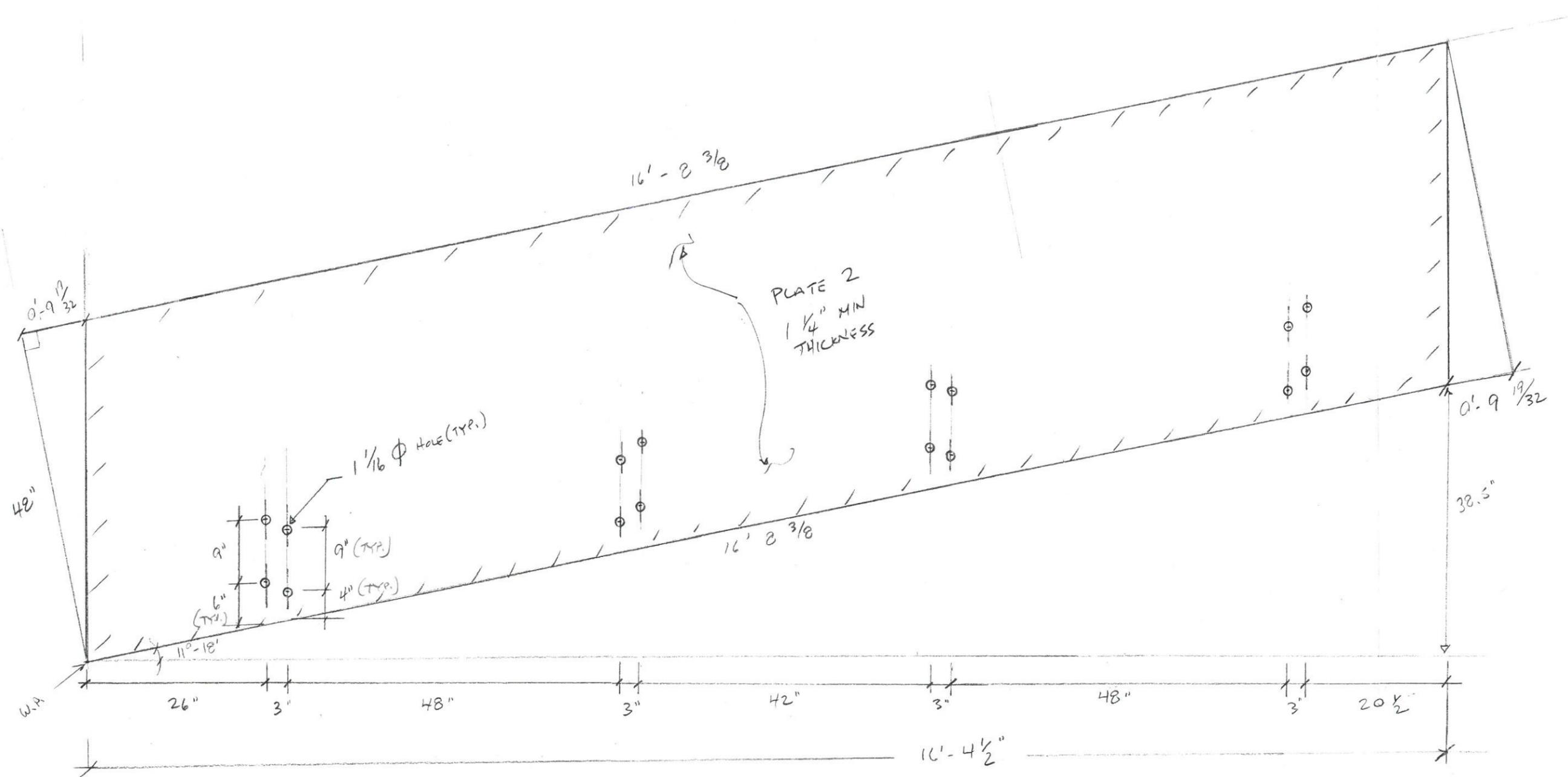


PLATE 2 FABRICATION
(3/4" = 1')

1 REQ'D

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Addendum No. 1
Page 15 of 17

Scale:

No.	Revision	By	Date

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By	Date	Checked	By	Date
JWH	3/14	RJA	RJA	3/14
		In Charge of	RAL	04/14

3/14

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MAINE TURNPIKE

THE GOLD STAR MEMORIAL HIGHWAY

MTA PROJECT MANAGER:

BRIDGE REPAIRS - FOREST AVENUE
SOUTH ABUTMENT - SOUTHBOUND
JOINT REPAIR

SHEET NUMBER: _____ OF _____

CONTRACT: 2014

Filename: working-mpc.dgn

Date: 3/28/2014

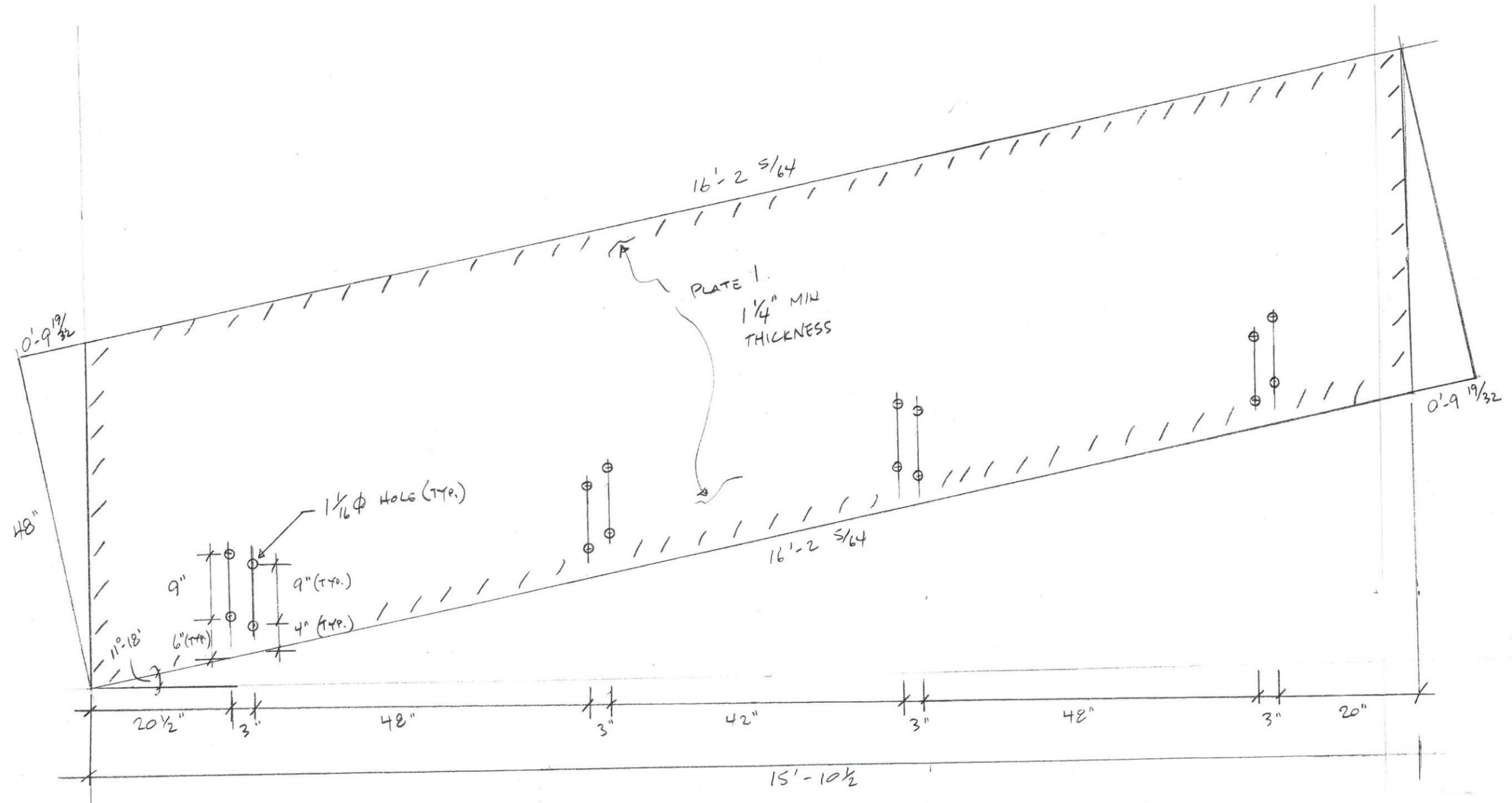


PLATE 1 FABRICATION
(3/4" = 1')

/ RJA'D

Contract 2014.14
Addendum No. 1
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Scale:

No.	Revision	By	Date

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HNTB

CONSULTANT PROJECT MANAGER: Robert J. Driscoll, P.E.

By	Date	By	Date
Designed <i>CH</i>	3/14	Checked <i>CH</i>	3/14
Drawn <i>CH</i>	3/14	In Charge of <i>RAL</i>	04/14

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**THE GOLD STAR
MEMORIAL HIGHWAY**

MTA PROJECT MANAGER:

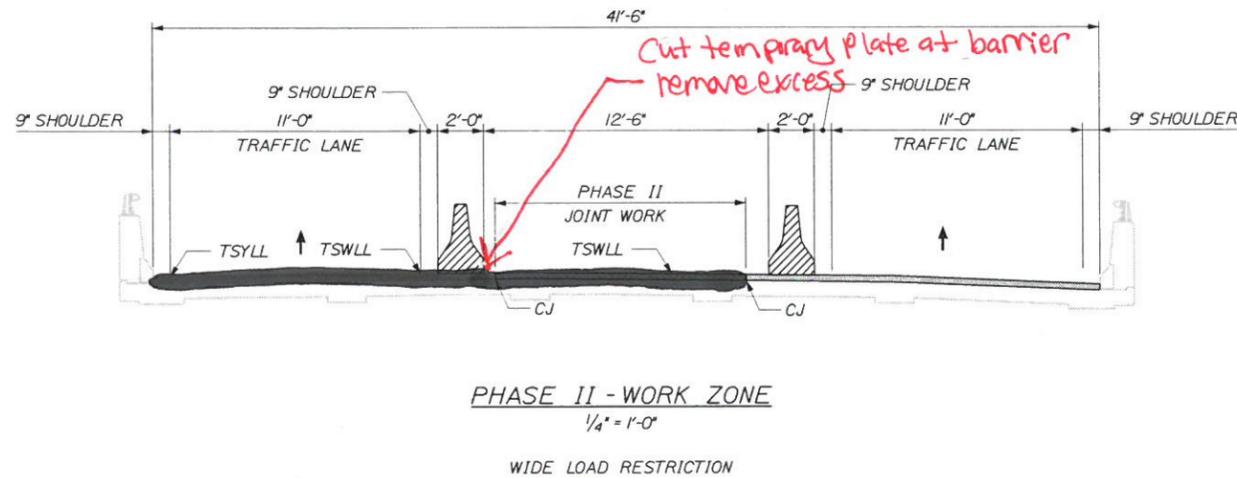
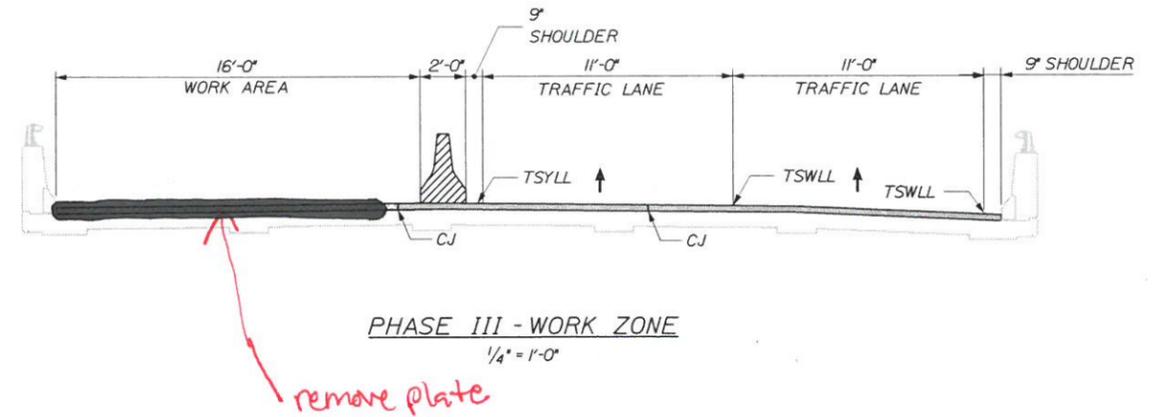
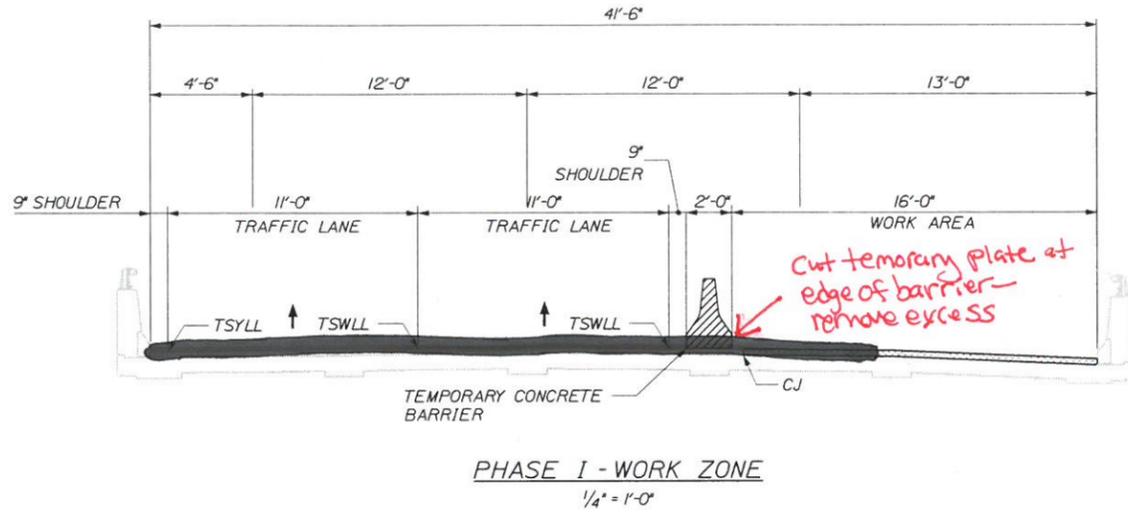
BRIDGE REPAIRS - FOREST AVENUE
SOUTH ABUTMENT - SOUTHBOUND
JOINT REPAIR

CONTRACT: 2014

SHEET NUMBER: _____ OF _____

Filename: working-mpc.dgn

Date: 3/14/2014



SEE SPECIFICATION 107.4.7 LIMITATIONS OF OPERATIONS FOR PHASE II ACCESS RESTRICTIONS.

- JOINT UNDER CONSTRUCTION
- JOINT COMPLETE
- CJ = CONSTRUCTION JOINT
- TSWLL = TEMPORARY SOLID WHITE LANE LINE
- TSYLL = TEMPORARY SOLID YELLOW LANE LINE

NOTE:
FINAL LIFT OF DECK PAVEMENT AND APPROACH SHIMMING SHALL BE PLACED NIGHTS AFTER PHASE IV IS COMPLETE. PAVEMENT JOINTS SHALL MATCH AT 25' AND 37' RIGHT OF MTA CENTERLINE.

NOTES:
PRE-PHASE I:
THE CONTRACTOR SHALL DETERMINE THE FINAL LOCATION OF THE PROPOSED BRIDGE DRAINS AND CORE A 3 INCH DIAMETER HOLE THRU THE DECK IN THE APPROXIMATE CENTER OF THIS LOCATION TO PROVIDE DECK DRAINAGE PRIOR TO PHASE I. MEASURES SHALL BE TAKEN TO CONTROL DRAIN WATER EROSION OF THE EROSION CONTROL MIX BELOW. THE CONTRACTOR SHALL ALSO REMOVE ACCUMULATED SAND AND DEBRIS FROM THE END OF THE ADJACENT ENDPOST FOR A MINIMUM OF 25 FEET AND REGRADE TO ALLOW WATER RUNOFF. THIS WORK SHALL BE CONSIDERED INCIDENTAL TO ITEM 502.70 BRIDGE DRAINS.

Contract 2014.14
Addendum No. 1
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Plate Phasing-Removal

Scale: AS NOTED			
No.	Revision	By	Date

Designed by: HNTB			
CONSULTANT PROJECT MANAGER: Robert J. Driscoll, P.E.			
Designed	By	Date	Checked
	TMH	03/14	CAH
Drawn	By	Date	In Charge of
	JRD	03/14	RAL

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**THE GOLD STAR
MEMORIAL HIGHWAY**

MTA PROJECT MANAGER: Ralph C. Norwood, IV, P.E., P.T.O.E.

BRIDGE REPAIRS - FOREST AVENUE AND
RIVERSIDE STREET
FOREST AVENUE
NORTHBOUND AND SOUTHBOUND

SHEET NUMBER: 3
CONTRACT: 2014.14
3 OF 40